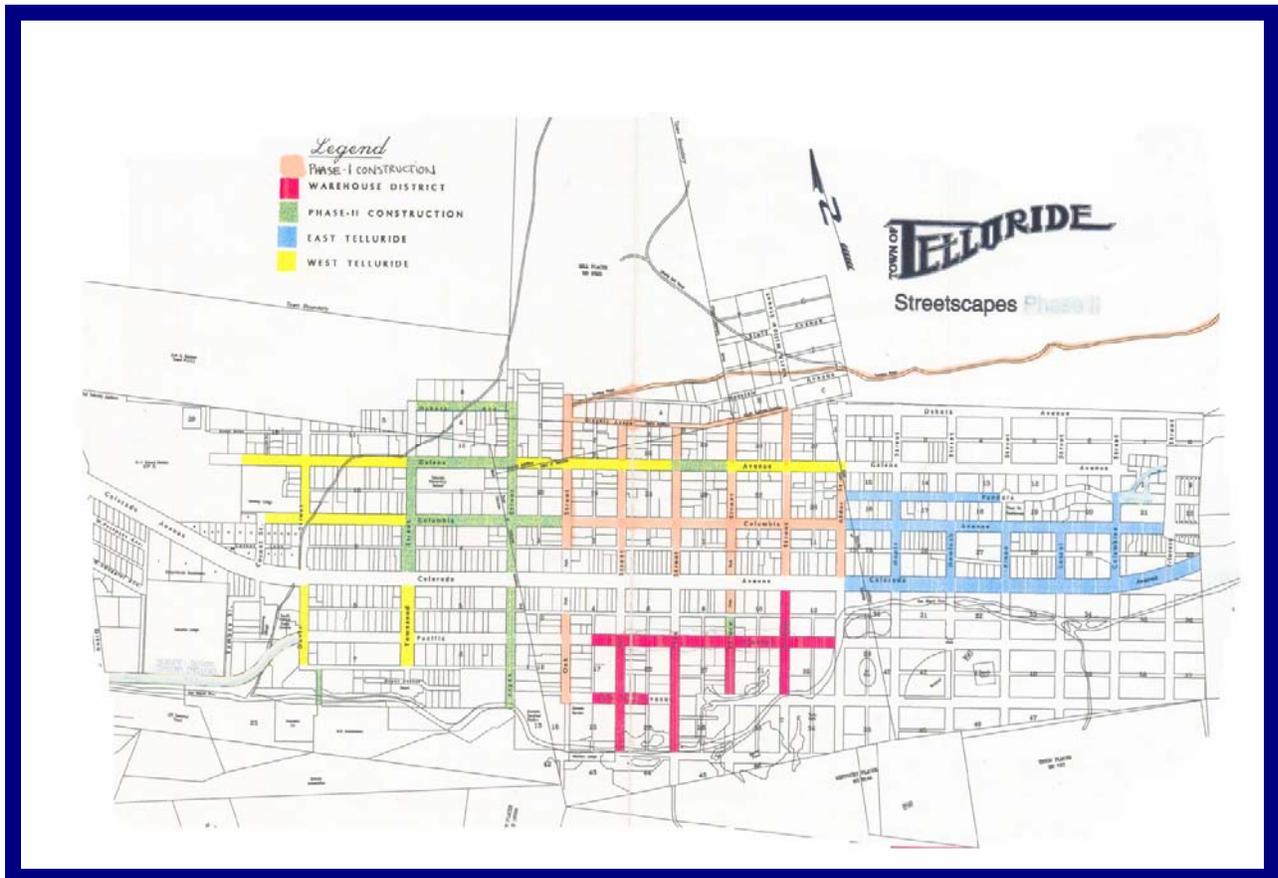


Town of Telluride

MANUAL OF STREETSCAPE STANDARDS

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ACKNOWLEDGEMENTS

This manual is the result of a painstaking and persistent ten-year effort by the Telluride Streetscape Task Force and Town staff. The following citizens deserve recognition for contributing their time, ideas, and expertise throughout this project:

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Introduction and Background

USE AND ADAPTATIONS OF THIS MANUAL

Town of Telluride Boards and Commissions, and the Public Works, Planning, and Building departments should use this Manual of Streetscape Standards as a reference and guide. Public projects should consult and rely upon the Standards throughout their planning, design, engineering, and construction phases. Private projects should consult and rely upon the Standards when determining the scope of appropriate public improvements that may be required by the Town's Land Use Code. Subsequent design, engineering, and construction shall then conform to these standards. Any conflict between the adopted design standards and the desire for specific improvements by new development shall be resolved based on the specifications of the standards, unless both the Planning and Public Works departments approve an amendment to the standards. To ensure consistency and clarity in the design of all street improvements, the adopted standards shall not be amended without clear findings supporting the need for any changes. These findings should be noted in this Manual and included in the Amendments Appendix. It is not anticipated that major changes will be necessary on a frequent basis.

OVERVIEW

Telluride's streets and rights of way are an essential community asset, and their design and use are a fundamental community concern. As such, street improvements must balance the needs of adjacent property owners with the desires of the broader community.

This manual represents the culmination of a ten-year effort by an appointed body of town citizens to ensure that work in the public right of way is completed in an efficient, functional, aesthetically pleasing, and practical manner. Work completed under these standards is intended to provide for a traditional, historic appearance, where appropriate, and a functionality that balances the varied interests of the Telluride community. Further, it is a primary goal of these standards to ensure that, after improvements take place, they be appropriately maintained.

Not every right of way in Telluride was part of this project. For example, the design of rights of way in more modern neighborhoods, such as Backman Village or The Depot has already been determined through the planning process. Figure 1 shows the streets that are included in this Manual of Streetscape Standards. For rights of way that are not included, it is possible that this manual may be a useful reference to support discussions about potential improvements and the approaches taken throughout the different areas of town.

Standards contained in this document are to be implemented through a combination of methods. Some streets have not seen significant infrastructure upgrades or adequate structural maintenance for many years. It is expected that these streets and alleys will be improved gradually through public and private efforts, such as citizen/council initiatives or the development of adjacent private land. Town staff should also work to secure funding for streetscape improvements through bond initiatives, grants, or other means.

For streets already exhibiting these standards, normal maintenance and occasional updates or improvements to existing conditions will be necessary as neighborhood uses and needs change.

Town staff should plan for this type of work when preparing annual town operating and capital budgets.

Engineered Designs developed for many Telluride streets as part of this effort are an important element of the Streetscape Standards. As such, they should be followed when any street improvements, private development, rehabilitation or maintenance is undertaken. To this end, it is intended that the Telluride Town Council, town boards and commissions, and staff use these standards to aid in making decisions affecting town rights of way.

It is important to note that the Engineered Designs have integrated a number of concerns. First, drainage was designed to balance the potential for large runoff events with the small scale of Telluride streets. Therefore, it is expected that certain precipitation events will temporarily flood the drainage system as it is designed. Second, "one at a time" streets were intentionally designed for specific neighborhoods to slow low volumes of through traffic and provide more green area for aesthetics and drainage. Finally, the Engineered Designs have attempted to maintain the "rough and natural" character of specific neighborhoods by specifying either asphalt or chip seal as the preferred travel way finish product. The Task Force has found that over time both products look the same from an aesthetic standpoint and it is likely that construction cost might ultimately dictate which product is used.

BACKGROUND

In 1996 the Telluride Town Council created and appointed the Streetscape Task Force. The task was charged with producing a comprehensive plan for rehabilitating Town streets and alleys. At the time street drainage and other infrastructure faced serious neglect due to historic under-funding. Citizen preferences for a "rough and natural" town character, though recognized as integral to preserving Telluride's aesthetic, contributed to the problem.

The first facet of the Task Force's work was to educate its members. Information gathering sessions ensured that members learned about the

existing conditions of Telluride's rights of way, new problem solving technology, and issues associated with public works and planning. Armed with this knowledge, the Task Force became qualified to formulate area-specific solutions that preserve a "rough and natural" character, while providing for safe and functional rights of way.

The second facet of the Task Force's work was soliciting extensive neighborhood input through numerous neighborhood meetings (*see Section 6*). Neighborhood input was considered as part of each decision to expand, upgrade, or retrofit public rights of way so that these design standards were developed with the greatest amount of citizen input possible.

The third facet was to supervise the design and construction of over \$4.5 million in street infrastructure improvements between 1998 and 2006. Direct task force involvement helped secure the success of two ballot initiatives to fund these improvements. Congestion Mitigation Air Quality Grants and Enhancement Grants from the Colorado Department of Transportation helped, as well.

A fourth facet of the Streetscape Task Force's work was to supervise the engineering design of street upgrades.

The fifth and final facet was to develop this Manual of Streetscape Standards. Adoption of these standards by Town Council is intended to ensure their use and enforcement by various town boards and commissions, and staff.

Telluride's Manual of Streetscape Standards is based on a street-by-street analysis that sought the most appropriate solutions for a wide array of existing conditions. Some areas required significant attention from the Task Force and staff, while other areas did not. The following is a list of ten design elements that are addressed:

1. Street surfacing
2. Street width
3. Sidewalk surfacing or no sidewalk
4. Sidewalk width
5. Edge of travel way treatment
6. Drainage
7. Streetlight number, location, and style
8. Landscaping location, configuration, and size
9. Parking location, width, and aspect
10. Snow storage location

GOALS AND OBJECTIVES

A broad principal objective of the Streetscape Standards is to preserve and enhance the rustic and historic character of the Town (i.e., maintenance of historic sidewalks and street widths, use of historic scoring patterns, and rejection of modern embellishments). The preservation of Tomboy Road as an unpaved four-wheel-drive road is a specific example of this broad objective.

The Streetscape Standards strive to balance this broad, overarching goal with equally important, more specific goals for improving and protecting the quality of life for residents and visitors. These goals include protecting the environment, maximizing efficiency of street function, encouraging pedestrianization, and preserving aesthetics. Specific objectives for achieving these goals were compiled from the individual recommendations of task force participants at a meeting held on February 29, 1996.

The Streetscape Standards also strive to be consistent with and move forward on core ideas of the Telluride 2006 Master Plan. The most significant is to "retain the character of Telluride through ... [page 5] simple streets and alleys, sidewalks limited to where necessary, native landscapes, [and] undefined visual edges between public and private property ...". A second is to "[page 7] maintain and enhance the pedestrian orientation of Telluride, furthering the ability of residents and visitors to walk throughout the community in all seasons and to experience social interactions at various gathering points around Town." A third is to "[page 4] preserve and restore the natural environment", which is achieved by the designs in this manual by minimizing impervious surfaces and retaining and cleaning runoff whenever possible.

Goal 1

Protect The Environment

Objective 1.1 - Reduce air pollution. Poor air quality traced to particulates from unpaved streets and alleys has been an historic problem. Surfacing streets with asphalt and chip seal decreases dust that is kicked up by motor vehicle tires and wind. Of course, paved streets must also be swept regularly to ensure that improvements to air quality can be maintained over time.

Getting citizens and guests out of motor vehicles and onto their feet is also a way to improve air quality. Providing a safe, easily traveled walkway along rights of way encourages pedestrian travel. (*Indicator of Success: Lower peak and annual average PM10 or PM2.5 concentrations, as measured at the Miramonte Building.*)

Objective 1.2 - Reduce water pollution. Poor water quality traced to excess sediment in runoff from unpaved streets and alleys has been an historic problem for the San Miguel River. Surfacing streets with asphalt and chip seal and constructing drainage that adequately conveys rain and snowmelt without creating erosion decreases the amount of sediment that is entrained by runoff and transported into local waterways. Storing snow in smaller piles where it can melt onto green space, installing runoff treatment before discharge into local waterways, and providing a riparian buffer whenever possible are other ways that the Streetscape Standards have

attempted to improve and protect water quality. (*Indicator of Success: Decreasing sediment concentrations in runoff to local streams.*)

Goal 2

Maximize Efficiency Of Street Function

The publicly owned right of way serves citizens and visitors in a number of ways. Pedestrians use the right of way to move from one place in town to another. Bicyclists and motorists use the right of way to move goods and services and to temporarily store transport vehicles (i.e., bike, car, and truck parking). Citizens use portions of the right of way for leisure, recreation (e.g., pocket parks), and vending (e.g., near Elks, Oak Street, and Spruce Street parks in the Commercial Core). Public Works requires portions of the street for traffic and parking signage, storing snow temporarily or permanently during winter, and to transport runoff efficiently, with minimal flooding. Utilities are routed along its airspace and subsurface. Each of these uses must be considered and balanced when designing a right of way for maximum efficiency of street function.

Objective 2.1 - Surface travel ways and walkways with materials appropriate to use and neighborhood character. Street paving and pedestrian surfacing materials should be economical, serviceable, and easy to repair, and their variety should be kept to a minimum. It is anticipated that different surface treatments may be appropriate and desirable depending on neighborhood character. For example, the black surface created by asphalt and chip seal better facilitates snowmelt during winter on south facing slopes than light-colored concrete. Or, the rougher, unfinished look of chip seal provides a more rustic aesthetic for a neighborhood and has historically been less expensive to install than asphalt.

Objective 2.1 - Allow for efficient clearance, storage, and removal of snow.

2.1.a. Clearing snow from the travel way during or after a winter storm is a very high priority for the Public Works Department, residents, and visitors. Having a nearby location where snow from the travel way can be temporarily or permanently stored after clearing is essential as there is limited space available at the Public Works' Snow Storage Area and transporting tons of snow away from streets and alleys to the Public Works Facility is very resource-intensive. The south side of many rights of way is an ideal place for snow storage because it allows the north, sunny side to melt off and be a safe, mostly ice-free walkway for pedestrians.

2.1.b. Snow removal priorities should be established and should follow the basic pattern of street use intensity. Delivery of these services is not intended to be uniform throughout town.

2.1.c. Design parking, pedestrian, landscaping, plaza, and street furnishing improvements with snow storage and ease of snow removal in mind.

Objective 2.2 - Adequately plan for sidewalk repair, maintenance, and infill.

2.2.a. Under the Municipal Code 12-04, "Sidewalk Construction and Maintenance", the owner of a lot fronting a right of way is responsible for repairing and maintaining (i.e., removing ice and snow) a sidewalk along that frontage. Town staff shall strive to enforce Municipal Code 12-04 in a fair and efficient manner.

2.2.b. Where no sidewalks currently exist, the Municipal Code requires that the frontage property owner construct the improvements detailed in this manual. Sometimes sidewalks will be specified; sometimes no sidewalks will be specified.

2.2.c. Where no sidewalks currently exist but are needed, they are to be constructed based on intensity of public use or concurrently with other street improvements.

Objective 2.3 - Keep alley design simple. The simple, utilitarian purposes of an alley shall be maintained, but opportunities to enhance their efficiency, as delivery areas, drainage ways, and alternative pedestrian routes shall not be overlooked. If special improvement district plans are developed for alleys, the Public Works and Planning departments staff should review and approve the engineered plans.

Objective 2.4 - Construct street and alley improvements to minimize maintenance. Upgrades and retrofits of existing streets should strongly consider long-term implications and costs. In general, work that can be readily understood to require replacement or redesign within 3 to 5 years should not be undertaken.

Objective 2.5 - Ensure adequate resources for street maintenance. Each year, the Town should examine Public Works' ability to adequately maintain street infrastructure, as it exists and as it may be upgraded. Staffing and equipment needs should be addressed to the maximum extent practical.

Objective 2.6 - Maintain existing on-street parking, when possible. Parking impacts the streetscape by decreasing the width of the travel way, the sidewalk, and/or the landscape strip. The Streetscape Standards maintain existing parking conditions along most streets; however, required private off-street parking spaces should be maintained and used as designed and approved. As well, resident permit parking will greatly improve the streetscape and should be implemented and enforced.

Goal 3

Enhance The Ease, Safety, And Beauty Of The Pedestrian Experience

Telluride would like to remain a community in which the pedestrian experience is preferred, and the resident and visitor does not need an automobile.

Objective 3.1 - Design streets with safe, useable sidewalks, where appropriate. Streets and rights of way should be safe, serve multiple uses, and offer varied amenities to pedestrians, such as public art, street furniture, landscaping, and handicap accessibility. They shall be designed and built with the intent of encouraging pedestrian and bicycle movement as an alternative to motorized vehicles. However, construction and maintenance of sidewalks shall not be required along every right of way so as to preserve the rustic character of the streets and to recognize the limited vehicle traffic in many locations. This should not be interpreted as discouraging pedestrian use of the street.

Objective 3.2 - Operate streets with safety as a priority. Town rights of way shall allow for reasonable pedestrian access, without the physical threat of automobile movement. A key component of this is the town-wide 15-mph speed limit.

Objective 3.2 - Recognize and facilitate use of existing, informal pedestrian corridors. The Streetscape Standards are intended to enhance right of way interfaces with non-motorized pathways and trails, such as the River Park Trail Corridor and pathways between streets along unimproved rights of way that cross Cornet Creek and connect residential areas to area schools.

Objective 3.3 - Support Public Transportation. The streetscape task force recognizes the importance of public transportation and its role in decreasing motor vehicle volume and therefore increasing pedestrian safety. Although public transportation is generally not within their purview, the task force recommends that bus stops be integrated into adjacent buildings designs so as not to impede pedestrian flow along sidewalks. In addition, when conflicts between public transportation and other vehicular right of way uses occur, they should be resolved in favor of assuring efficient and reliable public transportation.

Goal 4

Consider Aesthetics Of Street Improvements

Objective 4.1 - Maintain Neighborhood Character. When applicable, use neighborhood character as a criterion for determining whether a specific design treatment is appropriate. Pedestrian, plaza, landscaping, and street furnishing improvements should not be allowed to compete with the town's historic buildings or mountain vistas. In established areas, street improvements should be visually subdued and support what already exists. In developing areas (such as the western and eastern edges), improvements should be designed to

compliment the existing character of these areas, and not compete with the character of established areas (2006 Telluride Master Plan).

Objective 4.2 - Ensure sufficient street lighting for safe use of the right of way, while minimizing light pollution. Establish a lighting plan and regulations that direct more lighting onto the sidewalks and less into the night sky.

Objective 4.3 - Underground all existing aboveground utilities. Telluride requires that new utilities be placed underground, preferably in alley rights of way. As streetscape improvements are constructed, town staff shall coordinate the undergrounding of utilities with the construction.

Objective 4.4 - Include landscaping elements in the right of way to cut down on dust and increase greenery. Landscape or green strips are intended to separate motor vehicles and pedestrians, act as snow storage areas, and to align neighboring blocks. Frontage property owners are responsible for installing and maintaining (e.g., watering and weeding) right of way green strips. The Right of Way Landscaping Guidelines (Section 4) in this manual shall be followed to the greatest extent possible.

Objective 4.5 - Allow pocket parks as an appropriate public use of the right of way. Not every portion of the right of way should be developed. Pocket parks can be a legitimate use that disconnects impervious street surfaces, allows for runoff infiltration, moderates traffic flow, and enhances neighborhood aesthetics and the pedestrian experience.

Objective 4.6 - Minimize the amount and size of signage. Too much signage not only destroys the beauty of a neighborhood, it is also ineffective at conveying information. The reader becomes inured.

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Conceptual Street Designs

STREETSCAPES	STREETS
1	Alder 100-200 N
2	Aspen 100-400 N
3	Aspen 100-200 S
4	Colorado 400-900 E
5	Columbia 100-600 W
6	Columbia 100-900 E
7	Columbine 100-200 N
8	Dakota
9	Davis 200 N
10	Davis 100-200 S
11	Fir 100-300 N
12	Fir 200-300 S
13	Galena 100-600 W
14	Galena 100-300 E
15	Gregory 100-200 E
16	Gregory 100-200 W
17	Laurel 100 N
18	Hemlock 100 N
19	Maple 100-200 N
20	Oak 100-300 N
21	Oak 100-200 S
22	Pacific 100-300 E
23	Pacific 1/2 100-200 W
24	Pandora 400-600 E
25	Pine 100-300 N
26	Pine 200-300 S
27	Pinon 100 N
28	San Juan 100-300 W
29	Spruce 100-300 N
30	Spruce 100-200 S
31	Tomboy Road
32	Townsend 100-200 N
33	Townsend 100-200 S
34	Willow 100-300 N
35	Willow 100-200 S

NORTH ALDER STREET - 100 BLOCK

50' Right of Way

Neighborhood Meetings: July 16, 1996; February 24, 1997

BACKGROUND

Existing Street Conditions. This block was improved during Streetscape Phase I, 1998, Construction. Paved with asphalt, it slopes to the south. The concrete curb and gutter along the west side of the block is in good condition, as is the 3' concrete v-pan that borders the edge of asphalt along the east side. An alley intersects the block, but there are no other curb cuts. There is a 5'-wide sidewalk on the west side of the block. South of the alley there is a 3'-wide green strip that separates the sidewalk from the travel way; north of the alley the green strip is on the residential side to accommodate historic encroachments. There is a 15.5'-wide vegetated berm along the entire length of the east side to protect nearby residences from large runoff flows that are directed onto this block. Runoff management is a continued challenge on this block and the north end and at the east and west alley curb cuts. Re-grading and re-contouring the travel way and widening the western 3' v-pan to 5' are two possibilities for improving drainage flow.

Environment/Drainage/Snow Storage. While this block, as it is configured, is consistent with Telluride's snow storage and drainage plans and the existing curb and gutter drainage system effectively handles runoff during a typical runoff event, there is a continued flooding problem during larger runoff events. It should be noted that this block receives up to 70 percent of the runoff in Telluride. Re-grading and re-contouring the travel way and widening the western 3' v-pan to 5' are two possibilities for improving drainage flow. Two large cement sedimentation vaults were installed underneath the northern third of this block in 1998 to remove sediment from runoff flows. The surface and subsurface drainage system requires special maintenance to ensure the fugitive sediment that settles out here does not adversely impact air and water quality. To this end, the drop inlets and sedimentation vaults are completely cleaned with a vacuum truck every six months. Snow is typically stored along the west side in winter.

Historic Character. There is one contributing structure on the west side of this block. Any alterations to the right of way should not negatively impact this historic resource.

Interfacing Streets. The interfacing streets function well with this block. The interfacing alleys create sediment and flooding problems that require an engineering remedy.

Lighting. There are three standard commercial light fixtures on this block. There is one each on the southeast and southwest corner and one at the west alley entrance. These are consistent with the Streetscape Lighting Plan.

Neighborhood Concerns. The neighborhood has expressed concern about flooding during large storms.

Parking. Parallel parking is only allowed along the west side of this block. Intensity of on-street parking is moderate to heavy, due to North Alder's proximity to the commercial core and residential need. Despite the parking pressure, current parking configurations should be maintained. Use of existing off-street parking in this vicinity should be maximized.

Pedestrian Concerns. The existing west side sidewalk effectively handles the moderate pedestrian traffic along this block. There is handicap access.

Street Furniture and Public Art. No street furniture or amenities exist on the block. None are recommended.

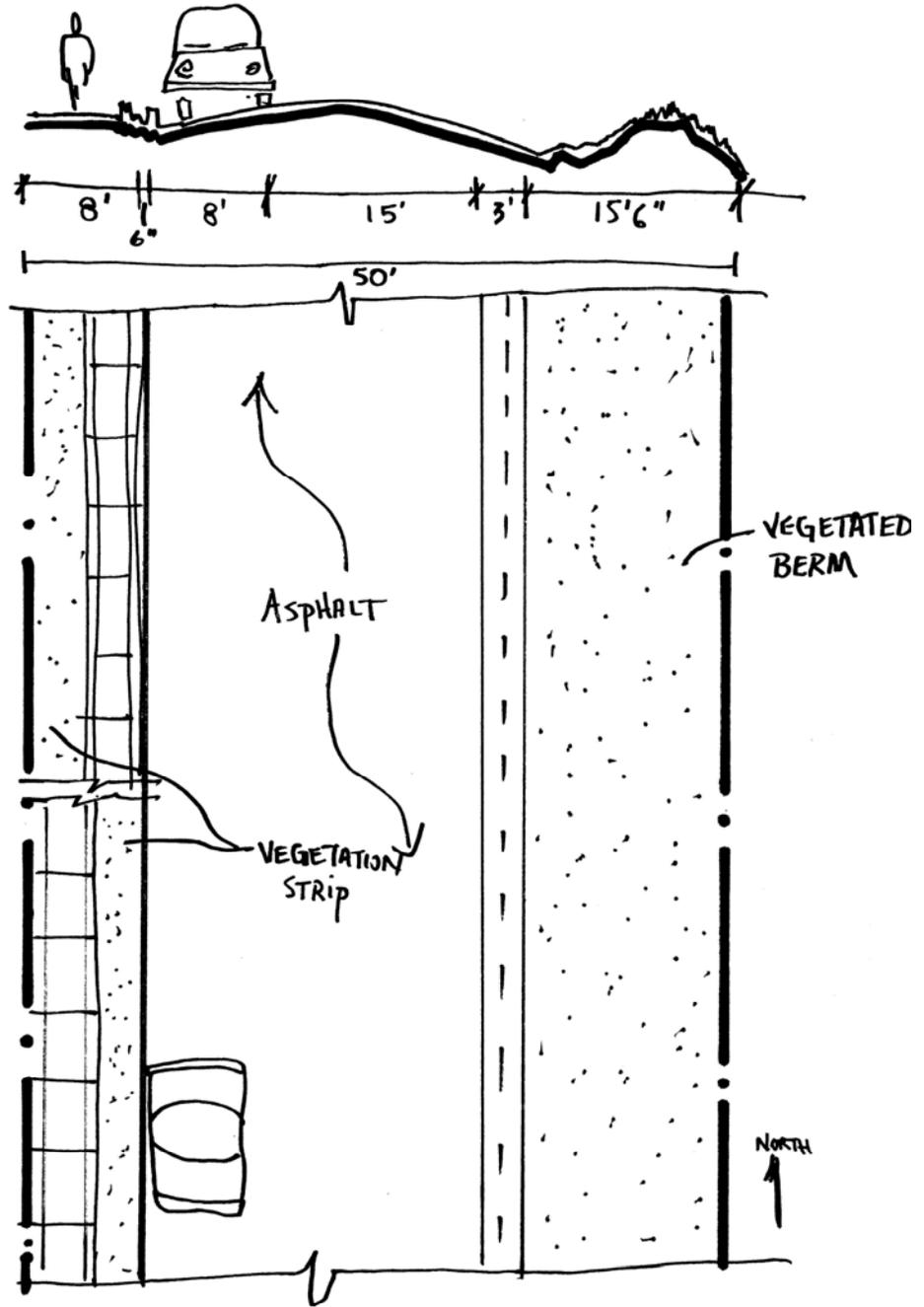
Traffic Circulation. Vehicular traffic on this block is moderate to heavy, and carrying capacity is usually adequate for intensity of use. Emergency vehicle access also appears adequate.

Utilities. Minimize or eliminate above ground utilities. If possible, locate electricity transformer boxes on private property. If they are located in the right of way, they should be as inconspicuous as possible, and concrete bollards will be needed to protect them from vehicle impact.

RECOMMENDED IMPROVEMENTS

1. Re-engineer street grades (e.g., add a more significant crown) to improve drainage from travel way.
2. Replace the 3'-wide drain pan along the west side with a 5'-wide pan.

Refer to Streetscape Phase I, 1998, Engineered Street Designs and adjacent street profiles for additional information.



NORTH ALDER STREET - 100 BLOCK
(BETWEEN COLORADO & COLUMBIA)

NORTH ALDER STREET - 200 BLOCK

50' Right of Way

Neighborhood Meetings: July 16, 1996; February 24, 1997

BACKGROUND

Existing Street Conditions. This block, improved during Streetscape Phase I, 1998, Construction, was paved with asphalt to Pandora and tined concrete north of Pandora. It slopes steeply to the south. There are 3'-wide concrete drain pans along the east and west side. The travel way and drainage systems are in good condition. There are no sidewalks or special handicap access on this block. The green strip along the west side is 13' wide. The width of the green strip along the east side varies as the width of the travel way varies: 22' wide at the south end and 10' wide at the north end.

Environment/Drainage/Snow Storage. The existing drainage system effectively handles runoff and the green strips are useful for snow storage in winter. This block does not negatively affect air and water quality. It is consistent with Telluride's snow storage and drainage plans.

Historic Character. There are numerous rated and non-rated structures on this block. Any alterations to the right of way should not negatively impact these historic resources.

Interfacing Streets. Interfacing streets function well with this block.

Lighting. There is currently one residential pole-mounted light fixture on this block at the southeast corner that should be replaced with a fixture that is consistent with the Streetscape Lighting Plan.

Parking. Due to the extremely steep grade, parallel parking is not allowed on either side of this block. Intensity of on-street parking is light and current parking configurations should be maintained. But residential parking pressure requires that the use of existing off-street parking be maximized.

Pedestrian Concerns. Pedestrian traffic along this block is mainly local residents and is therefore light. Pedestrians often use the green strips or the middle of the street. There is no special handicap access.

Street Furniture and Public Art. No street furniture or amenities exist on the block. None are recommended.

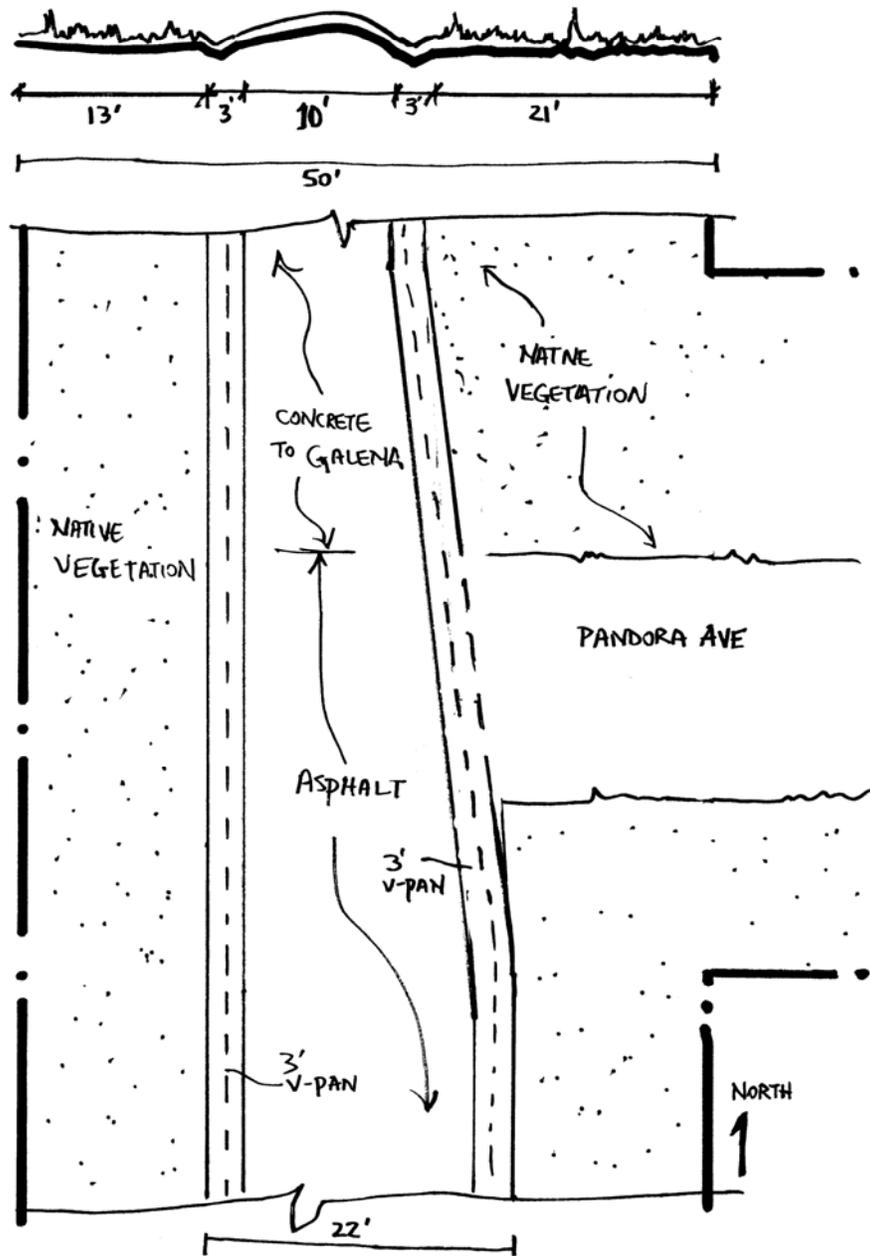
Traffic Circulation. Vehicular traffic on this block is mostly local, due to the very steep grade, and capacity appears adequate for the intensity of use and emergency vehicle access.

Utilities. Minimize or eliminate above ground utilities. If possible, locate electricity transformer boxes on private property. If they are located in the right of way, they should be as inconspicuous as possible and concrete bollards will be needed to protect them from vehicle impact.

RECOMMENDED IMPROVEMENTS

1. Steep driveway entrances could be paved or concreted to decrease sediment loading to drain pans.

Refer to Streetscape Phase I, 1998, Engineered Street Designs and adjacent street profiles for additional information.



NORTH ALDER STREET - 200 BLOCK
(BETWEEN COLUMBIA & GALENA)

NORTH ASPEN STREET - 100 BLOCK

50' Right of Way

Neighborhood Meetings: November 3, 1999; January 19, 2000

BACKGROUND

Existing Street Conditions. This block was improved during Streetscape Phase II, 2000, Construction. Paved with asphalt, it slopes down to the south. The concrete curb and gutter along the east and west sides is in good condition and efficiently drains runoff south to Colorado Avenue. An alley intersects the block, and there are two other curb cuts. There is a 7'-wide sidewalk on the west side of the block and a dirt and gravel path on the east side. There are no green strips.

Environment/Drainage/Snow Storage. The existing curb and gutter drainage system effectively handles runoff. Snow is temporarily stored along the east side during winter. This block does not negatively affect air and water quality and is consistent with Telluride's snow storage and drainage plans.

Historic Character. There are numerous rated and non-rated structures on this block. Any alterations to the right of way should not negatively impact these historic resources.

Interfacing Streets. All interfacing streets function well with this block.

Lighting. There is currently one residential pole-mounted light fixture on this block at the northwest corner, which should be replaced with a fixture that is consistent with the Streetscape Lighting Plan. There is also a standard commercial street light on the southwest corner, which should remain.

Parking. Parallel parking is allowed on both sides of this block, except in winter, when parking is not permitted on the east side to allow for temporary snow storage. Intensity of on-street parking is heavy due to North Aspen's proximity to the commercial core. Despite the parking pressure, existing parking configurations should be maintained and use of existing off-street parking should be maximized.

Pedestrian Concerns. Moderate to heavy pedestrian traffic along this block will eventually require that a sidewalk be constructed on the east side. The unimproved east side pathway hinders safe pedestrian travel through the area, particularly in winter. As well, the building on the northeast corner of North Aspen and West Colorado sheds snow in winter, making walking underneath hazardous. There is handicap access.

Street Furniture and Public Art. No street furniture or amenities exist on the block. None are recommended.

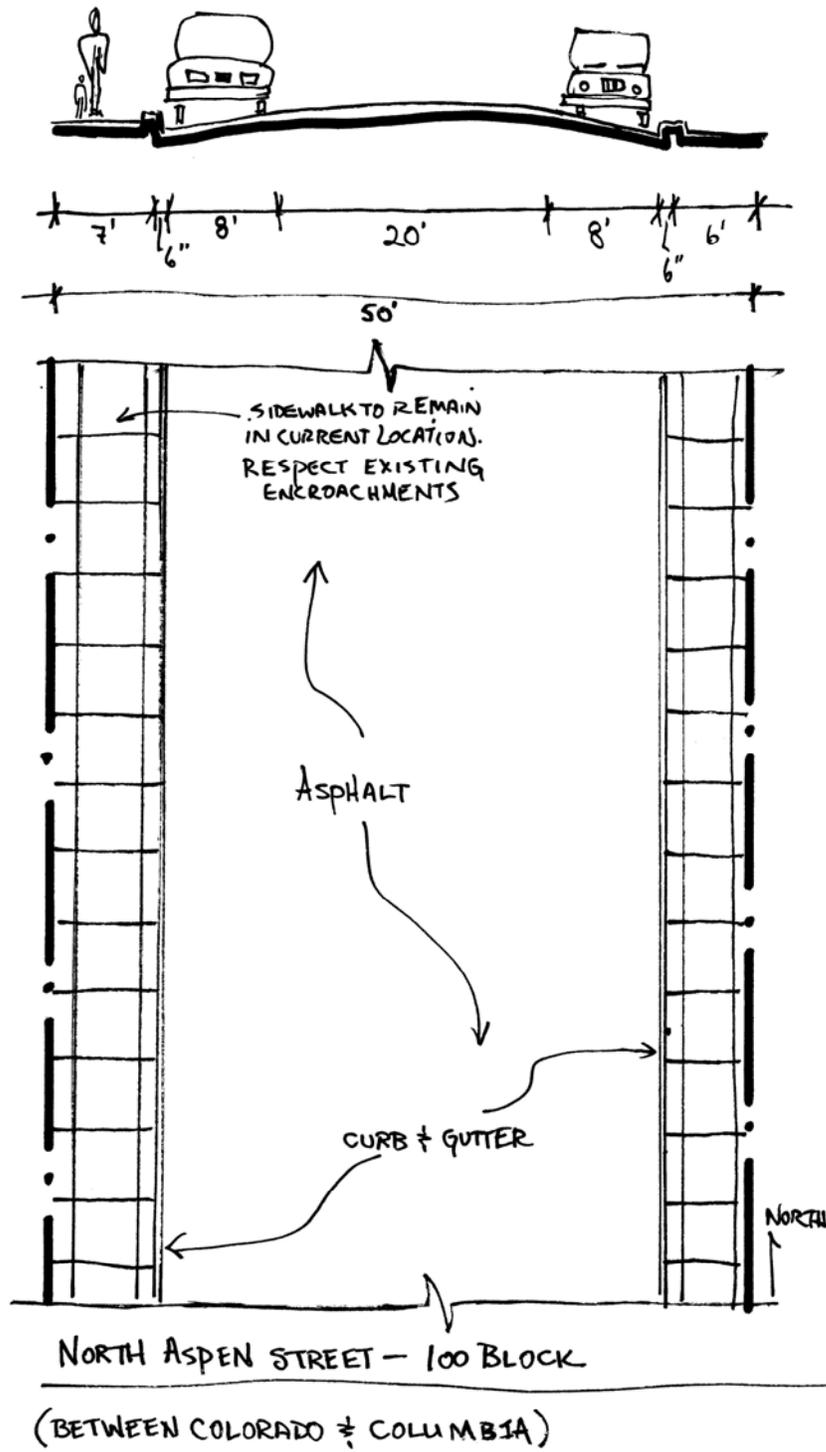
Traffic Circulation. Vehicular traffic on this block is moderate to heavy. On occasion, Public Works closes the travel way under extremely icy conditions to prevent cars from sliding onto West Colorado Avenue. Capacity appears adequate for intensity of use and emergency vehicle access.

Utilities. No utility concerns are mentioned in meeting notes. However, the two utility poles located in the gutter on the west side of the block should be relocated out of the drainage, and aboveground utilities should be minimized or eliminated. If possible, locate electricity transformer boxes on private property. If they are located in the right of way, they should be as inconspicuous as possible and concrete bollards will be needed to protect them from vehicle impact.

RECOMMENDED IMPROVEMENTS

1. Construct a new 6'-wide sidewalk on east side.
2. Require 335 West Colorado Avenue to retain snow on its roof or otherwise ensure the safety of pedestrians in winter regarding their roof shed.
3. Underground utilities or relocate the existing utility poles out of the drainage and without obstructing pedestrian traffic.
4. Replace the existing residential pole-mounted light fixture with a fixture consistent with the Streetscape Lighting Plan.

Refer to Streetscape Phase II, 2000, Engineered Street Designs and adjacent street profiles for additional information.



NORTH ASPEN STREET – 200 BLOCK

50' Right of Way

Neighborhood Meetings: November 3, 1999; January 19, 2000

BACKGROUND

Existing Street Conditions. This block was improved during Streetscape Phase II, 2000, Construction. Paved with asphalt, it slopes south. The concrete curb and gutter along both sides of the block is in good condition and effectively drains runoff to Columbia Avenue and onto the 100 Block of North Aspen Street. There is a 7'-wide sidewalk along the west side and a 5'-wide sidewalk along the east side. Both are in good condition. There are no green strips.

Environment/Drainage/Snow Storage. The existing curb and gutter drainage system effectively handles runoff. In winter, snow is stored temporarily on the west side. This block does not negatively affect air and water quality and is consistent with Telluride's snow storage and drainage plans.

Historic Character. There are numerous rated and non-rated structures on this block. Any alterations to the right of way should not negatively impact these historic resources.

Interfacing Streets. The interfacing streets appear to function well with this block.

Lighting. There are no streetlights on this block and none are required.

Parking. Parallel parking is allowed on both sides of this block. In winter, parking is not permitted along the west side to allow for snow storage. Intensity of on-street parking is moderate to heavy, due in part to proximity to the commercial core. Despite the parking pressure, current parking configurations should be maintained and use of existing off-street parking should be maximized.

Pedestrian Concerns. Pedestrian traffic on this block is moderate to heavy. Existing sidewalks effectively handle pedestrian traffic. There is handicap access.

Street Furniture and Public Art. No street furniture or amenities exist on the block. None are recommended.

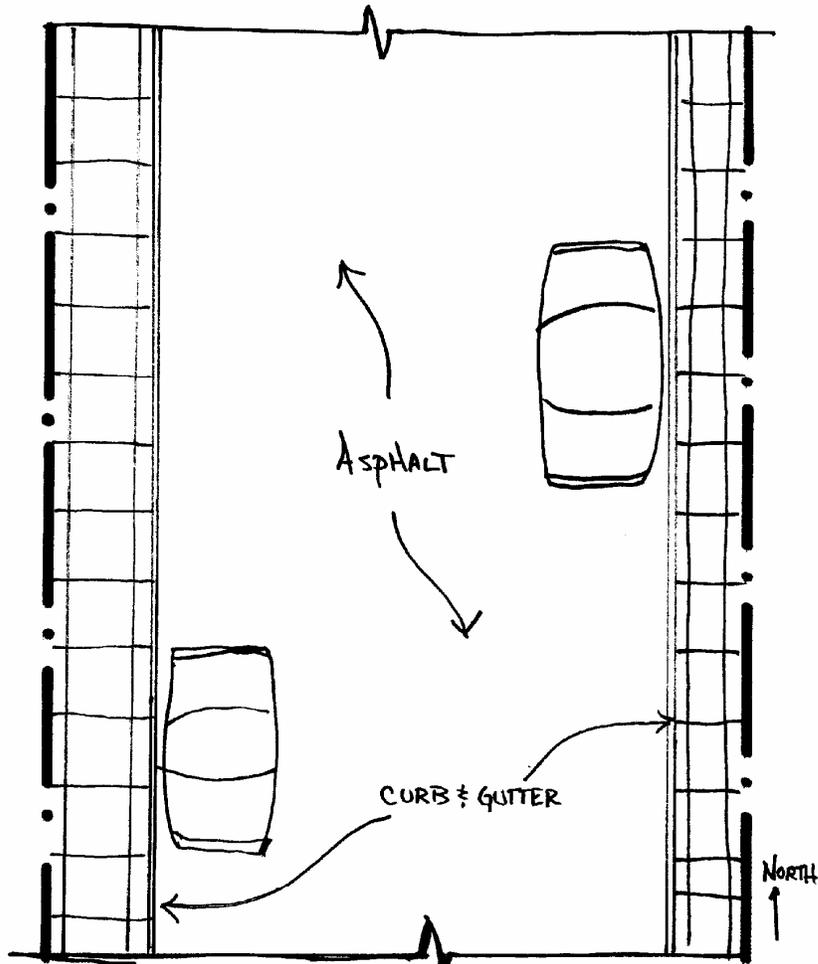
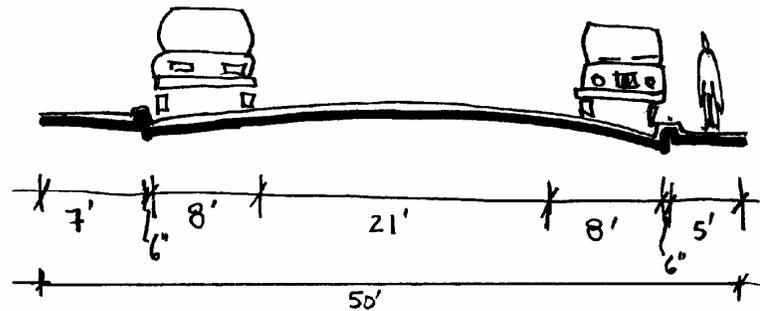
Traffic Circulation. This block is vital to traffic associated with the Elementary School and all decisions regarding design, parking, and traffic flow should take this into consideration. Carrying capacity appears adequate for the moderate to heavy vehicular traffic. Emergency vehicle access also appears adequate.

Utilities. No utility concerns are mentioned in meeting notes. Minimize or eliminate above ground utilities. If possible, locate electricity transformer boxes on private property. If they are located in the right of way, they should be as inconspicuous as possible and concrete bollards will be needed to protect them from vehicle impact.

RECOMMENDED IMPROVEMENTS

No improvements are recommended at this time.

Refer to Telluride Streetscape Phase II, 2000, Engineered Street Designs and adjacent street profiles for additional Information.



NORTH ASPEN STREET - 200BLOCK
(BETWEEN COLUMBIA & GALENA)

NORTH ASPEN STREET – 300 BLOCK

50' Right of Way

Neighborhood Meetings: November 3, 1999; January 19, 2000

BACKGROUND

Existing Street Conditions. This block was improved during Streetscape Phase II, 2000, Construction. Paved with asphalt, it slopes moderately to the south. There is a concrete curb and gutter and 5'-wide sidewalk along the west side. Both are in good condition. Along the east side, there is a 6'-wide strip of asphalt at the edge of the travel way separated from the rest of the travel way by a 3'-wide concrete drain pan. This design allows vehicles to park on the east side of the street so that they straddle the drain pan, preventing tires from blocking drainage flow. The drainage system effectively moves runoff south to Galena Avenue and onto the 200 Block of North Aspen Street. There are numerous encroachments along east side of the right of way. Because of this, the eastern 10' of right of way is designated as green space and is not part of the travel way. The west side of this block is in the Boulder Village PUD, which does not have traditional alleys and has numerous driveway cuts.

Environment/Drainage/Snow Storage. The existing curb and gutter and drain pan effectively handle runoff. Snow is stored along the west side in winter. This block does not negatively affect air and water quality and is consistent with Telluride's snow storage and drainage plans.

Historic Character. There are two rated structures on this block. Any alterations to the right of way should not negatively impact these historic resources.

Interfacing Streets. The interfacing streets function well with this block.

Lighting. The one residential pole-mounted light fixture on this block, located at the southeast corner, should be replaced with a fixture that is consistent with the Streetscape Lighting Plan.

Neighborhood Concerns. No major neighborhood concerns have been expressed.

Parking. Permit parallel parking is allowed on both sides of this block. In winter, parking is not permitted on the west side to allow for snow storage. Intensity of on-street parking is moderate, due in part to the limited parking availability on Dakota Avenue, which causes spillover from that area to this block. People using the Jud Wiebe Trail also tend to park here. Despite the parking pressure, the current parking configurations should be maintained and use of existing off-street parking maximized.

Pedestrian Concerns. The west sidewalk effectively handles the moderate amount of pedestrian traffic on this block. There is handicap access.

Street Furniture and Public Art. No street furniture or amenities exist on the block. None are recommended.

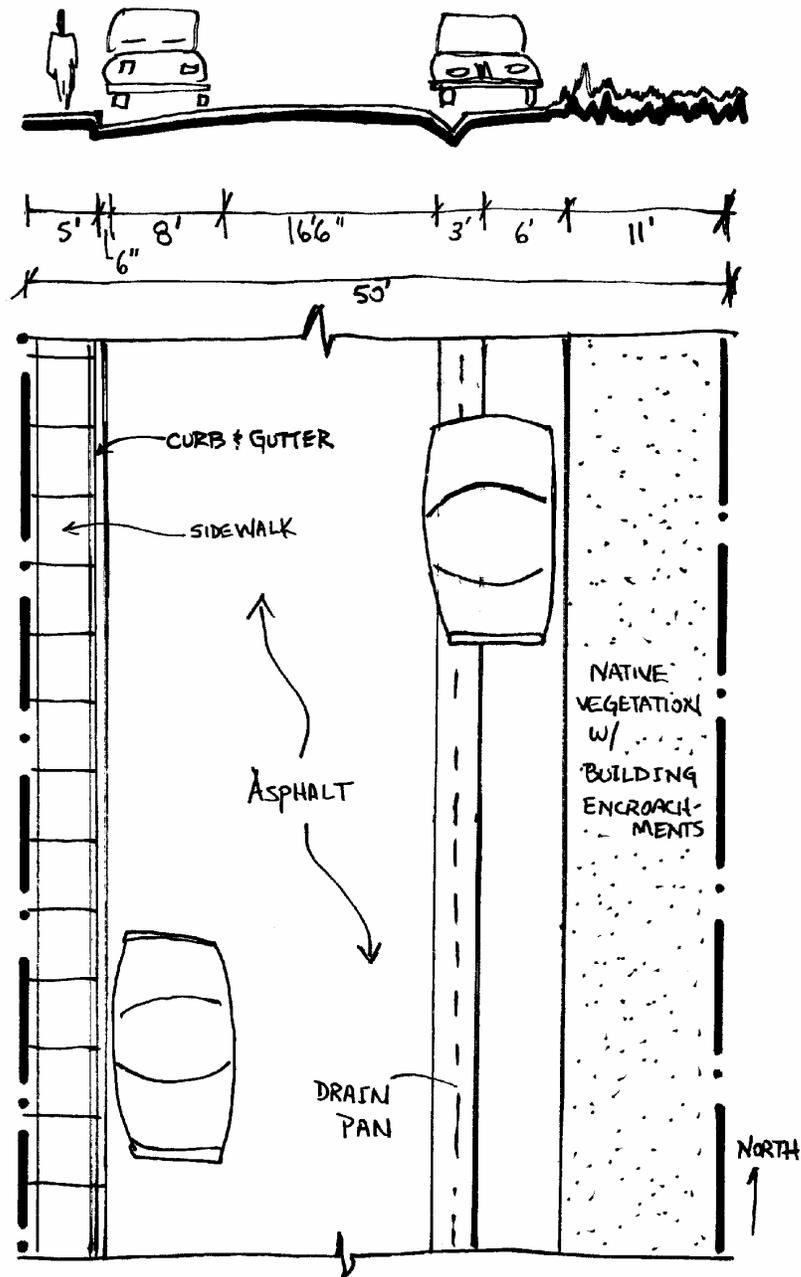
Traffic Circulation. Capacity appears adequate for the moderate vehicular traffic on this block. Emergency vehicle access also appears adequate.

Utilities. No utility concerns are mentioned in meeting notes. Minimize or eliminate above ground utilities. If possible, locate electricity transformer boxes on private property. If they are located in the right of way, they should be as inconspicuous as possible and concrete bollards will be needed to protect them from vehicle impact.

RECOMMENDED IMPROVEMENTS

1. Replace the existing residential pole-mounted light fixture with a fixture that is consistent with the Streetscape Lighting Plan.

Refer to Streetscape Phase II, 2000, Engineered Street Designs and adjacent street profiles for additional information.



NORTH ASPEN STREET - 300 BLOCK
(BETWEEN GALENA & DAKOTA)

NORTH ASPEN STREET - 400 BLOCK

50' Right of Way

Neighborhood Meetings: November 3, 1999

BACKGROUND

Existing Street Conditions. This block was improved during Streetscape Phase IIA, 2002, Construction. It is not typical of other streets in town as it is a short, narrow dead end travel way that provides access to a few houses and empty lots and serves as access to the Jud Wiebe Trail. Several large boulders mark the northern extent of the travel way and the beginning of the Wiebe. Sloping steeply to the south, this block is surfaced with gravel and bordered along the west by a 3' concrete and cobble drainage swale. There are no sidewalks. Drainage from this block does not appear efficient. The green strips are natural in character. There are numerous encroachments into the right of way.

Environment/Drainage/Snow Storage. Runoff tends to carry gravel and silt into the western drainage swale and onto the 300 Block. This sediment can negatively impact air and water quality, if the street and drainage is not regularly cleaned. Surfacing the travel way with asphalt or grooved concrete, depending on the steepness of the grade, may solve this problem. This block is consistent with Telluride's snow storage and drainage plans.

Historic Character. There are no rated structures on this block. The right of way neither contributes to nor detracts from the historic character of the area.

Interfacing Streets. Currently the drainage from this block is inefficient as it connects to the drainage system on the 300 Block of North Aspen Street. Regular maintenance of the western drain pan and/or surfacing of the travel way may correct this problem.

Lighting. There are no streetlights on this block and none are required.

Parking. A narrow, steep travel way that terminates at a dead end makes on street parking impractical. Therefore, no on-street parking is currently permitted and none should be permitted in the future, regardless of improvements. Existing off-street parking at each residence should therefore be maintained and future residences should be sure to provide sufficient off-street parking, as well.

Pedestrian Concerns. As this block provides access to one end of the Jud Wiebe Trail, pedestrian traffic is substantial. There are no sidewalks, and due to low vehicular traffic and a desire to maintain the "rough and natural" character of the block, none are required. There is no special handicap access.

Street Furniture and Public Art. Existing informational signs at the north end of this block at the head of the Jud Wiebe Trail should be maintained. Additional street furniture or amenities could be considered at this location.

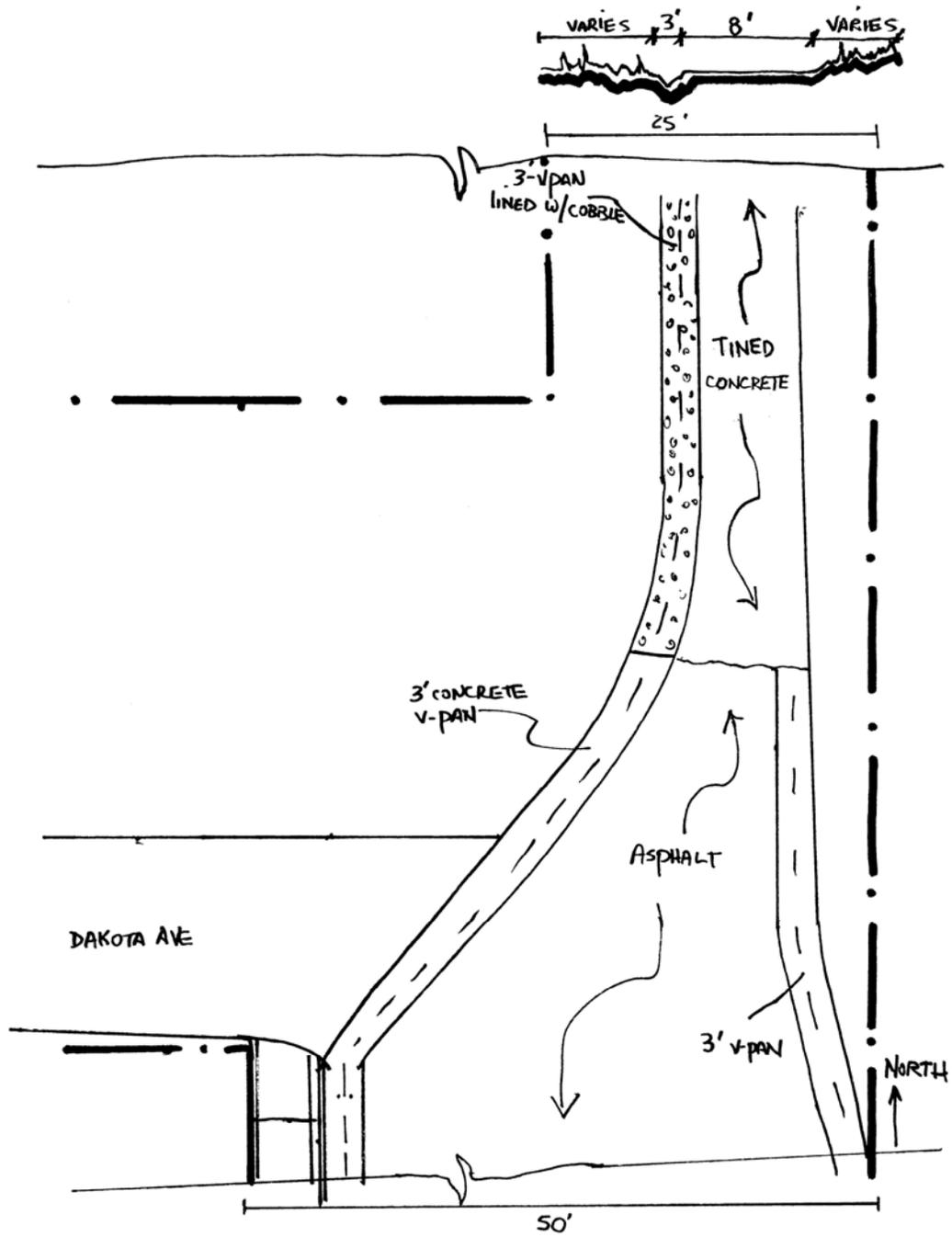
Traffic Circulation. Vehicular traffic intensity is low—attributed to owners and guests of the few houses on the block. The block is very narrow and steep and, in its current condition, emergency vehicle access appears difficult.

Utilities. No utility concerns are mentioned in meeting notes. Minimize or eliminate above ground utilities. If possible, locate electricity transformer boxes on private property. If they are located in the right of way, they should be as inconspicuous as possible and concrete bollards will be needed to protect them from vehicle impact.

RECOMMENDED IMPROVEMENTS

1. Surface entire block with tined concrete to accommodate the extremely steep grade.

Refer to Streetscape Phase IIA, 2002, Engineered Street Designs and adjacent street profiles for additional information.



NORTH ASPEN STREET - 400 BLOCK
(BETWEEN DAKOTA & JUD WIEBE TRAILHEAD)

SOUTH ASPEN STREET – 100 BLOCK

50' Right of Way

Neighborhood Meetings: July 24, 1996; November 3, 1999; January 19, 2000

BACKGROUND

Existing Street Conditions. This block was improved during Streetscape Phase II, 2000, Construction. Paved with asphalt it slopes moderately south. The curb and gutter along the east and west side drains runoff to West Pacific Avenue. An east-west alley whose entrances do not align bisects the block. There is a 6'-wide sidewalk along the east side, and a 7'-wide sidewalk along the southwest side, which was extended to the corner at West Colorado in fall 2006. All existing infrastructure is in good condition. There are no green strips.

Environment/Drainage/Snow Storage. The existing curb and gutter effectively handles runoff. Snow is stored temporarily along the west side in winter. Traffic on the unpaved alleys track mud to the paved street, which negatively impacts air and water quality. This block is consistent with Telluride's snow storage and drainage plans.

Historic Character. There is one rated structure on this block. Any alterations to the right of way should not negatively impact this historic resource.

Interfacing Streets. All interfacing streets appear to function well with this block.

Lighting. There is currently one standard commercial streetlight on the northeast corner and one on the southwest corner of this block. This is consistent with the Streetscape Lighting Plan.

Parking. Parallel parking is allowed on both sides of this block, except in winter, when parking is not permitted on the west side to allow for temporary snow storage. Intensity of on-street parking is heavy due to South Aspen's proximity to the commercial core. Despite the parking pressure, current parking configurations should be maintained and use of existing off-street parking should be maximized.

Pedestrian Concerns. There is moderate to heavy pedestrian traffic along this street because it provides a natural pedestrian path from the River Trail and the Gondola Plaza businesses north to the commercial core. This intense usage requires that the missing northwest section of sidewalk be constructed quickly, as its currently unimproved condition hinders safe pedestrian travel through the area, particularly in winter. There is adequate handicap access.

Street Furniture and Public Art. No street furniture or amenities exist on the block. None are recommended.

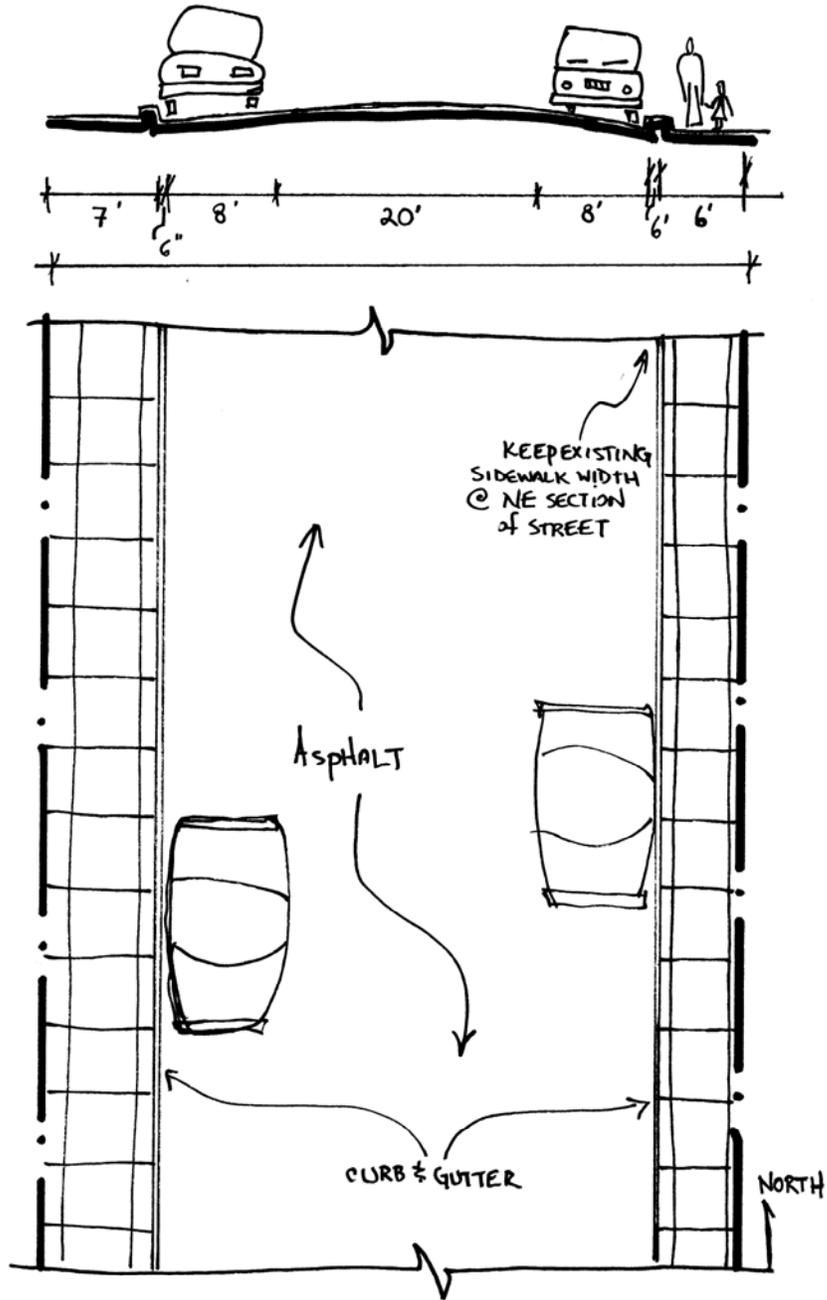
Traffic Circulation. Vehicular traffic on this block is moderate to heavy. A good deal of commercial traffic for the east-west alley enters from this block. Capacity appears adequate for intensity of use and emergency vehicle access. However, parking along the west side should be prohibited within 30 feet of the north limit to improve the ability of vehicles turning right off of Colorado to successfully negotiate the intersection.

Utilities. No utility concerns are mentioned in meeting notes. Minimize or eliminate above ground utilities along the alley. If possible, locate electricity transformer boxes on private property. If they are located in the right of way, they should be as inconspicuous as possible and concrete bollards will be needed to protect them from vehicle impact.

RECOMMENDED IMPROVEMENTS

1. Pave the intersecting alleys to decrease the amount of sediment tracked onto this block.
2. Consider improving the east drainage, which flows up the alley curb cut and onto the sidewalk.

Refer to Streetscape Phase II, 2000, Engineered Street Designs and adjacent street profiles for additional information.



SOUTH ASPEN STREET - 100 BLOCK
(BETWEEN COLORADO & PACIFIC)

SOUTH ASPEN STREET - 200 BLOCK

50' Right of Way

Neighborhood Meetings: July 24, 1996; November 3, 1999; January 19, 2000

BACKGROUND

Existing Street Conditions. This block was improved during Streetscape Phase II, 2000, Construction. Paved with asphalt it slopes to the south. The curb and gutter along the east and west side drains runoff to two drop inlets that lead to an underground sediment vault and then to the San Miguel River. There is a 6.5'-wide sidewalk along the east side, an 8.5'-wide sidewalk along the southwest side, and no walkway along the northwest side. The large cottonwood trees that are along this northwest section should be protected. All existing infrastructure is in good condition. There are no green strips.

Environment/Drainage/Snow Storage. The existing curb and gutter effectively handles runoff and the underground sediment vault removes over half of the sediment and oil and grease that are in the runoff prior to discharging it to the San Miguel River. Snow is stored temporarily along the west side in winter. This block does not negatively affect air and water quality and is consistent with Telluride's snow storage and drainage plans.

Historic Character. There is one rated structure on this block. Any alterations to the right of way should not negatively impact this historic resource.

Interfacing Streets. All interfacing streets appear to function well with this block.

Lighting. There is currently one standard commercial streetlight on the northeast corner and one on the alley on the west side of this block. This is consistent with the Streetscape Lighting Plan.

Parking. Parallel parking is allowed on both sides of this block, except in winter, when parking is not permitted on the west side to allow for temporary snow storage. Intensity of on-street parking is heavy due to South Aspen's proximity to the commercial core and the Gondola Plaza. Despite the parking pressure, current parking configurations should be maintained and use of existing off-street parking should be maximized.

Pedestrian Concerns. There is moderate to heavy pedestrian traffic along this street because it provides a natural pedestrian path from the River Trail and the Gondola Plaza businesses north to the commercial core. This intense usage requires that the missing northwest section of sidewalk be constructed quickly, as its currently unimproved condition hinders safe pedestrian travel through the area, particularly in winter. There is adequate handicap access.

Street Furniture and Public Art. The array of benches at the Depot Avenue cul-de-sac and across from the Columbia Hotel appears to be sufficient. No additional street furniture or amenities are recommended.

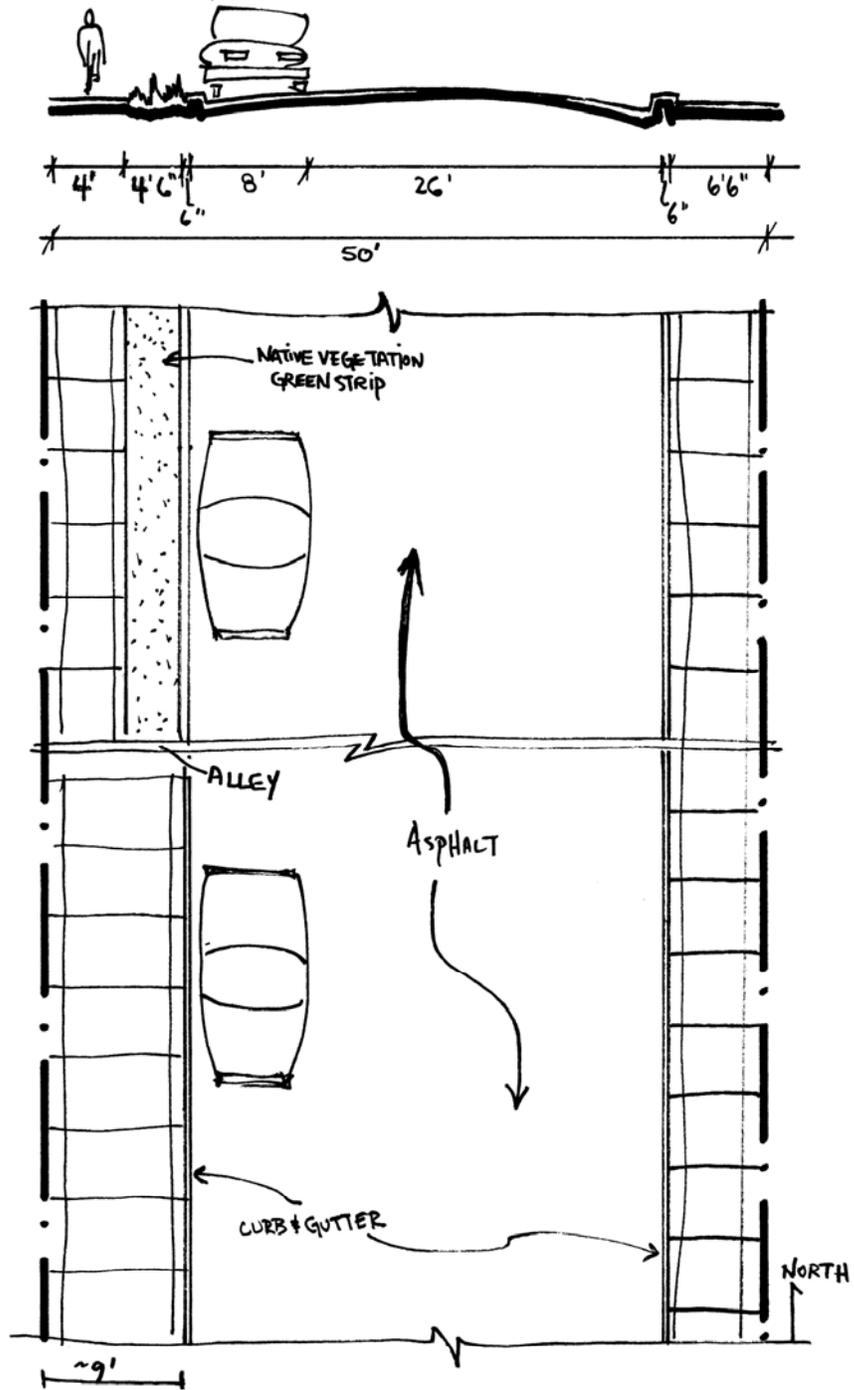
Traffic Circulation. Vehicular traffic on this block is moderate to heavy. This block is integral to the Galloping Goose Transit Town Loop. Capacity appears adequate for intensity of use and emergency vehicle access.

Utilities. No utility concerns are mentioned in meeting notes. Minimize or eliminate above ground utilities along the alley. If possible, locate electricity transformer boxes on private property. If they are located in the right of way, they should be as inconspicuous as possible and concrete bollards will be needed to protect them from vehicle impact.

RECOMMENDED IMPROVEMENTS

1. Construct a 4-foot wide section of sidewalk along the northwest half of the block, making cut outs if needed for the large cottonwoods that are in the right of way.

Refer to Streetscape Phase II, 2000, Engineered Street Designs and adjacent street profiles for additional information.



SOUTH ASPEN STREET - ZOO BLOCK
(BETWEEN PACIFIC & W SAN JUAN)

EAST COLORADO AVENUE – 400 BLOCK

80' Right of Way

Neighborhood Meetings: July 16, 1996; January 20, 1999; June 28, 2005; Jun 29, 2005; July 21, 2005

BACKGROUND

Existing Street Conditions. This block of East Colorado Avenue is the transition between the commercial core and the residential section of East Telluride. The north side is lined with houses; to the south are Town Park and the San Miguel River. The Town and River Park Master Plan describe the south side, beyond the right of way, in some detail. This block is currently unimproved with approximately 24' of asphalt in the center, a wide area of asphalt up to a curb and gutter that delineates Town Park parking on the south, and a 9' dirt and gravel shoulder and wetland drainage on the north. Festival delivery trucks and buses rely on the expanse of asphalt to the south of the travel way. This section of East Colorado Avenue slopes lightly to the west. There is no sidewalk along the north side. The River Trail and Town Park parking are adjacent to the right of way on the south; however, there is a missing segment of sidewalk between the 300 Block of East Colorado Avenue and this block. There is a green strip that separates this parking from the travel way.

Environment/Drainage/Snow Storage. The south side curb and gutter and the north side drainage swale is fairly effective at handling runoff. On the south side, the drainage connection between East Colorado Avenue and the Alder Runoff Treatment Wetland needs to be improved as a large puddle forms whenever there is runoff. Runoff from this block of East Colorado Avenue is treated by the wetland prior to its discharge to the San Miguel River. Snow is stored temporarily along the south side in winter. This block does not negatively affect air and water quality and is consistent with Telluride's snow storage and drainage plans.

Historic Character. There are no rated structures on this block. The right of way neither contributes to nor detracts from the historic character of the area.

Interfacing Streets. All interfacing streets appear to function well with this block.

Lighting. There is currently one standard commercial streetlight on the northeast corner and one on the northwest corner of this block. These are consistent with the Streetscape Lighting Plan.

Neighborhood Concerns. The neighborhood is very concerned with the parking overflow from Town Park activities and motor vehicle speeding.

Parking. No parking is allowed on either side of this block to ensure that Galloping Goose Transit buses and delivery trucks have sufficient room to turn around at this location. Current parking configurations should be maintained and use of existing off-street parking should be maximized.

Pedestrian Concerns. There is moderate to heavy pedestrian traffic along this block because it links the commercial core with the Town Park, east end residences, and east end recreation. Therefore, a sidewalk, with handicap access is needed on the north side and a clearly delineated crosswalk that announces the main entrance to Town Park and minimizes conflicts between pedestrians and motor vehicles. The missing section of sidewalk that would connect the 300 Block of East Colorado on the south side is important to complete as the poor drainage pushes a large number of pedestrians into Main Street traffic. There is a need to slow vehicle traffic on this block.

Street Furniture and Public Art. No street furniture or amenities are recommended within the right of way. Sufficient benches are available at the Bus Stop at the Town Park entrance.

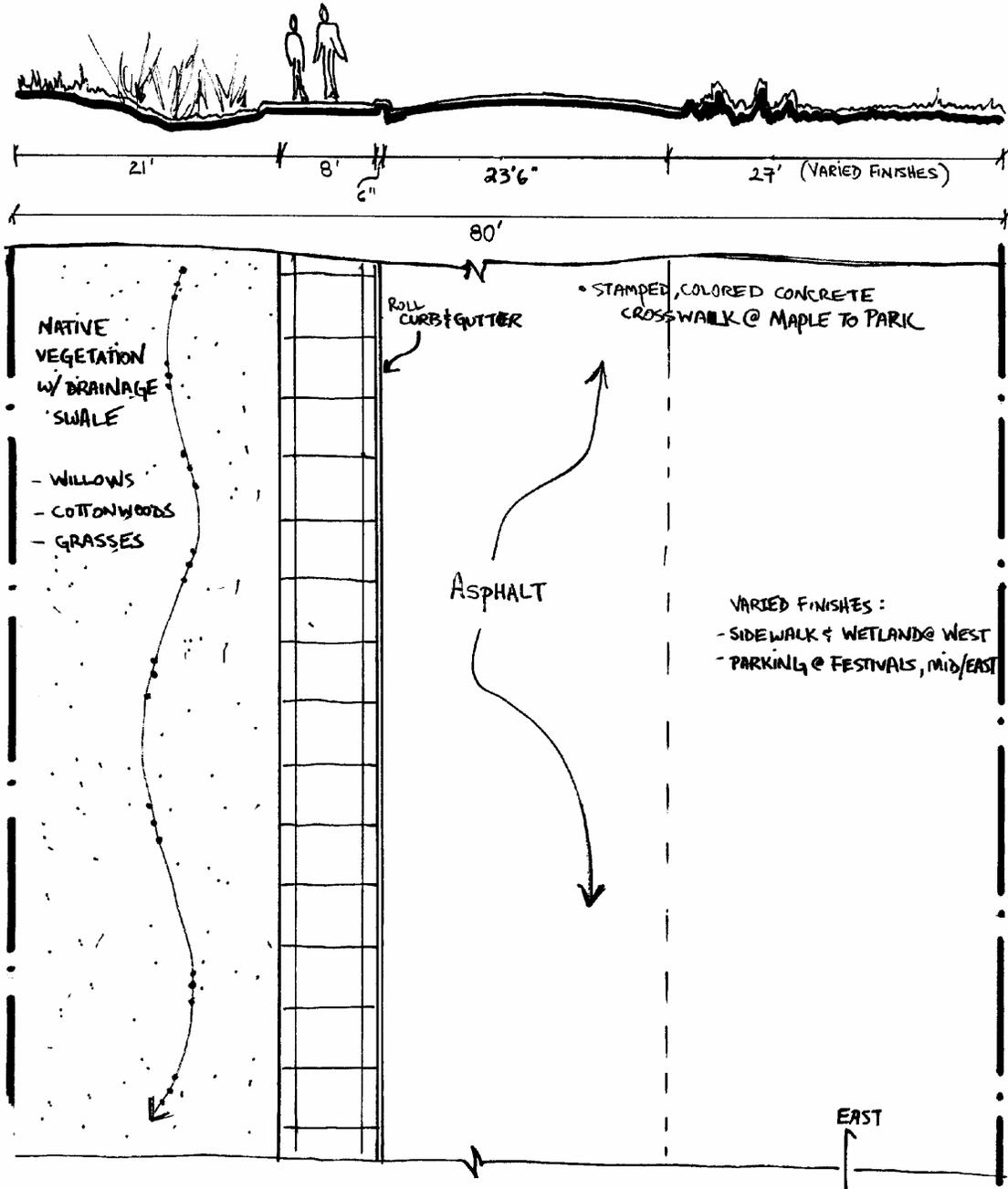
Traffic Circulation. Vehicular traffic on this block is moderate to heavy and it tends to exceed the speed limit upon exiting the commercial core. This block is integral to the Galloping Goose Transit Town Loop and there is a bus turnaround incorporated into the Town Park parking area. Capacity appears adequate for intensity of use and emergency vehicle access.

Utilities. No utility concerns are mentioned in meeting notes. All utilities are underground.

RECOMMENDED IMPROVEMENTS

1. Narrow the travel way width.
2. Construct an 8'-wide sidewalk on the north side.
3. Construct a sidewalk connection between the 300 Block of East Colorado and the sidewalk that exists at the Alder Street Wetlands.

Refer to Streetscape East Telluride District, 2006, Engineered Street Designs and adjacent street profiles for additional information.



EAST COLORADO AVENUE - 400 BLOCK
(BETWEEN ALDER & MAPLE)

EAST COLORADO AVENUE – 500 & 600 BLOCKS

80' Right of Way

*Neighborhood Meetings: July 15, 1996; July 16, 1996; January 20, 1999;
June 28, 2005; June 29, 2005; July 21, 2005*

BACKGROUND

Existing Street Conditions. The 500 and 600 Blocks of East Colorado begin the eastern most section of Telluride's Main Street. These blocks, however, are residential in character because they are east of the commercial core and the north side is lined with houses. Town Park borders these blocks along the south. The current Town and River Park master plans detail treatment of this southern border, including an 8'-wide drainage swale, infiltration sumps and extensive vegetation at the south edge of the right of way.

Although the right of way is 80'-wide, the asphalt travel way is only about 26' wide and slopes very slightly to the west. The asphalt edge is raveling and is difficult to see; although the travel way itself is in good condition. A 15'-wide gravel drainage swale borders the south side. The north side is compacted base course and is used for head in parking by adjacent residents. There are no sidewalks. Adjacent to the dirt surface on the north there are some willow stands, which are remnants of a wetland that formerly occupied this area.

Environment/Drainage/Snow Storage. The dirt located at each side of the paved traveled way, and the absence of curb and gutter on these blocks may negatively impact air and water quality. Drainage along the south side is efficient, but needs to be maintained. Drainage along the north side is inefficient so that pools tend to form at intersections. At the eastern half of the 500 Block, there is a wetland swale that connects with an 18" culvert, which discharges to the San Miguel River. This swale should be recreated along the entire length of these blocks. Snow storage is on the south side, which is compatible with the town snow storage plan.

Historic Character. There is a rated structure on the 600 Block. Therefore, improvements that are made should consider protecting the character of this historic resource.

Interfacing Streets. The bridges that connect the Town Park to East Colorado Avenue at Maple Street works well with the San Miguel River and in servicing the vehicle and pedestrian traffic that is heavy in this location. The interfaces at Hemlock and Pinon also work well.

Lighting. There is no street lighting along these blocks. The Streetscape Lighting Plan calls for installing a bollard at the southwest corner of Hemlock Street.

Neighborhood Concerns. The neighborhood is very concerned with the parking overflow from Town Park activities and motor vehicle speeding.

Parking. Currently there is no parking on the south side of these blocks, except for special events. No parking is allowed on the north side within 15 feet from the edge of pavement. Parallel parking on the street should be permitted on the 500 and 600 Blocks; however, except for one residence, all head-in parking should be eliminated. All other residences are required to have parking off of the alley.

Pedestrian Concerns. Because these blocks are part of Telluride's Main Street and close to Town Park, increased pedestrian access off of the travel way is important in order to safely separate pedestrians from increasing motor vehicle volume. A sidewalk on the south side is precluded by the goals of the Town of Telluride and the Town Park master plans. Therefore, a 6'- or 8'-wide sidewalk on the north side is most appropriate. It is preferable if the sidewalk is separated from the travel way with a green strip. The existing travel way should remain in its current location.

Street Furniture and Public Art. No street furniture or amenities exist on the block. None are recommended.

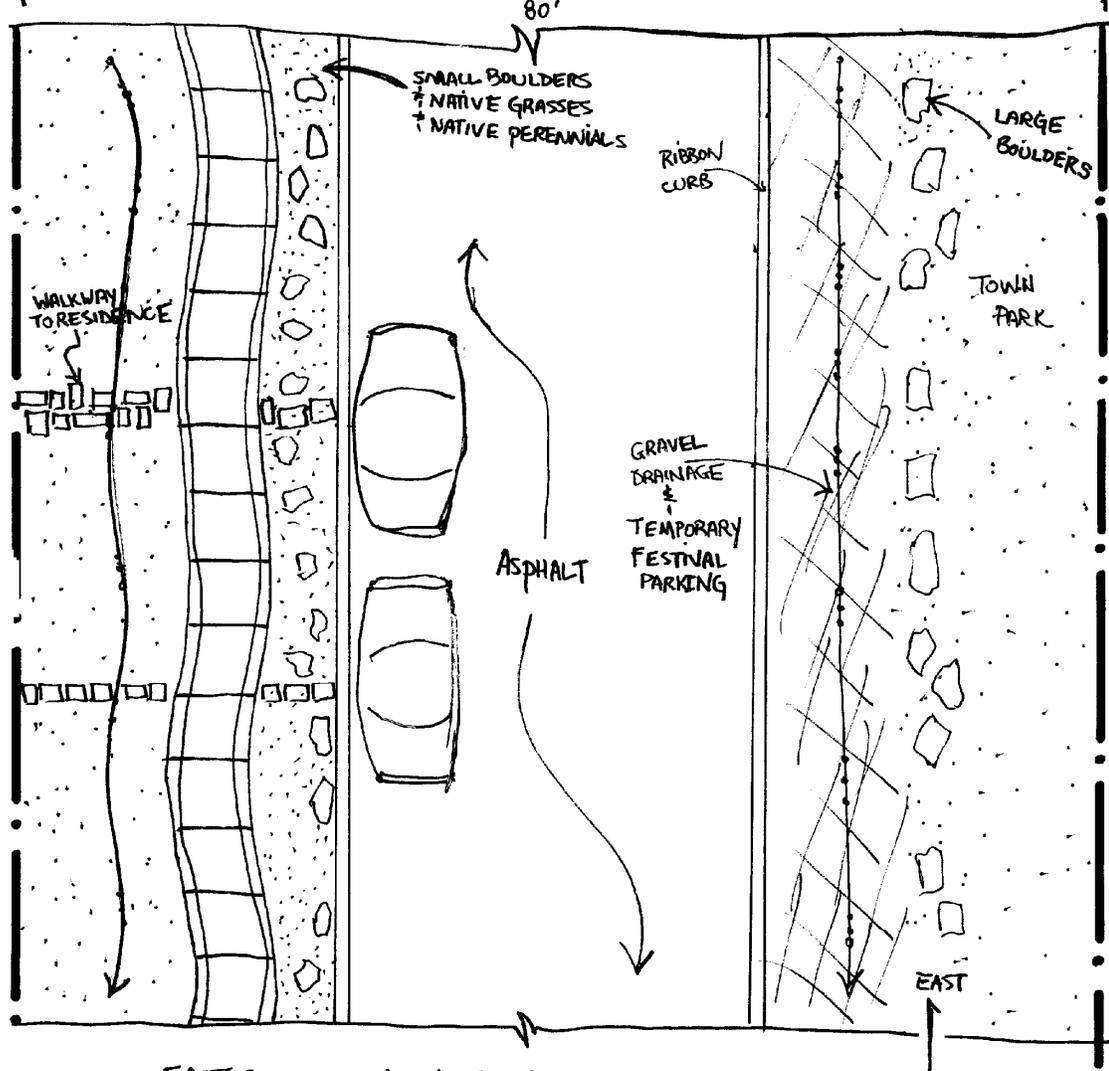
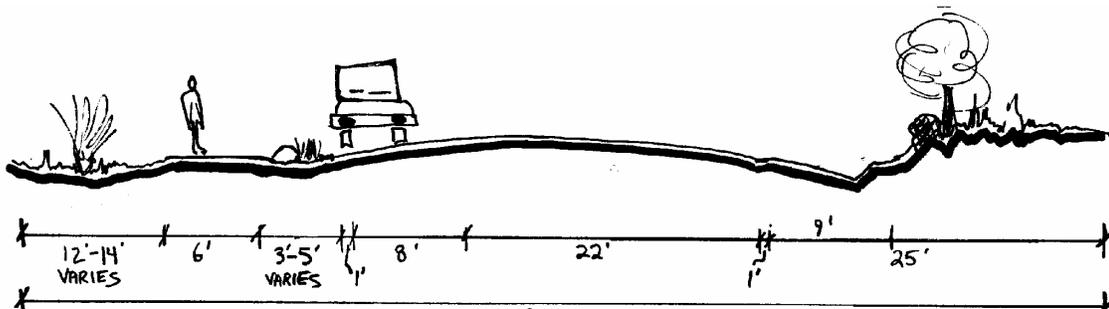
Traffic Circulation. The current light to moderate vehicular traffic is increasing over time as more people recreate at the east end of the valley and additional residences are built east of Town. As well, depending on the time of day and the season, some of the vehicular traffic can be attributed to park and festival users, as well as local residents. Current capacity appears adequate for the intensity of traffic and emergency vehicle access.

Utilities. No utility concerns are mentioned in meeting notes. All utilities are underground.

RECOMMENDED IMPROVEMENTS

1. Construct a 6'- or 8'-wide sidewalk and curb and gutter on the north side, separated from the travel way by parking and/or a green strip.
2. Narrow the travel way with curb and gutter or a ribbon curb on the north and south side.
3. Re-create a viable infiltration swale area that will eliminate ponding at intersecting streets.
4. Install a streetlight bollard at the southeast corner of Hemlock Street to be consistent with the Streetscape Lighting Plan.

Refer to Streetscape East Telluride District, 2006, Engineered Street Designs and adjacent street profiles for additional information.



EAST COLORADO AVENUE - 500 & 600 BLOCKS
(BETWEEN MAPLE & PINON)

EAST COLORADO AVENUE – 700 & 800 BLOCKS

80' Right of Way

Neighborhood Meetings: July 5, 1996; January 20, 1999; June 28, 2005; June 29, 2005; July 21, 2005

BACKGROUND

Existing Street Conditions. The 700 and 800 Blocks of East Colorado continue the eastern most section of Telluride's Main Street. These blocks are residential on the north side, although a wetland area has been preserved between the houses and the travel way. Town Park borders these blocks along the south. The current Town and River Park master plans detail treatment of this southern border, including an 8'-wide drainage swale, infiltration sumps and extensive vegetation at the south edge of the right of way.

Although this is an 80'-wide right of way, the asphalted travel way is approximately 26' wide and slopes very slightly to the west. The edge of asphalt is raveling and difficult to see; although the travel way itself is in good condition. There are no sidewalks on either side of the block. Adjacent to the dirt surface on the north there are dense stands of willows, cottonwoods, and other shrubs. This area is protected as wetland.

Environment/Drainage/Snow Storage. The dirt shoulder on either side of the travel way, and the absence of any curb may negatively impact air and water quality by allowing motor vehicles to track mud onto the asphalt surface. Drainage along the south side is efficient, but needs to be maintained. Drainage along the north side is also efficient, as runoff tends to infiltrate into the wetland area. The only problem area is at the southwest corner of Columbine, where a significant amount of sediment carried from upslope has settled out and covered the existing infrastructure. This area must be cleaned out and potentially re-engineered. Snow storage is on the south side, which is compatible with the town snow storage plan.

Historic Character. There are no rated structures on this block. The right of way neither contributes to nor detracts from the historic character of the area.

Interfacing Streets. All interfacing streets appear to function well with these blocks.

Lighting. There is a standard commercial streetlight at the southeast corner of Pinon, at the southwest corner of Laurel and at the southwest corner of Columbine. These should be replaced with the bollard lighting specified in the Streetscape Lighting Plan.

Neighborhood Concerns. The neighborhood is very concerned with the parking overflow from Town Park activities and motor vehicle speeding.

Parking. Currently there is no parking on the south side of these blocks, except for special events. No parking is allowed on the north side either. Other than during special events, parking pressure is very low and local residents park off of Shadow Lane.

Pedestrian Concerns. Because these blocks are part of Telluride's Main Street and close to Town Park, increased pedestrian access off of the travel way is important in order to safely separate pedestrians from increasing motor vehicle volume. A sidewalk on the south side is precluded by the goals of the Town of Telluride and the Town Park master plans. Therefore, a 6'- or 8'-wide sidewalk on the north side is most appropriate. It is preferable if the sidewalk is separated from the travel way with a green strip, but protecting the wetland area is paramount. The existing travel way should remain in its current location.

Street Furniture and Public Art. The cottonwood at the end of Pinon Street is to be protected as part of the neighborhood character. No street furniture or amenities exist on the block. None are recommended.

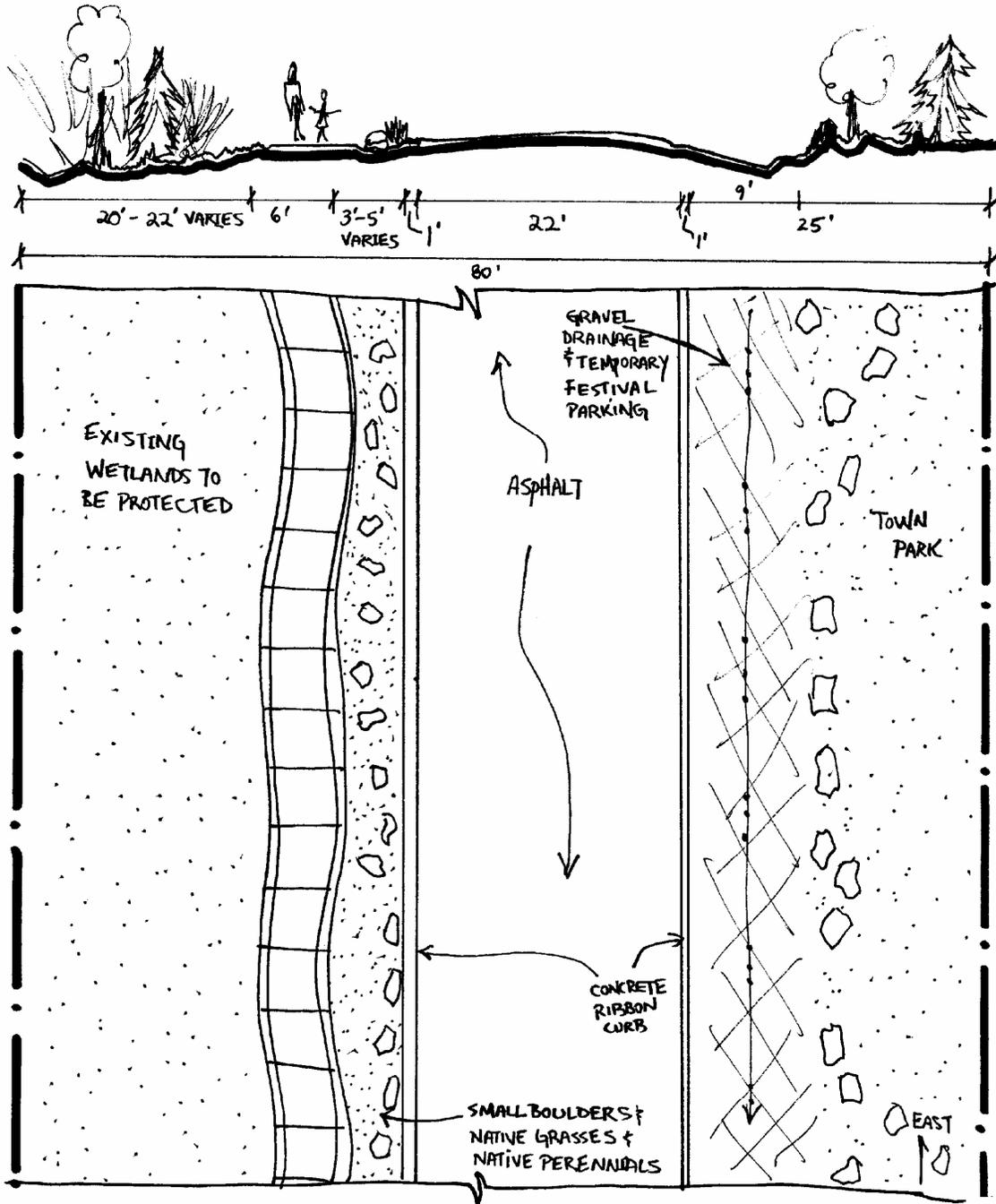
Traffic Circulation. Vehicular traffic is currently light, but is noticeably increasing over time as more people recreate at the east end of the valley and there will be additional residences east of Town. As well, depending on the time of day and the season, some of the vehicular traffic can be attributed to park and festival users, as well as local residents. Current capacity appears adequate for the intensity of traffic and emergency vehicle access.

Utilities. No utility concerns are mentioned in meeting notes. All utilities are underground.

RECOMMENDED IMPROVEMENTS

1. Construct a 6'- or 8'-wide sidewalk on the north side, separated from the travel way by parking and/or a green strip.
2. Narrow travel way width with curb and gutter or a flush ribbon curb on the north and south side.
3. Re-create a functional drainage system at the intersection of Columbine Street and East Colorado Avenue.
4. Install a streetlight bollard at the southeast corner of Pinon, Laurel, and Columbine streets, as called for by the Streetscape Lighting Plan.

Refer to Streetscape East Telluride District, 2006, Engineered Street Designs and adjacent street profiles for additional information.



EAST COLORADO AVENUE — 700 & 800 BLOCKS
(BETWEEN PINON & COLUMBINE)

EAST COLORADO AVENUE – 900 BLOCK

80' Right of Way

Neighborhood Meetings: July 15, 1996; June 28, 2005; June 29, 2005; July 21, 2005

BACKGROUND

Existing Street Conditions. This block is residential on the north side and is currently undeveloped on the south side beyond the Town Park River Trail boundary. Although this is an 80'-wide right of way, the asphalted travel way is only about 26' wide and slopes west. The edge of asphalt is raveling and is difficult to see; although the travel way itself is in good condition. Nearby residents heavily use the 10'-wide gravel and dirt shoulder on the north and south side for parking. There are no sidewalks on either side of the block; although there is a red dust dirt path along the north side, which connects to the River Trail outlet on the south. Grass is adjacent to the gravel and dirt shoulder. There is a deep vegetated swale that connects to a culvert at Columbine. There is no formal drainage along the south side.

Environment/Drainage/Snow Storage. The dirt shoulder on either side of the travel way, and the absence of any curb may negatively impact air and water quality by allowing motor vehicles to track mud onto the asphalt surface. Drainage along the south side may require some engineering to prevent erosion from runoff over time. The vegetated drainage swale along the north side appears to work well. This block is consistent with Telluride's drainage plan. Snow storage is on the south side, which is compatible with the town snow storage plan.

Historic Character. There are no rated structures on this block. The right of way neither contributes to nor detracts from the historic character of the area.

Interfacing Streets. All interfacing streets appear to function well with these blocks.

Lighting. There are no streetlights along this block and none are required at this time.

Neighborhood Concerns. The neighborhood is very concerned with having sufficient on-street parking and motor vehicle speeding.

Parking. Currently parking is permitted along both the north and south sides, except in winter when the south side is reserved for snow storage. Parking pressure can be high due to the higher density of housing at Wilkin Court.

Pedestrian Concerns. A pedestrian connection to the commercial core that is off of the travel way is important to safely separate pedestrians from increasing motor vehicle volume. A sidewalk on the south side is precluded by the goals of the Town of Telluride and the Town Park master plans. Therefore, a 6'- or 8'-wide sidewalk on the north side is most appropriate. It is preferable if the sidewalk is separated from the travel way with a green strip. The existing travel way should remain in its current location.

Street Furniture and Public Art. No street furniture or amenities exist on the block. None are recommended.

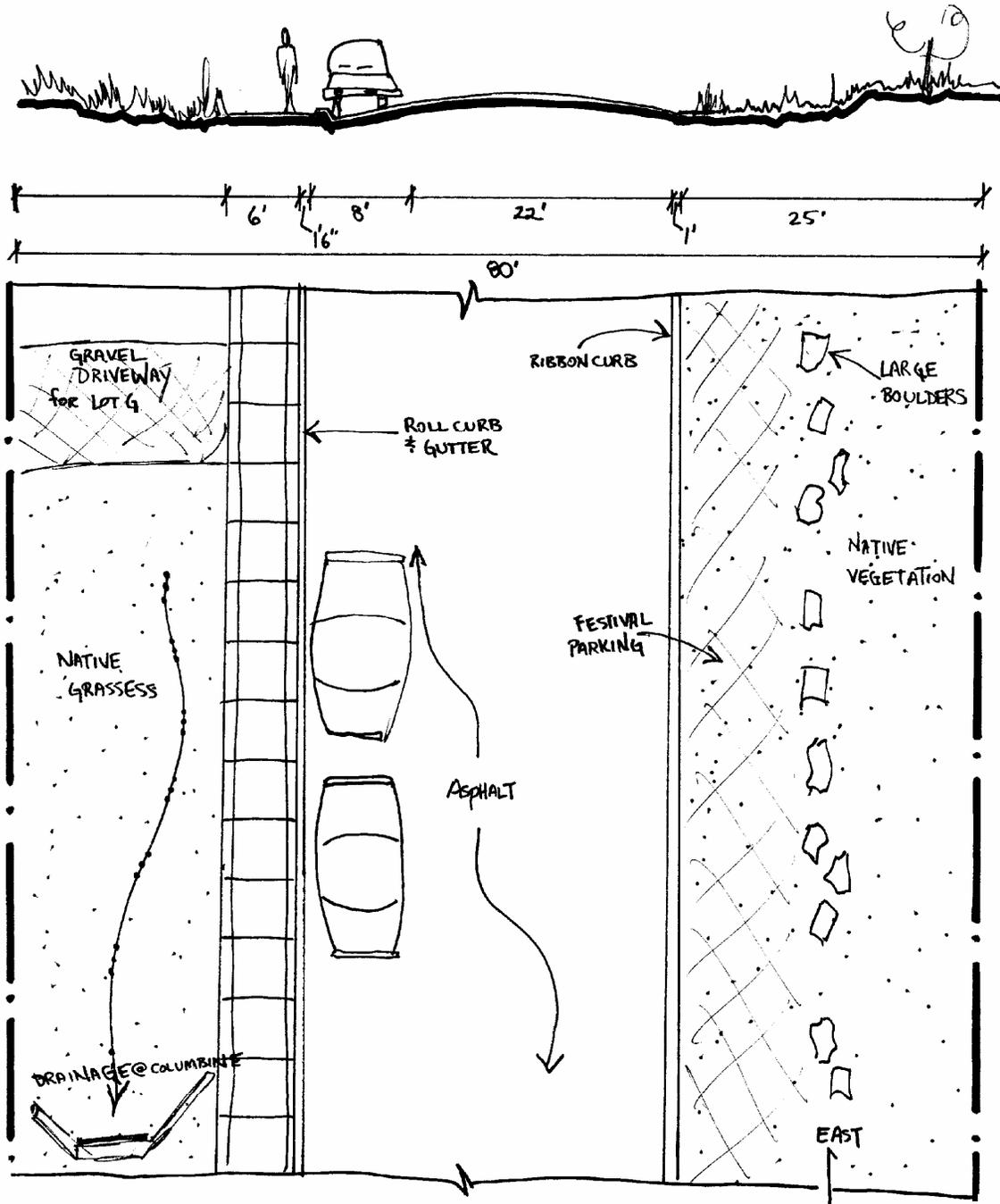
Traffic Circulation. Vehicular traffic is currently light, but is noticeably increasing over time as more people recreate at the east end of the valley and there will be additional residences east of Town. Current capacity appears adequate for the intensity of traffic and emergency vehicle access. A bus turnaround may be needed in the future when a formal east end bus stop is established.

Utilities. No utility concerns are mentioned in meeting notes. All utilities are underground.

RECOMMENDED IMPROVEMENTS

1. Construct a 6'- or 8'-wide sidewalk on the north side, separated from the travel way by parking and/or a green strip.
2. Narrow travel way width with curb and gutter or a flush ribbon curb on the north and south side.
3. Provide for parallel parking along the north side, allowing for driveway cuts at lots G and H.

Refer to Streetscape East Telluride District, 2006, Engineered Street Designs and adjacent street profiles for additional information.



EAST COLORADO AVENUE - 900 BLOCK
(BETWEEN COLUMBINE ≠ WILKIN COURT)

WEST COLUMBIA AVENUE – 100 BLOCK

60' Right of Way

Neighborhood Meetings: July 10, 1996

BACKGROUND

Existing Street Conditions. The Streetscape Task Force has identified Columbia Avenue as Telluride's "Residential Main Street." This block, improved during Streetscape Phase I, 1998, Construction, is paved with asphalt, slopes to the east, has a 5'-wide sidewalk with a 3'-wide green strip south of the sidewalk, and a 6'-wide sidewalk with a 3'-wide green strip between the sidewalk and the travel way on the north. Concrete and curb and gutter runs along the north and south sides. There is a large curb cut on the north side of the block in front of the Fire House, allowing for emergency vehicle access. The asphalt, and sidewalks, and green strip are in good condition.

Environment/Drainage/Snow Storage. Drainage from this block is efficient and the existing curb and gutter system effectively handles runoff. This block does not negatively affect air and water quality. In its current condition this block is consistent with Telluride's snow storage and drainage plans.

Historic Character. This right of way neither contributes to nor detracts from the historic character of the area. This block, however, contains numerous historic structures and any decisions to alter the right of way should not negatively affect these structures or the overall historic character.

Interfacing Streets. The main interfacing streets appear function effectively and pose no negative impacts.

Lighting. There are no light fixtures in the right of way and none are required. There is a single standard commercial light fixture in front of Town Hall. No changes to this fixture are necessary.

Parking. On street parallel parking is allowed along both sides of this block, except across from the Fire House and Town Hall. In winter, parking restrictions exist on the south. Though parking pressure is heavy due to this area's proximity to the commercial core, the Fire House, Old Town Hall, and Rebekah Hall, this parking configuration should be maintained. Existing off-street parking in this vicinity should be maximized.

Pedestrian Concerns. Existing sidewalks adequately handle the heavy pedestrian traffic on this block. There is a green strip between the travel way and the sidewalk on the north side, except where the curb cut allows access to the Fire House. There is no green strip on the south in order to accommodate moderately heavy traffic and parking needs. There are handicap ramps on all corners.

Street Furniture and Public Art. No street furniture or amenities exist on the block. None are recommended.

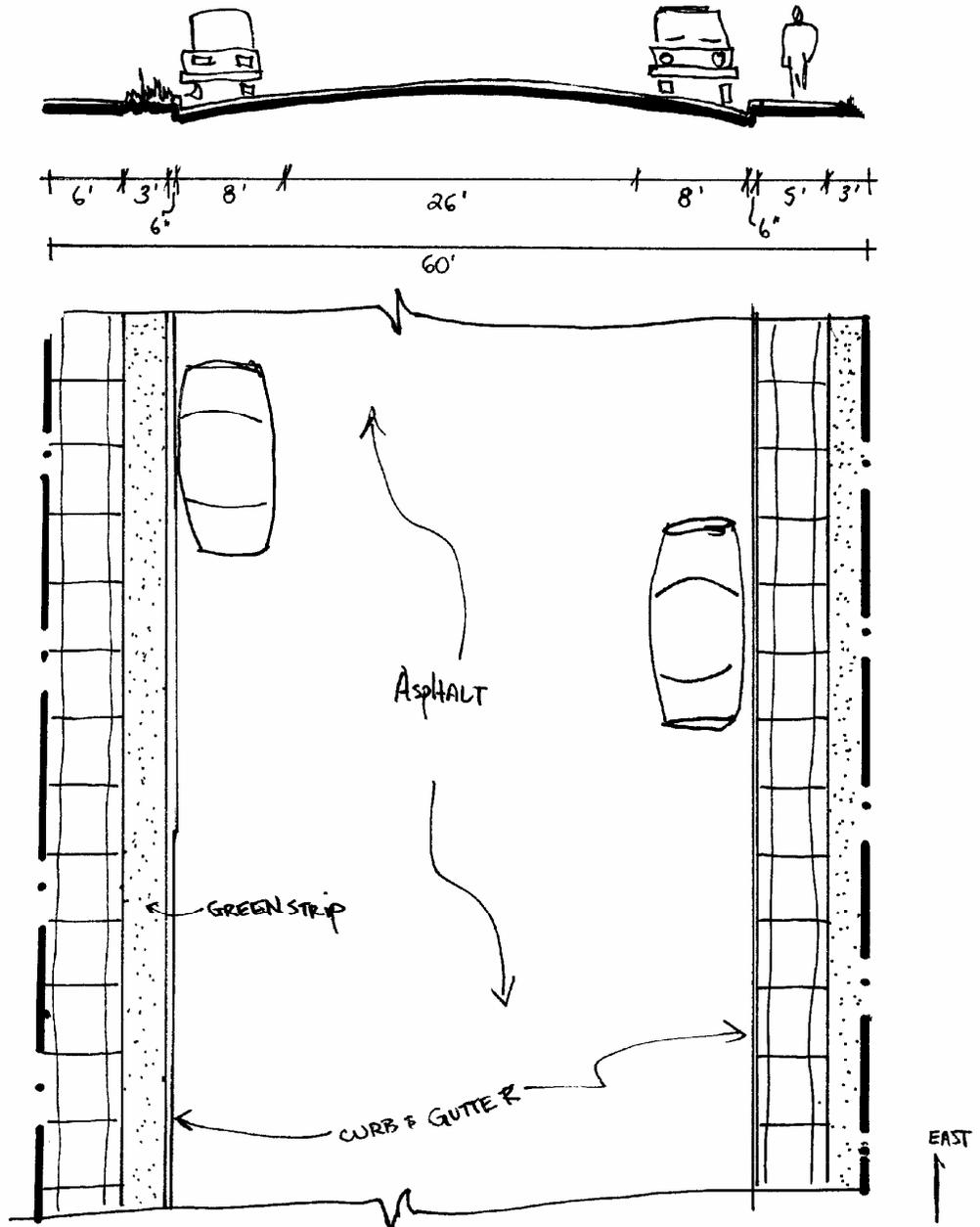
Traffic Circulation. Due to the location of Columbia Avenue one block north of Colorado Avenue, and the presence of businesses and Town of Telluride offices on this block, traffic is moderately heavy. Currently, it appears that carrying capacity and emergency vehicle access are adequate. Parking across from the Fire House is restricted to allow for ease of access for emergency vehicles and Fire House personnel.

Utilities. There are no concerns with regard to existing utilities.

RECOMMENDED IMPROVEMENTS

No improvements are recommended at this time.

Refer to Streetscape Phase I, 1998, Engineered Street Designs and adjacent street profiles for additional information.



WEST COLUMBIA AVENUE - 100 BLOCK
(BETWEEN FIR & PINE STREETS)

WEST COLUMBIA AVENUE – 200 BLOCK

60' Right of Way

Neighborhood Meetings: July 10, 1996; July 2, 1997; September 29, 1999; November 3, 1999

BACKGROUND

Existing Street Conditions. The Streetscape Task Force has identified Columbia Avenue as Telluride's "Residential Main Street." This block, improved during Streetscape Phase I Construction, is paved with asphalt, slopes east, and has curb and gutter and 5'-wide sidewalks along its north and south sides. There is a 5'-wide green strip between the travel way and the sidewalk on the north side. There is no green strip on the south. On the west ½ of the south side of the block is a 12"-high modified curb and gutter which receives drainage from a drain pan crossing on a slight diagonal from the north east corner of Oak Street and Columbia Avenue. The asphalt, and sidewalks, and green strip are in good condition.

Environment/Drainage/Snow Storage. Currently, drainage from this block appears efficient and the existing curb and gutters and drain pans effectively handle runoff the significant quantity of runoff that comes from uphill. The current design is a balance of aesthetics, runoff management, and flood control. The drainage system requires regular cleaning, as sediment from Tomboy Road drainage tends to drop out in this area, potentially impacting air and water quality. In its current condition this block is consistent with Telluride's snow storage and drainage plans.

Historic Character. As it exists, this right of way neither contributes to nor detracts from the historic character of the area. This block, however, contains numerous historic structures. Any alteration of the right of way should not negatively impact these historic resources.

Interfacing Streets. The main interfacing streets appear to pose no negative impacts to this block.

Lighting. The single existing residential pole-mounted light fixture at the northwest corner of Columbia Avenue and Fir Street should be replaced with a fixture that is consistent with the Streetscape Lighting Plan.

Parking. On street parallel parking is allowed on both sides of this block. In winter, parking restrictions exist on the south. Though this area experiences heavy parking pressure, this parking configuration should be maintained, and existing off-street parking in this vicinity should be maximized.

Pedestrian Concerns. Existing sidewalks adequately handle the heavy pedestrian traffic on this block. There is a green strip buffer between the travel way and the sidewalk on the north side of the street. There is no green strip on the south in order to accommodate moderately heavy traffic and parking needs. There are handicap ramps on all corners.

Street Furniture and Public Art. No street furniture or amenities exist on the block. None are recommended.

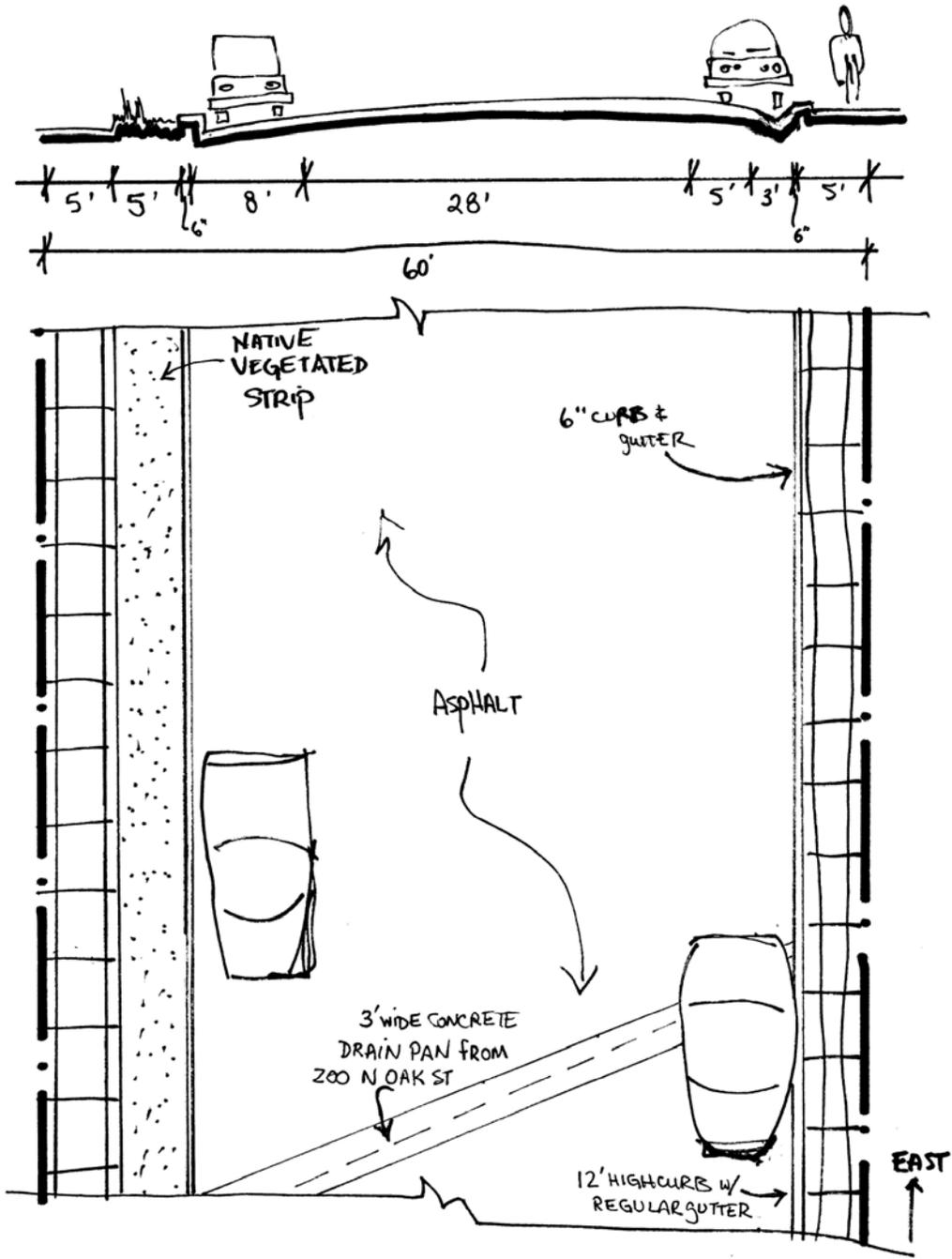
Traffic Circulation. Due to the location of Columbia Avenue one block north of Colorado Avenue, and the presence of businesses and Town of Telluride offices near this block, traffic is moderately heavy. Currently, however, carrying capacity and emergency vehicle access appears adequate.

Utilities. There are no concerns with regard to existing utilities.

RECOMMENDED IMPROVEMENTS

1. Replace the existing residential pole-mounted light fixture at the northwest corner of Columbia Avenue and Fir Street with a fixture that is consistent with Streetscape Lighting Plan.

Refer to Streetscape Phase I, 1998, Engineered Street Designs and adjacent street profiles for additional information



WEST COLUMBIA AVENUE - 200 BLOCK
(BETWEEN OAK & FIR STREETS)

WEST COLUMBIA AVENUE – 300 BLOCK

60' Right of Way

*Neighborhood Meetings: July 10, 1996; July 2, 1997; September 29, 1999;
November 3, 1999; January 19, 2000*

BACKGROUND

Existing Street Conditions. The Streetscape Task Force has identified Columbia Avenue as Telluride's "Residential Main Street." This block, improved during Streetscape Phase IIA 2002 Construction, is paved with asphalt, slopes to the east, and has curb and gutter and 5'-wide sidewalks along its north and south sides. There is a 5'-wide green strip between the travel way and the sidewalk on the north side. There is no green strip on the south. The asphalt, and sidewalks, and green strip are in good condition.

Environment/Drainage/Snow Storage. Currently, drainage from this block appears efficient and the existing curb and gutter and drain pans effectively handle runoff. There appears to be no adverse impact to air and water quality. In its current condition this block is consistent with Telluride's snow storage and drainage plans.

Historic Character. As it exists, this right of way neither contributes to nor detracts from the historic character of the area. This block, however, contains numerous historic structures and any decisions to alter the right of way should not negatively affect these structures or the overall historic character.

Interfacing Streets. The main interfacing streets appear to pose no negative impacts to this block.

Lighting. The single existing residential pole-mounted light fixture at the southwest corner of Columbia Avenue and Fir Street should be replaced with a fixture that is consistent with the Streetscape Lighting Plan.

Parking. On street parallel parking is allowed on both sides of this block. In winter, parking restrictions exist on the south. Though this area experiences heavy parking pressure, this parking configuration should be maintained, and existing off-street parking in this vicinity should be maximized.

Pedestrian Concerns. Existing sidewalks adequately handle the heavy pedestrian traffic on this block. There is a green strip buffer between the travel way and the sidewalk on the north side of the street. There is no green strip on the south in order to accommodate moderately heavy traffic and parking needs. There are handicap ramps on all corners.

Street Furniture and Public Art. No street furniture or amenities exist on the block. None are recommended.

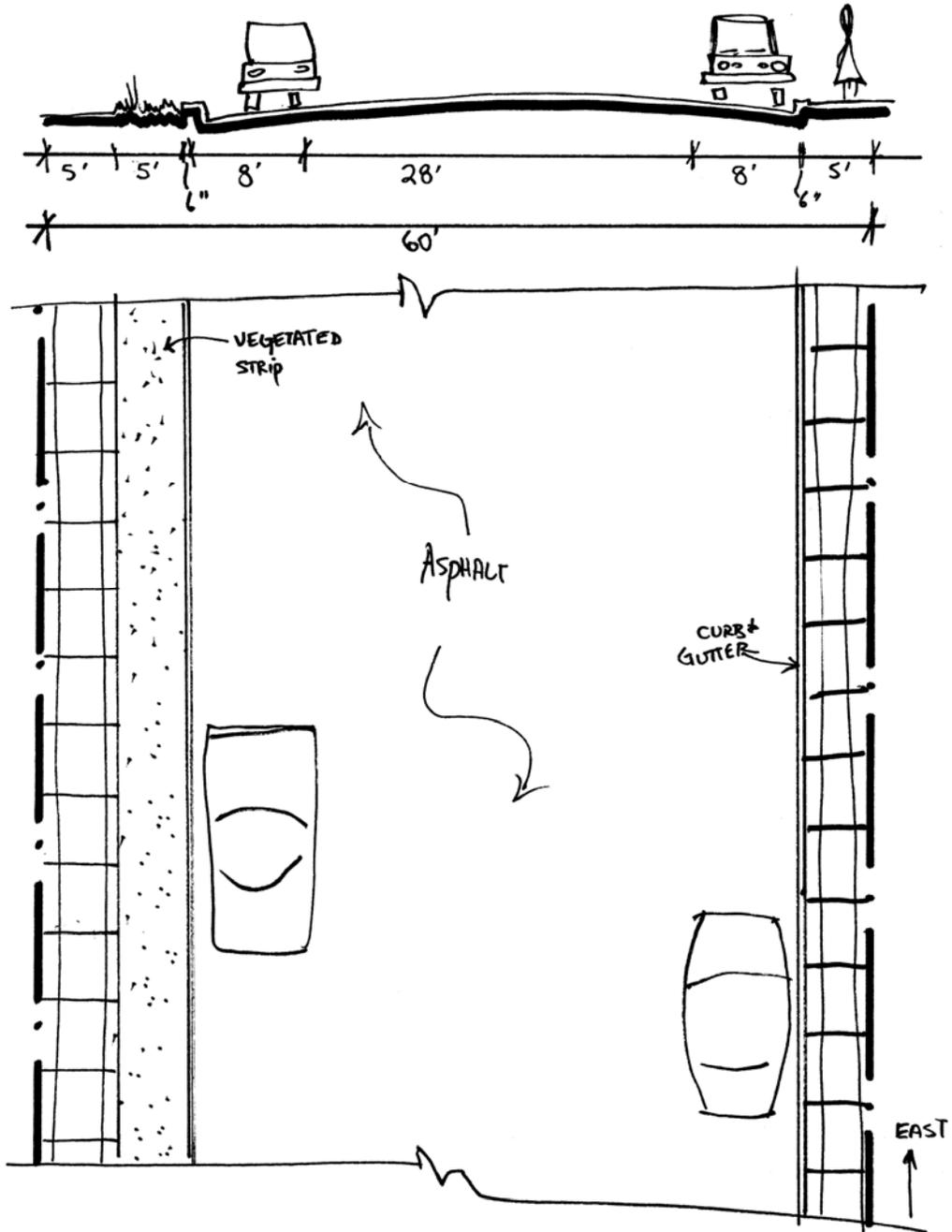
Traffic Circulation. Due to the location of Columbia Avenue one block north of Colorado Avenue, and the presence of businesses and Town of Telluride offices near this block, traffic is moderately heavy. Currently, however, carrying capacity and emergency vehicle access appears adequate.

Utilities. There are no concerns with regard to existing utilities.

RECOMMENDED IMPROVEMENTS

1. Replace the existing residential pole-mounted light fixture at the northwest corner of Columbia Avenue and Fir Street with a fixture that is consistent with Streetscape Lighting Plan.

Refer to Streetscape Phase IIA, 2002, Engineered Street Designs and adjacent street profiles for additional information.



WEST COLUMBIA AVENUE - 300 BLOCK
(BETWEEN ASPEN + OAK STREETS)

WEST COLUMBIA AVENUE – 400 BLOCK

60' Right of Way

*Neighborhood Meetings: July 9, 1996; July 2, 1997; September 29, 1999;
November 3, 1999; January 19, 2000*

BACKGROUND

Existing Street Conditions. The Streetscape Task Force has identified Columbia Avenue as Telluride's "Residential Main Street." This block, improved during Streetscape Phase IIA 2002 Construction, is paved with asphalt, slopes to the west, and has curb and gutter on the north and south sides. There is a 6' sidewalk with a 3.5'-wide green strip on the south side, and a 5' sidewalk with a 4.5'-wide green strip on the north side. At the elementary school, the sidewalk detours to the north to allow head in parking. The north side sidewalk is 9.5' wide from the head in parking west to the North Townsend Street intersection.

Environment/Snow Storage/Drainage. In its current condition this block is consistent with Telluride's snow storage and drainage plans. Snow is not stored on this block, therefore there are no winter parking restrictions.

Historic Character. As it exists, this right of way neither contributes to nor detracts from the historic character of the area. However, there are many historic structures along this block, including the landmark elementary school, and any decisions to alter the right of way should take this into consideration.

Interfacing Streets. The main interfacing streets appear to pose no negative impacts to this block.

Lighting. There are four residential pole-mounted light fixtures and three standard commercial fixtures on this block. The two residential pole-mounted light fixtures near the center of the block should be removed. The two residential pole-mounted light fixtures in the center of the block should be completely removed and the two fixtures that remain on the corners should be replaced with a fixture consistent with the Streetscape Lighting Plan.

Neighborhood Concerns. The neighborhood is concerned about vehicle speeding related to Student drop off at the Elementary School. The Elementary School and Town should continue to work together to resolve new issues that may arise.

Parking. On street parallel parking exists on both sides of this block. One exception to this is head in parking on the north side at the Elementary School. Though this area experiences moderately heavy parking pressures, this parking configuration should be maintained, and existing off-street parking should be maximized.

Pedestrian Concerns. For reasons stated in "Traffic Circulation" there is a heavy amount of pedestrian traffic on these blocks. Therefore, the existing sidewalks need to be maintained in good condition. There is adequate handicap access at every street corner.

Street Furniture and Public Art. While no street furniture or amenities currently exist on this block, it may be desirable to have street furniture or public art in the vicinity of the elementary school.

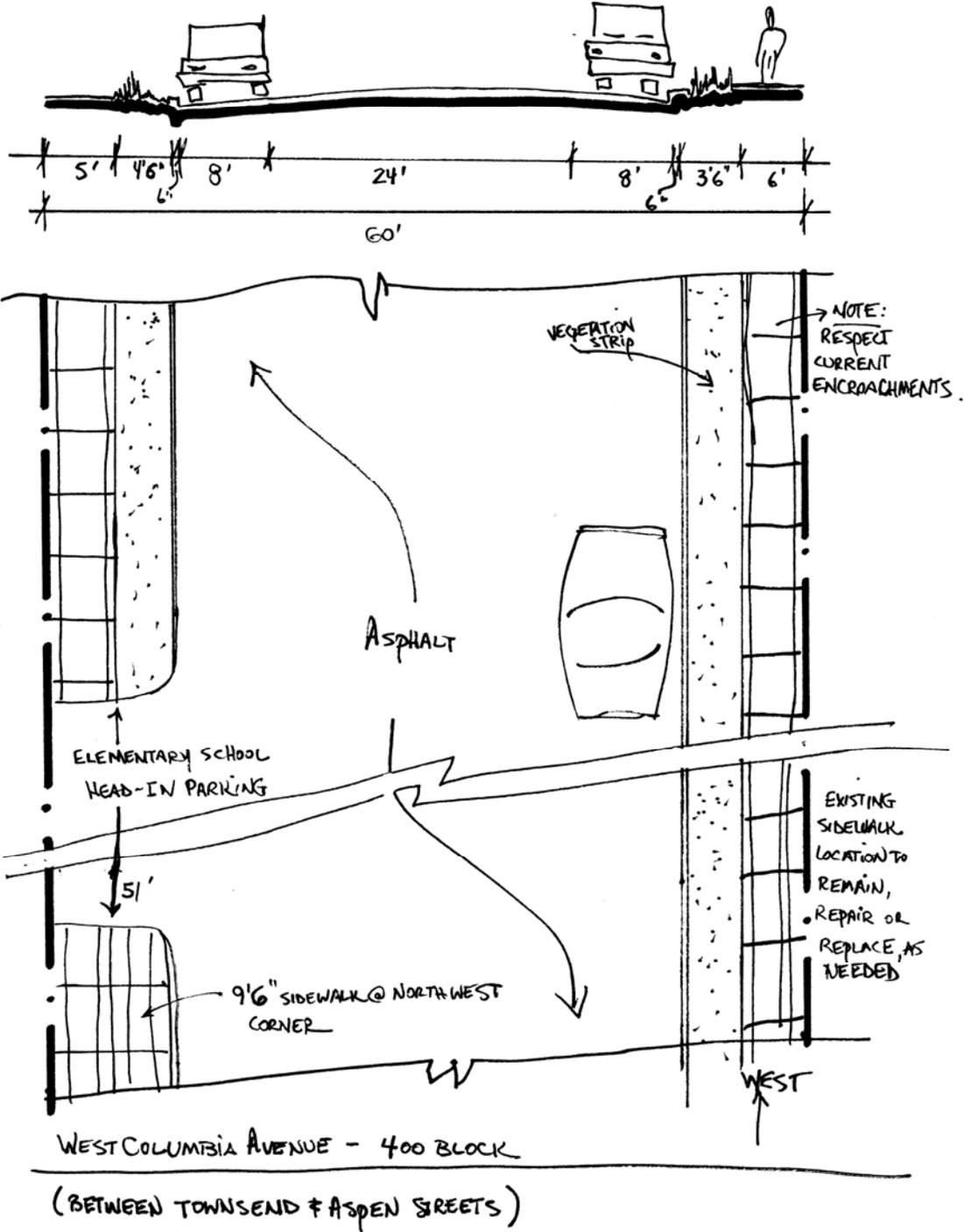
Traffic Circulation. Because Columbia Avenue is just one block north of Colorado Avenue and many parents pick up and drop off their children at the Elementary School, traffic on this block can be very heavy. Nevertheless, capacity and emergency vehicle access appear adequate.

Utilities. There are no concerns or requirements with regard to existing utilities.

RECOMMENDED IMPROVEMENTS

1. Remove the two pole-mounted light fixtures at the center of the block.
2. Update the pole-mounted residential light fixture at the southeast corner of the Townsend Street intersection and the southwest corner of the Aspen Street intersection with a fixture that is consistent with the Streetscape Lighting Plan.

Refer to Streetscape Phase IIA, 2002, Engineered Street Designs and adjacent street profiles for additional information.



WEST COLUMBIA AVENUE – 500 BLOCK

60' Right of Way

*Neighborhood Meetings: July 9, 1996; July 2, 1997; September 29, 1999;
November 3, 1999; January 19, 2000*

BACKGROUND

Existing Street Conditions. The Streetscape Task Force has identified Columbia Avenue as Telluride's "Residential Main Street." This block, improved during Streetscape Phase IIA 2002 Construction, is paved with asphalt, slopes to the west, and has curb and gutters 6' sidewalks, and 3.5'-wide green strips on the north and south sides. One exception to the sidewalk configuration is the absence of a sidewalk on the north side of the bridge over Cornet Creek. There are single drop inlets at the curb just east of Cornet Creek on the north and south side that direct untreated runoff from the street level into the Cornet Creek.

Environment/Snow Storage/Drainage. In its current condition this block is consistent with Telluride's snow storage and drainage plans.

Historic Character. This right of way neither contributes to nor detracts from the historic character of the area. However, there are many historic structures along this block and any decisions to alter the right of way should take this into consideration.

Interfacing Streets. The main interfacing streets appear to pose no negative impacts to this block.

Lighting. The one standard commercial streetlight on this block should be removed.

Neighborhood Concerns. The neighborhood has repeatedly raised concerns about vehicle speeding related to Student drop off at the Elementary School. The School and Town should continue to work toward resolving issues that arise.

Parking. On street parallel parking is allowed on both sides of the block, except on the Columbia Avenue Bridge, which crosses Cornet Creek. In winter, parking is restricted on the south side. While parking pressure in this area is moderately heavy, the current parking configuration should be maintained, and existing off-street parking should be maximized.

Pedestrian Concerns. For reasons stated in "Traffic Circulation" there is a heavy amount of pedestrian traffic on this block. Existing sidewalks should therefore be kept in good condition. The absence of a sidewalk on the north side of the bridge over Cornet Creek inhibits pedestrian travel at that location. There is handicap access on all corners.

Street Furniture and Public Art. No street furniture or amenities currently exist on this block and none are required.

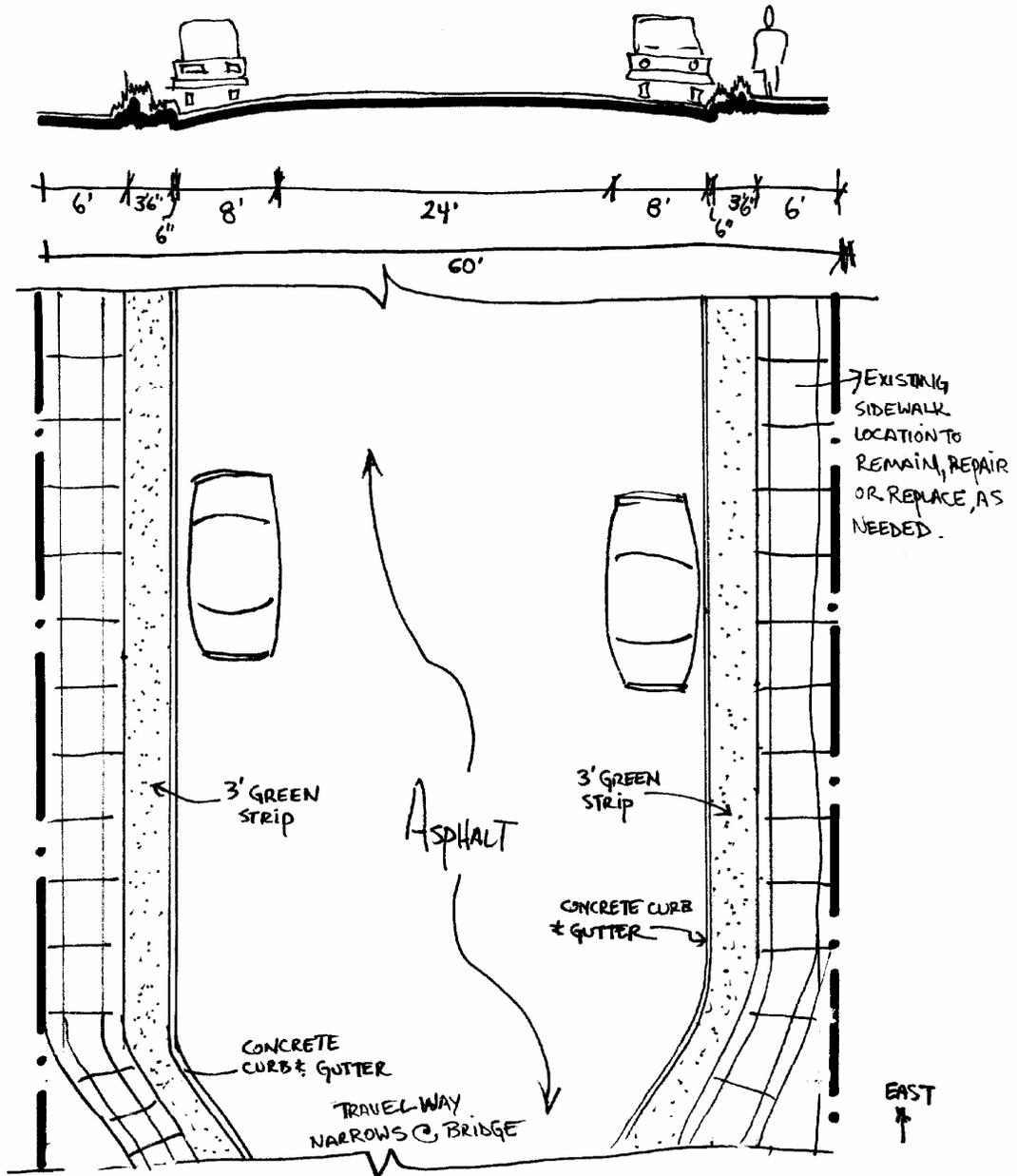
Traffic Circulation. Due to the location of Columbia Avenue one block north of Colorado Avenue, and the presence of the Elementary School on the west 400 Block, traffic on this block is moderately heavy. However, capacity and emergency vehicle access appear adequate.

Utilities. There are no concerns or requirements with regard to existing utilities.

RECOMMENDED IMPROVEMENTS

1. Remove the standard commercial streetlight to be consistent with the Streetscape Lighting Plan.

Refer to Streetscape Phase IIA, 2002, Engineered Street Designs and adjacent street profiles for additional information.



WEST COLUMBIA AVENUE - 500 BLOCK
(BETWEEN DAVIS & TOWNSEND STREETS)

WEST COLUMBIA AVENUE – 600 BLOCK

60' Right of Way

Neighborhood Meetings: July 9, 1996; July 2, 1997; September 29, 1999; November 3, 1999

BACKGROUND

Existing Street Conditions. Improvements to this block include a mixture of asphalt and chip seal surfacing, concrete sidewalks with handicap ramps, concrete curb and gutters, and three new standard commercial street lights. These are all in good condition. This block slopes down to the west and drainage is effective. There are no green strips in the right of way.

Environment/Drainage/Snow Storage. The existing street effectively handles runoff and there appears to be no negative impacts to air and water quality. The current configuration is consistent with Telluride's snow storage and drainage plans.

Historic Character. As it exists, this right of way neither contributes nor detracts from the historic character of the area. There are no historic structures on this block.

Interfacing Streets. The main interfacing streets appear to pose no negative impacts to this block. It should be made clear that the pedestrian extension west of this block is a public right of way.

Lighting. There are two standard commercial streetlights along the north side of this block, which is consistent with the Streetscape Lighting Plan.

Parking. On street parallel parking is allowed along the north and south sides of the street. Winter parking is restricted on the south side. There is substantial off street parking for subdivisions along this block. Parking is also located west of Cornet Street at the terminus of Columbia Avenue. For these reasons, on street parking appears adequate.

Pedestrian Concerns. Pedestrian traffic is moderately heavy due to the proximity of the high school and a hotel. Existing sidewalks appear to effectively handle the pedestrian volume. A paved pedestrian extension west of Cornet Street, just north of the hotel, completed in conjunction with the hotel, assists with access to the high school. There is handicap access to all sidewalks. There is access to the Cornet Creek trail from the south side in the middle of this block. The trail is not handicap accessible.

Street Furniture and Public Art. While no street furniture or amenities currently exist, some may be appropriate to better accommodate public use of this area. Signage may also be appropriate at the entrance to the abovementioned pedestrian extension to identify it as an easement access across private property.

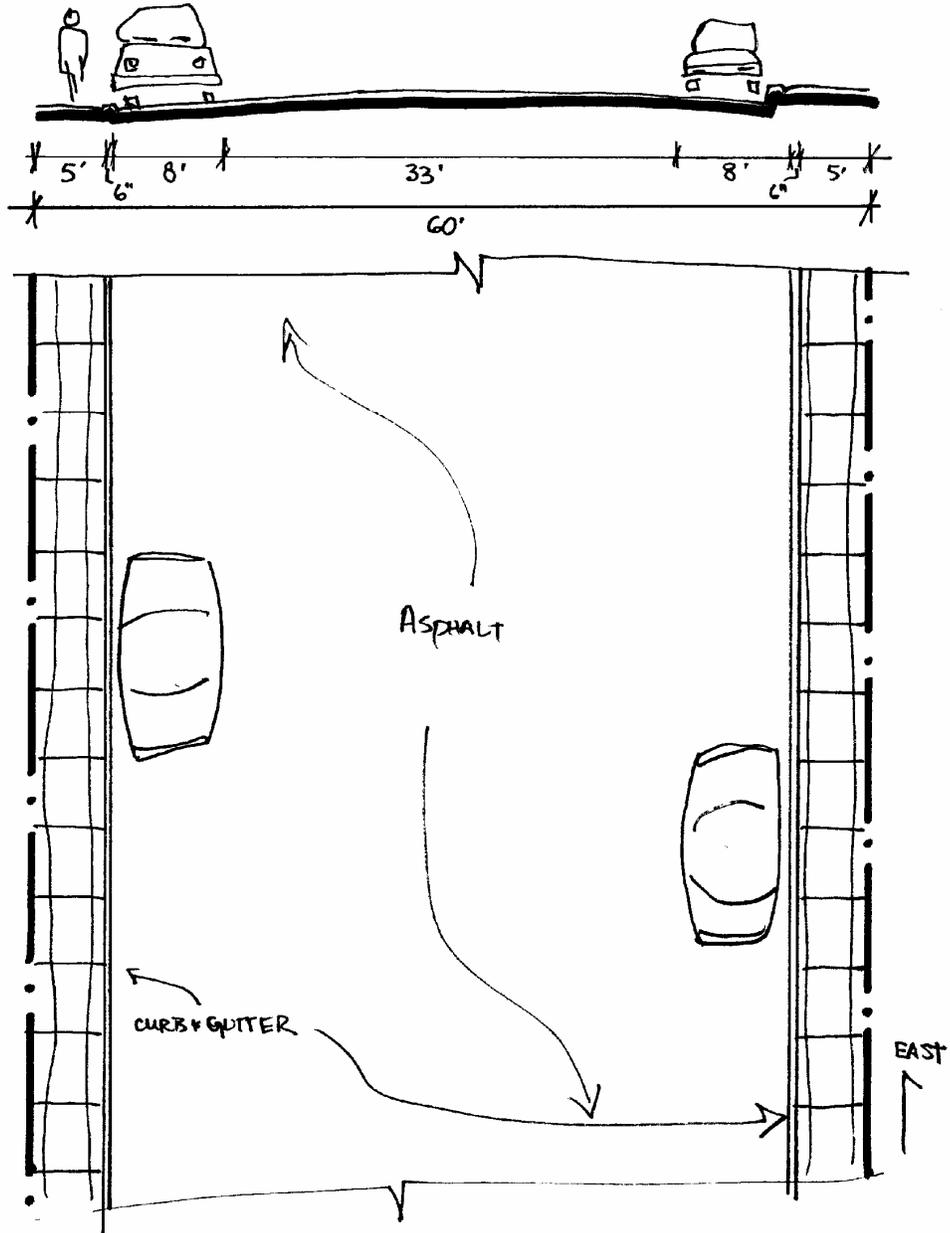
Traffic Circulation. Traffic on this block is moderately heavy due to the intensity of uses in the vicinity, and because this street is used as an alternative to Colorado Avenue. It serves as a feeder to 200 North Davis and the 500, 600 and 700 blocks of West Galena. Capacity and emergency vehicle access appear adequate.

Utilities. There are no concerns or requirements with regard to existing utilities.

RECOMMENDED IMPROVEMENTS

1. Provide a handicap ramp on the curb on the south at the access to the Cornet Creek Trail.

Refer to adjacent street profiles for additional information. No Engineered Street Designs exist for this block.



WEST COLUMBIA AVENUE - 600 BLOCK

(BETWEEN CORNET CREEK & DAVIS STREETS)

EAST COLUMBIA AVENUE – 100, 200, & 300 BLOCKS

60' Right of Way

Neighborhood Meetings: July 10, 1996; July 19, 1999

BACKGROUND

Existing Street Conditions. Columbia Avenue has been identified by the Streetscape Task Force as Telluride's "Residential Main Street." These blocks, improved during Streetscape Phase I 1998 Construction, are paved with asphalt and slope down to the east. There is a 5'-wide sidewalk on the north separated from the travel way by a 5'-wide green strip. The sidewalk is in poor condition in many locations. South of the travel way is a 14.5'-wide green strip. A 5'-wide drain pan runs the length of these three blocks and terminates at two storm drains at the southeast corner of Columbia Avenue and Alder Street. Numerous curb cuts exist on both sides of these three blocks. The asphalt street surface, curbs, gutters, and drain pans are in good condition.

Environment/Drainage/Snow Storage. Currently drainage from these blocks is efficient and the existing curb and gutters and drain pans effectively handle runoff. It is important to maintain the 1.5'-high berm on the south side to assist with flood protection for the south side houses. These blocks do not adversely impact air or water quality. They are consistent with Telluride's snow storage and drainage plans.

Historic Character. As they exist these three blocks of East Columbia Avenue neither contribute to nor detract from the historic character of the area. These blocks, however, contain numerous historic structures and any decisions to alter the right of way should not negatively affect these structures or the overall historic character.

Interfacing Streets. All interfacing streets appear to function well with these blocks.

Lighting. Three pole-mounted residential light fixtures—one at the southeast corner at Pine Street, one at the southeast corner at Spruce Street, and one at the northeast corner at Willow Street—should be replaced with a fixture that is consistent with the Streetscape Lighting Plan.

Parking. On street parallel parking is allowed on the north and south sides. In winter, parking on the south is prohibited to allow for snow storage. Because the area is close to the commercial core, parking pressure is heavy. However, therefore the current parking configuration should be maintained and existing off-street parking should be maximized. This street was originally designed for no south side parking; however, now that parking has been permitted on the south side, it has become a one car-at-a-time street.

Pedestrian Concerns. For reasons stated in "Traffic Circulation" there is a heavy amount of pedestrian traffic on these blocks. The existing improved sidewalks on the north adequately handle this traffic with the exception of portions of sidewalk in poor condition. These portions of sidewalk in poor condition should be repaired or replaced. There is a green strip buffer between the travel way and the sidewalk on the north side of the street. Where this green strip is steeply sloped, particularly on the East 300 Block, it may be appropriate to construct steps at limited locations

Street Furniture and Public Art. No street furniture or amenities currently exist and none are recommended.

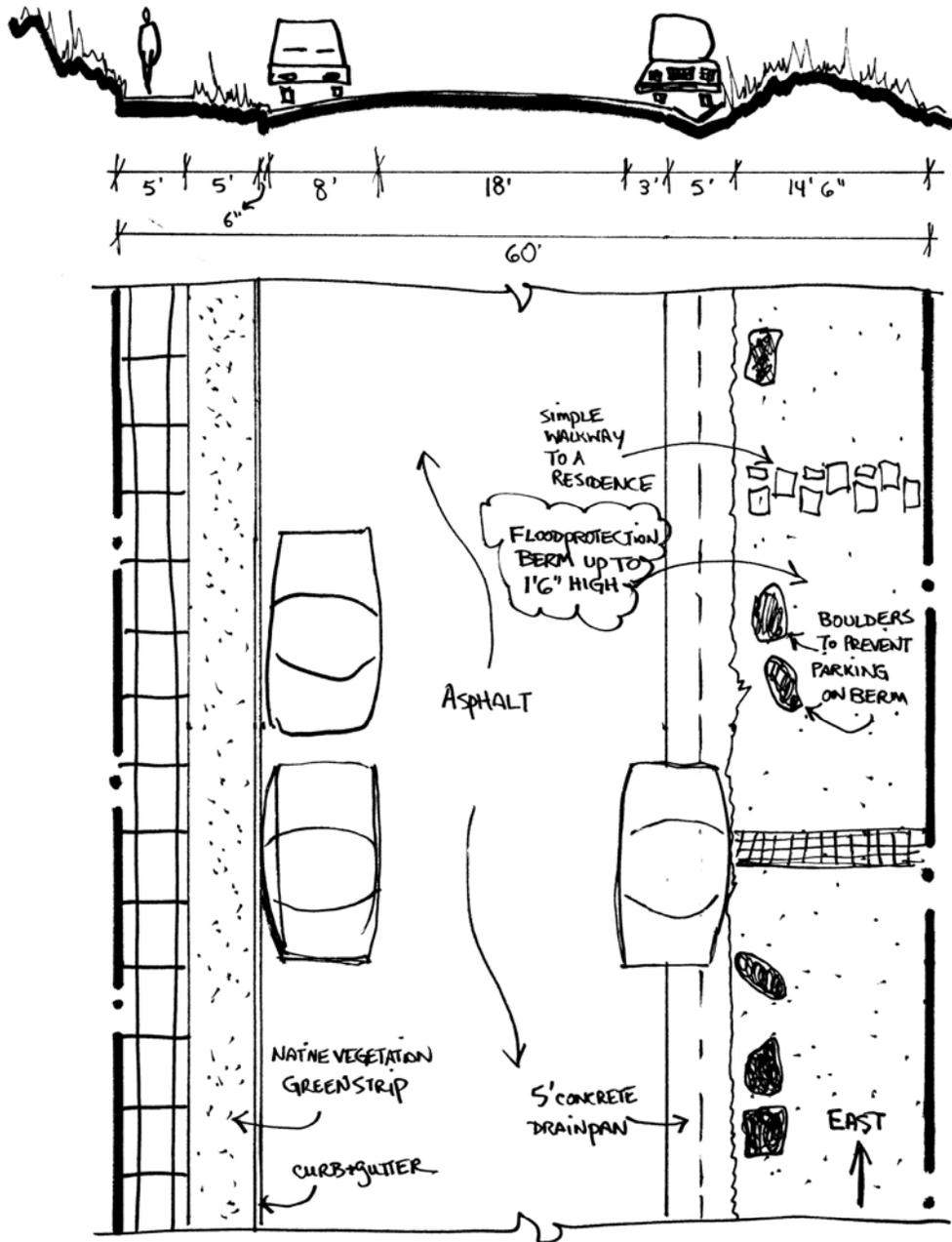
Traffic Circulation. Because the commercial core is just one block south, traffic is moderate, though less heavy than traffic on the four blocks west of Pine Street. Because of parking on both sides of these blocks the travel way is narrow but sufficient for the traffic volume. The narrower travel way slows vehicle speed. Emergency vehicle access appears adequate. There is handicap access.

Utilities. No utility concerns are mentioned in meeting notes. Many, but not all utilities are underground.

RECOMMENDED IMPROVEMENTS

1. Replace all sidewalks that are in poor condition.
2. Replace the three existing pole-mounted light residential fixtures with a fixture that is consistent with the Streetscape Lighting Plan.

Refer to Streetscape Phase I, 1998, Engineered Street Designs and adjacent street profiles for additional information.



EAST COLUMBIA AVENUE - 100, 200, & 300 BLOCKS
(BETWEEN PINE & ALDER STREETS)

EAST COLUMBIA AVENUE – 400 & 500 BLOCKS

60' Right of Way

Neighborhood Meetings: July 16, 1996; November 12, 1997; January 6, 1999

BACKGROUND

Existing Street Conditions. The Streetscape Task Force has identified Columbia Avenue as Telluride's "Residential Main Street." These blocks have not been formally improved as part of Streetscape. They are likely the lowest area in the east half of town, however, with the culvert at Maple Street as the only outflow. As a result, these blocks experience low level flooding. These blocks are paved with chip seal and are fairly flat sloping slightly east in the 400 Block and slightly west in the 500 Block. There is an intermittent 5'-wide sidewalk along the north side that is often in poor condition and no curb and gutter. Parking occurs on the dirt shoulder. There is an intermittent vegetated drainage swale along the south side. The south side edge of the travel way is poorly defined. There are no alleys intersecting this block.

Environment/Drainage/Snow Storage. There is no curb and gutter on the north side of these blocks, and runoff is directed into the unpaved north portion of the right of way. The Streetscape Task Force recommends constructing a traditional curb and gutter along this north side with a 5'-wide green strip between parked vehicles and the sidewalk. As well, Streetscape recommends that the vegetated/wetland drainage swale along the south be maintained for infiltration and for transporting flows to Maple Street, when needed. Snow is stored on the south side. Snow storage on these blocks is consistent with Telluride's snow storage plan. Drainage requires work in order to be consistent with Telluride's drainage plan.

Historic Character. Numerous contributing structures are located on this block. Any decisions to alter the right of way should protect the character of these historic resources.

Interfacing Streets. All interfacing streets appear to function well with these blocks.

Lighting. The two pole-mounted streetlights at the northwest corners of Alder and Maple streets should be replaced with fixture that is consistent with the Streetscape Lighting Plan.

Parking. On street parallel parking is permitted on the north and south sides. In winter, parking is prohibited on the south to allow snow storage. It may be helpful to place small boulders 9' south of the travel way to protect vegetation in the swale. The existing parking configuration should be maintained to accommodate moderate on street parking pressure. Existing off-street parking should be maximized.

Pedestrian Concerns. The intermittent sidewalks along the north side do not serve the moderate pedestrian traffic that travels along these blocks. Most pedestrians walk in the vehicle travel way. To allow for pedestrian traffic on the north side, north sidewalks should be installed and/or replaced. Furthermore, a 5'-wide green strip should be constructed between the travel way and the north sidewalk. Handicap ramps should be included with any sidewalk improvements.

Street Furniture and Public Art. No street furniture or amenities currently exist and none are recommended.

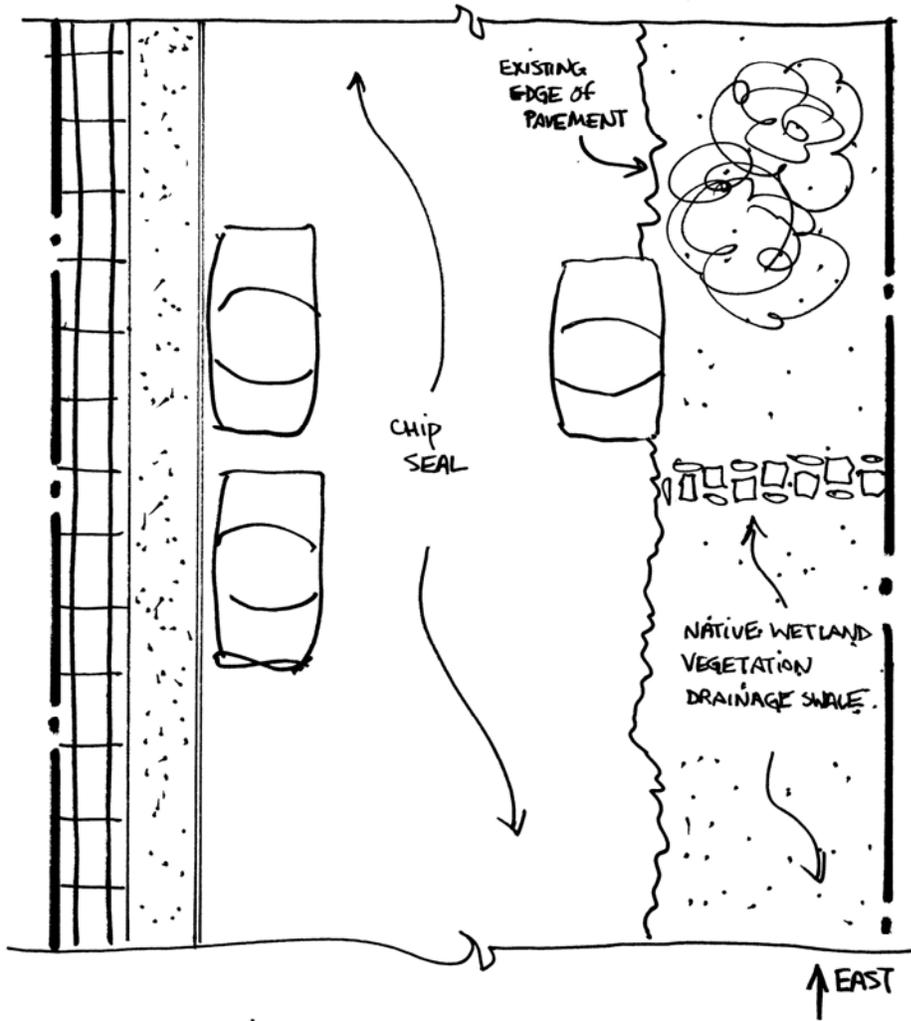
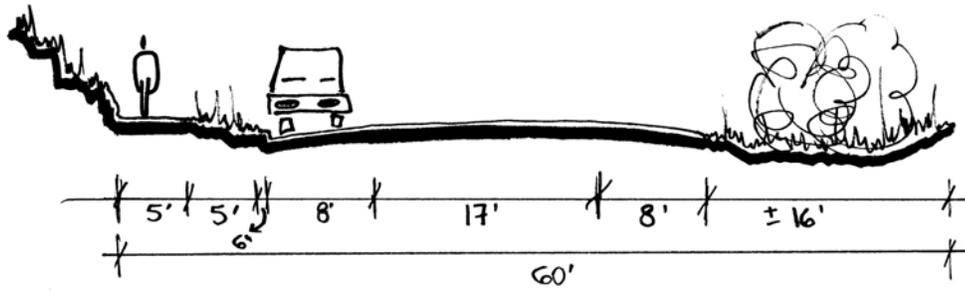
Traffic Circulation. Due to the location of this block east of the commercial core and one block north of Colorado Avenue, and the residential nature of area, the vehicular traffic is moderate. The capacity appears adequate for intensity of use and emergency vehicle access.

Utilities. No utility concerns are mentioned in meeting notes. All utilities are underground.

RECOMMENDED IMPROVEMENTS

1. Repair or replace sidewalks in their current location on the north side of these blocks. Construct new 5'-wide sidewalks on the north where none currently exist.
2. Install a 5'-wide green strip between the north sidewalk and the travel way.
3. Construct a concrete curb and gutter along the north side.
4. Provide a 16'-wide vegetated swale on the south side that uses native, wetland-compatible vegetation. Place boulders in the swale area to delineate the edge of parking.
5. Update existing pole-mounted residential light fixtures with fixtures consistent with the Streetscape Lighting Plan.

Refer to Streetscape East Telluride District, 2006, Engineered Street Designs and adjacent street profiles for additional information.



EAST COLUMBIA AVENUE - 400 + 500 BLOCKS
(BETWEEN ALDER + HEMLOCK STREETS)

EAST COLUMBIA AVENUE – 600, 700, & 800 BLOCKS

60' Right of Way

Neighborhood Meetings: July 15, 1996; October 8, 1997; January 6, 1997

BACKGROUND

Existing Street Conditions. The Streetscape Task Force has identified Columbia Avenue as Telluride's "Residential Main Street." Extending from Hemlock to Columbine streets, these blocks have not been formally improved as part of Streetscape. They are paved with chip seal and are in good condition, in general. The west half of the 600 Block slopes west toward Hemlock Street and the east half slopes east toward Pinon Street. The 700 Block slopes west toward Pinon Street, and the 800 Block slopes east toward Columbine street. The Laurel Street right of way separates the 700 and 800 blocks but has not been improved and a pocket park has been established, so there is no intersection with Columbia Avenue and none is anticipated.

There are no sidewalks on any of these blocks and no formal drainage. Poorly defined green strips of varying widths extend along the north and south sides. The East 600 Block has a steep grade falling from the travel way down to the south at the center of the block. Drainage swales at intersections carry drainage south. This is particularly evident on the north side of the East 800 Block near the Laurel Street right-of-way where a gravel swale and a concrete drain pan exist.

Environment/Drainage/Snow Storage. Due to the absence curb and gutter on all of these blocks they do not effectively handle drainage. However because there are drainage swales and a steep grade in some locations, drainage is directed away from the travel way and toward intersecting streets. Drainage could be improved, by installing gravel or concrete swales when appropriate along the north and south sides that direct drainage more effectively toward intersections. This would alleviate sediment that is currently entering v-pans at intersections. These blocks are consistent with the Town's snow storage plans and the Town Drainage Master Plan.

Historic Character. These blocks neither detract from nor contribute to the historic character of the Town.

Interfacing Streets. All interfacing streets appear to function well with these blocks.

Traffic Circulation. The relatively light level of vehicular traffic is due to local residents. Capacity appears adequate for intensity of use and emergency vehicle access.

Lighting. The existing pole-mounted residential light fixture on the northeast corner of the Hemlock intersection should be replaced with a fixture that is consistent with the Streetscape Lighting Plan.

Parking. Parallel parking is permitted along on both sides of these blocks. In winter, parking is prohibited along the south side to allow snow storage. Most residents have and use existing off-street parking. Therefore on street parking pressure is light. The existing parking configuration should be maintained and use of off street parking maximized.

Pedestrian Concerns. Pedestrian traffic on these blocks is light, attributed mainly to neighborhood residents. No sidewalks currently exist on these blocks. This condition causes little or no conflict with vehicular traffic. For these reasons, and in an attempt to preserve the "rough and natural" character of the area, no sidewalks are recommended. Therefore, the width of green strips can be maximized, allowing for both a separation from the travel way and private property, and adequate travel way width to accommodate emergency vehicles.

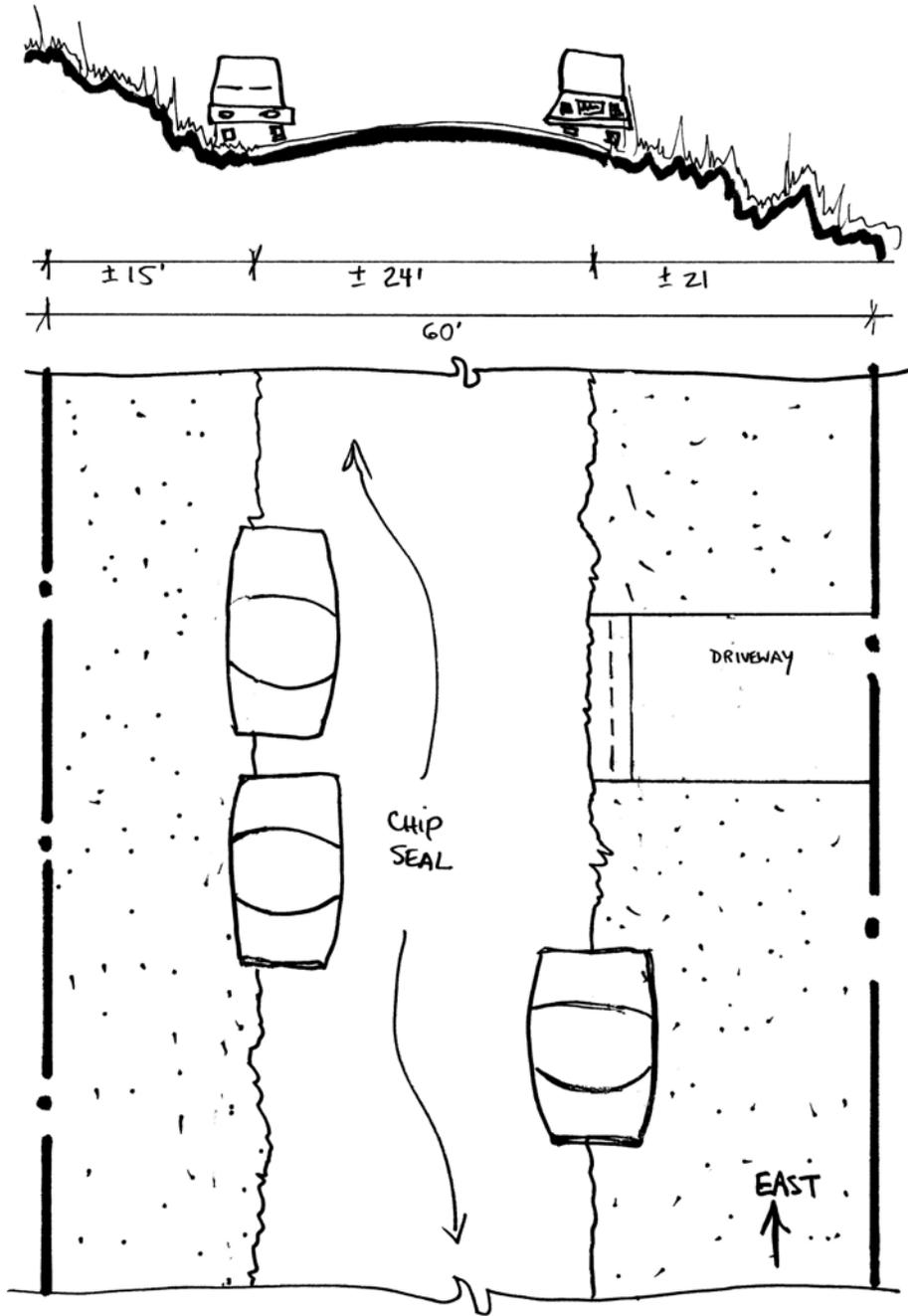
Street Furniture and Public Art. There is no street furniture or amenities and none are recommended.

Utilities. There are no utility concerns for these blocks at this time.

RECOMMENDED IMPROVEMENTS

1. Improve drainage by constructing gravel drainage swales where none currently exist, if needed..
2. Replace the existing pole-mounted residential light fixture at the northeast corner of the Hemlock intersection with a fixture that is consistent with the Streetscape Lighting Plan.

Refer to Streetscape East Telluride District, 2006, Engineered Street Designs and adjacent street profiles for additional information.



EAST COLUMBIA AVENUE - 600, 700, & 800 BLOCKS
(BETWEEN HEMLOCK & COLUMBINE STREETS)

EAST COLUMBIA AVENUE – 900 BLOCK

60' Right of Way

Neighborhood Meetings: July 15, 1996; October 8, 1997; January 6, 1997

BACKGROUND

Existing Street Conditions. The Streetscape Task Force has identified Columbia Avenue as Telluride's "Residential Main Street." From Columbine Street to where it dead ends at the Cemetery, this block slopes to the west and south, is surfaced with gravel, and is in fair condition. There are no sidewalks. A 3'-wide drain pan borders the southwest side of the travel way in order to protect residences to the south that are below the right of way grade and in danger of flooding from runoff. Snow is stored at the east end of the right of way and allowed to melt at this location. There is head-in parking to the south of this drain pan.

Environment/Drainage/Snow Storage. The drain pan appears to effectively handle runoff at this time. If additional problems are created as lots to the north of the travel way are developed, a berm between the south parking area and the private property may be a next step. Drainage could also be improved by a gravel drainage swale on the north that directs runoff toward Columbine Street. At the east side of the Columbine Street/Columbia Avenue intersection a drain pan should then be installed to carry the drainage from the north side of this block south, down the east side of Columbine Street. Since this block is not heavily traveled, it does not negatively affect air or water quality. With the exceptions noted above this block is consistent with Telluride's snow storage and drainage plans.

Historic Character. This block neither detracts from nor contributes to the historic character of the Town.

Interfacing Streets. Drainage between this block and Columbine Street should be improved.

Lighting. There is currently no street lighting on this block and none is required.

Neighborhood Concerns. The neighborhood has expressed strong concern with the management of drainage and the need to maximize parking.

Parking. No parking is currently allowed on the north side, and there is head in parking on the south. This configuration is insufficient to handle the heavy residential parking pressure on this block. In order to maximize availability of parking the Streetscape Task Force recommends that additional parking be allowed on the north. Existing off-street parking in this vicinity should be maximized.

Pedestrian Concerns. Pedestrian traffic, which is largely local residents, is low. There are no sidewalks on this block yet there appears to be little conflict with vehicular traffic. For these reasons, and in an attempt to preserve the "rough and natural" character of the area, the Streetscape Task Force does not recommend sidewalks for this block.

Street Furniture and Public Art. No street furniture or amenities currently exist and none are recommended.

Traffic Circulation. Because this block is residential and terminates at the cemetery, vehicular traffic is low. Capacity appears adequate for intensity of use and emergency vehicle access. However, emergency vehicles require that the travel way be at least 18'-wide to allow them to turn around.

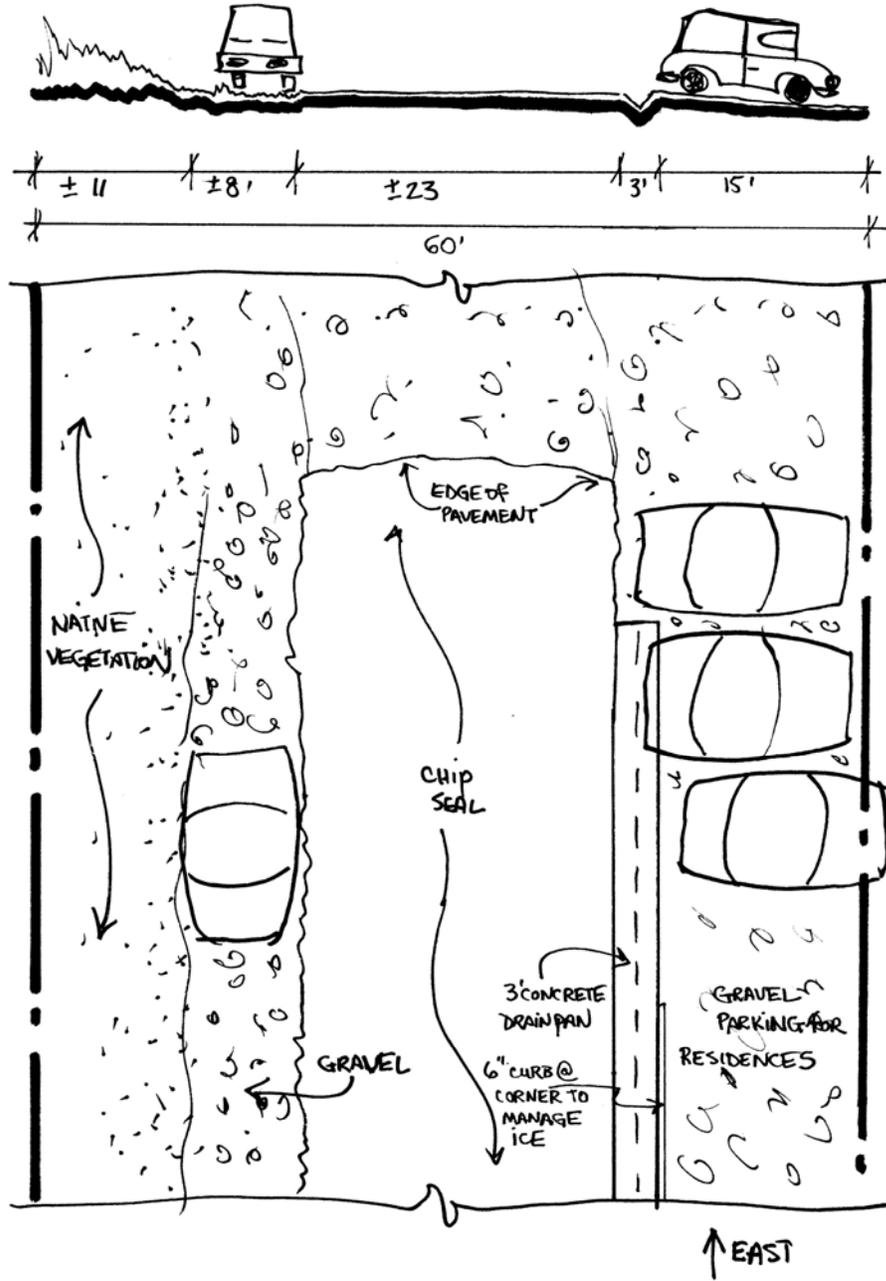
Utilities. There are no utility concerns for these blocks at this time.

RECOMMENDED IMPROVEMENTS

1. Improve drainage by constructing a gravel drainage swale on the north side of the block that leads to a drain pan on the east side of the Columbine/Columbia intersection.
2. Formalize head-in parking at the south side.
3. Surface this block with chip and seal, as conditions require.

NOTE: The designs for this block may need to change when the adjacent PUD is developed.

Refer to Streetscape East Telluride District, 2006, Engineered Street Designs and adjacent street profiles for additional information.



EAST COLUMBIA AVENUE - 900 BLOCK
(BETWEEN COLUMBINE STREET & CEMETARY)

NORTH COLUMBINE STREET – 100 BLOCK

50' Right of Way

Neighborhood Meetings: July 15, 1996

BACKGROUND

Existing Street Conditions. These blocks are currently surfaced with chip and seal and slope south toward East Colorado Avenue. South of the 100 Block intersection of Shadow Lane on the west and an alley on the east, there is a concrete curb and gutter on both sides and a 4'-wide sidewalk on the west. A drain pan, parallel with Columbine Street and crossing the Shadow Lane intersection, directs runoff into the gutter at the south end of the block. A diagonal drain pan near the intersection of the east alley and Columbine Street directs drainage from the alley into the east gutter. North of the Shadow Lane intersection is a portion of curb and gutter on the west side of the street that extends north about 20'. Drainage from the north portion of the 100 Block and the entire 200 Block is collected in gravel drainage swales and directed to the concrete drain pans and gutters to the south. Crossing Columbine Street diagonally from the east to west, just north of the Shadow Lane intersection, is a dip in the street that directs drainage to the west. Existing undefined green strips are planted in native vegetation.

Environment/Drainage/Snow Storage. While the existing drainage system adequately handles runoff from this block and the 200 Block of North Columbine, it should be improved. The absence of concrete drain pans on the steeper north sections negatively impacts water quality. Therefore, improvements to this block should include construction of concrete drain pans on the north half and changing the current curb and gutter configuration on the south half to a concrete drain pan. The existing drainage system at the southwest corner of the 100 Block should be maintained and potentially upgraded. The block is surfaced with chip and seal and does not negatively affect air quality. Snow storage along the west side of the block is consistent with Telluride's snow storage plan.

Historic Character. These blocks neither contribute to nor detract from the historic character of the area.

Interfacing Streets. All interfacing streets appear to function well with these blocks.

Lighting. There is a standard commercial light fixture at the southwest corner of East Colorado intersection. This should be replaced with a bollard that is consistent with the Streetscape Lighting Plan. No additional lighting is required at this time.

Parking. Parallel parking is allowed on both sides of the 100 Block, except in winter when the west side is used for snow storage. There is no parking along either side of the steep 200 Block. There is not a substantial amount of parking pressure in this area and therefore the existing parking configuration should be maintained. Existing off-street parking should continue to be maximized.

Pedestrian Concerns. Pedestrian traffic is largely local. The one short section of sidewalk that currently exists on the 100 Block is covered with gravel and does not appear to be commonly used. The lack of sidewalks on the majority of this block appears to create little or no conflict with vehicular traffic. For these reasons, and in an attempt to preserve the "rough and natural" character of the area, the Streetscape Task Force does not recommend additional sidewalks for this block. However, the existing sidewalk should be maintained to accommodate school children meeting the Galloping Goose Transit at East Colorado Avenue. There is no handicap access.

Street Furniture and Public Art. No street furniture or amenities currently exist and none are recommended.

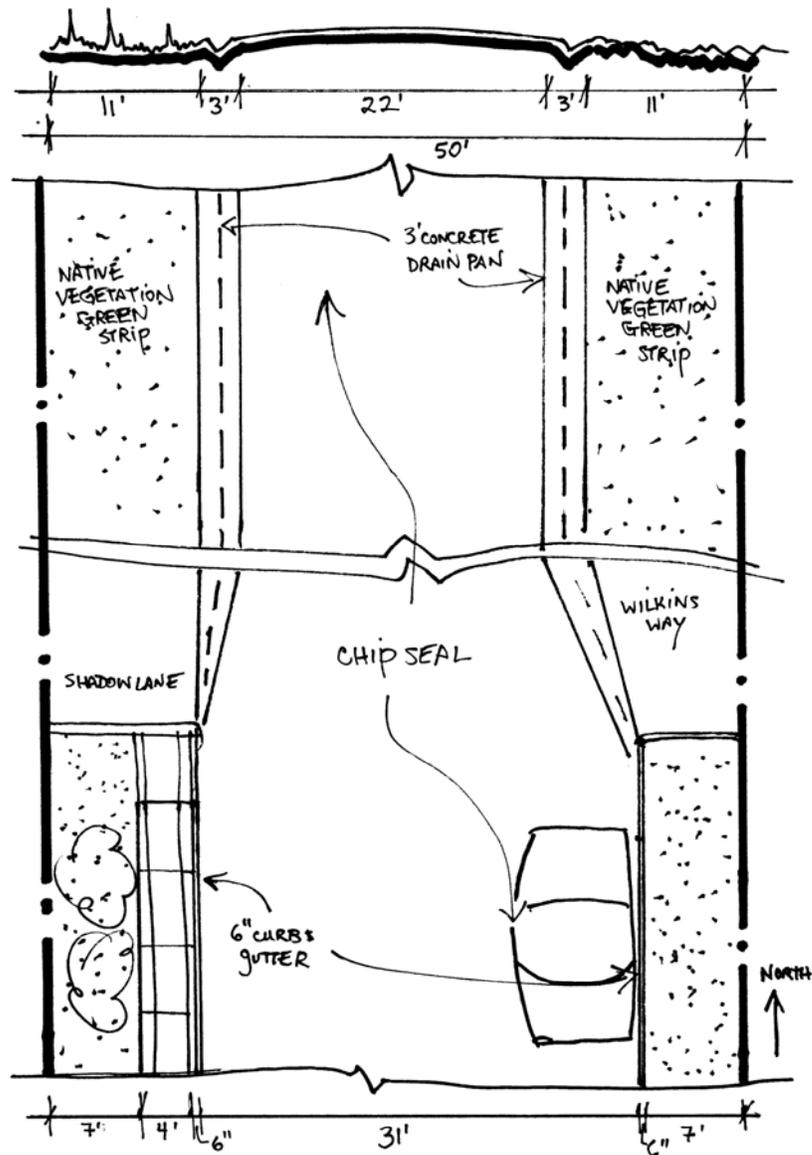
Traffic Circulation. This secondary street has moderate traffic intensity as it carries a fair amount of residential traffic to Road Hawk Circle and Pandora Avenue. Capacity appears adequate for intensity of use and emergency vehicle access.

Utilities. There are no utility concerns for these blocks at this time.

RECOMMENDED IMPROVEMENTS

1. Narrow the travel way width to 22 feet and construct 3'-wide concrete drain pans along the east and west sides of the north half of the 100 Block, tying the pans into the existing curb and gutter and the south half of the 100 Block. Eliminate the diagonal drain pan on the 100 Block.
2. Clean out and upgrade the drainage system at the southwest corner of the 100 Block to settle out sediment prior to discharge to the wetlands on the south side of East Colorado.
3. Landscape the east and west sides with native vegetation.

Refer to Streetscape East Telluride District, 2006, Engineered Street Designs and adjacent street profiles for additional information.



NORTH COLUMBINE STREET - 100 BLOCK
(BETWEEN E. COLORADO & E COLUMBIA AVENUES)

WEST DAKOTA AVENUE – 400 BLOCK

50' Right of Way

*Neighborhood Meetings: July 9, 1996; November 17, 1999;
September 27, 1999 (survey); January 19, 2000*

BACKGROUND

Existing Street Conditions. This block, improved during Streetscape Phase II 2000 Construction, is paved with asphalt, has a 3'-wide drain pan along the north side and along portions of the south side. This block slopes from its east and west ends toward a culvert bridge over Cornet Creek near its center. The bridge is in fair condition. There are no sidewalks or defined green strips.

Environment/Drainage/Snow Storage. This block is consistent with Telluride's snow storage and drainage plans.

Historic Character. As it exists this block contributes to the rough and natural character of town. Future improvements should strive to retain this neighborhood character.

Interfacing Streets. The only interfacing street, 300 North Aspen, poses no negative impacts.

Lighting. No street lighting currently exists on this block and none is required.

Neighborhood Concerns. Meeting notes indicate that the neighborhood is concerned about the need for resident permit parking and the possible need for a guardrail at the end of the street. Recent interaction with the neighborhood also indicates that there is a grave concern about "sinking" in the right of way and its effects on drainage. The slope appears to be shifting downhill.

Parking. On-street parallel parking is currently permitted on the north side where the travel way is wide enough. No parking is permitted on the south side of the block. Due to the numerous curb cuts on this block the amount of on-street parking is limited. Creation and use of off-street parking should be used maximized.

Pedestrian Concerns. Pedestrian traffic on this block is low, confined mainly to local residents. Due to the low vehicular and pedestrian traffic there is no need for sidewalks. Handicap access to this block is difficult.

Street Furniture and Public Art. No street furniture or amenities currently exist and none are recommended.

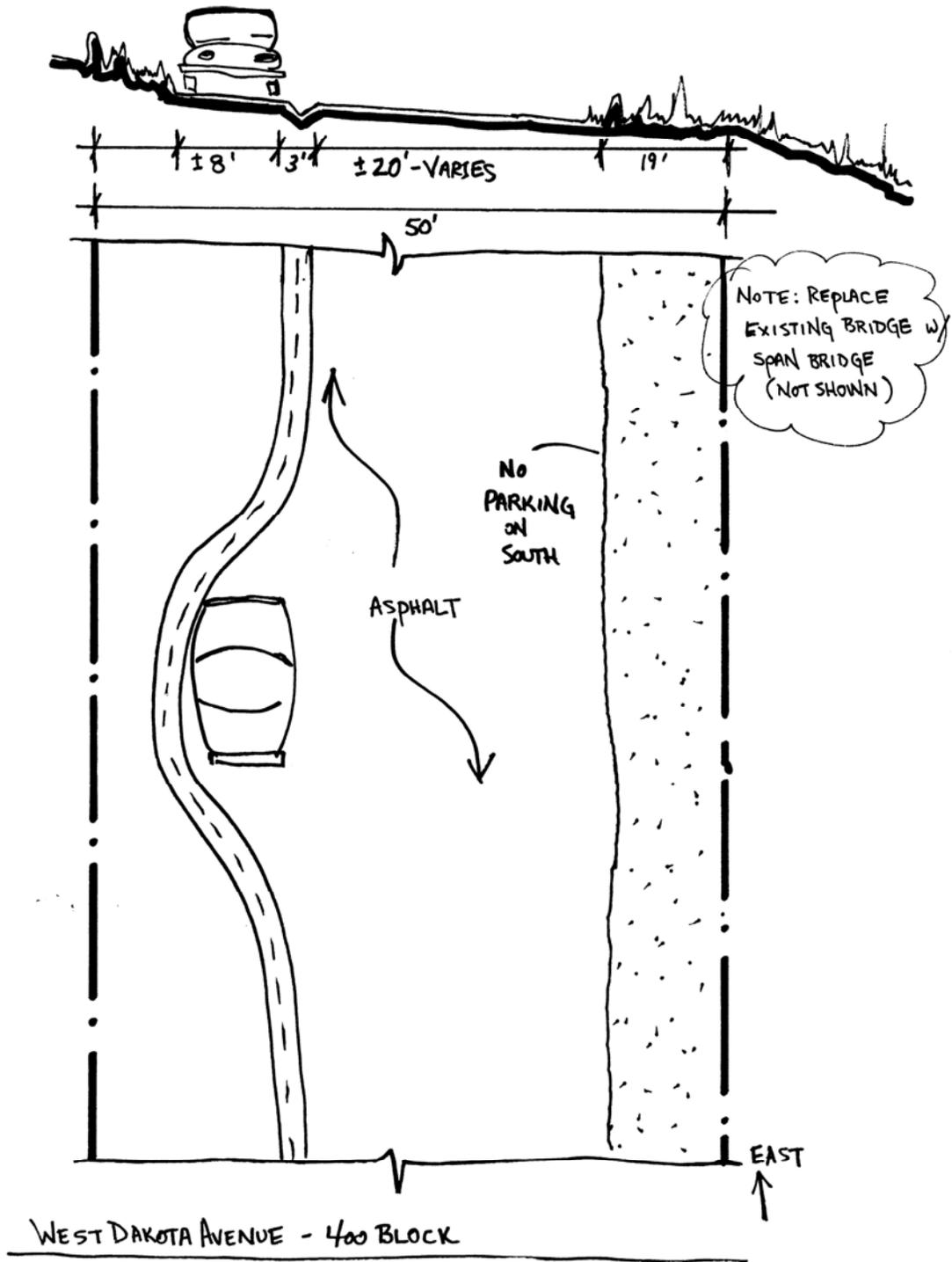
Traffic Circulation. As this street terminates at its west end, vehicular traffic is confined to local residents. Capacity appears adequate for intensity of use. Emergency vehicle access appears tight and adequate width should be ensured.

Utilities. There are no utility concerns for this block at this time.

RECOMMENDED IMPROVEMENTS

No additional improvements are recommended at this time.

Refer to Streetscape Phase II, 2000, Engineered Street Designs and adjacent street profiles for additional information.



NORTH DAVIS STREET – 200 BLOCK

50' Right of Way

Neighborhood Meetings: July 9, 1996

BACKGROUND

Existing Conditions. The block is surfaced with asphalt and slopes slightly south. There is a 5'-wide sidewalk and curb and gutter on the west side and a green strip along the east side. Improved and new sidewalks were installed on the west side as part of the Tomboy Subdivision. The task force did not see the necessity for additional sidewalks as pedestrian access to the school could be accomplished on the west. Existing infrastructure is in good condition.

Environment/Drainage/Snow Storage. Street conditions appear to cause no adverse impacts to air and water quality. Drainage could be improved by constructing a drain pan on the west side of the travel way. This block is consistent with Telluride's snow storage and drainage plans.

Historic Character. This block neither contributes to nor detracts from the historic character of the area. There are no historic structures.

Interfacing Streets. All interfacing streets appear to function well with these blocks.

Lighting. The one pole-mounted residential light fixture at the northeast corner should be updated with a fixture that is consistent with the Streetscape Lighting Plan.

Parking. Parallel parking is permitted on both sides of the street. In winter, parking restrictions exist on the west side to allow for snow storage. There is not a substantial amount of parking pressure on this block and the existing parking configuration should be retained. Existing off-street parking should be used to the greatest extent possible.

Pedestrian Concerns. This block is a primary pedestrian access point to the Telluride Middle/High School. Pedestrian traffic is attributed to students and local residents. The existing 5'-wide sidewalk on the west side appears to adequately handle pedestrian traffic. There is adequate handicap access.

Street Furniture and Public Art. No street furniture or amenities currently exist and none are required.

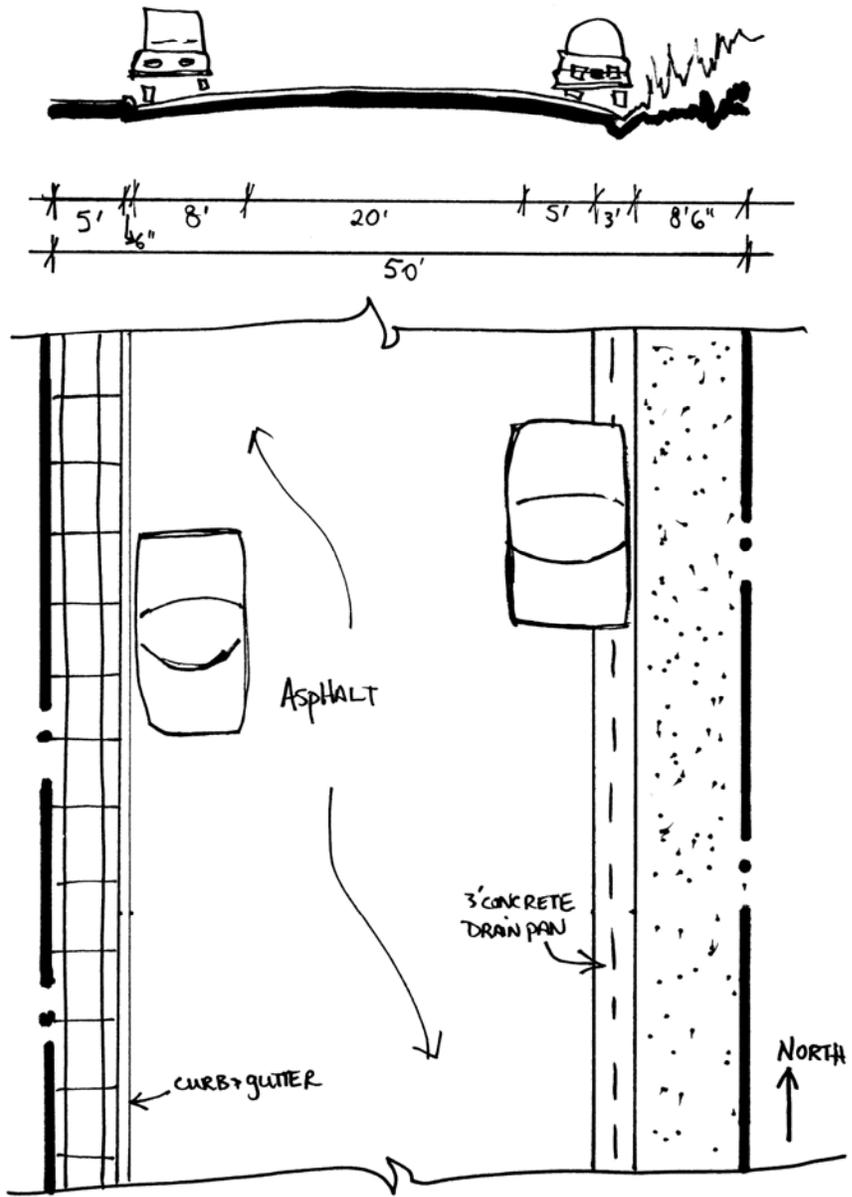
Traffic Circulation. This block provides primary access to portions of Galena Avenue west of Cornet Creek. It also provides access into the High/Middle School for buses. Therefore, the traffic intensity is moderate. The travel way width is adequate for the intensity of use. Emergency vehicle access also appears adequate.

Utilities. There are no utility concerns for these blocks at this time.

RECOMMENDED IMPROVEMENTS

1. Construct a drain pan on the west side of the block, if necessary. If done, the drain pan should be located so that the tires of parked cars are not in the drain pan.
2. Replace the existing pole-mounted residential light fixture with a fixture that is consistent with the Streetscape Lighting Plan.

Refer to Streetscape West Telluride District, 2006, Engineered Street Designs and adjacent street profiles for additional information.



NORTH DAVIS STREET - 200 BLOCK

(BETWEEN COLUMBIA & GALENA AVENUES)

SOUTH DAVIS STREET – 100 & 200 BLOCKS

50' Right of Way

Neighborhood Meetings: July 24, 1996; June 23, 1999

BACKGROUND

Existing Street Conditions. The 100 and 200 Blocks of South Davis Street are surfaced with asphalt and slope to the south. Pacific Avenue intersects these two blocks. The 200 Block of South Davis Street is half the distance of a typical street and terminates at an alley intersection. Both blocks contain 5'-wide sidewalks on both sides of the street. There is traditional curb and gutter along the east side of both blocks, and the west side of the 200 Block. There is a roll curb and gutter along the west side of the 100 Block adjacent to Cornet Creek Condominiums. There are no green strips.

Environment/Drainage/Snow Storage. The drainage system effectively handles runoff. The curb and gutter and asphalt surfacing do not appear to negatively impact air and water quality. These blocks are consistent with Telluride's snow storage and drainage plans.

Historic Character. There are two contributing structures on these blocks. Any alteration to the right of way should consider potential negative impacts these historic resources.

Interfacing Streets. The configuration of Pacific Avenue and the businesses that occupy these blocks lead to a high number of vehicles passing through this area. Traffic volumes can cause some trouble accessing westbound Colorado Avenue. Consideration should be given to this situation when managing traffic circulation for the Carhenge Intercept Parking Facility.

Lighting. The two existing standard commercial streetlights on this block should be kept in their current location.

Parking. On the 100 Block of South Davis Street, no parking is permitted on the west side at any time, due to the presence of almost continual curb cuts. Parallel parking is permitted along the east side, except in winter when parking is prohibited allow temporary snow storage. On the 200 Block, 15-minute parking is permitted on the east side to service Pacific Street Liquors and parallel parking is permitted on the west side, except in winter because of snow storage. Parking pressure in this area is substantial and the existing parking configuration should be maintained. Use of existing off-street parking should be maximized.

Pedestrian Concerns. The existing sidewalks effectively handle the moderate to high pedestrian traffic in this area. These blocks have handicap access.

Street Furniture and Public Art. No street furniture or amenities currently exist on these blocks. Furniture adjacent to the right of way might help to create a better neighborhood feel in this area. Such furniture should be kept simple and somewhat rustic.

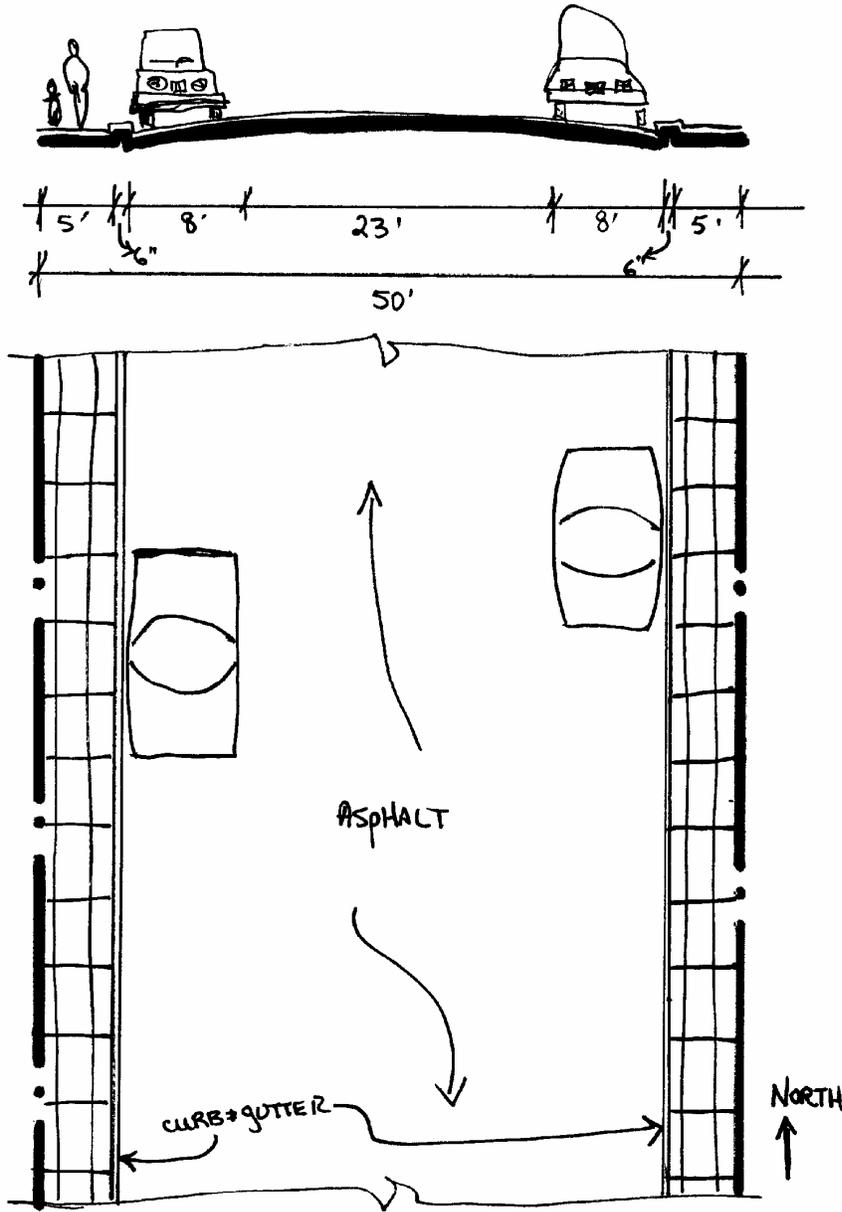
Traffic Circulation. Vehicular traffic is heavy on these blocks. This traffic is due to Pacific Street designated as one way in the easterly direction west of Davis Street. This causes traffic traveling from both the east and west to funnel onto South Davis Street. The width of the street effectively handles this traffic, but there is often a queue of two or three cars waiting to turn left onto Colorado Avenue from the 100 Block of South Davis Street. Emergency vehicle access appears adequate.

Utilities. There are no utility concerns for these blocks at this time.

RECOMMENDED IMPROVEMENTS

1. Continue to study the affect the Carhenge Intercept Parking Facility on traffic circulation and parking configuration on this block.

Refer to Streetscape West Telluride District, 2006, Engineered Street Designs and adjacent street profiles for additional information.



SOUTH DAVIS STREET - 100 & 200 BLOCKS
(BETWEEN COLORADO & ALLEY)

NORTH FIR STREET – 100 BLOCK

50' Right of Way

Neighborhood Meetings: July 10, 1996; February 24, 1997

BACKGROUND

Existing Street Conditions. This block, improved during Streetscape Phase I 1998 construction, is paved in asphalt and slopes south. There is a 6'-wide sidewalk on the east, and 7.5'-wide sidewalk on the west, and curb and gutter along both sides. All improvements are in good condition. An alley intersects the center of this block. There are no green strips.

Environment/Drainage/Snow Storage. In its current condition, this block is consistent with Telluride's snow storage and drainage plans. It does not appear to negatively impact air and water quality.

Historic Character. There are several historic structures on this block, such as the historic First National Bank Building. Any alteration of the right of way should not negatively affect the historic resources on this block.

Interfacing Streets. All interfacing streets appear to function well with this block.

Lighting. There are no streetlights on this block and none are required.

Parking. Paid parallel parking is permitted on both sides of the block. Parking pressure is heavy due to the proximity of the commercial district. There are no parking restrictions in winter. The current parking configuration should be maintained.

Pedestrian Concerns. Existing sidewalks appear to effectively handle the moderate to heavy pedestrian traffic along this block. There is handicap access.

Street Furniture and Public Art. No street furniture or amenities currently exist and none are recommended.

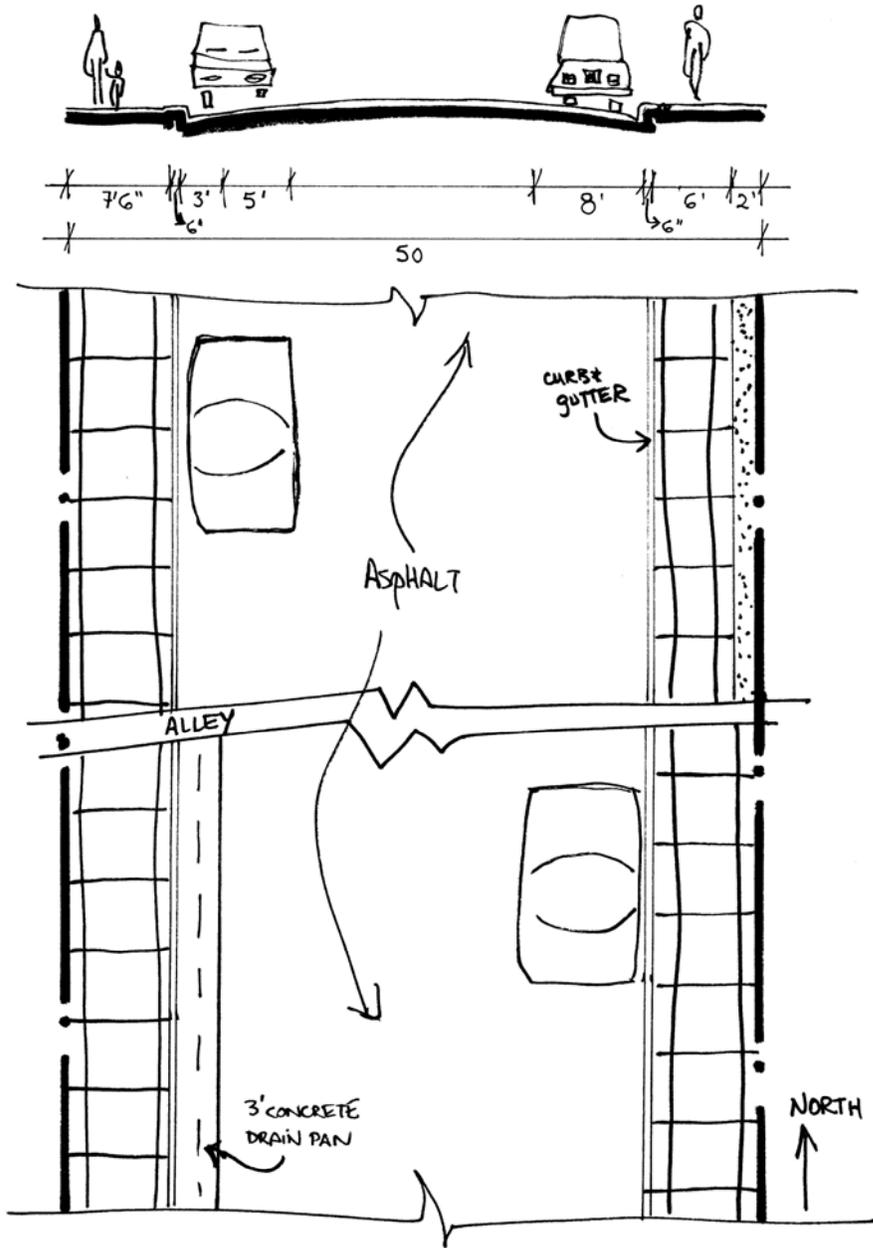
Traffic Circulation. Vehicular traffic is moderate to heavy. Travel way width appears adequate for the intensity of use and emergency vehicle access.

Utilities. There are no utility concerns for these blocks at this time.

RECOMMENDED IMPROVEMENTS

No improvements are needed at this time.

Refer to Streetscape Phase I, 1998, Engineered Street Designs and adjacent street profiles for additional information.



NORTH FIR STREET - 100 BLOCK

(BETWEEN COLORADO & COLUMBIA AVENUES)

NORTH FIR STREET – 200 & 300 BLOCKS

50' Right of Way

Neighborhood Meetings: July 10, 1996; February 24, 1997

BACKGROUND

Existing Street Conditions. These blocks, improved during Streetscape Phase I 1998 construction, are paved with asphalt, slope south, and have traditional curb and gutter, 5'-wide sidewalks, and 2.5'-wide green strips along their east and west sides. All improvements are in good condition.

Environment/Drainage/Snow Storage. In their current condition, these blocks are consistent with Telluride's snow storage and drainage plans. Good drainage and little on-street sediment result in little adverse impact to air and water quality.

Historic Character. The right of way itself neither contributes to nor detracts from the historic character of the area, which is attributed to the historic Town Hall and other rated structures. However, these blocks are the entrance to the Telluride Museum and frame it as it stands at the end of the roadway. The Museum was once a hospital. Any alteration to these blocks must consider the historic resources.

Interfacing Streets. All interfacing streets appear to function well with this block.

Lighting. The single pole-mounted residential light fixture on the southwest corner of the 200 Block should be replaced with a fixture that is consistent with the Streetscape Lighting Plan.

Parking. Parking pressure is heavy due to the proximity of these blocks to the commercial district. Two-hour parallel parking is allowed on both sides of the street, except in winter when the east side is used for temporary snow storage. The current configuration should be maintained and use of off-street parking should be maximized.

Pedestrian Concerns. Many pedestrians use these blocks to travel up to the Museum from Colorado Avenue. Existing sidewalks appear to effectively handle the moderate pedestrian traffic that is typical along these blocks. This block has handicap access.

Street Furniture and Public Art. No street furniture or amenities currently exist and none are recommended.

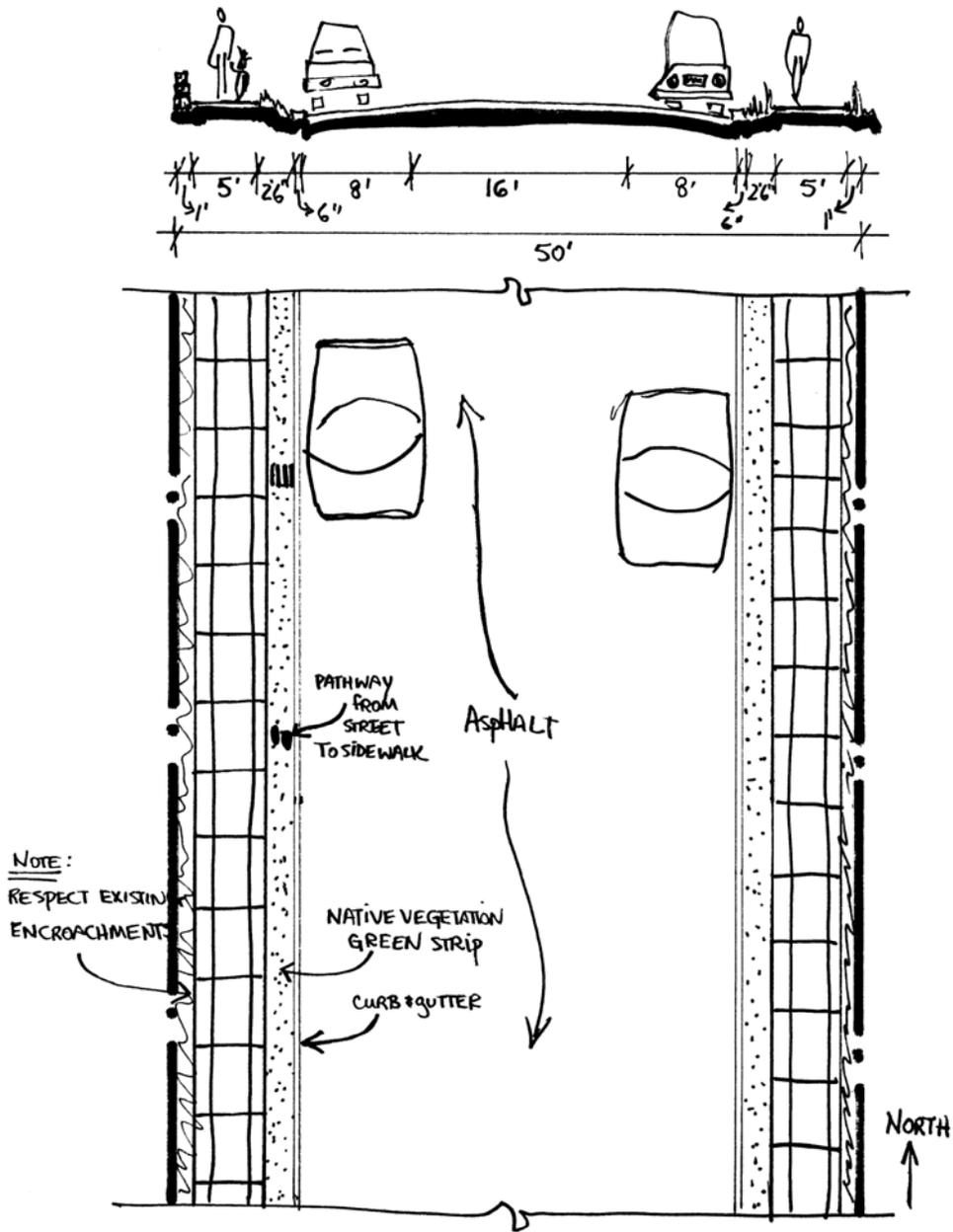
Traffic Circulation. Vehicular traffic on these blocks is moderate. Travel way width appears adequate for intensity of use and emergency vehicle access.

Utilities. There are no utility concerns for these blocks at this time.

RECOMMENDED IMPROVEMENTS

1. Replace the existing pole-mounted light fixture at the southwest corner of the block with a pole-mounted light fixture that is consistent with the Streetscape Task Force Lighting Plan.

Refer to Streetscape Phase I, 1998, Engineered Street Designs and adjacent street profiles for additional information.



NORTH FIR STREET - 200 + 300 BLOCKS
(BETWEEN COLUMBIA + GREGORY AVENUES)

SOUTH FIR STREET – 100 BLOCK

50' Right of Way

Neighborhood Meetings: July 23, 1996; May 27, 1998; November 17, 1998

BACKGROUND

Existing Street Conditions. This southward sloping block is paved with asphalt. It has curb and gutter and a concrete sidewalk along the entire east side. It has a curb and gutter along the north half of the west side and a 4-foot wide concrete drain pan along the south half of the west side. Due to grade issues, there is a concrete stairway along the sidewalk on the north half of the west side. The sidewalk on the south half of the west side has no curb. There are no green strips.

Environment/Drainage/Snow Storage. Drainage appears to be effective along this block. In its current condition, this block is consistent with Telluride's snow storage and drainage plans.

Historic Character. Any future improvements to the right of way should consider historic resources.

Interfacing Streets. All interfacing streets appear to function well with this block.

Lighting. There is a standard commercial street light on the southwest corner of this block and in the middle of the east side. Fixture types and locations are consistent with the Streetscape Light Plan.

Parking. Demand for parking in this area is very high as it is part of the commercial core. Paid diagonal parking is permitted all year along the east side of the street. The current parking configuration should be maintained and off-street parking should be maximized.

Pedestrian Concerns. Pedestrian traffic through this area is high as it is a natural pedestrian path from the Gondola Plaza to Main Street. While the east side sidewalk is wide and can adequately handle pedestrian traffic volumes, the stairway on the west side can make passage difficult. It is not possible for handicapped people to use the west side safely because of the stairs. Given the extreme change in grades on the west side, there appears to be no other viable engineered solution. Despite heavy vehicle traffic, pedestrians often walk in the travel way.

Street Furniture and Public Art. No street furniture or amenities currently exist and none are recommended.

Traffic Circulation. Vehicular traffic is quite heavy at this location due to the many businesses and hotels in the area. Nevertheless, the travel way width appears adequate for the intensity of use and emergency vehicle access.

Utilities. There are no utility concerns for this block at this time.

RECOMMENDED IMPROVEMENTS

No improvements are recommended at this time.

Refer to adjacent street profiles for additional information. There are no engineered plans.

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SOUTH FIR STREET – 200 BLOCK

50' Right of Way

Neighborhood Meetings: July 23, 1996; October 28, 2006

BACKGROUND

Existing Street Conditions. This block was improved in 2005 as part of the South Fir and West San Juan Street Improvements Project. Sloping southward, it is paved with asphalt and has curb and gutter along the east and west side. There is a 7'-wide concrete sidewalk along the east side and a 9'-wide base coarse walkway behind the curb along the north half of the west side. Sidewalk improvements along the entire west side are awaiting redevelopment of this side of the block. There are no green strips.

Environment/Drainage/Snow Storage. Drainage appears to be effective along this block. In its current condition, this block is consistent with Telluride's snow storage and drainage plans.

Historic Character. This block is important to the historic character of the area, as the entire west side is made up of the Transfer Building and the Stronghold Building and there are two contributing structures on the east side. Any future improvements to the right of way should consider these historic resources.

Interfacing Streets. All interfacing streets appear to function well with this block.

Lighting. There is a standard commercial street light on the southwest corner of this block and in the middle of the east side. Fixture types and locations are consistent with the Streetscape Light Plan.

Neighborhood Concerns. No major neighborhood concerns have been expressed.

Parking. Demand for parking in this area is quite high as it is part of the commercial core. Paid parallel parking is permitted along the east and west sides of the street, except in winter when the west side is needed to store snow. The current parking configuration should be maintained and off-street parking should be maximized.

Pedestrian Concerns. Pedestrian traffic through this area is high as it is a natural pedestrian path from the Gondola Plaza to Main Street. While the east side sidewalk is wide and can adequately handle pedestrian traffic volumes, it will be important to provide a contiguous sidewalk along the west side of this block. Despite heavy vehicle traffic, pedestrians often walk in the travel way next to parked cars rather than fully crossing from west to east.

Street Furniture and Public Art. No street furniture or amenities currently exist and none are recommended.

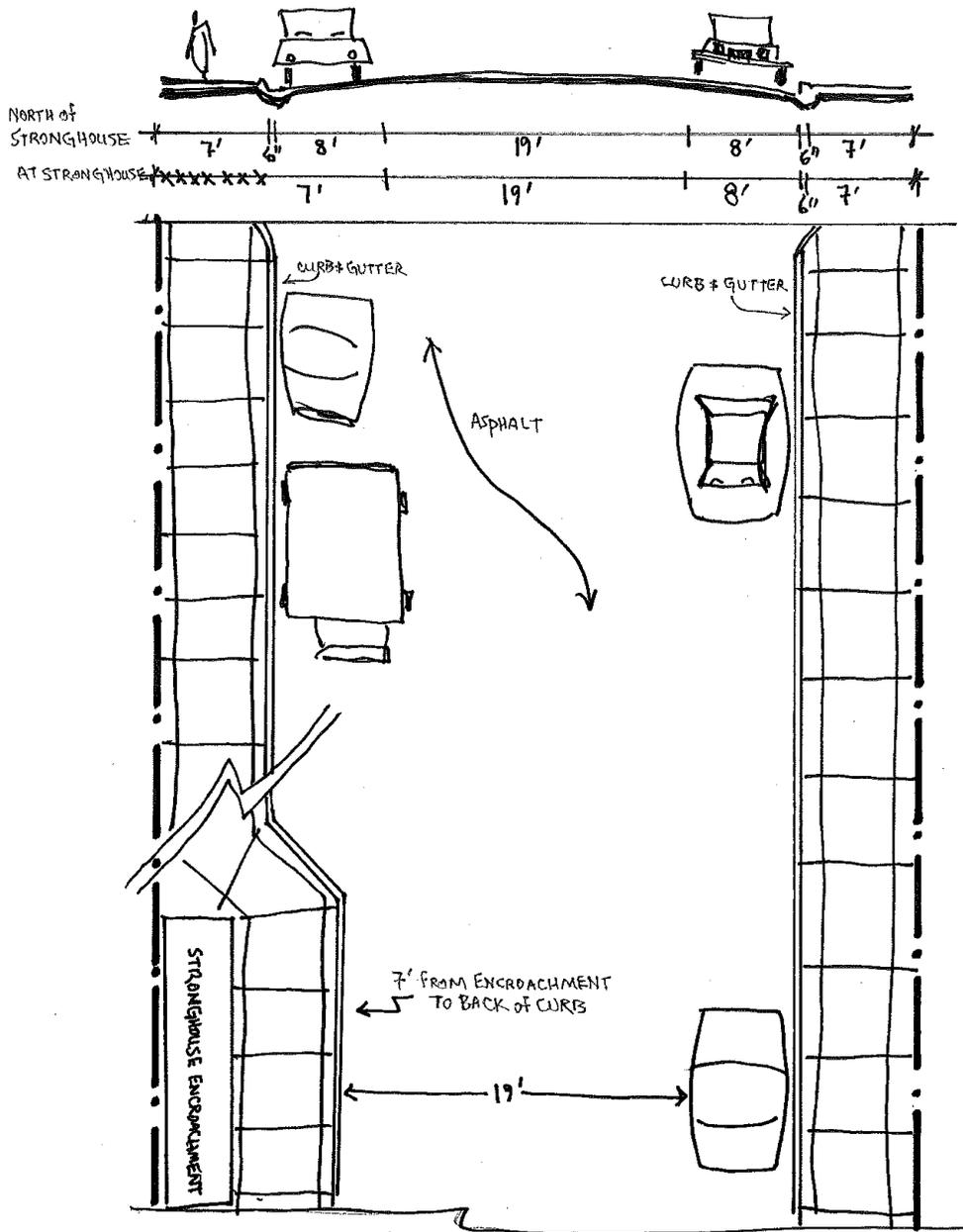
Traffic Circulation. Vehicular traffic is quite heavy at this location due to the many businesses and hotels in the area. This block is also a segment of Telluride's Galloping Goose Bus Town Loop. The travel way width appears adequate for the intensity of use and emergency vehicle access.

Utilities. There are no utility concerns for this block at this time.

RECOMMENDED IMPROVEMENTS

No improvements are recommended at this time.

Refer to Streetscape Warehouse District, 2006, Engineered Street Designs and adjacent street profiles for additional information.



SOUTH FIR STREET - 200 BLOCK
(BETWEEN PACIFIC & W SAN JUAN)

- REVISED 2016 -

SOUTH FIR STREET – 300 BLOCK

50' Right of Way

Neighborhood Meetings: July 23, 1996; October 28, 1998; November 12, 1998

Existing Street Conditions. This block was improved in 2005 as part of the South Fir and West San Juan Street Improvements Project. Sloping southward from West San Juan Avenue to the San Miguel River, it is paved with asphalt, has curb and gutter along the east side that terminates with a 3'- wide drain pan, and a 3'-wide drain pan along the west side. There is a 7.5'-wide green strip along the west side, which may become a sidewalk when the adjacent property develops. The block's south terminus has a small pocket park that provides a riparian buffer for the San Miguel River and houses an underground sedimentation vault for treating runoff prior to discharging it to the San Miguel River.

Environment/Drainage /Snow Storage. Drainage appears to be effective along this block. Although, the transition from curb and gutter to a 3' drain pan on the west side encourages sediment to drop out and block runoff flow. At the south end of this block there is an underground sediment vault that discharges runoff into the Ice House Pond for final polishing prior to release into the San Miguel River. Snow is store at the block's terminus. In its current condition, this block is consistent with Telluride's snow storage and drainage plans.

Historic Character. This block neither contributes to nor detracts from the historic character of the Town.

Interfacing Streets. All interfacing streets appear to function will with this block.

Lighting. There are no streetlights along this block and none are required.

Parking. Demand for parking in this area is very high due to the Ice House hotel, which has off-street parking, and the Manitou Lodge and Condominiums, which are south of the river and have no off street parking. Diagonal parking is permitted along the west side of the block throughout the year. If possible, the current parking configuration should be maintained and creati9on and use of additional off-street parking should be maximized.

Pedestrian Concerns. Pedestrian traffic through this area is moderate as it connects the River Trail to the commercial core. The east side sidewalk appears adequate for the pedestrian volume, at this time.

Street Furniture and Public Art. No street furniture or amenities currently exist and none are recommended; although benches may be desirable at the pocket park.

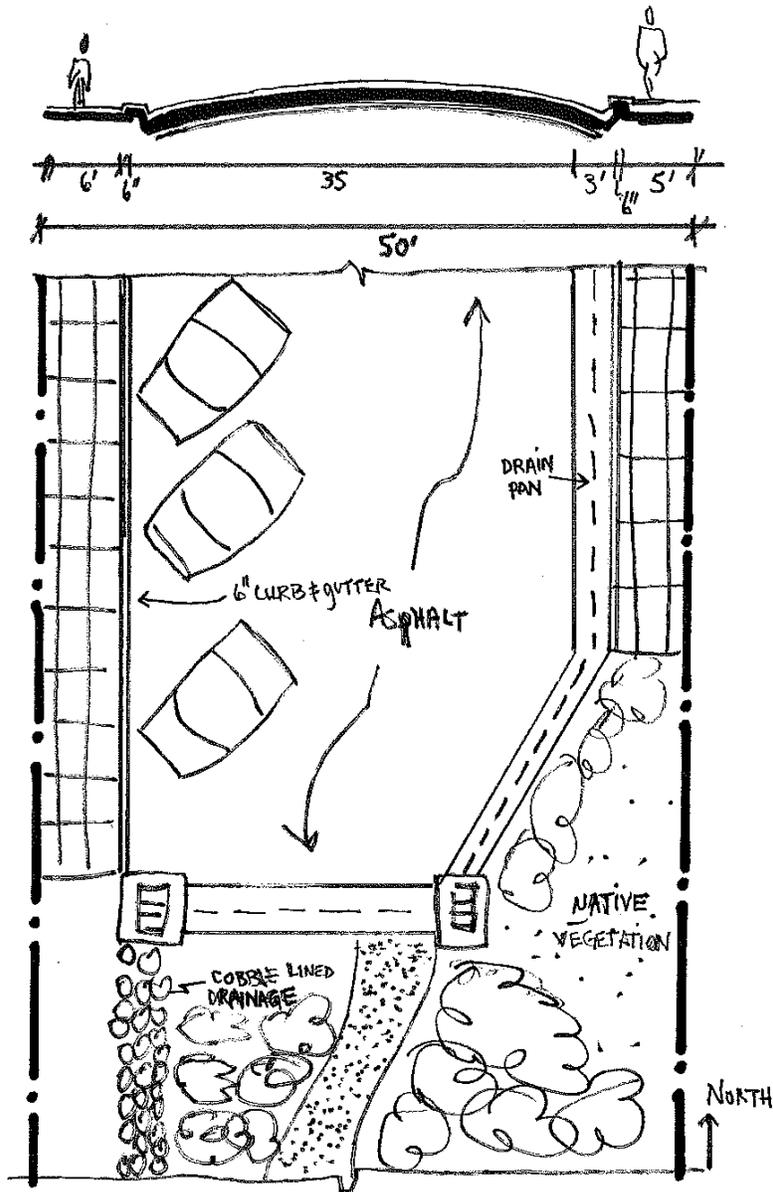
Traffic Circulation. Vehicular traffic is moderate, but intense due to the dense parking and narrow travel way. Nevertheless, the travel way width appears adequate for the intensity of use and emergency vehicle access.

Utilities. There are no utility concerns for this block at this time.

RECOMMENDED IMPROVEMENTS

1. The 3'- wide concrete drain pan along the west side should be changed to a traditional curb and gutter with a more aggressive slope, if possible, to maintain the velocity of runoff flows and bring sediment to the sediment vault at the south end of the block.

Refer to Streetscape Warehouse District, 2006, Engineered Street Designs and adjacent street profiles for additional information.



SOUTH FIR STREET - 300 BLOCK
(BETWEEN W SAN JUAN & RIVER)

- REVISED 2016 -

**WEST GALENA AVENUE – 600 & 700 BLOCKS
DAVIS STREET WEST TO MIDDLE/HIGH SCHOOL PLAYING FIELDS**

60' Right of Way

Neighborhood Meetings: July 9, 1996; February 17, 1999; March 31, 1999

BACKGROUND

Existing Street Conditions. This paved street slopes slightly downward to the west and is in fair condition. Improvements are required to improve drainage and pedestrian access. Currently, runoff sheet flows across the High/Middle School driveway, creating a hazard. A sidewalk is needed on the north to improve pedestrian access to the High/Middle School. There is a green strip along the south side of the right of way. A wider travel way may be needed to accommodate school bus traffic.

Environment/Drainage/Snow Storage. These blocks minimally impact air and water quality. Meeting minutes state that the "crown of Galena will improve drainage from the alley." Drainage is currently to the west and south. As they exist, these blocks are consistent with Telluride's snow storage and drainage plans.

Historic Character. These blocks neither contribute to nor detract from the historic character of the Town.

Interfacing Streets. All interfacing streets appear to function well with this block.

Lighting. The pole-mounted residential light fixture at the southwest corner should be replaced with a fixture that is consistent with the Streetscape Lighting Plan.

Neighborhood Concerns. Meeting minutes contain no mention of neighborhood concerns; however, recent development of the Tomboy PUD has noted the need to preserve access to parking and service areas in the PUD.

Parking. Parallel parking is permitted on both sides of the street. Parking is limited by extensive curb cuts on the south near the school playing fields. Off-street parking is prevalent; therefore on-street parking is not heavily used. In winter, parking is prohibited on the south side to allow for snow storage. Curb cuts may be needed on the north side in the future.

Pedestrian Concerns. Pedestrian traffic is high during the school year and pedestrian improvements are needed, as a result. A 6-foot-wide sidewalk on the south side of the street is proposed. Handicap access is also needed.

Street Furniture and Public Art. No street furniture or amenities currently exist and none are required.

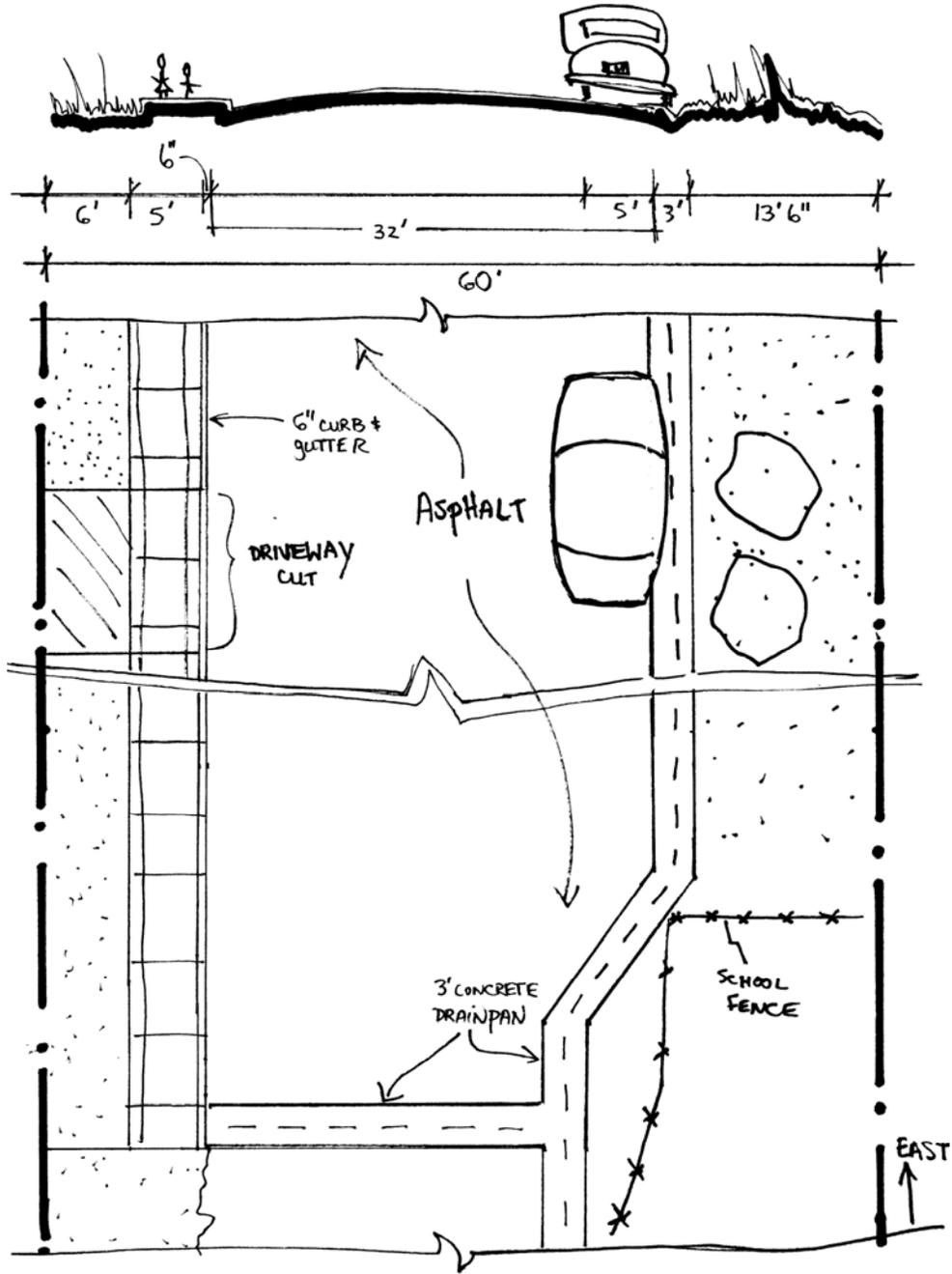
Traffic Circulation. While these blocks dead end at the school playing fields, school buses are increasingly using this street to access the school maintenance facility and drop children off. Capacity is not adequate for the intensity of use. Emergency vehicle access appears adequate. Vehicles can easily turn around in the High/Middle School lot.

Utilities. Underground all utilities. If possible, locate electric transformer boxes on private property. If they must be in the right-of-way, they should be as unobtrusive as possible. Concrete bollards will be required to protect them from vehicle impact.

RECOMMENDED IMPROVEMENTS

1. Construct a 6-foot-wide sidewalk on the north side of the street to improve pedestrian access to the school.
2. Re-grade these blocks and install drainage features that will improve runoff management by crowning the street in conjunction with any resurfacing activities.
3. Increase travel way width in conjunction with any resurfacing and re-grading activities.
4. Update existing residential pole-mounted light fixture at west end of street.
5. Determine whether current traffic circulation patterns are adequate for emergency vehicle access.

Refer to Streetscape West Telluride District, 2006, Engineered Street Designs and adjacent street profiles for additional information.



WEST GALENA AVENUE - 600 & 700 BLOCKS

(BETWEEN DAVIS & SCHOOL PLAYING FIELDS)

WEST GALENA AVENUE – 500 BLOCK

60' Right of Way

*Neighborhood Meetings: July 9, 1996; June 4, 1997; June 18, 1997;
September 10, 1997; February 17, 1999*

BACKGROUND

Existing Conditions. Cornet Creek divides the 500 Block of West Galena into east and west sections. The east section is unpaved. The west section is surfaced with chip seal. The east section right of way is 60' wide, although Cornet Creek does run in and along it. The east section drains south and west toward the creek. Most of the west section drains west and south toward North Davis Street. There is currently no safe access, vehicular or pedestrian, across Cornet Creek; although, this block is frequently used by pedestrians and school children who are traveling east or west across the creek. There are no sidewalks along either section of this block. There is landscaping on north side of the right of way and Cornet. There is public open space east of Cornet.

Environment/Drainage/Snow Storage. Cornet Creek water quality is adversely impacted by untreated runoff from the east. Because the east section is unpaved, airborne dust is a potential problem. However, very limited traffic at this location minimizes this problem. The east section drains south toward the houses and then west toward Cornet Creek. It is inefficient and improvements are needed. The west section drains toward the south and west. While inefficient, generally this runoff reaches North Davis Street. Currently this block complies with Telluride's snow storage and drainage management plans.

Historic Character. There are several historic structures along this block. Any alteration to the right of way should not negatively affect these historic resources.

Interfacing Streets. To the east, North Townsend Street has been improved with asphalt and a concrete drain pan has been constructed along its west side. This directs runoff south and away from West Galena at this location. Therefore, North Townsend does not impact the 500 Block of West Galena negatively.

Lighting. There is one light fixture on the west end of the block. It is an existing pole-mounted light that will be replaced with an updated fixture.

Parking. On the east section, there is parking on both the north and south sides, and at the end at the pocket park providing ample on-street parking for residents in non-winter months. If the block continues to be separated by the creek, no parking problems are foreseen. No on-street parking is permitted on this block during winter, although cars do park there. The west section has on-street parking on both the north and south, although there are winter parking restrictions for the south side. All available off-street parking should be used to the greatest extent possible.

Pedestrian Concerns. This is an important pedestrian thoroughfare that requires a "safe connection". Currently there is no formal pedestrian access across Cornet Creek that would connect the east and west sections. Nevertheless, when creek flows permit, pedestrians—particularly children—tend to cross the creek using an informal, sawed-board crossing. There is no handicap access.

Neighborhood Concerns. All neighborhood residents on record oppose installing a formal pedestrian bridge over the creek at this location. Nevertheless, the Task Force recommends a pedestrian bridge at this location.

Street Furniture and Public Art. No street furniture or public art currently exists on this block. A sign may be desirable at the entrance to the pocket park identifying it as public space. Park benches could be considered.

Traffic Circulation. Vehicular traffic on the east and west sections is limited to local residents, as there is no through circulation. The current configuration of the east and west sections is difficult for emergency vehicles, which have to either back in or back out of the block.

Utilities. All utilities are to be put underground. If possible, locate electric transformer boxes on private property. If they must be in the right of way, they should be as unobtrusive as possible. Concrete bollards will be required to protect them from vehicle impact.

RECOMMENDED IMPROVEMENTS

West Section.

1. The street must remain two-way.
2. North Side. Establish diagonal head in parking. Establish green space with native vegetation and boulders, to prevent parking on the green space.
3. South side. Construct a 3-foot-wide drain pan on the south side draining to North Davis Street.
4. Replace existing pole-mounted light fixture with an updated fixture.

East Section.

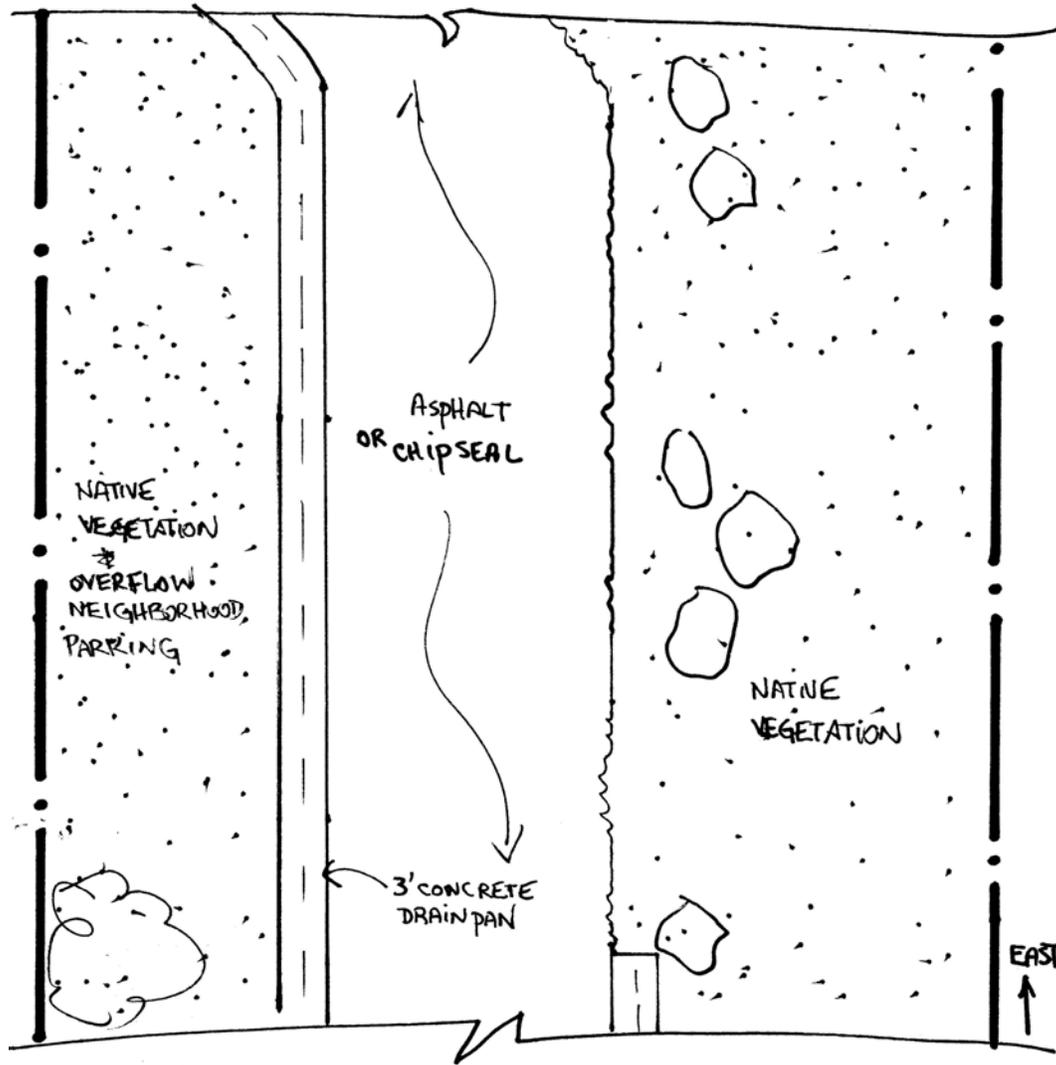
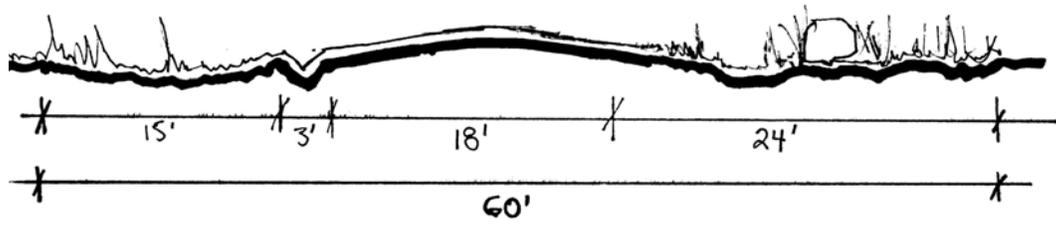
1. Construct a simple, rustic footpath along the south side of the pocket park.
2. Contour the street and pocket park to prevent drainage from flowing onto private property south of the block.
3. Create an infiltration garden in the pocket park to intercept and treat street drainage before it enters Cornet Creek.

4. Install a sign at the east entrance to the pocket park identifying it as public space and prevent parking from blocking the entrance.
5. Allow parking where the street terminates at the park, but keep a clear view corridor along the pedestrian entry to the park.

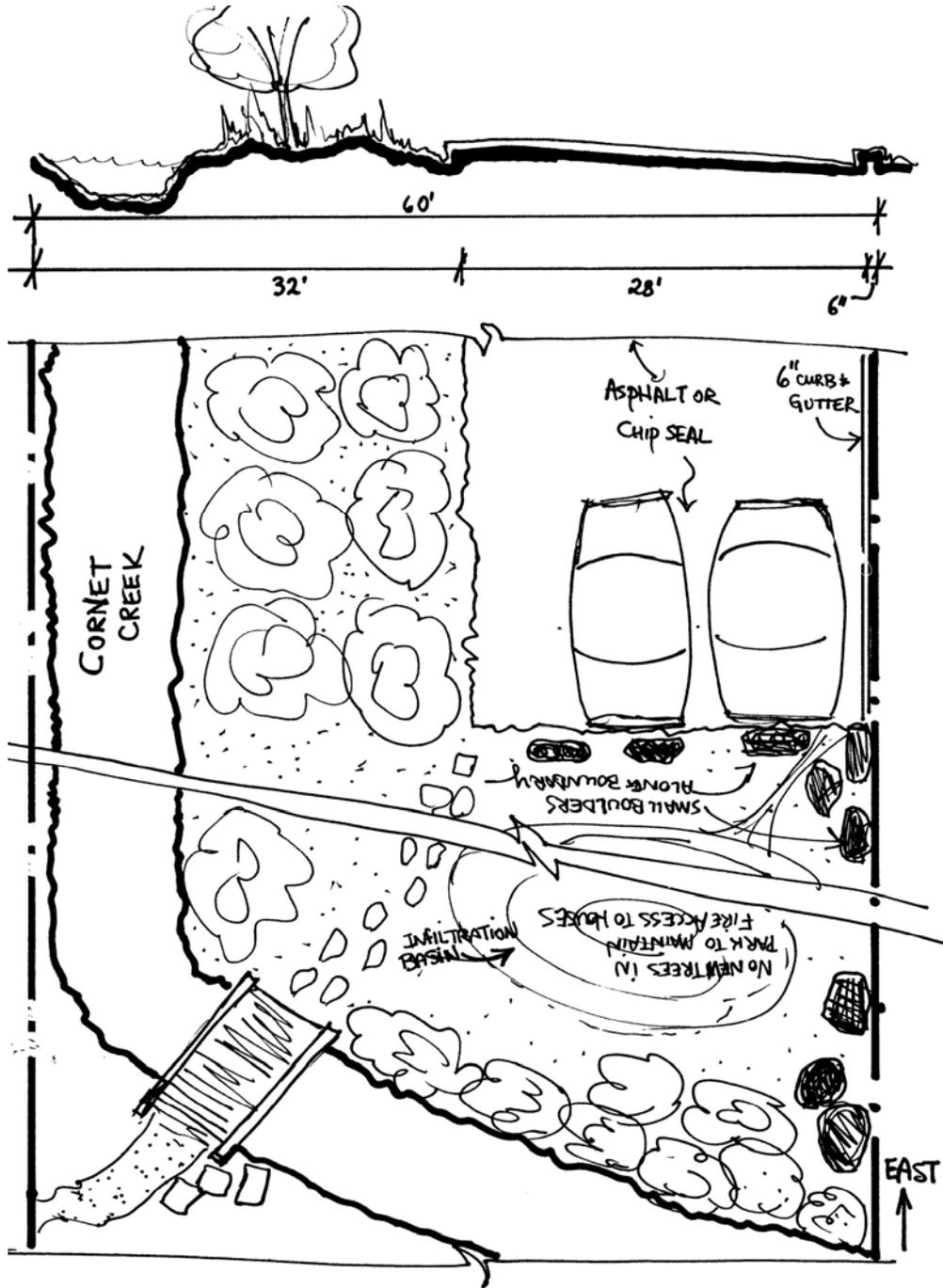
Over Cornet Creek.

1. Design and install a pedestrian bridge over Cornet Creek to facilitate safe pedestrian access to both sections of the block.

Refer to Streetscape West Telluride District, 2006, Engineered Street Designs and adjacent street profiles for additional information.



WEST GALENA AVENUE - 500 BLOCK, WEST SECTION
(DAN'S STREET TO CORNET CREEK)



WEST GALENA AVENUE - 500 BLOCK, EAST SECTION

(CORNET CREEK TO TOWNSEND STREET)

WEST GALENA AVENUE – 400 BLOCK

60' Right of Way

*Neighborhood Meetings: July 9, 1996; June 18, 1997; March 31, 1999
September 29, 1999; November 3, 1999*

BACKGROUND

Existing Street Condition. The block is in excellent condition. Public Works oversaw asphalt surfacing, concrete curb and gutter, and concrete sidewalk improvements in the summer and fall of 2000, in accordance with Streetscape Design Standards. The travel way slopes west with a crown offset primarily north of center. Two sections of sidewalk are missing on the north. A fall 1999 survey of this area revealed several right of way encroachments that required the Streetscape Task Force to revise its original designs, as follows: allow the curb line on the north to match existing (approximately 12 feet from the north property line). Place the face of curb 8 feet from the south property line maintaining a 40-foot curb-to-curb dimension. Install an 8-foot sidewalk adjacent to the school and a 5-foot sidewalk and 2.5-foot green strip (adjacent to the curb) adjacent to private property on the southeast section of the block.

Environment/Drainage/Snow Storage. This block effectively handles current runoff volumes. Asphalt surfacing and concrete curb and gutter along the north and side sides has minimized adverse impacts to air and water quality caused by sediment. The block is consistent with Telluride's snow storage and drainage plans.

Lighting. There are two pole-mounted light fixtures. The first, at the west end, will be replaced with an updated fixture. The second, on the north side in the center, will be removed to be consistent with the Streetscape Lighting Plan. Proposed lighting appears adequate.

Historic Character. Current improvements neither contribute to nor detract from the area's historic character. There are several historic structures on this block, in addition to the Elementary School.

Interfacing Streets. All interfacing streets appear to function well with this block.

Parking. Parking shall remain on both sides of the street allowing for a 24-foot travel way. Winter parking regulations restrict south side parking for part of the year. On-street parking appears adequate for this low-density neighborhood, except during special functions at the Elementary School.

Pedestrian Concerns. Pedestrian use of this block is heavy due to its proximity to the Elementary School. While existing sidewalks effectively handle the current pedestrian traffic, a continuous south and north sidewalk would optimize pedestrian access. There is no handicap accessibility.

Street Furniture and Public Art. No street furniture or public art currently exists and none is recommended.

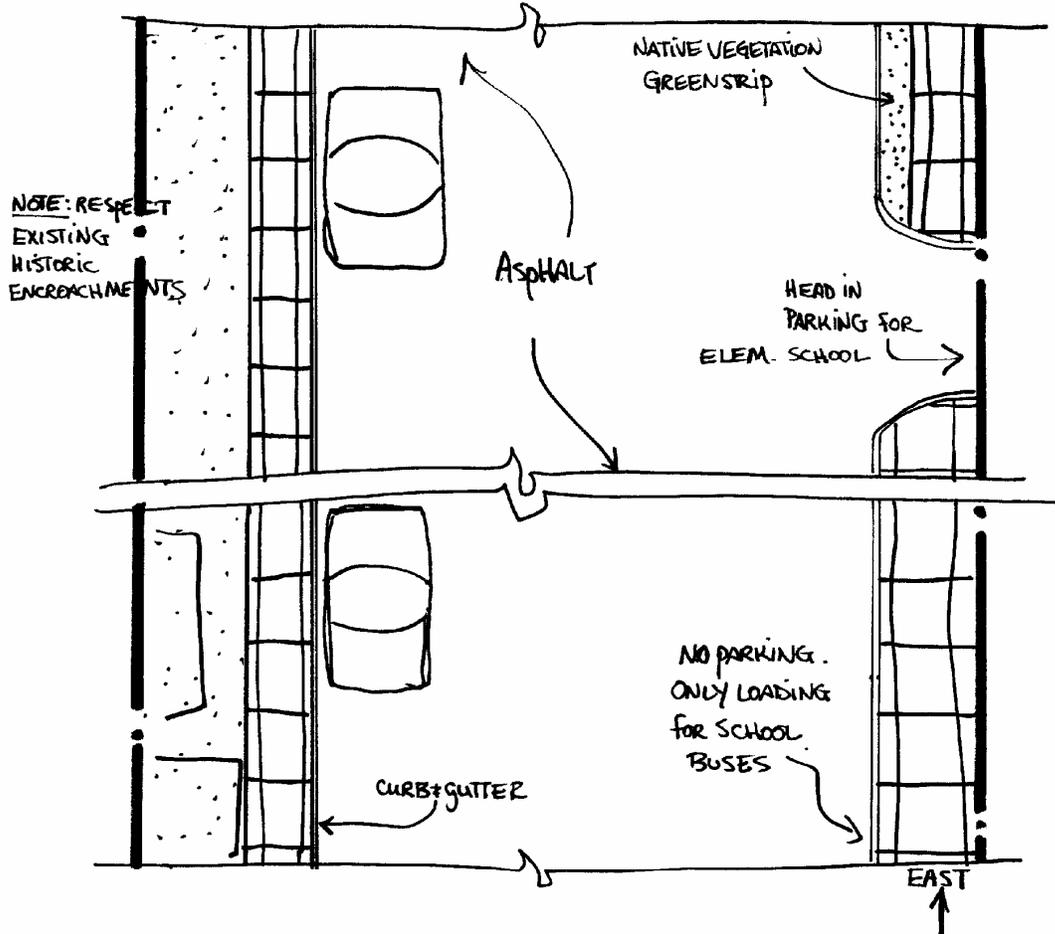
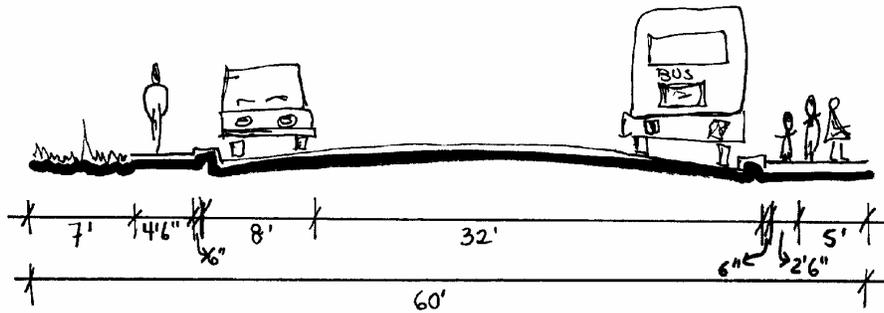
Traffic Circulation. This secondary travel way is heavily used for access to the elementary school and Dakota Avenue. Capacity is adequate for the intensity of use. Emergency vehicle access also appears adequate.

Utilities. There are no utility concerns at this time.

RECOMMENDED IMPROVEMENTS

1. Fronting property owners should construct the missing sections of sidewalk on the north and southwest sides as they develop.
2. Create handicap ramps at intersections.
3. Replace the existing pole-mounted residential light fixture at the west end of the block with an updated fixture.
4. Remove the existing pole-mounted residential light fixture on the north in the center of the block.

Refer to Streetscape Phase II, 2000, Engineered Street Designs and adjacent street profiles for additional information.



WEST GALENA AVENUE - 400 BLOCK
(BETWEEN ASPEN & TOWNSEND STREETS)

WEST GALENA AVENUE – 300 BLOCK

50' Right of Way

Neighborhood Meetings: July 10, 1996; June 18, 1997; March 31, 1999; November 3, 1999

BACKGROUND

Existing Conditions. Surfaced with chip seal, this block slopes mainly east. A small west end section slopes west. The block is in fair condition with improvements along most of the north side, which include a concrete sidewalk, curb and gutter, and landscaping. A sidewalk and curb and gutter are needed in front of 335 West Galena. On the south section, there is space to construct a 6'-wide green strip along the entire length. Incorporating a berm into the south green strip would minimize drainage flow onto the adjacent southeast lot.

NOTE: A 10' by 67.5' section of the northeast portion of this block was vacated to benefit the owner of Lot 1A, Block 3 T.O.T., making the right of way 50' wide along this block. The Task Force realizes that this section will be inconsistent in terms of alignment with the other blocks of Galena Avenue.

Environment/Drainage/Snow Storage. Drainage is mainly to the east and into a concrete drain pan at Oak Street. Minimal drainage flows west into the Aspen Street drain pan. The alley that bisects on the north side is a continual drainage and sediment problem because of an annual spring that originates north of the right of way. Surfacing even a portion of this alley and possibly creating an underground drainageway for the spring should be considered. This block is consistent with Telluride's snow storage and drainage plans.

Historic Character. As it exists, this block neither contributes to nor detracts from the historic character of the area, which is attributed mainly to contributing houses on the northeast and southeast corners and a supporting house on the northwest corner.

Interfacing Streets. All interfacing streets appear to function well with this block. However the alley on the north side is a sediment and drainage problem.

Lighting. The pole-mounted residential light fixture on the northeast and northwest corners should be replaced with a fixture that is consistent with the Streetscape Lighting Plan.

Neighborhood Concerns. There is concern regarding the drainage that flows from the right of way into the lot at the southeast corner of this block, potentially damaging the house. There is also concern about the spring that runs down the north alley.

Parking. There is parallel parking on the north and south sides. Curb cuts provide access to the north and south alleys at the center of the block, a driveway west of the north alley, and a driveway east of the south alley. In winter, the south side stores snow, prohibiting parking. Use of on-street parking is moderate. Existing off-street parking use should be maximized to maintain on-street parking availability.

Pedestrian Concerns. Pedestrian traffic is moderate and includes neighborhood residents traveling to the elementary and high schools. A continuous, safe sidewalk is therefore needed on the north (sunny) side of the street. Although there is no handicap access to this block, there are handicap ramps on the adjacent sidewalks to the north and south on Oak Street and to the south on Aspen Street. Sidewalk access through the green strip on the north may be needed and the grade may necessitate stairs occupying the green strip.

Street Furniture and Public Art. No furniture or public art currently exists and none are recommended.

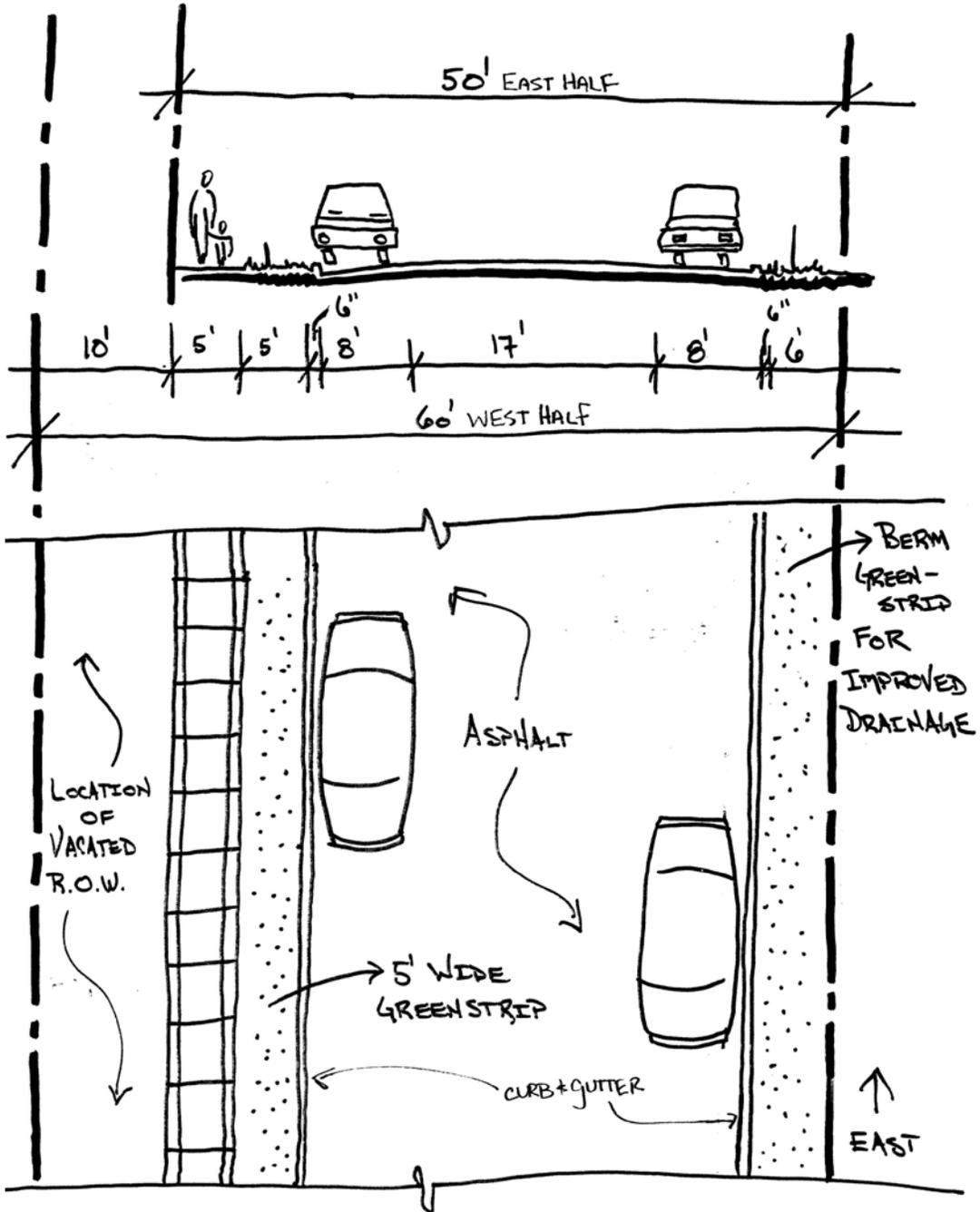
Traffic Circulation. Travel way width is adequate for the medium-to-low traffic volumes on this secondary street. Emergency vehicle access also appears adequate.

Utilities. The five utility poles on this block—four on the north and one on the south—should be undergrounded. If possible, locate electric transformer boxes on private property. If they must be in the right of way, they should be as unobtrusive as possible. Concrete bollards will be required to protect them from vehicle impact.

RECOMMENDED IMPROVEMENTS

1. Construct a 5'-wide sidewalk, a 5'-wide green strip, and a traditional curb and gutter on the north side of the street, west of the alley, in alignment with the existing sidewalk east of the alley on the north, and using the same scoring pattern.
2. Construct a 6'-wide green strip on the south side, with a berm minimizing drainage flowing to the lot on the southeast portion of the block. A drain pan should be installed on the south, if necessary.
3. Surface the north side alley with tined concrete due to the steep slope and manage the spring water.
4. Surface the block with asphalt.
5. Replace existing pole-mounted light fixtures with updated fixtures.
6. Bury all utilities.

Refer to Streetscape West Telluride District, 2006, Engineered Street Designs and adjacent street profiles for additional information.



WEST GALENA AVENUE - 300 BLOCK
(BETWEEN ASPEN & OAK STREETS)

WEST GALENA AVENUE – 200 BLOCK

60' Right of Way

Neighborhood Meetings: July 10, 1996; January 14, 1998; March 31, 1999; January, 19, 2000

BACKGROUND

Existing Conditions. Surfaced with chip seal, this block slopes east. Two drain pans direct runoff to the south, one at the west end at Oak Street; the other at the east end at Fir Street. A drain pan directs runoff into this block from the north alley, as well. The alley is paved. There is no sidewalk on the south side of the street. On the north side there is a 6'-wide sidewalk with a 4.5'-wide green strip that separates the walk from the travel way. There is minimal landscaping along the south side bordering the travel way.

Environment/Drainage/Snow Storage. The drain pan at the intersection of this block and Fir Street is laden with residual chip seal material, which can adversely impact water quality if this material is entrained and transported to local waterways. Drainage is carried efficiently to the east into the existing drainage swale where Galena Avenue intersects Fir Street. The block is consistent with Telluride's snow storage and drainage plans.

Historic Character. Currently, this block neither contributes to nor detracts from the historic character of the area, which is attributed mainly to two contributing structures and one structure listed as non-contributing with qualifications.

Interfacing Streets. All interfacing streets appear to function well with this block.

Lighting. The one pole-mounted light fixture at the northeast corner should be replaced with a fixture that is consistent with the Streetscape Lighting Plan.

Parking. Parallel parking is permitted on both sides of this block, except in winter when the south side is used for snow storage. The only curb cuts that exist access alleys to the north and south. Parking demand is moderate, consisting mainly of neighborhood residents. Maximizing use of existing off-street parking is essential for maintaining on-street parking availability.

Pedestrian Concerns. A sidewalk is needed to safely accommodate the moderate pedestrian traffic on this block, which is confined mainly to neighborhood residents. There is no handicap access. A new sidewalk should address this. There is handicap access to the sidewalk headed south at the Oak Street intersection.

Street Furniture and Public Art. No street furniture or amenities current exist and none are required.

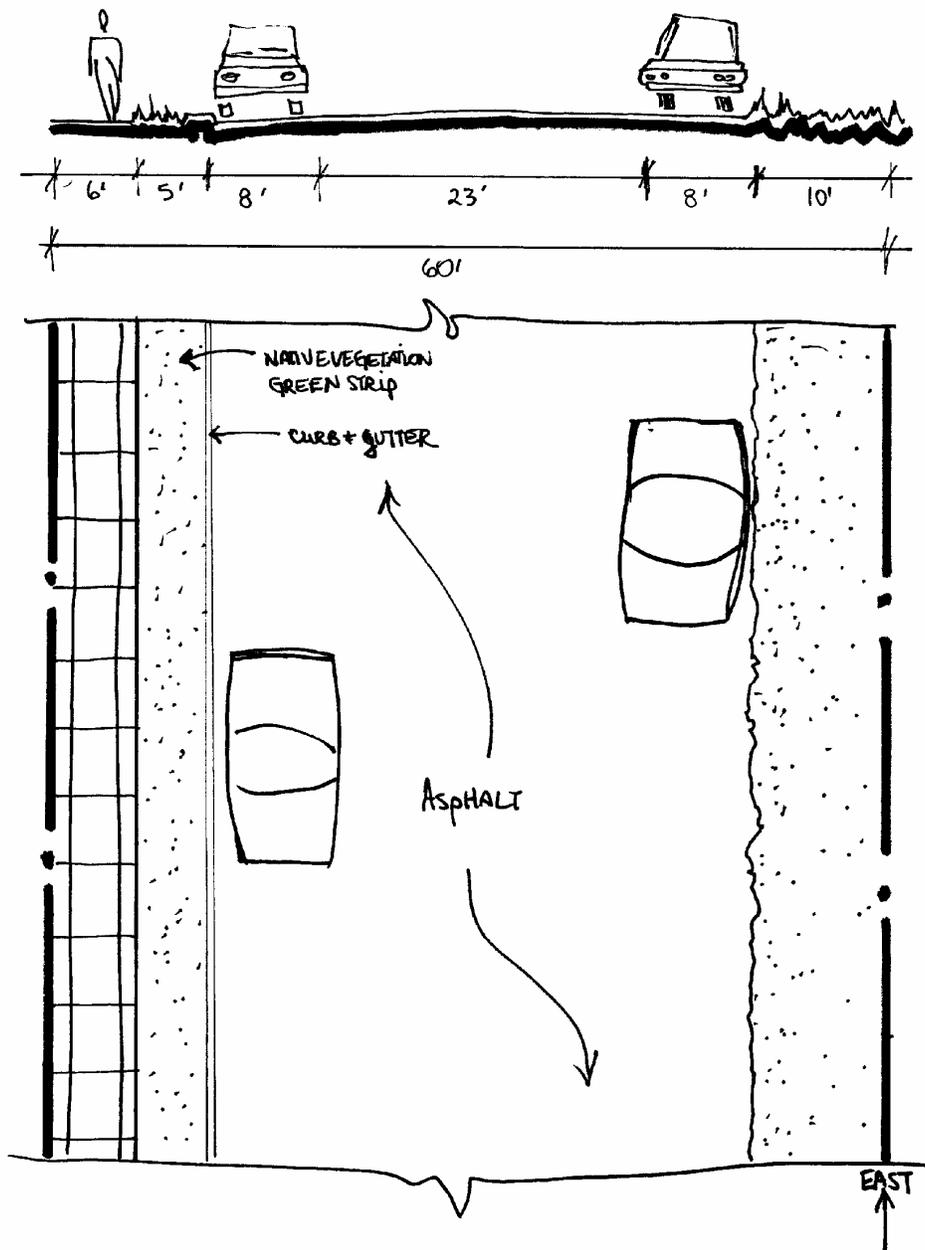
Traffic Circulation. Travel way width is adequate for the moderate traffic intensity on this secondary street and emergency vehicle access.

Utilities. Minimize or eliminate above ground utilities. If possible, locate electric transformer boxes on private property. If they must be in the right of way, they should be as unobtrusive as possible. Concrete bollards will be required to protect them from vehicle impact.

RECOMMENDED IMPROVEMENTS

None improvements are recommended at this time.

Refer to Streetscape West Telluride District, 2006, Engineered Street Designs and adjacent street profiles for additional information.



WEST GALENA AVENUE - Zoo Block
(BETWEEN OAK & FIR STREETS)

WEST GALENA AVENUE – 100 BLOCK

60' Right of Way

Neighborhood Meetings: July 10, 2006; January 14, 1998; March 31, 1999

BACKGROUND

Existing Conditions. Surfaced with chip seal, this block slopes to the east. At the east end, runoff travels south down Pine Street. There is no drain pan or swale directing runoff at the west end. The sidewalk and curb and gutter along the south, east of the alley, are in good condition. West of the alley, on the south, there is no sidewalk. There is a sidewalk along the north side of the block; however, a guardrail blocks the sidewalk at the driveway for 135 West Galena, forcing pedestrians into the travel way. There is minimal landscaping along either side of the block. Some mature vegetation is in the right of way in front of lots A and B, Block 25.

Environment/Drainage/Snow Storage. The drain pans along this block are laden with residual chip seal material, which can adversely impact water quality if this material is entrained and transported to local waterways. The alley on the north side is paved and takes carries drainage from Gregory Street. Drainage is generally inefficient and should be improved in conjunction with that on Pine Street at the intersection of these two streets. In its current condition, this block is consistent with Telluride's snow storage and drainage plans.

Historic Character. Currently this block neither contributes to nor detracts from the historic character of the area, which is attributed mainly to a landmark structure at the southeast corner of the block, and two other supporting structures.

Interfacing Streets. Drainage at the Pine Street and Galena Avenue intersection requires more aggressive management.

Lighting. The one pole-mounted light fixture at the northeast corner should be replaced with a fixture that is consistent with the Streetscape Lighting Plan.

Parking. There is parallel parking on both sides of this block, which is limited by two curb cuts and alley entrances on the north and south. In winter, snow is stored on the south and parking is prohibited there. On-street parking demand is moderate, consisting mainly of neighborhood residents, although there is some parking pressure from nearby businesses. Maximizing use of existing off-street parking is essential for maintaining on-street parking availability.

Pedestrian Concerns. A safe sidewalk is needed to accommodate the moderate neighborhood pedestrian traffic on this block and to be consistent with the blocks east and west. There is no handicap access. The guardrail that blocks the sidewalk on the north side is problematic. The severe grade at the southeast corner has required a set of stairs to connect the sidewalk to Pine Street. There may also be a need for sidewalk access through the green strip on the south, and the grade may necessitate stairs occupying the green strip.

Street Furniture and Public Art. There is currently no street furniture or public art and none is required.

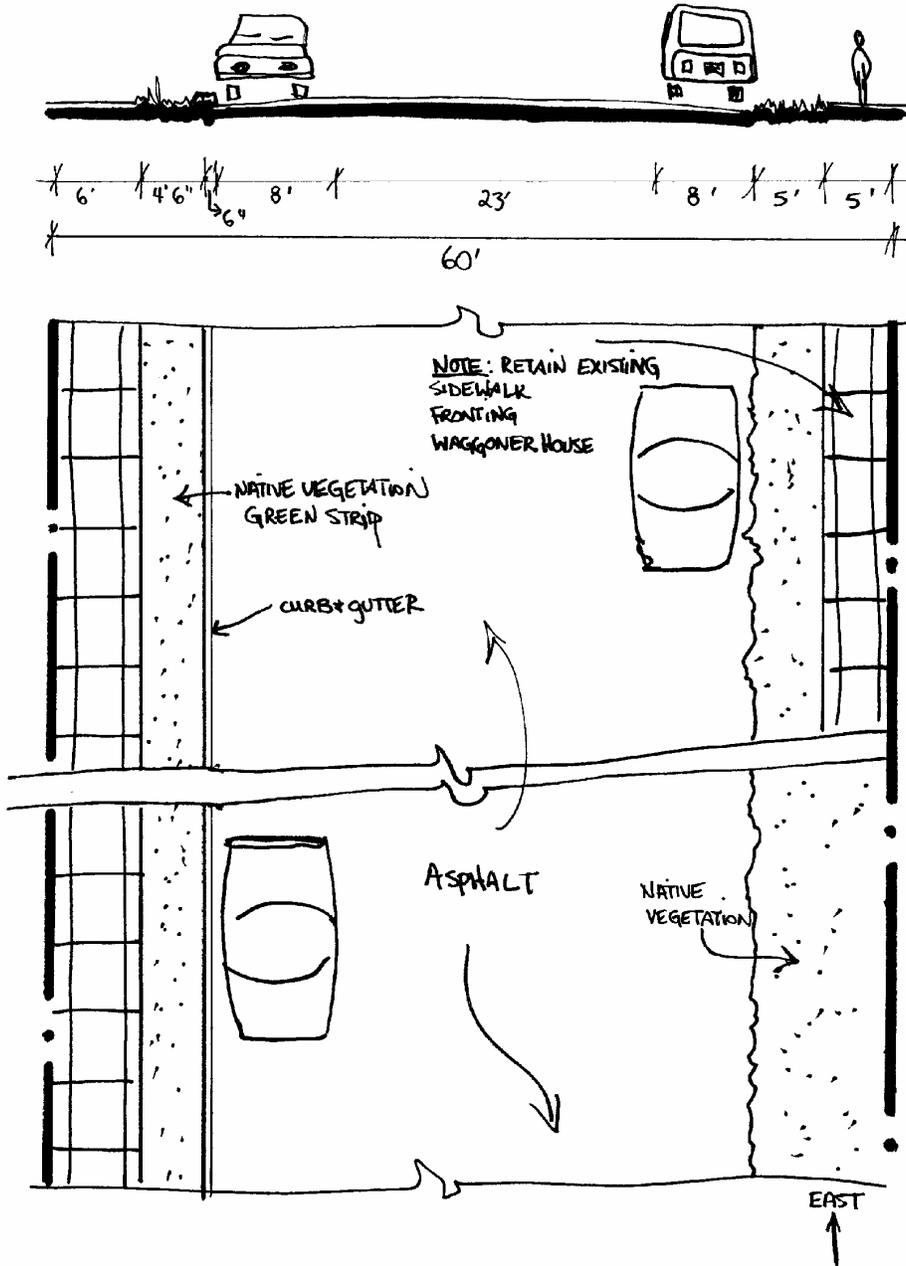
Traffic Circulation. Capacity is adequate for the low traffic intensity on this secondary street. Emergency vehicle access also appears adequate.

Utilities. It is desirable to minimize or eliminate above ground utilities. If possible, locate electric transformer boxes on private property. If they must be in the right of way, they should be as unobtrusive as possible. Concrete bollards will be required to protect them from vehicle impact.

RECOMMENDED IMPROVEMENTS

1. Trim shrub on northeast side of street where it blocks the sidewalk.
2. Surface travel way with asphalt.
3. Replace the existing pole-mounted light fixture with an updated fixture.
4. Establish a green strip of native vegetation 10'-wide along the south side, except at the east end where the existing sidewalk shall remain.

Refer to Streetscape West Telluride District, 2006, Engineered Street Designs and adjacent street profiles for additional information.



WEST GALENA AVENUE - 100 BLOCK
(BETWEEN FIR & PINE STREETS)

EAST GALENA AVENUE – 100 BLOCK

60' Right of Way

Neighborhood Meetings: July 10, 1996; January 14, 1998; March 31, 1999

BACKGROUND

Existing Conditions. This block, which slopes east, is paved with asphalt and has curb and gutter and a 5.5'-wide concrete sidewalk along the north side. These improvements are in excellent condition. There is no green strip on the north side, but along the south side there is a 10'-wide green strip that is in good condition. The edge of asphalt is not defined along the south side. At the east end of this block, there is a diagonal 3'-wide concrete drain pan that carries runoff from the north side onto the west side of Spruce Street.

Environment/Drainage/Snow Storage. This block appears to effectively handle runoff with no negative impacts to air and water quality. It is consistent with Telluride's snow storage and drainage plans.

Historic Character. As it exists, this block neither detracts from nor contributes to the historic character of the area, which is attributed mainly to a landmark structure on the northeast corner—St. Patrick's Church, a contributing structure on the southeast corner, and various other designated structures.

Interfacing Streets. All interfacing streets appear to function well with this block.

Lighting. There is currently no street lighting on this block, which is consistent with the Streetscape Lighting Plan.

Parking. Currently there is parallel parking on both sides of this block, which is limited by wide curb cuts. In winter, the south side is used to store snow and parking is prohibited. Intensity of on-street parking is moderate and consists mainly of neighborhood residents, though there is some parking pressure from St. Patrick's Church. Maximizing use of existing off-street parking in this vicinity will help to maintain on street parking availability. A wooden curb along the south side may prevent vehicles from driving/parking on the green strip.

Pedestrian Concerns. The north sidewalk and its associated handicap ramps safely accommodate the moderate neighborhood pedestrian traffic on this block.

Street Furniture and Public Art. While there is no street furniture or amenities, St. Patrick's Church on the northeast corner may warrant a bench or two.

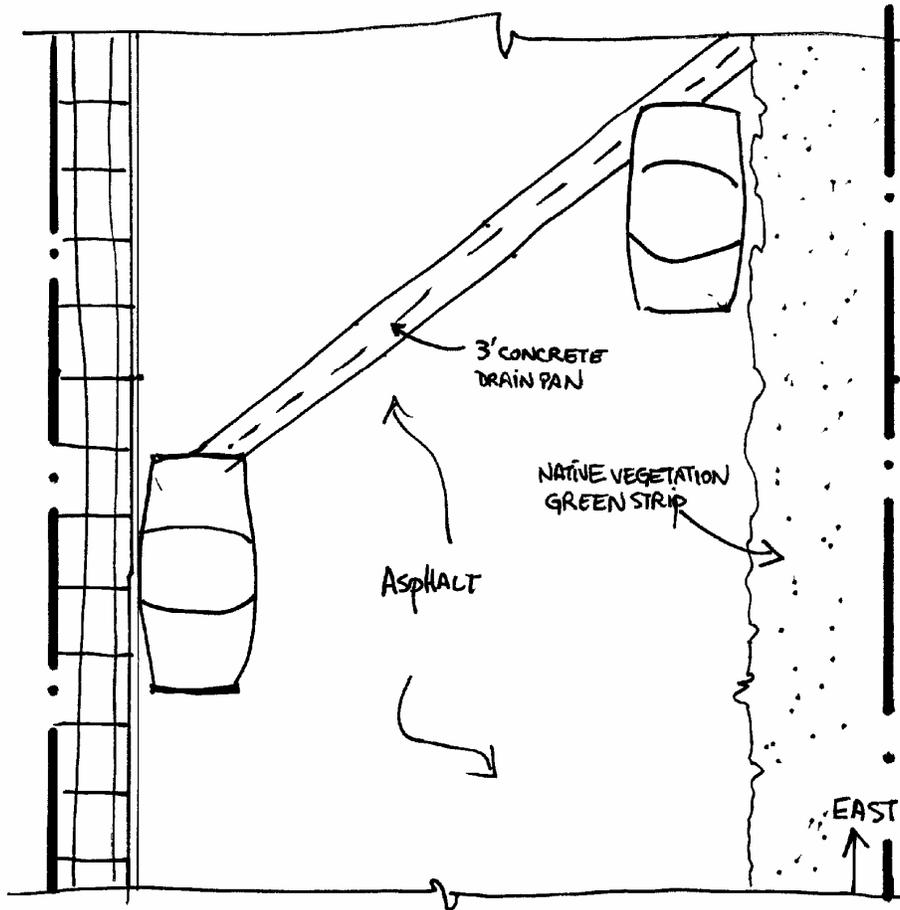
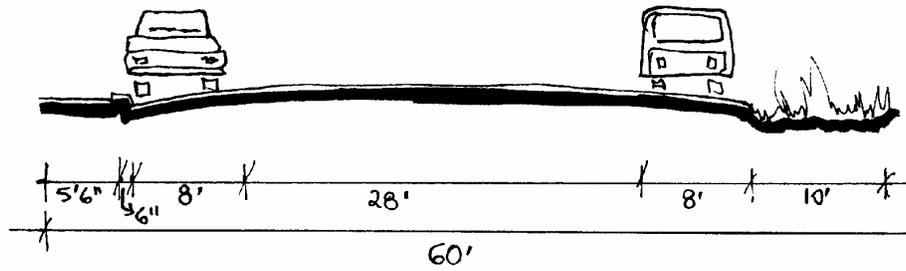
Traffic Circulation. This secondary street has low traffic intensity. Capacity appears adequate for intensity of use and emergency vehicle access.

Utilities. Currently, there are no utility concerns for this block.

RECOMMENDED IMPROVEMENTS

- 1 Possibly install a wooden curb on the south side of the travel way to prevent people from driving or parking on the grass.

Refer to Streetscape West Telluride District, 2006, Engineered Street Designs and adjacent street profiles for additional information.



EAST GALENA AVENUE - 100 BLOCK
(BETWEEN PINE & SPRUCE STREETS)

EAST GALENA AVENUE – 200 BLOCK

60' Right of Way

Neighborhood Meetings: July 10, 1996; January 14, 1998; March 31, 1999

BACKGROUND

Existing Conditions. Sloping fairly steeply to the west, this asphalted travel way is in poor condition. North of the travel is a narrow, poorly defined green strip in poor condition with two large electric transformer boxes. There are four curb cuts east of the alley on the north side. One is not deep enough to accommodate a parked vehicle without encroaching into the right of way. On the south there is a wide green strip that slopes steeply south. An alley, running diagonally from the southeast, intersects with the southeast corner of North Spruce Street and East Galena Avenue. This alley originates at Columbia Avenue and runs north-to-south between Spruce and Willow streets. As it approaches Galena, the steep grade prevents it from intersecting Galena at the center of the block. Therefore, it turns west and terminates at the Spruce-Galena intersection. There are no sidewalks.

Environment/Drainage/Snow Storage. There is a drain pan, directing flow south at the west end of the block at the Spruce Street intersection. There is also a drain pan that flows south at the east end of the block where it intersects with Willow Street. These provide adequate north-south drainage. A defined vegetated drainage swale should connect north v-pans. Snow is stored along the south side. Currently this block is consistent with Telluride's snow storage and drainage plans.

Historic Character. As it exists, this block contributes to the "rough and natural" historic character of town. There are two Supporting structures and three Non-Contributing without Qualifications structures on this block.

Interfacing Streets. All interfacing streets appear to function well with this block.

Lighting. The one pole-mounted residential light fixture at the northwest corner of this block should be updated with a fixture that is consistent with the Streetscape Lighting Plan.

Neighborhood Concerns. There is a high demand for parking on this block.

Parking. Currently year-round parallel parking is permitted on the north side. There is very little space (width) for any parking on the south. Due to curb cuts and an intersecting alley on the north, on-street parking is minimal. The curb cuts are shallow and parking in these areas encroaches into the right of way. In winter, the south side stores snow and parking is prohibited. There is space for diagonal head in parking south of the south alley where it intersects Spruce Street and Galena Avenue. This space often contains two vehicles. Parking is at a premium and is difficult due to the steep grade. Neighborhood residents, specifically residents north of this block, have difficulty parking on their blocks due to steep grades and dangerous winter conditions. This block and the 300 Block of East Galena absorb much of the parking spillover from other block, thereby increasing parking demand.

Pedestrian Concerns. Pedestrian traffic is light on this block, confined mainly to a few local residents. Due to low vehicular and pedestrian traffic intensity there is no need for a sidewalk. There is handicap access, but it is difficult to manage.

Street Furniture and Public Art. There is no street furniture or amenities and none are recommended.

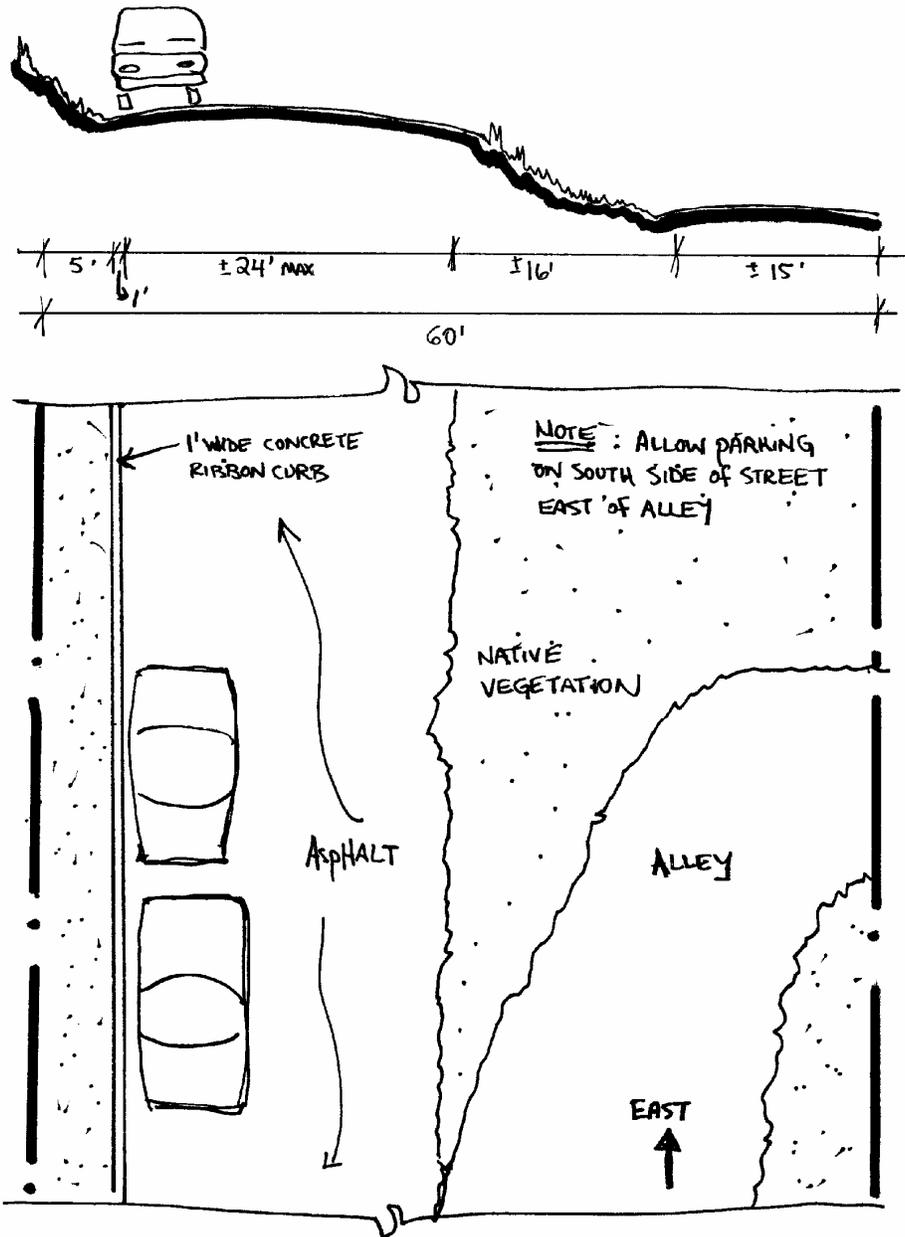
Traffic Circulation. Capacity appears adequate for intensity of use on this secondary street. Due to the steep grade, the block may pose difficulties for emergency vehicle access during heavy snow.

Utilities. If possible, locate electric transformer boxes on private property. If they must be in the right of way, they should be as unobtrusive as possible. Concrete bollards will be required to protect them from vehicle impact.

RECOMMENDED IMPROVEMENTS

1. Re-pave street surface with asphalt.
2. Install a ribbon curb on the north side.
3. Improve the north green strip by widening it to 5 feet, planting native vegetation, and defining the traveled way with the flush (ribbon) curb.
4. During summer, allow parallel parking on the south side, east of where the south alley would intersect the block.
5. Update existing pole-mounted light fixture with a fixture that is consistent with the Streetscape Lighting Plan.

Refer to Streetscape West Telluride District, 2006, Engineered Street Designs and adjacent street profiles for additional information.



EAST GALENA AVENUE - 200 BLOCK
(BETWEEN SPRUCE + WILLOW STREETS)

EAST GALENA AVENUE – 300 BLOCK

60' Right of Way

Neighborhood Meetings: July 10, 1996; January 14, 1998

BACKGROUND

Existing Conditions. The task force determined that “status quo” is appropriate for this block. Approximately the western three-quarters of this block slopes west toward Willow Street, while the eastern quarter slopes east toward Alder Street. The block is surfaced with chip seal and is in fair condition. There are no curbs, gutters, or sidewalks; although 3'-wide drain pans carry runoff south at the east and west intersections. There is an unimproved alley right of way on the north side. Landscaping is minimal and is mainly native vegetation on both the north and south. There is a large transformer box in the north green strip. Two curb cuts within the right of way on the south near the center allow for head in parking.

Environment/Drainage/Snow Storage. There are no formal drain pans or curbs along the length of this block, but current drainage appears adequate. There is a drain pan at the west end of the block, at the Willow Street intersection, and the east end of the block at the Alder Street intersection. Both drain pans direct runoff south. Due to light traffic the chip seal surface does not appear to negatively affect air quality. Snow is stored on the south and is commonly pushed down the steep green strip. Currently this block is consistent with Telluride's snow storage and drainage plans.

Historic Character. This block contributes to the “rough and natural” historic character of town. There is one Contributing structure, one Non-Contributing without Qualifications house, and two Non-Contributing without Qualification sheds on this block. One of the sheds is near the north edge of the right of way. Any alternation to the right of way should not negatively affect the historic resources on this block.

Interfacing Streets. All interfacing streets appear to function well with this block.

Lighting. There is one pole-mounted residential light fixture at the northwest corner that should be updated with a fixture that is consistent with the Streetscape Task Plan.

Neighborhood Concerns. There is a high demand for parking on this block.

Parking. Parallel parking is allowed on the north side. Head in parking on the south near the center of the block is within the right of way and is used by neighborhood residents, as needed, depending on the condition of Willow Street and other areas difficult to access in snow. In winter the south side is used for snow storage and parking is prohibited, except for the head in parking mentioned above. Neighborhood residents, specifically residents north of this block, have difficulty parking on their blocks due to steep grades and dangerous winter conditions. This block and the 200 Block of East Galena take much of this parking pressure, thereby increasing parking demand. The current parking configuration is inadequate for the demand.

Pedestrian Concerns. Pedestrian traffic is light because it is confined mainly to local residents. Due to low vehicular and pedestrian traffic intensity, there is no need for a sidewalk. There is no handicap access.

Street Furniture and Public Art. There currently is no street furniture or amenities. None are required.

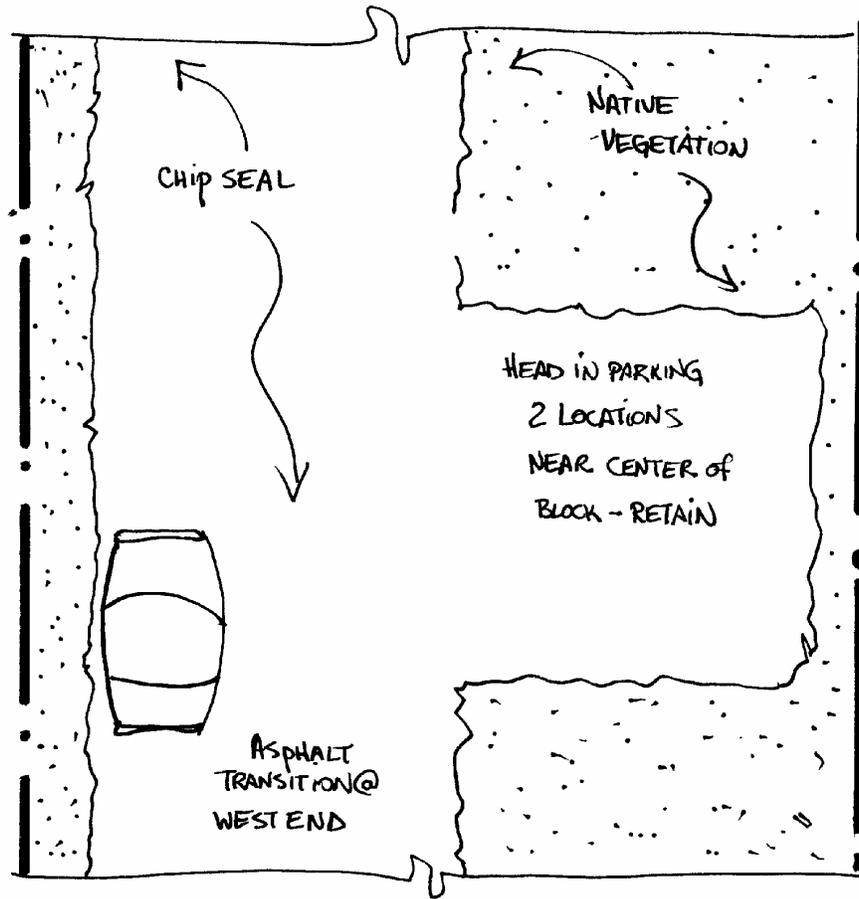
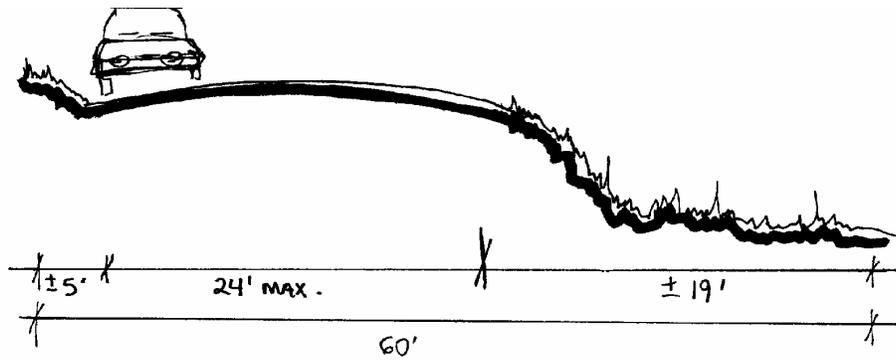
Traffic Circulation. Capacity appears adequate for handling the low traffic volumes along this secondary street. Emergency vehicle access also appears adequate.

Utilities. If possible, locate electric transformer boxes on private property. If they must be in the right of way, they should be as unobtrusive as possible. Concrete bollards will be required to protect them from vehicle impact.

RECOMMENDED IMPROVEMENTS

1. Surface the west end of the block with asphalt to provide for a transition at this steep section, preventing cars from spinning out and depositing chip seal surfacing into the drain pan below.
2. Explore the possibility of formalizing the existing head-in parking to maximize efficiency, or determine whether on-site parking is possible upon redevelopment.
3. Update existing pole-mounted light fixture with a fixture that is consistent with the Streetscape Lighting Plan.

Refer to Streetscape West Telluride District, 2006, Engineered Street Designs and adjacent street profiles for additional information.



EAST GALENA AVENUE - 300 BLOCK
(BETWEEN WILLOW & ALDER STREETS)

WEST GREGORY AVENUE - 100 & 200 BLOCKS

*25' Right of Way
Neighborhood Meetings: July 10, 1996*

BACKGROUND

Existing Conditions. These blocks were improved as part of Streetscape Phase I 1998 construction. The West 200 Block slopes eastward and is paved with chip seal. The West 100 Block slopes westward and is paved with asphalt. Both blocks are bordered along their north side with a 3'-wide concrete drain pan, and along the south with a 4'-wide green strip. Those intending to go to or leave from the Historical Museum, which is at the top of North Fir Street, use these blocks regularly.

Environment/Drainage/Snow Storage. These blocks are designed carry runoff along the north side in the 3'-wide drain pan toward North Fir Street, where it is drained south. Regular motor vehicle traffic compacts snowfall, which creates an icing that can be hazardous for vehicles and pedestrians and inhibits winter drainage. On occasion, gravel is placed on the travel way, which facilitates winter travel but creates difficulties throughout the rest of the year for pedestrians and vehicles maneuvering on the steep slopes. The gravel is also a source of sediment to the drainage system. Currently these blocks are consistent with Telluride's snow storage and drainage plans. Management should be reviewed periodically.

Historic Character. These blocks contribute to the "rough and natural" historic character of town. There are several rated structures, including the Historical Museum. Any alterations to the right of way should not negatively affect these historic resources.

Interfacing Streets. All interfacing streets appear to function well with these blocks; although the interfacing streets connect at a steep grade.

Lighting. No residential lighting is recommended.

Neighborhood Concerns. There is a high demand for parking throughout the year. Icing is very bad in winter.

Parking. Parallel parking is permitted along these blocks in summer, due to extremely high demand. However, the narrow right of way and even narrower travel way, when combined with parking, tend to make these one-at-a-time roads. Winter parking is not permitted due to the hazardous, icy conditions. The current parking configuration should be maintained, use of existing off-street parking should be maximized, and creation of off-street parking should be an important priority.

Pedestrian Concerns. Pedestrian traffic is light to moderate. It is made up of local residents, as well as Historical Museum visitors. Due to low vehicular and pedestrian traffic intensity, there is no need for a sidewalk, however steep grades and winter icing can make these blocks treacherous. As there are no sidewalks, there is no handicap access.

Street Furniture and Public Art. There currently is no room for street furniture or amenities. None are recommended.

Traffic Circulation. Capacity appears adequate for handling the low traffic volumes along these secondary blocks. Visitors to the Historical Museum impact the volume of traffic on these blocks. Emergency vehicle access also appears adequate; although, steep grades and icing may make winter access difficult.

Utilities. If possible, locate electric transformer boxes on private property. If they must be in the right of way, they should be as unobtrusive as possible. Concrete bollards will be required to protect them from vehicle impact.

RECOMMENDED IMPROVEMENTS

1. Regularly revisit winter and spring maintenance protocols for these blocks.

Refer to Streetscape Phase I, 1998, Engineered Street Designs and adjacent street profiles for additional information.

EAST GREGORY AVENUE – 100 BLOCK

25' Right of Way

Neighborhood Meetings: July 10, 1996

BACKGROUND

Existing Conditions. This block was paved with asphalt as part of Streetscape Phase I 1997 Construction. Sloping lightly west, it is bordered along the north side with a 3'-wide concrete drain pan. There is no formal treatment along the south side.

Environment/Drainage/Snow Storage. This block is designed to sheet flow to the north. The north side drain pan is intended to capture runoff from the hillside to its north and east. Runoff is carried east to west and then drained south at Pine Street. Regular motor vehicle traffic compacts snowfall, which creates an icing problem that creates a hazard for vehicles and pedestrians and inhibits winter drainage. On occasion, gravel is placed on the travel way, which facilitates winter travel but creates difficulties throughout the rest of the year for pedestrians and vehicles maneuvering on the steep slope. The gravel is also a source of sediment to the drainage system. Currently this block is consistent with Telluride's snow storage and drainage plans. Management should be reviewed periodically.

Historic Character. This block contributes to the "rough and natural" historic character of town. There are several rated structures. Any alterations to the right of way should not negatively affect these historic resources.

Interfacing Streets. All interfacing streets appear to function well with these blocks; although the intersections are very steep.

Lighting. No residential lighting is proposed for this block.

Neighborhood Concerns. There is a high demand for parking throughout the year. Icing is very bad in winter.

Parking. On-street parking is only permitted on this block in summer. Use of existing off-street parking should be maximized, and creation of off-street parking should be an important priority.

Pedestrian Concerns. Pedestrian traffic is light because it is confined mainly to local residents. Due to low vehicular and pedestrian traffic intensity, there is no need for a sidewalk, however winter icing can make this block treacherous. As there is no sidewalk, there is no handicap access.

Street Furniture and Public Art. There currently is no room for street furniture or amenities. None are recommended.

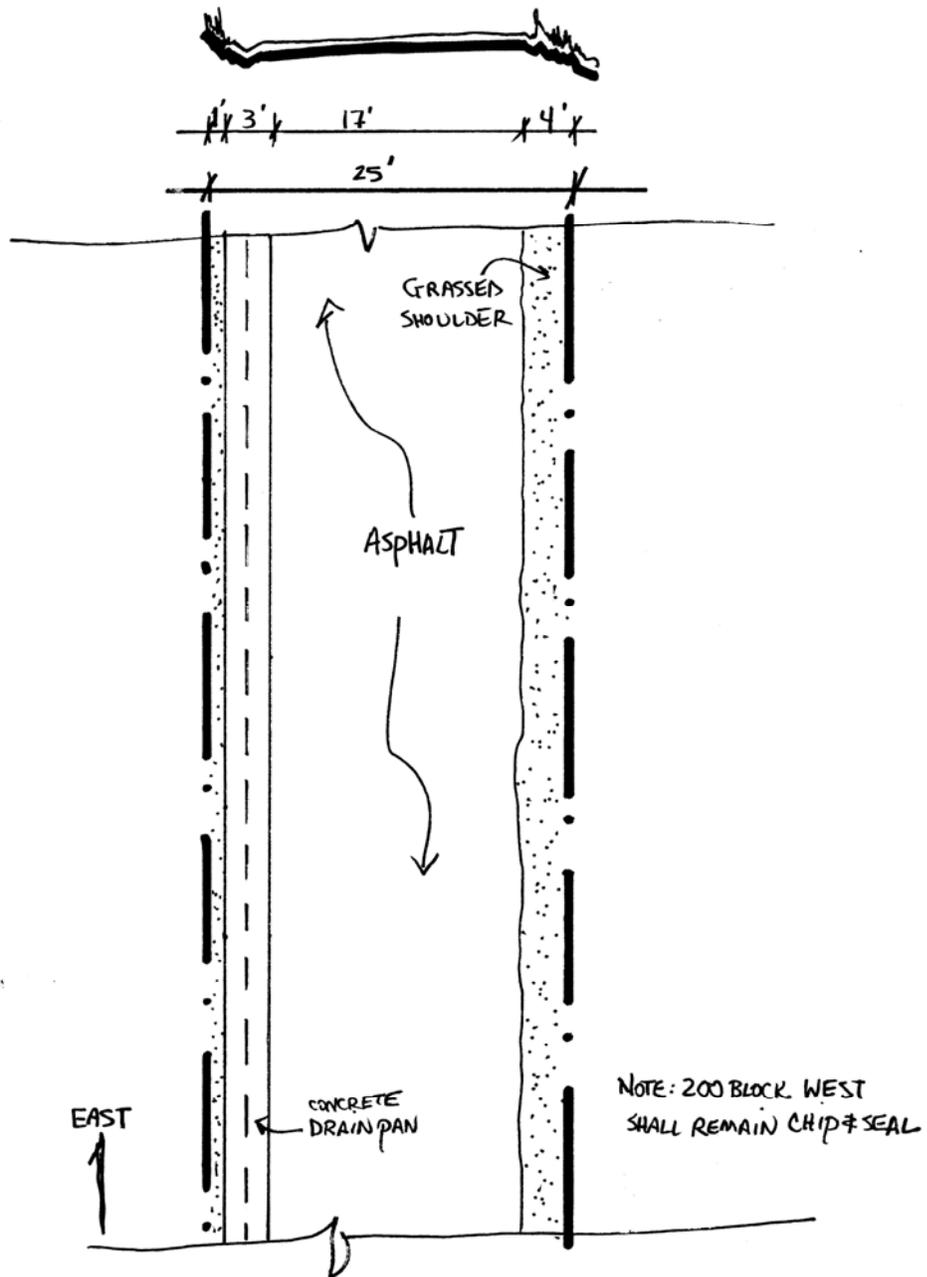
Traffic Circulation. Capacity appears adequate for handling the low traffic volumes along this secondary block. Emergency vehicle access also appears adequate; although, icing may make winter access difficult.

Utilities. If possible, locate electric transformer boxes on private property. If they must be in the right of way, they should be as unobtrusive as possible. Concrete bollards will be required to protect them from vehicle impact.

RECOMMENDED IMPROVEMENTS

1. Regularly revisit winter and spring maintenance protocols for this block.
2. Remove accumulated gravel regularly to decrease spring and summer difficulties.

Refer to Streetscape Phase I, 1997, Engineered Street Designs and adjacent street profiles for additional information.



GREGORY AVENUE - 100 & 200 BLOCKS WEST & 100 BLOCK EAST
(BETWEEN OAK & SPRUCE STREETS)

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EAST GREGORY AVENUE – 200 BLOCK

15' Right of Way

Neighborhood Meetings: July 10, 1996

BACKGROUND

Existing Conditions. This block was paved with tined concrete as part of Streetscape Phase I 1997 construction. Sloping very steeply to the west, it is bordered along the north side with a 3'-wide concrete drain pan. Right of way width narrows to 15' on this block; travel way width varies from 10' on the west portion to 15' as it nears the intersection with North Willow Street. In some places on the south side, road improvements encroach onto private property.

Environment/Drainage/Snow Storage. This block is designed to sheet flow to the north. Any shading of the right of way results in significant icing and a very slick travel way. Runoff is taken south at North Spruce Street. Regular motor vehicle traffic compacts snowfall, which creates an icing problem that creates a hazard for vehicles and pedestrians and inhibits winter drainage. On occasion, gravel is placed on the travel way, which facilitates winter travel but creates difficulties throughout the rest of the year for pedestrians and vehicles maneuvering on the steep slope. The gravel is also a source of sediment to the drainage system. Currently this block is consistent with Telluride's snow storage and drainage plans. Management should be reviewed periodically.

Historic Character. This block contributes to the "rough and natural" historic character of town. There are several rated structures. Any alterations to the right of way should not negatively affect these historic resources.

Interfacing Streets. All interfacing streets appear to function well with these blocks; although the intersection at North Willow Street and the alley west of Willow are extremely steep.

Lighting. No residential lighting is proposed for this block.

Neighborhood Concerns. There is a high demand for parking throughout the year. Icing is very bad in winter.

Parking. Parallel parking is permitted along this block in summer due to the extremely high demand. However, the narrow right of way and even narrower travel way, when combined with parking, tend to make this a one-at-a-time road. Winter parking is discouraged due to the hazardous, icy conditions. The current parking configuration should be maintained, use of existing off-street parking should be maximized, and creation of off-street parking should be an important priority.

Pedestrian Concerns. Pedestrian traffic is light because it is confined mainly to local residents. Due to low vehicular and pedestrian traffic intensity, there is no need for a sidewalk, however steep grades and winter icing can make this block treacherous. As there is no sidewalk, there is no handicap access.

Street Furniture and Public Art. There currently is no room for street furniture or amenities. None are recommended.

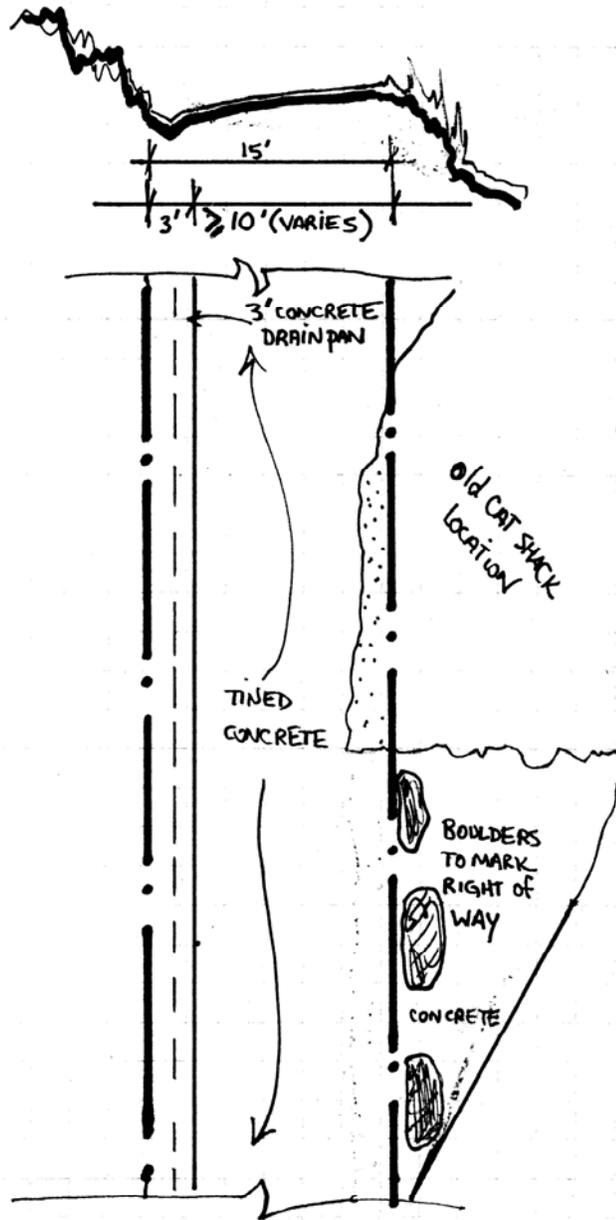
Traffic Circulation. Capacity appears adequate for handling the low traffic volumes along this secondary block. Emergency vehicle access also appears adequate; although, steep grades and icing may make winter access difficult.

Utilities. If possible, locate electric transformer boxes on private property. If they must be in the right of way, they should be as unobtrusive as possible. Concrete bollards will be required to protect them from vehicle impact.

RECOMMENDED IMPROVEMENTS

1. Regularly revisit winter and spring maintenance protocols for this block.
2. Remove accumulated gravel regularly to decrease spring and summer difficulties.

Refer to Streetscape Phase I, 1997, Engineered Street Designs and adjacent street profiles for additional information.



EAST GREGORY AVENUE - 200 BLOCK

(BETWEEN SPRUCE & WILLOW STREETS)

NORTH LAUREL STREET – 100 BLOCK

50' Right of Way

Neighborhood Meetings: July 15, 1996

BACKGROUND

Existing Street Conditions. This short block extends from Colorado Avenue to Shadow Lane, where the travel way terminates. The Laurel Street right of way extends further north, but largely remains undeveloped as a neighborhood pocket park with a footpath to connect Columbia Avenue. To the north of Shadow Lane is a dead-end section of pavement about 20' deep that allows 4 vehicles to park. Surfaced with asphalt, both sides of the block contain curb and gutter that directs runoff south into storm drains near Colorado Avenue. This drainage discharges to the willow carr along the north side of East Colorado Avenue. On the east and west sides of the block, there are 4.5'-wide sidewalks along the travel way with 5'-wide green strips behind them. All infrastructure is in good condition.

Environment/Drainage/Snow Storage. The existing drainage system effectively handles runoff from this block. Since this block is paved with chip and seal that is in good condition, the existing street profile does not negatively affect air and water quality. The existing street profile is consistent with Telluride's snow storage and drainage plans.

Historic Character. This block neither contributes to nor detracts from the historic character of the area.

Interfacing Streets. All interfacing streets appear to function well with these blocks.

Lighting. Currently there are two standard commercial light fixtures on this short block. One is located at the southwest corner and should be replaced with a bollard that is consistent with the Streetscape Lighting Plan. The other, at the southeast corner of Laurel Street and Shadow Lane, should remain.

Parking. Parallel parking is allowed on both sides of this block. In winter, parking restrictions exist on the west side to allow for snow storage. Parking also exists at the dead end north of Shadow Lane. There is head-in parking for 4 vehicles at this location. Parking pressure is not substantial on this block and therefore the current parking configuration should be maintained. Existing off-street parking in this vicinity should be maximized.

Pedestrian Concerns. The existing sidewalks appear to adequately handle the light pedestrian traffic, which is attributed mainly to neighborhood residents. There is no special handicap access.

Street Furniture and Public Art. No street furniture or amenities currently exist on this block and none are recommended.

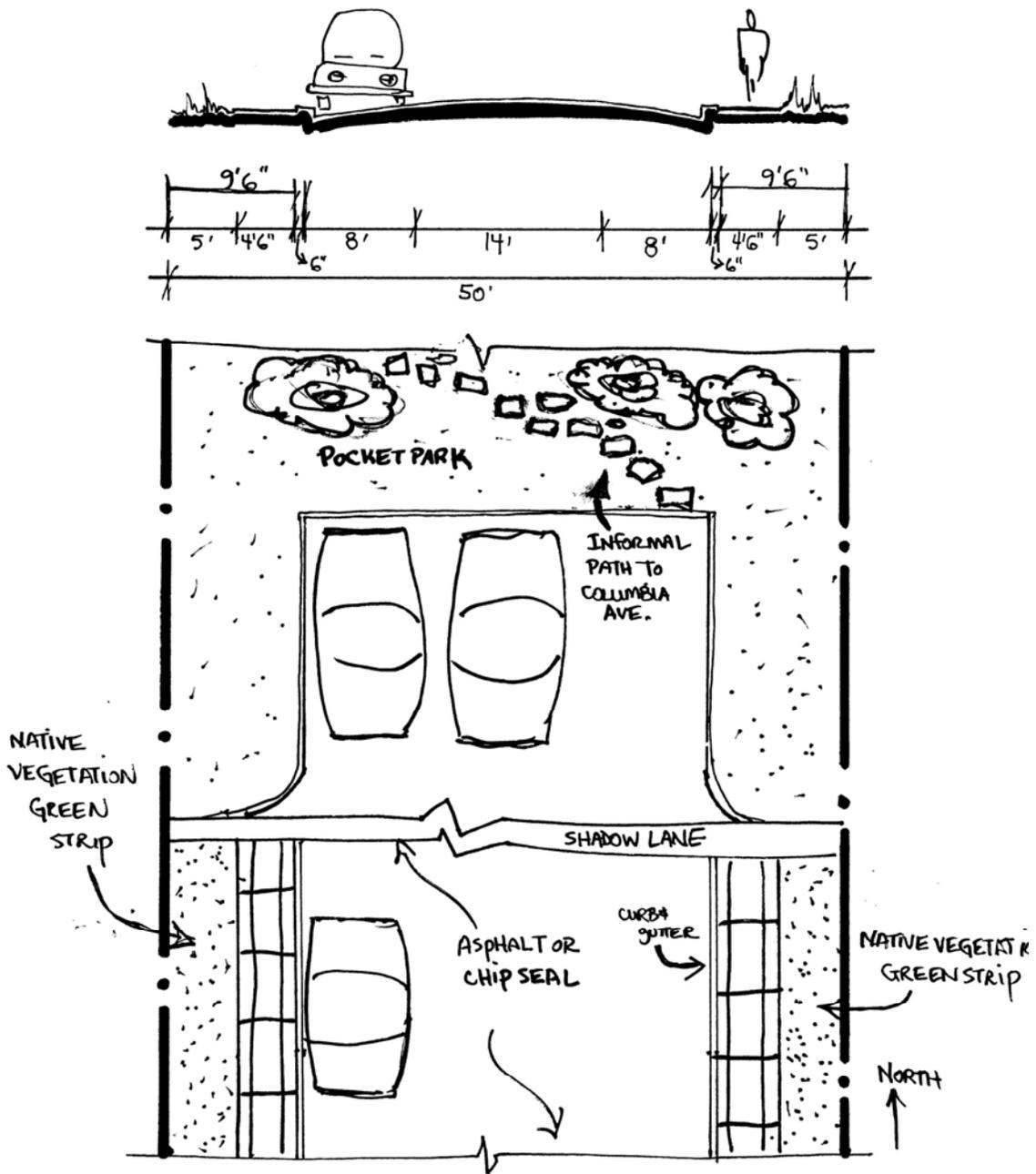
Traffic Circulation. This secondary street has low traffic intensity. Travel way width appears adequate for intensity of use and emergency vehicle access.

Utilities. There are currently no utility concerns for this block.

RECOMMENDED IMPROVEMENTS

No improvements are recommended at this time.

Refer to Streetscape East Telluride District, 2006, Engineered Street Designs and adjacent street profiles for additional information



NORTH LAUREL STREET - 100 BLOCK
(BETWEEN COLORADO + COLUMBIA AVENUES)

NORTH HEMLOCK AVENUE – 100 BLOCK

50' Right of Way

Neighborhood Meetings: July 15, 1996

BACKGROUND

Existing Street Conditions. Surfaced with chip seal, this block slopes very gently south. The edges of the traveled way are not well defined and are covered with gravel and/or grass. The intersecting alleys are offset. The east alley enters slightly south of center and there is a drain pan running north and south along the alley width. This drain pan is filled with gravel and does not function adequately. The west alley enters at the center of the block with no drain pan to guide drainage. There is an east-west drain pan across the width of Hemlock Street at the Colorado Avenue intersection that is also filled with gravel and dirt and does not function adequately. Runoff tends to pool at this location. There are no sidewalks.

Environment/Drainage/Snow Storage. This block does not appear to negatively impact air and water quality. It is also consistent with Telluride's snow storage plan. However, better drainage management is needed. As this is a low spot in town, most of the drainage management will likely focus on infiltration areas along either side of the travel way and along East Colorado Avenue, to where this block drains.

Historic Character. This block neither contributes to nor detracts from the historic character of the area.

Interfacing Streets. Columbia Avenue appears to function well with this block. However, the travel way should be more clearly defined at the Colorado Avenue interface, where motor vehicles often cut the corner and drive on the green strip area. As well, drainage at the Colorado Avenue interface should be improved.

Lighting. There are currently no light fixtures on this block. A bollard-type light fixture that is consistent with the Streetscape Lighting Plan should be installed.

Neighborhood Concerns. Poor alley drainage is a significant neighborhood concern.

Parking. Parallel parking is permitted on both sides of this block. In winter, parking is prohibited on the east side to allow for snow storage. Except during Special Events at the Town Park, parking pressure is light and therefore, the existing parking configuration should be maintained. To maximize the amount of green space for infiltration, parking isles should be constructed of materials that prevent compaction and allow for infiltration. Use of existing off-street parking should be maximized.

Pedestrian Concerns. Pedestrian traffic on this block is light and attributed mainly to neighborhood residents. There are no sidewalks on this block, and because both vehicular and pedestrian volumes are low, none are required. There is no special handicap access.

Street Furniture and Public Art. No street furniture or amenities exist and none are recommended.

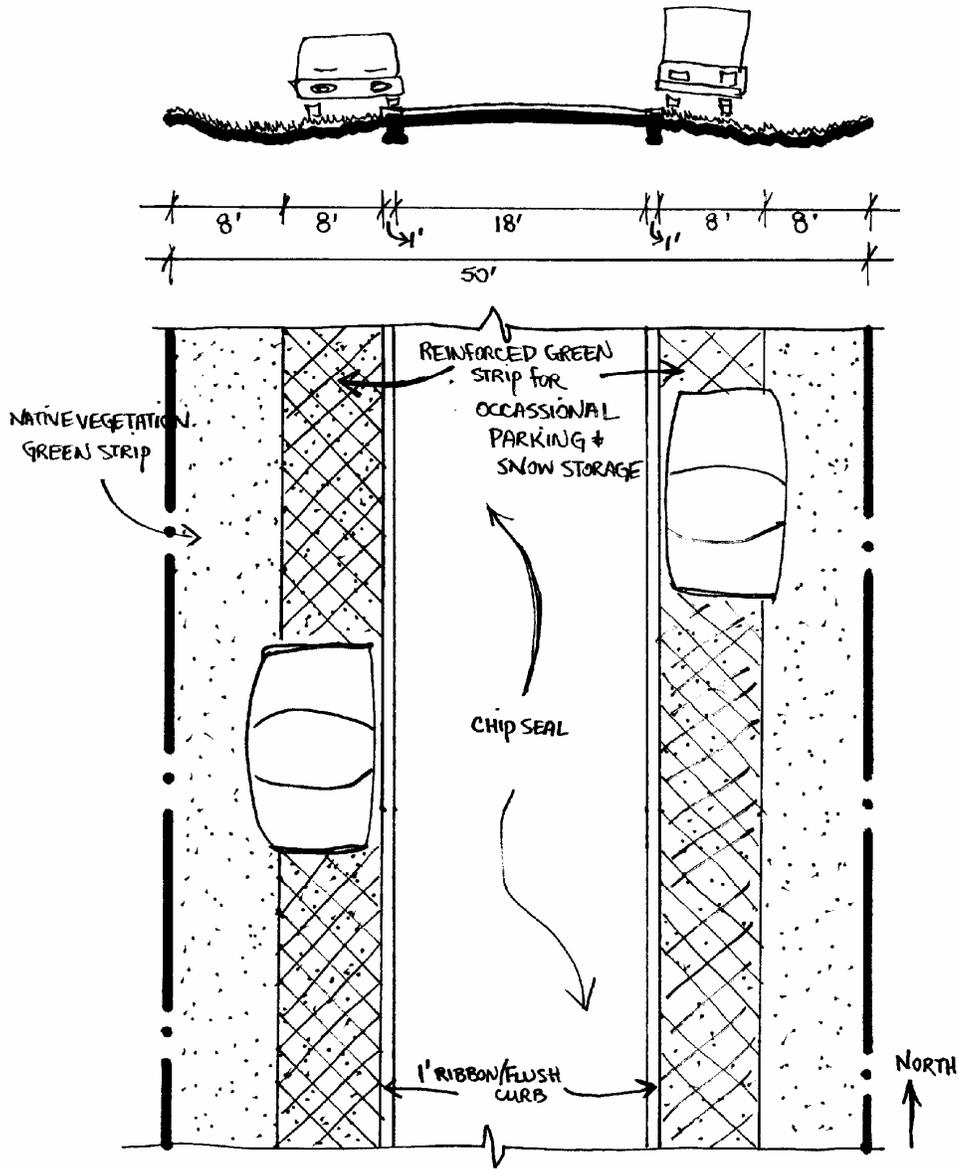
Traffic Circulation. It is anticipated that a travel way width of 18' will be adequate for intensity of use and emergency vehicle access.

Utilities. There are no utility concerns for this block at this time.

RECOMMENDED IMPROVEMENTS

1. Improve drainage management along this block by creating vegetated infiltration swales along the 16'-wide east and west green strips.
2. Better define an 18'-wide chip seal travel way by installing a flush (ribbon curb) at its edge.
3. Install gravel or grasscrete for parking.
4. Address the poor drainage in the alleys on either side.
5. Install a bollard light fixture that is consistent with the Streetscape Lighting Plan at the southwest corner of the block.

Refer to Streetscape East Telluride District, 2006, Engineered Street Designs and adjacent street profiles for additional information.



1. NORTH MAPLE STREET - 100 BLOCK
 2. NORTH HEMLOCK STREET - 100 BLOCK
- (BETWEEN COLORADO + COLUMBIA AVENUES)

NORTH MAPLE STREET – 100 BLOCK

50' Right of Way

Neighborhood Meetings: July 16, 1996

BACKGROUND

Existing Street Conditions. This block is the low spot within the surrounding neighborhood. It is also very flat and drainage tends to pool at its midpoint. It is surfaced with chip seal, with no curb and gutter or drain pans. There are also no sidewalks. The edge of the travel way is poorly defined and the green strips on either side vary in their plantings and condition. Two drain pans within the Columbia Avenue right of way drain runoff south onto Maple Street. There is an 18"-diameter culvert at the southeast corner of the right of way that discharges to the San Miguel River only during extreme events.

Environment/Drainage/Snow Storage. This block does not appear to negatively impact air and water quality. It is also consistent with Telluride's snow storage plan. However, better drainage management is clearly needed. As this is a low spot in town, most of the drainage management will have to focus on creating better infiltration areas along either side of the travel way and better transporting flows into the southeast culvert or along East Colorado Avenue into the Alder Street Managed Wetland

Historic Character. This block neither contributes to nor detracts from the historic character of the area.

Interfacing Streets. Columbia Avenue appears to function well with this block. However, the travel way should be more clearly defined at the Colorado Avenue interface, where motor vehicles often cut the corner and drive on the green strip area. As well, drainage at the Colorado Avenue interface should be improved.

Lighting. There is a standard commercial streetlight at the southwest corner at Colorado Avenue that should be replaced with a bollard fixture that is consistent with the Streetscape Lighting Plan.

Neighborhood Concerns. Poor alley drainage is a significant neighborhood concern.

Parking. Parallel parking is permitted on both sides of this block. In winter, parking is prohibited on the west side to allow for snow storage. Except during Special Events at the Town Park, parking pressure is light and therefore, the existing parking configuration should be maintained. To maximize the amount of green space for infiltration, parking isles should be constructed of materials that prevent compaction and allow for infiltration. Use of existing off-street parking should be maximized.

Pedestrian Concerns. Pedestrian traffic on this block is light and attributed mainly to neighborhood residents. There are no sidewalks on this block, and because both vehicular and pedestrian volumes are low, none are required. There is no special handicap access.

Street Furniture and Public Art. No street furniture or amenities exist and none are recommended.

Traffic Circulation. It is anticipated that a travel way width of 18' will be adequate for intensity of use and emergency vehicle access.

Utilities. There are no utility concerns for this block at this time.

RECOMMENDED IMPROVEMENTS

1. Improve drainage management along this block by creating vegetated infiltration swales along the 16'-wide east and west green strips.
2. Better define an 18'-wide chip seal travel way by installing a flush (ribbon curb) at its edge.
3. Install gravel or grasscrete for parking.
4. Improve alley drainage.
5. Install a bollard light fixture that is consistent with the Streetscape Lighting Plan at the southwest corner of the block.

Refer to Streetscape East Telluride District, 2006, Engineered Street Designs and adjacent street profiles for additional information

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NORTH MAPLE STREET – 200 BLOCK

50' Right of Way

Neighborhood Meetings: July 16, 1996

BACKGROUND

Existing Street Conditions. This block slopes steeply south. The width of the travel way, which is paved with asphalt, varies. The edge of the travel way is not defined and there is a green strip of varying width along either side. There are no sidewalks and no formal drainage except for a vegetated swale. It is in good condition.

Environment/Drainage/Snow Storage. This block does not appear to negatively impact air and water quality. It has good drainage. It is consistent with Telluride's drainage and snow storage plans.

Historic Character. There is one designated historic structure with qualifications on this block. Any alteration to the right of way should not negatively affect this historic resource.

Interfacing Streets. All interfacing streets appear to function well with this block.

Lighting. There is a pole-mounted residential light fixture at the southwest corner at Columbia Avenue that should be replaced with a fixture that is consistent with the Streetscape Lighting Plan.

Neighborhood Concerns. No major neighborhood concerns have been expressed.

Parking. Due to its steep grade, there is little parallel parking along either side of this block. is prohibited on the west side to allow for snow storage. Except during Special Events at the Town Park, parking pressure is light and therefore, the existing parking configuration should be maintained. To maximize the amount of green space for infiltration, parking isles should be constructed of materials that prevent compaction and allow for infiltration. Use of existing off-street parking should be maximized.

Pedestrian Concerns. Pedestrian traffic on this block is light and attributed mainly to neighborhood residents. There are no sidewalks on this block, and because both vehicular and pedestrian volumes are low, none are required.

Street Furniture and Public Art. No street furniture or amenities exist and none are recommended.

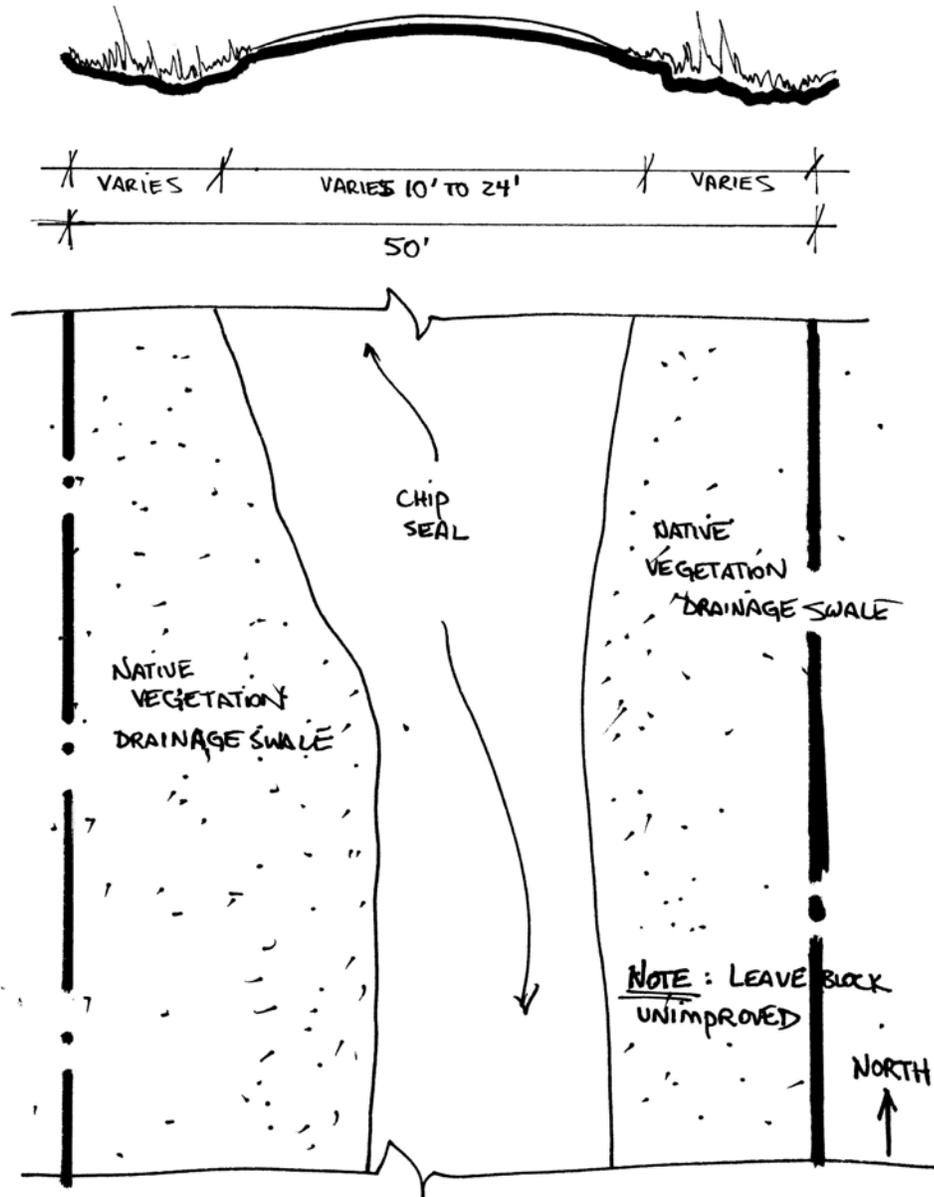
Traffic Circulation. It appears that the current travel way width is adequate for intensity of use and emergency vehicle access. The steep grade made make emergency vehicle access difficult in winter.

Utilities. There are no utility concerns for this block at this time.

RECOMMENDED IMPROVEMENTS

No improvements are recommended at this time.

Refer to Streetscape East Telluride District, 2006, Engineered Street Designs and adjacent street profiles for additional information.



NORTH MAPLE STREET - Zoo Block
(BETWEEN COLUMBIA + PANDORA AVENUES)

NORTH OAK - 100 BLOCK

50' Right of Way

Neighborhood Meetings: July 10, 1996; February 24, 1997

BACKGROUND

Existing Street Conditions. This block was improved during Streetscape Phase I, 1997, Construction. The north half is paved with asphalt and slopes south. The south half is a park, which results in this being a dead end at the east-west intersecting alley. The park is landscaped with grass and trees and contains picnic tables, benches and a bike rack. Wide sidewalks are located on both the east and west sides of the park. There are 5'-wide sidewalks along the north half of the block, where vehicular traffic is permitted. There are also green strips between the sidewalk and the curb. Four large cottonwood trees are located within the east green strip near the alley. The green strip is in fair to poor condition. The north half of the block contains curb and gutter that directs drainage south to drain pans that cross the alley and then into a trench drain connects to an infiltration trench that is located underneath a portion of the park. A traditional curb separates the travel way from the park. While the travel way and sidewalk are in good condition, the drainage system at the dead end requires attention.

Environment/Drainage/Snow Storage. This block does not appear to negatively impact air and water quality. Maintenance and a re-construction of the south curb in summer 2006 improved the ability of the existing drainage system to effectively handle runoff. Historically, runoff often drained east past the trench drain, south down the east side of the block, onto the handicap ramp on the east side at the intersection with the park, down the east sidewalk through the park, and into the west wall of the historic Sheridan Opera House. This block is consistent with Telluride's drainage and snow storage plans.

Historic Character. The block neither contributes to nor detracts from the historic character of the area, attributed to the San Miguel County Courthouse, the Sheridan Opera House, and other historic structures. Any alteration to this block must take these buildings into consideration and no alterations shall take place that negatively impact these buildings.

Interfacing Streets. All interfacing streets appear to function well with this block.

Lighting. The pole-mounted residential light fixture located at the northwest corner of the block should be replaced with a fixture that is consistent with the Streetscape Lighting Plan.

Neighborhood Concerns. The operators of the Sheridan Opera House expressed concern regarding the drainage that flowed along their building. Pedestrians expressed concern in winter regarding icing on the sidewalk.

Parking. Parallel parking is permitted on both sides of the north half of this block. In winter, parking is not permitted on the east side to allow for snow storage. Proximity to the commercial core makes parking pressure high on this block. However, on-street parking options are limited and therefore the current parking configuration should be maintained. Use of existing off-street parking should also be maximized to maintain availability of on-street parking.

Pedestrian Concerns. Pedestrian traffic on this block is moderate to high due to its location at the center of the commercial district. The sidewalks on this block effectively handle pedestrian traffic. There is handicap access.

Street Furniture and Public Art. There are currently benches, picnic tables, and a bike rack in the park at the south end of this block. These should remain. Additional street furniture and public art could be considered within the park.

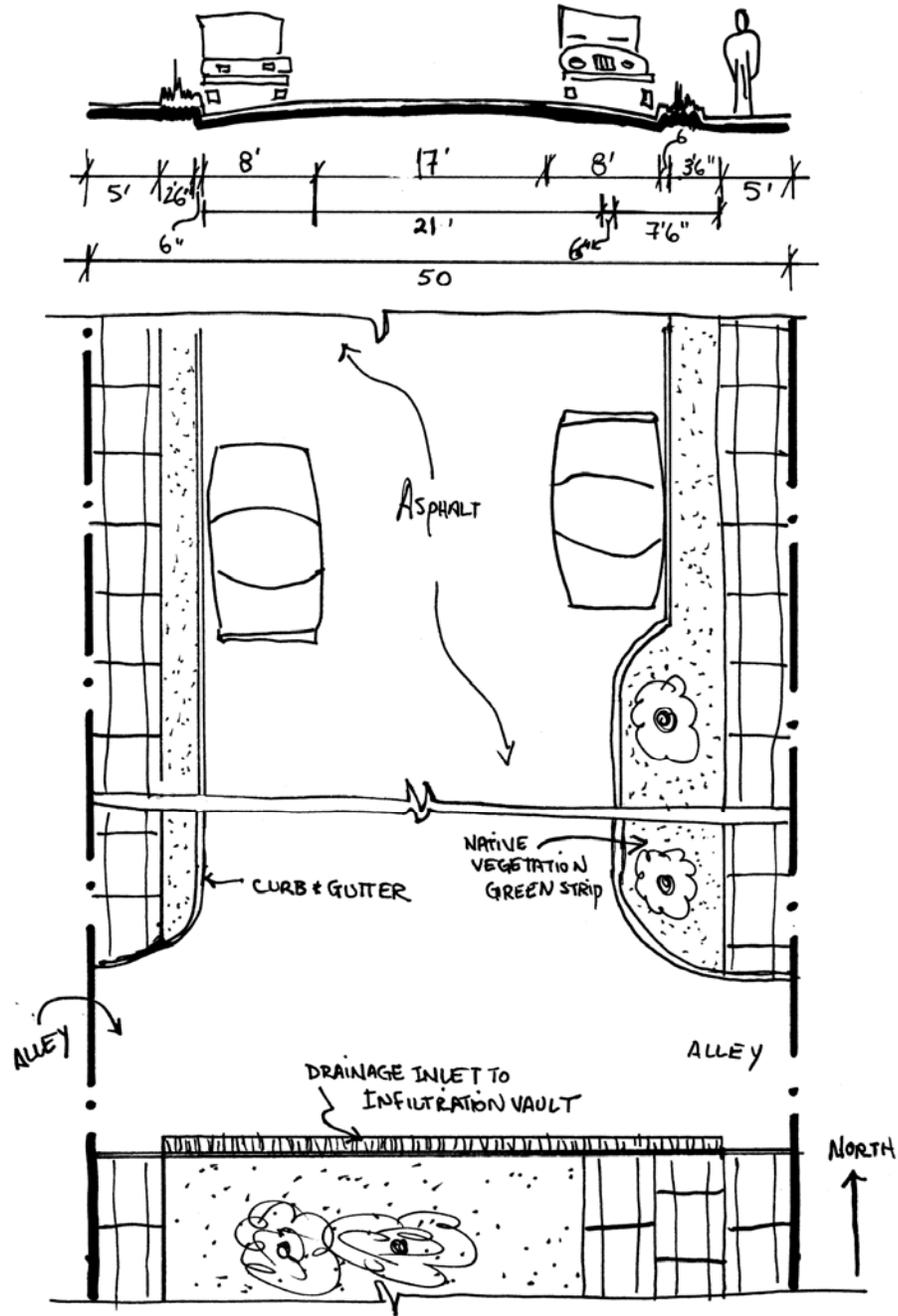
Traffic Circulation. Since this is a dead end street, the traffic coming down the block to the south either must turn around or turn east or west into the intersecting alley. The proximity of this block to the commercial core causes a high number of vehicles to enter the block looking for parking, however, and vehicular traffic intensity is moderate. Travel width appears adequate for intensity of use and emergency vehicle access.

Utilities. There are no utility concerns for this block at this time.

RECOMMENDED IMPROVEMENTS

1. Replace the pole-mounted residential light fixture at the northwest corner of the block with a fixture that is consistent with Streetscape Lighting Plan.

Refer to Streetscape Phase I, 1997, Engineered Street Designs and adjacent street profiles for additional information.



NORTH OAK STREET - 100 BLOCK
(BETWEEN COLORADO + COLUMBIA AVENUES)

NORTH OAK STREET – 200, 300, & 400 BLOCKS

50' Right of Way

Neighborhood Meetings: July 10, 1996; February 24, 1997

BACKGROUND

Existing Street Conditions. These asphalt-paved blocks were improved as part of Streetscape Phase I 1997 Construction. Sloping southward, they have curb and gutter, 5'-wide sidewalks, and 2.5'-wide green strips on both sides. All improvements are in good condition. The drainage system functions effectively, except at the handicap ramps on the north end of the block that allow runoff to flow onto the sidewalks and into the green strips rather than down the gutter, as designed. The west side green strip slopes steeply downward from the sidewalk to the street. The green strips appear to be in good condition.

Environment/Drainage/Snow Storage. These blocks do not appear to negatively impact air and water quality. The drainage system effectively handles drainage during low flow events. During moderate to high flow events the handicap ramps at the north end allow runoff to flow onto the sidewalks and into the green strips. This should be corrected by reconstructing a curb at the corner where the drain pans are and locating the handicap ramps off the corners and facing in toward the middle of the block (Type II Handicap Ramps). As well, during large runoff events, drainage from Tomboy Road flows onto the east side of Oak Street, jumps the curb and flows onto the sidewalk and in the green strip. Tomboy Road runoff is heavily laden with sediment, that gets deposited along the east side drain pan and often along the eastern half of the travel way. Managing this drainage and the associated sediment in a more effective manner will be important. In its current condition, this block is consistent with Telluride's snow storage and drainage plans.

Historic Character. As it exists these blocks contribute to the historic character of the area due to the numerous rated structures located there. Any future alterations must take these buildings into consideration. No alterations shall take place that negatively impacts these buildings.

Interfacing Streets. Most interfacing streets appear to function well with these blocks. The high volume of runoff draining from Tomboy Road, however, is problematic and should be addressed.

Lighting. The existing pole-mounted residential light fixture at the southwest corner of the 300 Block should be replaced with a fixture that is consistent with the Streetscape Lighting Plan.

Neighborhood Concerns. No major neighborhood concerns currently exist.

Parking. Parallel parking is allowed on both sides of the street. Parking pressure is moderate to heavy due in part to the proximity of this 200 block to the commercial district and the 300 and 400 blocks to Tomboy Road, which accesses recreation in the backcountry. In winter, parking is prohibited on the east side to allow temporary snow storage. Although the availability of parking on this block is low, the current configuration should be maintained. Use of existing off-street parking should be maximized to maintain on-street parking availability.

Pedestrian Concerns. Existing sidewalks appear to effectively handle the moderate pedestrian traffic on these blocks. As the west green strip slopes steeply from the sidewalk to the street, some residents have constructed steps within the green strip to facilitate access to the sidewalk from the street. There is handicap access at all corners.

Street Furniture and Public Art. No street furniture or amenities currently exist on these blocks and none are required.

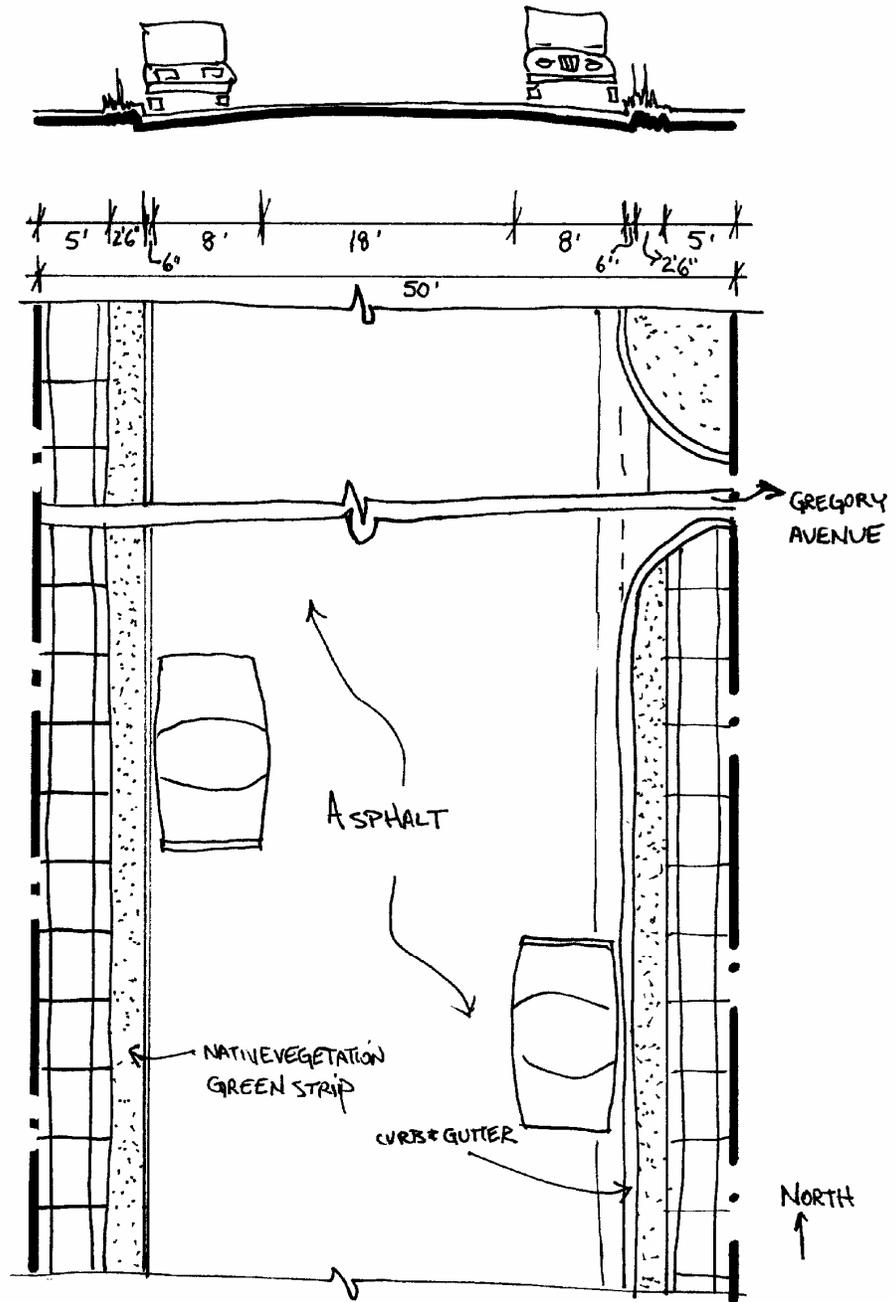
Traffic Circulation. Vehicular traffic on these blocks is moderate. Travel way width appears adequate for the moderate vehicular traffic volumes and emergency vehicle access.

Utilities. There are no utility concerns for these blocks at this time.

RECOMMENDED IMPROVEMENTS

1. Replace the existing handicap ramps at the north end of the block with a curb. The handicap ramps should be located just south of the north corners of the block and slope downward toward the middle of the street.
2. Increase the frequency of maintenance to clean sediment from the eastern drain pans and the east half of the travel way.
3. Improve the management of drainage from Tomboy Road.
4. Replace the existing pole-mounted residential light fixture at the southwest corner of the 300 Block with a fixture that is consistent with the Streetscape Lighting Plan.

Refer to Streetscape Phase I, 1997, Engineered Street Designs and adjacent street profiles for additional information.



NORTH OAK STREET - 200, 300, & 400 BLOCKS
(BETWEEN COLUMBIA AVE & TOMBOY ROAD)

SOUTH OAK STREET - 100 BLOCK

50' Right of Way

Neighborhood Meetings: July 23, 1996; February 24, 1997

BACKGROUND

Existing Street Conditions. This block was improved as part of Streetscape Phase I 1997 construction. It is an integral link between the Gondola Plaza and the commercial core. The north half is Elks Park. The half south of the alley is a travel way that is paved with asphalt, has curb and gutter along the east and west sides, has a 6'-wide sidewalk along the west side, and a 7'-wide sidewalk with a 5'-wide green strip along the east side. The entire right of way slopes to the south. All improvements are in good condition.

Environment/Drainage/Snow Storage. This block does not appear to negatively impact air and water quality. The drainage system appears to effectively handle runoff volumes. Snow is not stored on this heavily used block; it is hauled to the Public Works Facility. In its current condition, this block is consistent with Telluride's snow storage and drainage plans.

Historic Character. As it exists this block contributes to the historic character of the area due to the numerous rated structures located there. Any future alterations to the right of way should not negatively affect the historic resources on this block.

Interfacing Streets. All interfacing streets and alleys appear to function well with this block.

Lighting. There are two standard commercial streetlight fixtures on the south half of this block. These are consistent with the Streetscape Lighting Plan and should remain.

Parking. Parallel parking is permitted on both sides of the street, which makes through travel somewhat difficult. Parking pressure is quite heavy because this block is in the middle of the commercial core. It is believed that on-street parking has been maximized and the current configuration should be maintained. Use of existing off-street parking should also be maximized.

Pedestrian Concerns. Existing sidewalks appear to effectively handle the moderate pedestrian traffic on these blocks. There is handicap access.

Street Furniture and Public Art. A picnic table and informal seating are provided in Elks Park, which is used by a variety of events that would conflict with any permanent structures in its center or on its south side. Therefore, no additional amenities are encouraged. However, there are two heavily used bicycle racks south of the southeast corner of the Pocket Park. Due to consistent demand, additional capacity for bicycle parking should be considered. No street furniture or amenities currently exist on the south half of this block and none are required.

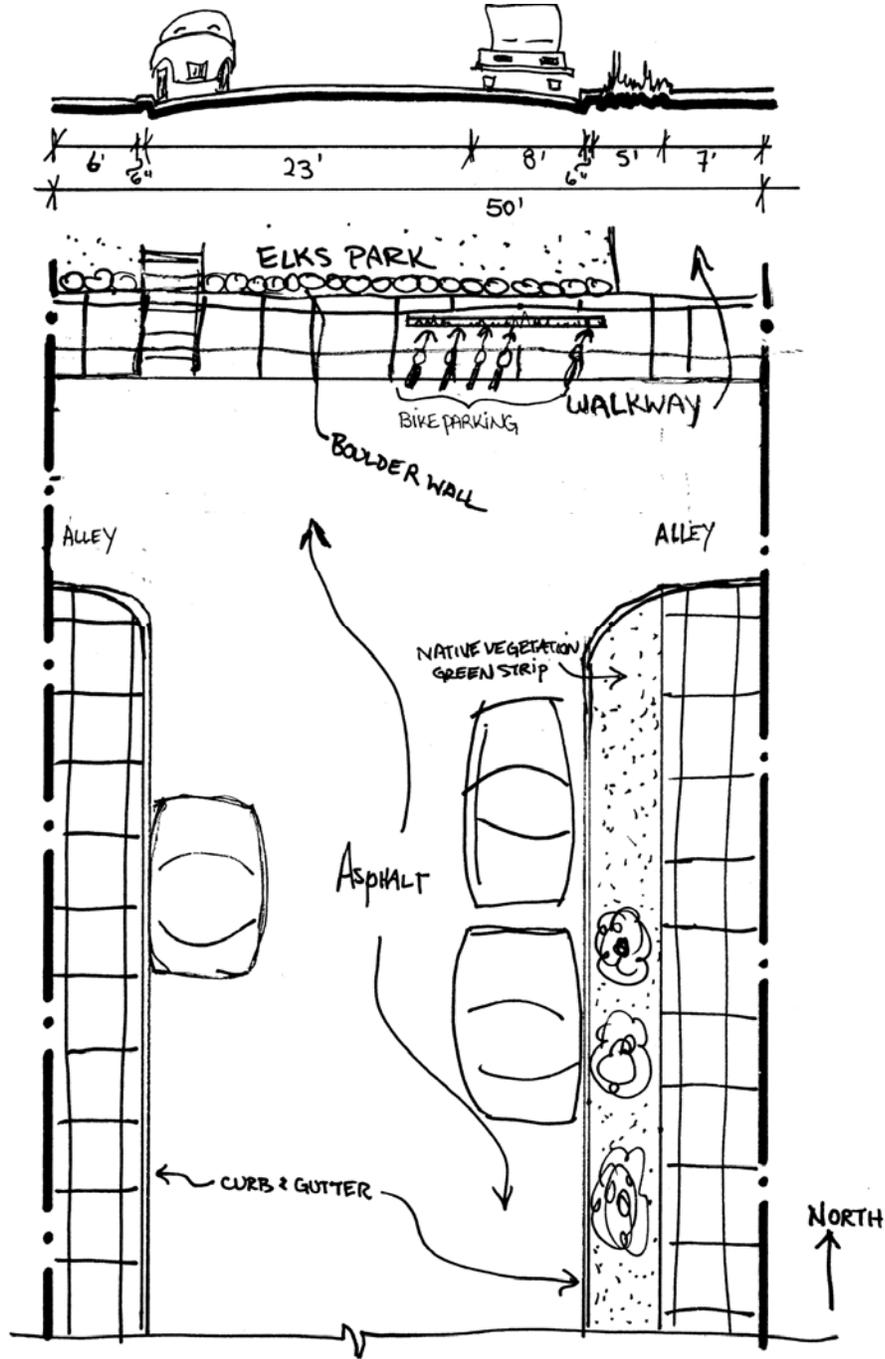
Traffic Circulation. Travel way width appears adequate for the moderate to heavy vehicular traffic volumes and emergency vehicle access.

Utilities. There are no utility concerns for this block at this time.

RECOMMENDED IMPROVEMENTS

No improvements are recommended at this time. However, it is anticipated that the retaining wall at the south end of Elks Park will be modified in the future.

Refer to Streetscape Phase I, 1997, Engineered Street Designs and adjacent street profiles for additional information.



SOUTH OAK STREET - 100 BLOCK
(BETWEEN PACIFIC + ELKS PARK)

SOUTH OAK STREET – 200 BLOCK

*50' Right of Way
Neighborhood Meeting*

BACKGROUND

Existing Street Conditions. This south sloping block was improved as part of Streetscape Phase I 1997 construction and the Gondola Plaza Project. It is an integral link between the Gondola Plaza and the commercial core. Paved with asphalt, there are 6'-wide sidewalks and curb and gutter along both sides of the block. There is a 24" diameter cottonwood on the southwest corner of the block. The sidewalk has been narrowed at this location to protect this valuable asset, which lends shade and a casual, neighborhood atmosphere to this busy area. All improvements are in good condition.

Environment/Drainage/Snow Storage. This block does not appear to negatively impact air and water quality. The drainage system appears to effectively handle a range of runoff volumes. Snow is not stored on this heavily used block; it is hauled to the Public Works Facility. In its current condition, this block is consistent with Telluride's snow storage and drainage plans.

Historic Character. As it exists this block contributes to the historic character of the area due to the numerous rated structures located there. Any future alterations must take these buildings into consideration. No alterations shall take place that negatively impacts these buildings.

Interfacing Streets. All interfacing streets appear to function well with this block.

Lighting. The five standard commercial streetlight fixtures along this block from Pacific and West San Juan Avenue are consistent with the Streetscape Lighting Plan and should remain.

Parking. Paid parallel parking is permitted on both sides of the street. Parking pressure is heavy because this block is in the middle of the commercial core. It is believed that on-street parking has been maximized and the current configuration should be maintained. Use of existing off-street parking should also be maximized.

Pedestrian Concerns. Existing sidewalks appear to effectively handle the moderate to heavy pedestrian traffic. There is handicap access.

Street Furniture and Public Art. No street furniture or amenities currently exist and none are required as adequate artwork and seating are provided in Gondola Plaza at its south end.

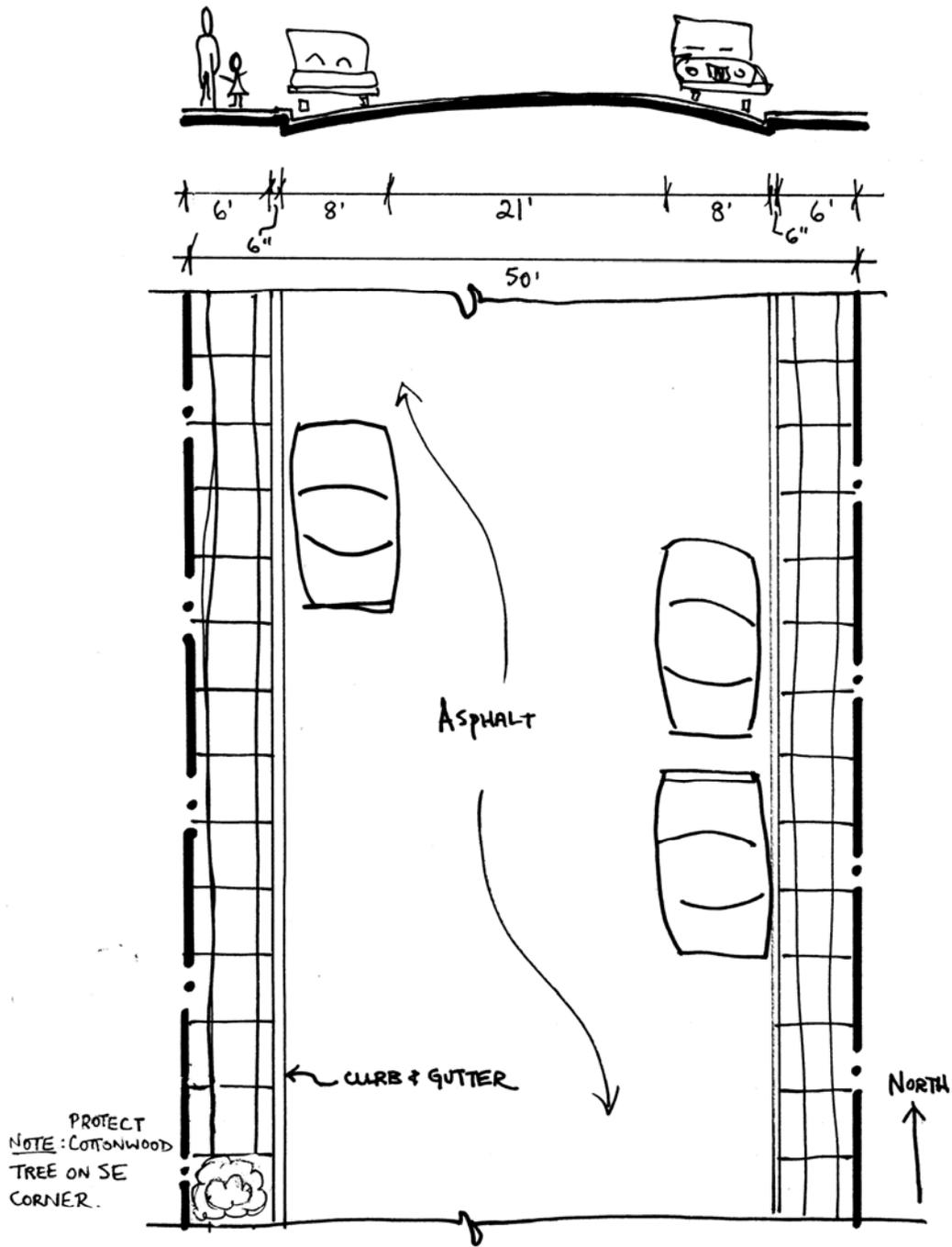
Traffic Circulation. Travel way width appears adequate emergency vehicle access and for the moderate to heavy vehicular traffic volumes on this block.

Utilities. All utilities should be put underground along this block. If possible, locate electric transformer boxes on private property. If they must be in the right of way, they should be as unobtrusive as possible. Concrete bollards will be required to protect them from vehicle impact.

RECOMMENDED IMPROVEMENTS

Underground all above-ground utilities.

Refer to Streetscape Phase I, 1997, Engineered Street Designs and adjacent street profiles for additional information.



SOUTH OAK STREET - ZOO BLOCK
(BETWEEN PACIFIC & SAN JUAN AVENUES)

WEST PACIFIC AVENUE – 100 BLOCK

60' Right of Way

Neighborhood Meetings: July 23, 1996; May 27, 1998; June 10, 1998; June 24, 1998; September 30, 1998; November 17, 1998; April 21, 1999; December 1, 1999

BACKGROUND

Existing Street Conditions. This east sloping block is paved with asphalt. There is 9.5'-wide sidewalk with a concrete curb and gutter along the north side, a 7.5'-wide sidewalk and curb and gutter in front of the Wilkinson Public Library, and a 7.5'-wide base coarse walkway and curb and gutter west of the alley. A 5'-wide drain pan runs perpendicularly across Pacific Avenue at the west side of the Pine Street intersection, and one runs perpendicularly across Pacific Avenue at the east side of the Fir Street intersection. There are no green strips.

Environment/Drainage/Snow Storage. This block does not appear to negatively impact air and water quality. Drainage is effective. Snow storage is on the south side of the block. This is consistent with Telluride's snow storage and drainage plans.

Lighting. There is one standard commercial streetlight east of the alley on the north side and one on the southeast corner. These should remain. A third standard type fixture should be installed on the south side of the street between the alley and Fir Street. These changes are required to be consistent with the Streetscape Lighting Plan.

Historic Character. The contributing structures on the north side of this should be protected. Any improvements should respect these structures, and their relationship to the street.

Interfacing Streets. All interfacing streets appear to function well with this block.

Parking. Parking demand is moderate to high on this block due to the library and its proximity to numerous businesses. Paid parallel parking is permitted on both the north and south sides of this block. Parking is prohibited on the south in winter to allow for snow storage. This parking configuration appears adequate. Use of off-street parking should be maximized.

Pedestrian Concerns. Pedestrian traffic is heavy along this major thoroughfare, due to the presence of the library, the grocery and other nearby establishments, and Main Street. Existing sidewalks appear adequate; although missing sections of concrete are needed to ensure that pedestrians prefer the sidewalk to the travel way. A 7.5' concrete sidewalk should be constructed on the west half of the south side of the block where none currently exists.

Street Furniture and Public Art. There is currently no street furniture or amenities and none are recommended.

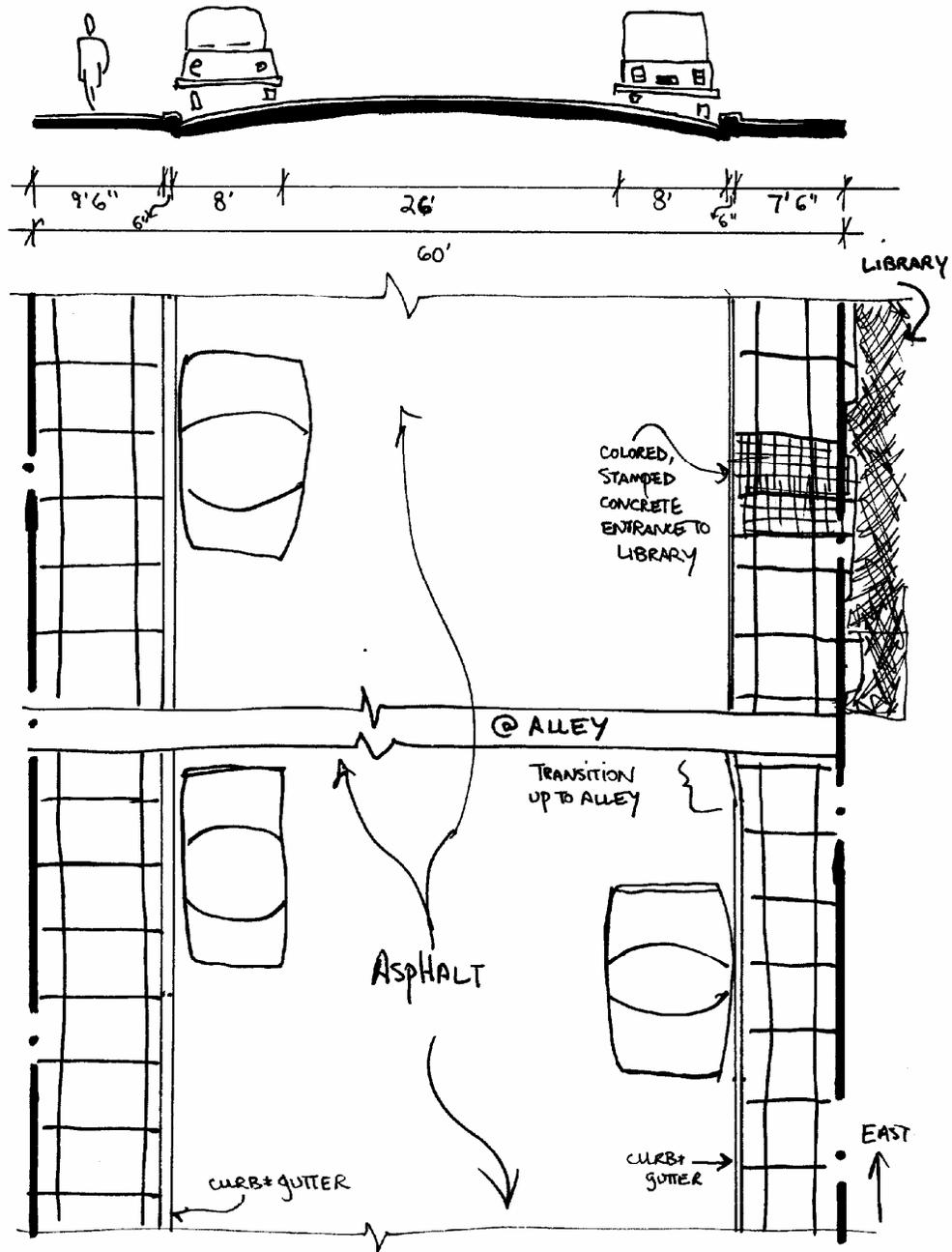
Traffic Circulation. Traffic is heavy along this major thoroughfare. However, travel width is adequate for intensity of use and emergency vehicle access.

Utilities. No utility concerns have been expressed.

RECOMMENDED IMPROVEMENTS

1. Construct a 7.5'-wide concrete sidewalk with curb and gutter on the south side of the street west of the alley.
2. Install a standard type light fixture on the south side of the block between the alley and Fir Street.

Refer to Streetscape Warehouse District, 2005, Engineered Street Designs and adjacent street profiles for additional information.



WEST PACIFIC AVENUE - 100 BLOCK
(BETWEEN FIR & PINE STREETS)

WEST PACIFIC AVENUE – EAST ½ OF 200 BLOCK

60' Right of Way

*Neighborhood Meetings: May 27, 1998; June 10, 1998; June 24, 1998; September 30, 1998;
November 17, 1998; April 21, 1999; December 1, 1999*

BACKGROUND

Existing Street Conditions. This east sloping block is paved with chip seal. There is 6.5'-wide concrete sidewalk with a curb and gutter on the north side of the block and a curb and gutter and compacted base coarse walkway on the east half of the south side of the block. A 5'-wide drain pan runs perpendicularly across Pacific Avenue at the west side of the Pine Street intersection, and one runs perpendicularly across Pacific Avenue at the east side of the Fir Street intersection. There are no green strips.

Environment/Drainage/Snow Storage. This block does not appear to negatively impact air and water quality. Drainage is effective. Snow storage is on the north side of the block. This is consistent with Telluride's snow storage and drainage plans.

Lighting. There are two standard commercial streetlights on this ½ block. One is east of the alley on the south side and the other is at the northeast corner near Village Market. These should remain, as they are consistent with the Streetscape Lighting Plan.

Historic Character. This block is important to the historic character of the area, as there are numerous contributing structures along the north and south side of this block. Any improvements should respect these structures, and their relationship to the street.

Interfacing Streets. All interfacing streets appear to function well with this block.

Neighborhood Concerns. Meeting notes mention no neighborhood concerns.

Parking. Parking demand is high on this ½ block due mainly to the adjacent grocery. Paid parallel parking is permitted on both the north and south sides. Parking is prohibited on the north side in winter to allow temporary snow storage. This parking configuration appears adequate. Use of off-street parking should be maximized.

Pedestrian Concerns. Pedestrian traffic is heavy on this block, due in part to the proximity of this block to the library, the proximity of numerous businesses, and the use of this area to move from the Gondola Plaza onto Main Street and onto Town Park. Existing sidewalks adequately handle pedestrian traffic. The areas where there are no sidewalks impede efficient pedestrian traffic. A 6.5' concrete sidewalk should be constructed on the west half of the south side of the block where none currently exists, making accommodations for the historic Transfer Building.

Street Furniture and Public Art. There is currently no street furniture or amenities, and none are recommended.

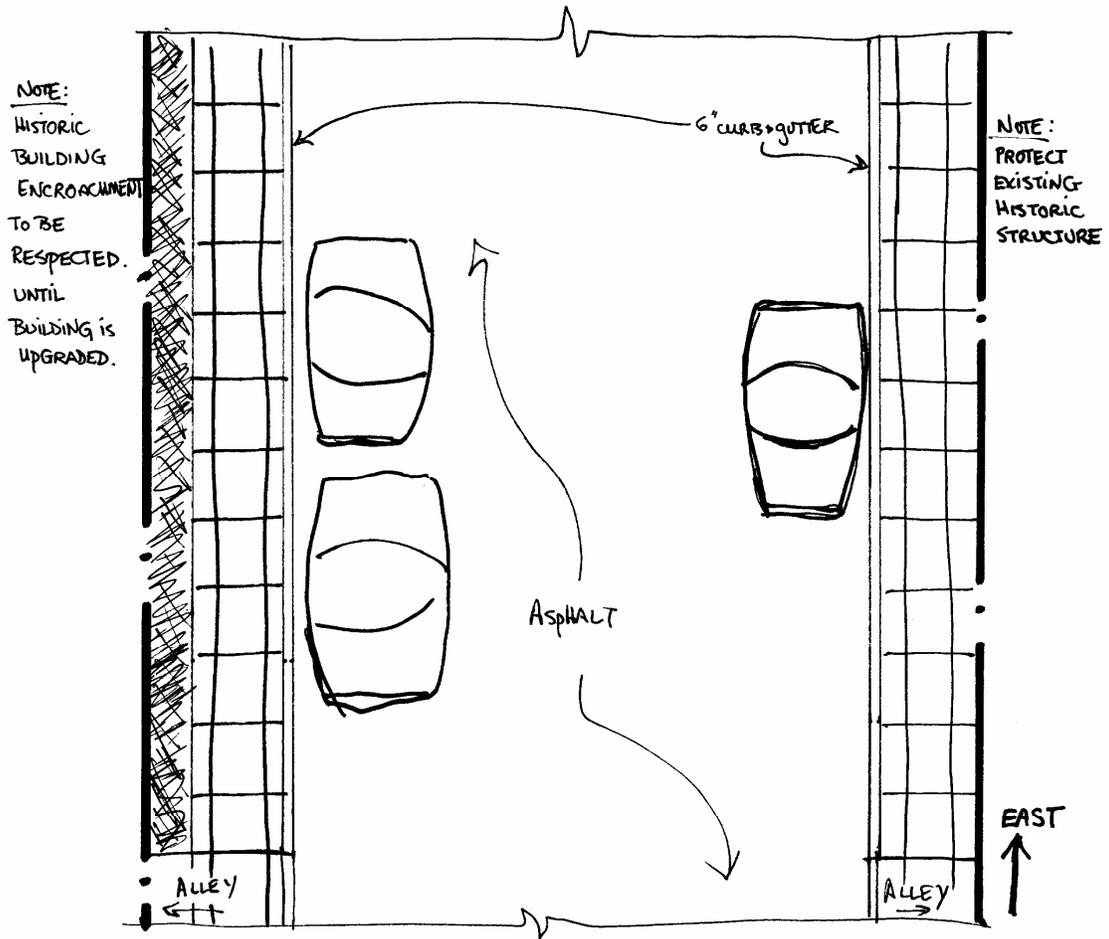
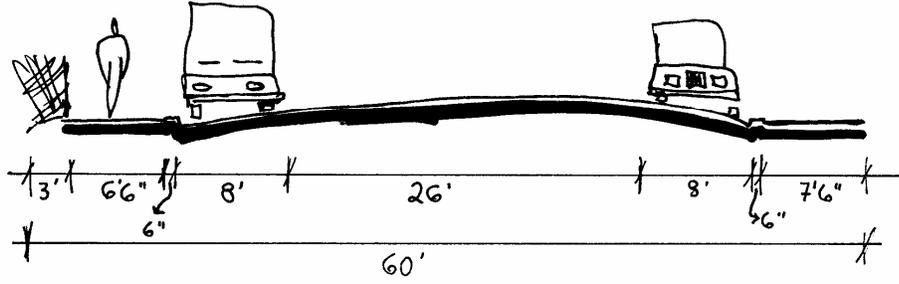
Traffic Circulation. Pacific Avenue is a major thoroughfare and traffic is heavy. As well, delivery vehicles for the Village Market often use a portion of the travel way to unload. For this reason, the travel way is wider here than along most of Pacific Avenue. As a result, the capacity of this ½ block is adequate for intensity of use and emergency vehicle access.

Utilities. There are currently no utility concerns for this ½ block.

RECOMMENDED IMPROVEMENTS

1. Construct a 6.5"-wide concrete sidewalk behind the existing curb and gutter on the south side of the street east of the alley.

Refer to Streetscape Warehouse District, 2005, Engineered Street Designs and adjacent street profiles for additional information.



WEST PACIFIC AVENUE - EAST 1/2 200 BLOCK
(BETWEEN ALLEY + FIR STREET)

EAST PACIFIC AVENUE – 100 BLOCK

60' right-of way

Neighborhood Meetings:

May 27, 1998; June 10, 1998; June 24, 1998; September 30, 1998; November 17, 1998

BACKGROUND

Existing Street Conditions. This major thoroughfare is east sloping and paved with chip seal. There is concrete curb and gutter and associated sidewalk on the west $\frac{3}{4}$ of the block on the south side, and in the west $\frac{1}{2}$ of the block on the north side. There is no other sidewalk or curb and gutter on this block. On the east half of the north side of the block the Cribs are constructed to the edge of the right of way and steps to these structures encroach into the right of way. The same is true for the Silver Bell Building. A 5'-wide drain pan runs diagonally across Pacific Avenue at the west side of the Spruce Street intersection. There are no formal green strips on this block.

Environment/Drainage/Snow Storage. Drainage is poor due to lack of curb and gutter along the down-slope end of this block. Poor drainage negatively impacts air and water quality. Therefore, it is recommended that curb and gutter be constructed when sidewalks are constructed. There is a diagonal 5'-wide drain pan crossing Pacific Avenue at the west side of the intersection with Spruce Street. This should be removed and a 5'-wide drain pan that is perpendicular to Pacific Avenue and drains into the curb and gutter designed for Spruce Street should be constructed. The block should be surfaced with asphalt to decrease air and water quality impacts. These improvements will make this block compatible with Telluride's snow storage and drainage plans.

Historic Character. There are three local landmark structures on this block. Any improvements should respect these structures, and their relationship to the street.

Interfacing Streets. All main interfacing streets appear to function well with this block.

Lighting. There are three standard commercial streetlights on the block. Two are located west of the alley on the north side and one is on the south side at the east corner of the alley intersection. To be consistent with the Streetscape Lighting Plan, the fixture farthest east on the north side should be relocated to the northwest corner of the block. The other two should remain.

Parking. Paid parallel parking is permitted on the north and south sides of this block. No parking is permitted on the north side in winter to allow for snow storage. This parking configuration appears adequate. Use of off-street parking should be maximized.

Pedestrian Concerns. Pedestrian traffic is heavy on this block as it is the main pedestrian thoroughway to the Post Office and into Town Park. The sidewalks on the south side are narrow and do not adequately handle pedestrian traffic. The areas where there are no sidewalks impede pedestrian traffic. As a result, pedestrians often walk in the travel way and conflict with motor vehicles. Sidewalks should therefore be constructed on both sides of the block. The north side should have a 9.5'-wide sidewalk west of the alley and a 5.0'-wide sidewalk east of the alley due to encroachments of historic structures into the right of way. The property between this sidewalk and private property should be a green strip. The south side of the block requires a new 7.5'-wide sidewalk, requiring the removal of the existing narrower sidewalk, which is in poor repair. (See engineered plans).

Street Furniture and Public Art. No street furniture or amenities currently exist on this block and none are required.

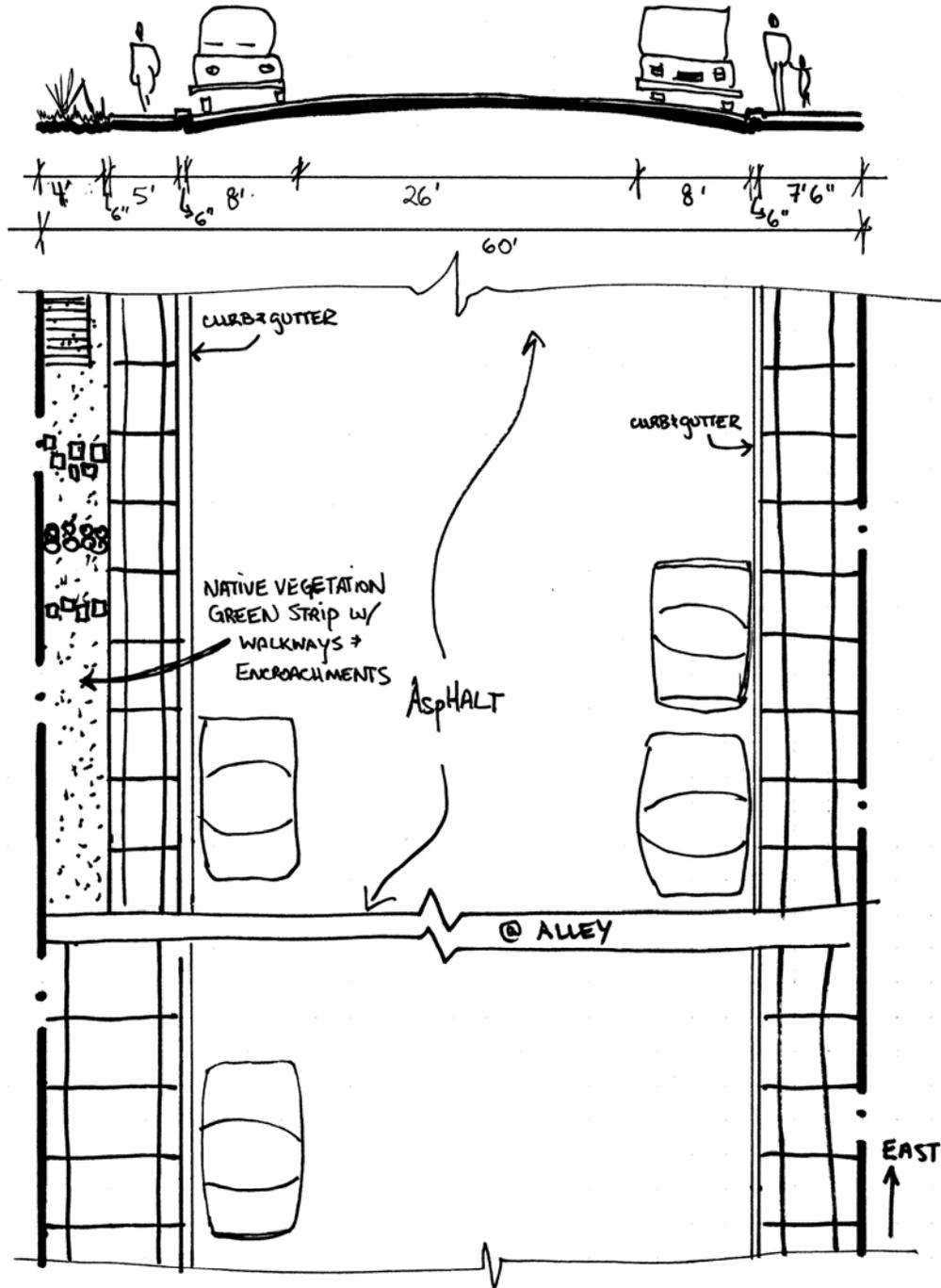
Traffic Circulation. Pacific Avenue is a major thoroughfare and traffic is heavy. However, the travel width of this block is adequate for intensity of use and emergency vehicle access.

Utilities. There are no utility concerns for this block.

RECOMMENDED IMPROVEMENTS

1. Construct a 10'-wide concrete sidewalk and curb and gutter on the north side of the street west of the alley, and a 5.0'-wide sidewalk and curb and gutter east of the alley. Coordinate with the owner of the Silver Bell Building regarding the encroachment of their stairs on the corner where there may be a conflict with a Type I handicap ramp.
2. Construct a 7.5'-wide concrete sidewalk and curb and gutter along the entire south side of the street.
3. Remove the diagonal drain pan on the west side of the intersection of Pacific and Spruce and construct a drain pan in this location that is perpendicular to Pacific Avenue.
4. Relocate the easternmost light fixture on the north side to the northeast corner of the block.

Refer to Streetscape Warehouse District, 2005, Engineered Street Designs and adjacent street profiles for additional information.



EAST PACIFIC AVENUE - 100 BLOCK
(BETWEEN PINE & SPRUCE STREETS)

EAST PACIFIC AVENUE – 200 BLOCK

60' Right of Way

Neighborhood Meetings:

May 27, 1998; June 10, 1998; June 24, 1998; September 30, 1998; November 17, 1998

BACKGROUND

Existing Street Conditions. This major thoroughfare through town slopes eastward and is paved with asphalt. There is a concrete curb and gutter and associated sidewalk that extends east of the alley on the south side. There are sidewalks and curb and gutter along the entire north side. At the northwest end are an historic shed and iron fence that encroach into the right of way. At the northeast end of the block an historic structure also encroaches into the right of way. There is a curb cut on the south side of the block between the alley and Spruce Street. A 5'-wide drain pan is located west of the Willow Street intersection crossing Pacific Avenue. There are no formal green strips.

Environment/Drainage/Snow Storage. Drainage is adequate, except at the Spruce Street intersection where there is no curb and gutter. As a result, air and water quality are negatively impacted. To improve this condition, curb and gutter should be constructed in association with the sidewalk construction. The 5'-wide drain pan crossing Pacific Avenue at the west side of the intersection with Willow Street should be replaced because has heaved and is ineffective. The majority of runoff drains across the north side of Pacific Avenue and then south along the drain pan on the east side of the intersection. Surfacing the block with asphalt will decrease negative impacts on air and water quality. These improvements will make this block compatible with Telluride's snow storage and drainage plans.

Historic Character. There are two historic structures on this block. Any improvements should respect these structures, and their relationship to the street.

Interfacing Streets. All interfacing streets appear to function well with this block.

Lighting. Two standard commercial streetlights that are consistent with the Streetscape Lighting plan should be installed: one on the north side and one on the south side.

Parking. Parking pressure is moderate to heavy on this block. Paid parallel parking is permitted on the north and south sides of the block and is often full. No parking is permitted on the north side in winter to allow for snow storage. This parking configuration appears adequate. Use of off-street parking should be maximized.

Pedestrian Concerns. Pedestrian traffic is heavy on this block as it is the main pedestrian thoroughway to the Post Office and into Town Park. Where there is no sidewalk, pedestrian travel is impeded and pedestrians tend to walk in the travel way and conflict with motor vehicles. Therefore, completing sidewalks is a very high priority. The north side should have a 9.5'-wide sidewalk due to the number of pedestrians that use this side of the street. However, where historic structures or fences encroach into the right of way, the sidewalk at these locations should be narrower to prevent damage to these resources. Where a sidewalk is missing on the south side of the block, a new 7.5'-wide sidewalk should be constructed.

Street Furniture and Public Art. There is no street furniture or amenities on this block. None are recommended.

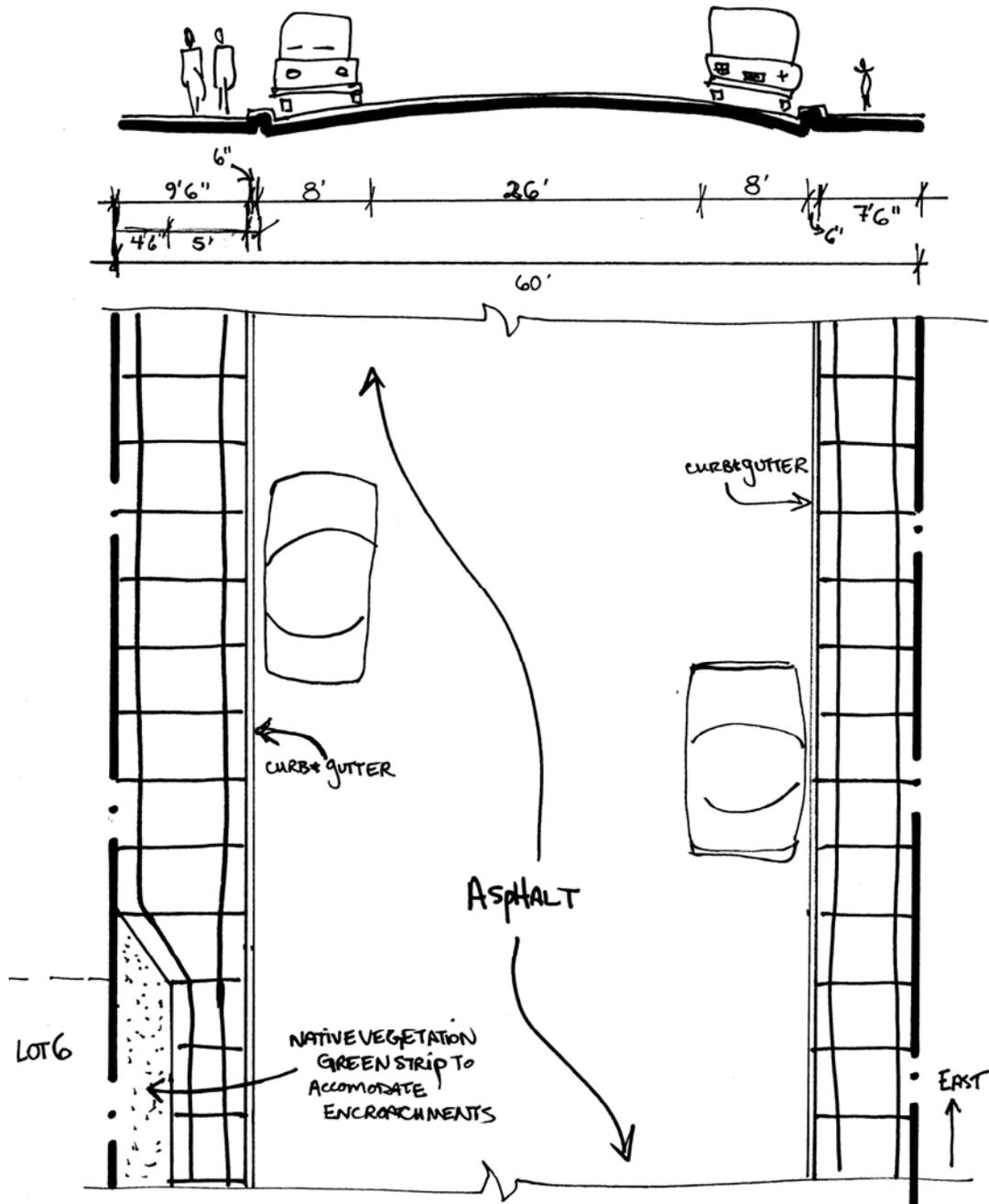
Traffic Circulation. Pacific Avenue is a major thoroughfare and traffic is heavy, particularly on this block due to its proximity to the Post Office. Nevertheless, capacity of this block is adequate for the intensity of use and emergency vehicle access.

Utilities. There are currently no utility concerns for this block.

RECOMMENDED IMPROVEMENTS

1. Construct a 9.5'-wide concrete sidewalk with a traditional curb and gutter on the north side. Where historic resources encroach into the right of way, narrow the sidewalk to accommodate.
2. Construct a 7.5'-wide concrete sidewalk with a traditional curb and gutter on the south side.
3. Remove the drain pan on the west side of the intersection of Pacific and Willow.
4. Install two standard type light fixtures, one on the north and one on the south side of the block. See the Streetscape Lighting Plan for more information.

Refer to Streetscape Warehouse District, 2005, Engineered Street Designs and adjacent street profiles for additional information.



EAST PACIFIC AVENUE - ZOO BLOCK

(BETWEEN SPRUCE + WILLOW STREETS)

EAST PACIFIC AVENUE – 300 BLOCK

60' Right of Way

Neighborhood Meetings: May 27, 1998; June 10, 1998; June 24, 1998; November 17, 1998

BACKGROUND

Existing Street Conditions. This is a half block that extends from Willow Street east to a dead end where Pacific Avenue intersects with Town Park. It slopes west and is surfaced with asphalt. There is an 8'-wide sidewalk and a curb and gutter on the north side of the block adjacent to the post office. There is a curb and gutter and landscaping broken by short concrete walks along the south side of the block. All drainage flows west into a drain pan that crosses Pacific Avenue at the east side of the Willow Street intersection. Bollards and boulders at the entrance to Town Park are intended to prevent motor vehicles from entering. The bollards are in poor condition and should be replaced with an improved design that is easily removed as needed.

Environment/Drainage/Snow Storage. As a result of improvements made in conjunction with the development of the Post Office, this block does not appear to negatively impact air and water quality. Drainage is effective, except at the Willow Street intersection. This block is consistent with the Telluride's snow storage and drainage plans.

Historic Character. There is a contributing structure at the north alley on this block. This historic resource should be protected.

Interfacing Streets. All interfacing streets appear to function well with this block.

Lighting. There is a standard commercial streetlight at the northwest corner of this block, which is consistent with the Streetscape Lighting Plan. This fixture should remain.

Neighborhood Concerns. The drainage at Willow Street should be improved. No other major neighborhood concerns have been expressed.

Parking. Short-term head-in parking is permitted on the north side of the block and no parking is permitted on the south side; although people do park there on occasion. This on-street parking configuration appears to be somewhat adequate and should be maintained or improved. Use of off-street parking should be maximized.

Pedestrian Concerns. Pedestrian traffic on this block is moderate and attributed to pedestrians accessing the Town Park and going to and from the post office. The existing sidewalk on the south side of the block does not effectively handle pedestrian traffic because it is not continuous. The sidewalk on the north side, where the majority of the pedestrian traffic is concentrated, does effectively handle pedestrian needs. This north sidewalk contains handicap ramps.

Street Furniture and Public Art. Benches located alongside the Post Office and in Town Park are appropriate and should remain. Public art could be considered in front of the Post Office if it does not interrupt the flow of pedestrian or vehicular traffic.

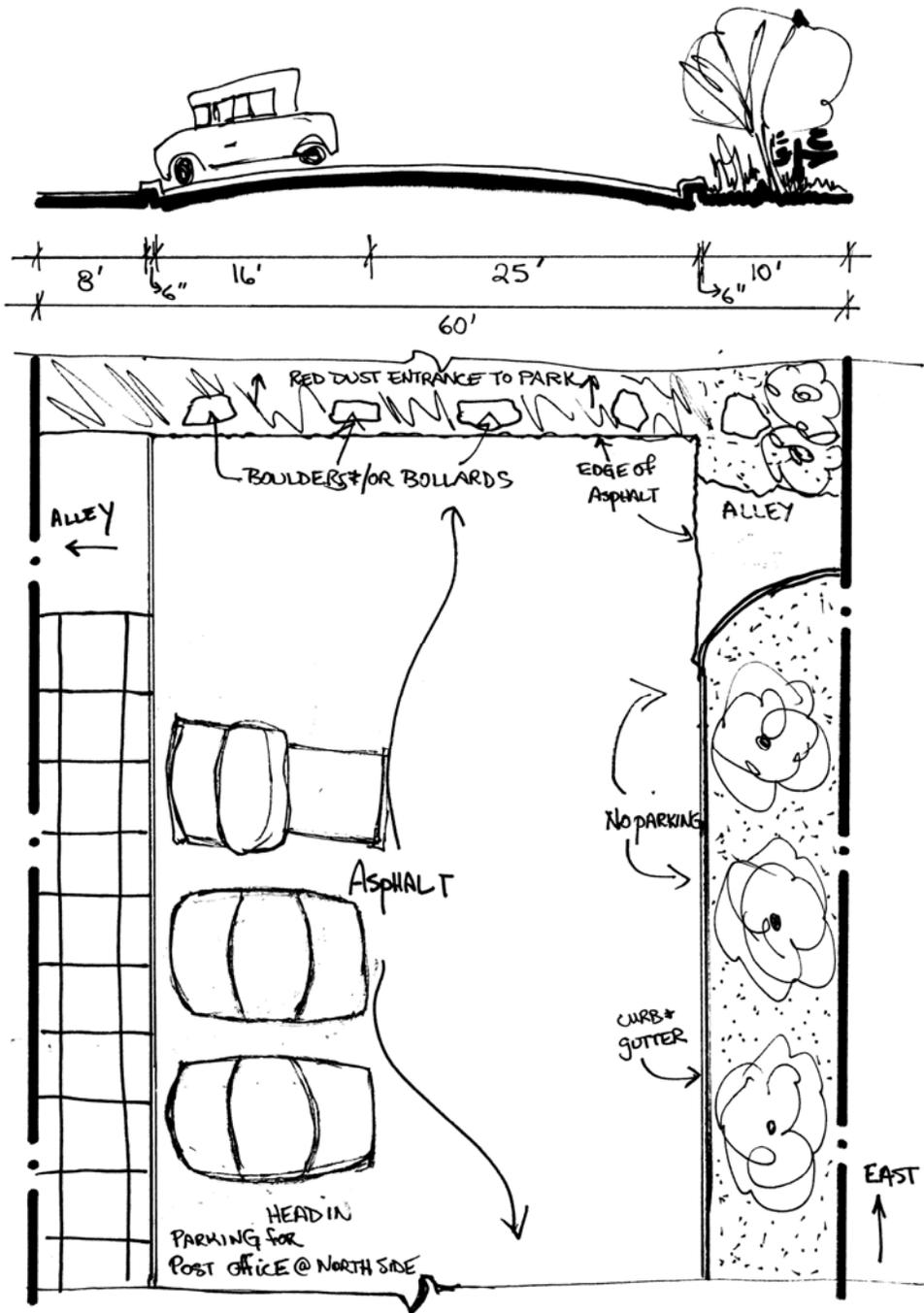
Traffic Circulation. Although this is a dead end, traffic volumes are high, due to vehicles accessing the head in short term parking for Post Office patrons. Access into and out of these head in parking spaces requires some caution, but the capacity is adequate for the intensity of use. Emergency vehicle access also appears adequate.

Utilities. There are no utility concerns at this time.

RECOMMENDED IMPROVEMENTS

1. Replace the existing bollards with a sturdier alternative.

Refer to Streetscape Warehouse District, 2005, Engineered Street Designs and adjacent street profiles for additional information.



EAST PACIFIC AVENUE - 300 BLOCK
(BETWEEN WILLOW STREET + SAN MIGUEL RIVER)

EAST PANDORA AVENUE - 400 BLOCK

50' Right of Way

Neighborhood Meetings: July 16, 1996; December 30, 1998

BACKGROUND

Existing Street Conditions. This block is fairly flat along its west half and slopes east along its east half. At its intersection with Alder Street, it slopes steeply to west and south. Surfaced with chip seal, the travel way is narrow, but in fair condition. South of the travel way, the right of way slopes down steeply. This slope is interrupted at the west by a parking area and near the center by a small parking platform. On the north, there is space for diagonal parking on the west end, one head in space between trees, and then more diagonal parking on the east. A north/south concrete drain pan at the west end carries drainage south along Alder Street. There is a drain pan at the southeast corner of the block, but gravel obscures how far west the pan extends. There is no curb and gutter or sidewalk.

Environment/Drainage/Snow Storage. This block mainly drains east and then south down Maple Street. The Alder Street drain pan effectively collected any drainage to the west. The diagonal drain pan near the center of the block that directs flow southeast toward Maple Street is not effective, largely because the block is flat near the center. Creating a cross-slope to the south would better direct runoff. The drain pan at the southeast corner needs cleaning to improve drainage. Depending on the length of this pan, it may be practical to install an active drainage system along the south edge of the travel way. In addition, paving the steep west portion with asphalt may improve drainage efficiency and quality. South side snow storage is consistent with Telluride's snow storage plan.

Historic Character. As it exists, this block contributes to the "rough and natural" historic character of the Town. There are three contributing structures and a contributing shed on the south side of the block, but are oriented to Columbia Avenue. Any right of way improvements should not negatively affect these historic resources.

Interfacing Streets. Access to this block is mainly from the south downhill side, and because of this, vehicles turning onto the street from Alder tend to deposit street surfacing behind them into the Alder Street drain pan. To prevent this, the westernmost section of the block should be surfaced in asphalt.

Lighting. There is no street lighting on this block and none is recommended.

Parking. Residents create moderate demand for parking. Parking consists of diagonal parking on the north side of the block, interrupted by landscape areas containing large trees and a single access to a private garage. The south side contains a flat parking area at the west end and a parking platform for two cars near the middle. There is parallel parking on the south side from April 1 to Oct 31, and none in winter to allow for snow storage. Due to the relatively narrow width of the block, and the need for an 18'-wide travel way for emergency vehicles, it may be necessary to eliminate parallel parking on the south side of this block.

Pedestrian Concerns. Because there pedestrian and vehicle traffic on this block is low--confined mainly to local residents--no sidewalks are needed. Handicap access to this block is difficult.

Traffic Circulation. Traffic volume is low and, therefore, despite the narrow travel way width, capacity appears adequate for intensity of use and emergency access. This is due to the north side parking configuration, which keeps parked vehicles out of the travel way. Parallel parking on the south side from spring through fall may make the travel way too narrow for effective traffic circulation, which should be considered when this block is improved.

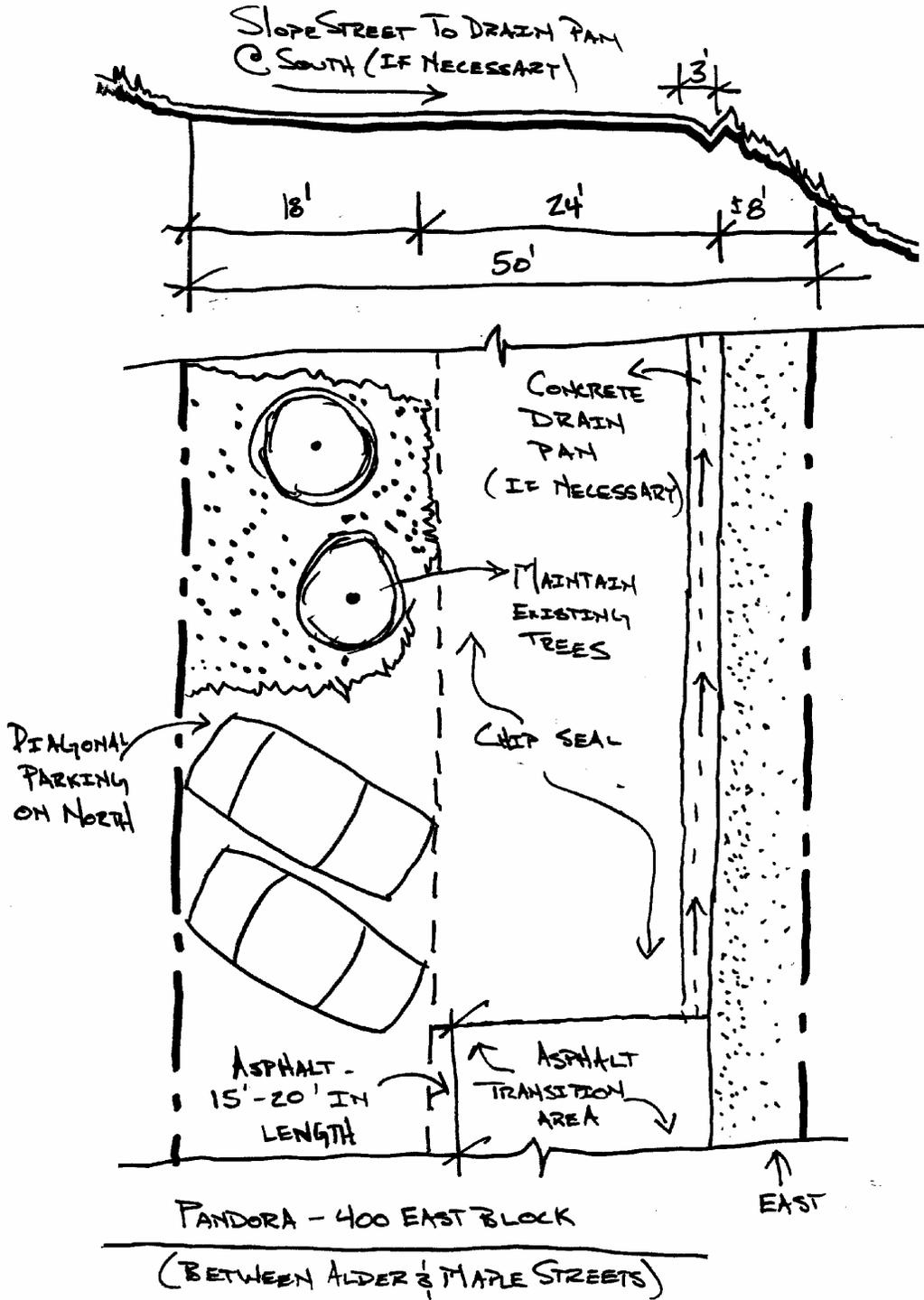
Street Furniture and Public Art. No street furniture or amenities currently exist and none are recommended.

Utilities. If possible, electricity transformer boxes should be located on private property. If they are located within the right of way, they should be as unobtrusive as possible and protected from impact by concrete bollards.

RECOMMENDED IMPROVEMENTS

1. Surface the block with asphalt and grade the block so the majority of the drainage is directed to the south and east. (Drainage from the westernmost section of the block should flow to the southwest and onto Alder Street.)
2. Clean out the drain pan at the southeast corner of this block. Determine how far west this drain pan extends and how effectively it handles drainage. If necessary, install a drain pan along the south edge of the travel way.
3. Maintain the existing diagonal parking on the north side of the block, but provide at least an 18' travel way to allow for emergency vehicle access. This may require the elimination of parallel parking on the south side.

Refer to Streetscape East Telluride District, 2006, Engineered Street Designs and adjacent street profiles for additional information.



EAST PANDORA AVENUE – 500 BLOCK

50' Right of Way

Neighborhood Meetings: July 16, 1996; December 30, 1998

BACKGROUND

Existing Street Conditions. This block is unique because it is two one-way streets divided by a green strip that slopes steeply to the south. The divided street begins at the east side of the Maple Street intersection and continues past where the unimproved Hemlock Street right of way would intersect. Both sections are surfaced with chip seal and slope west. The north (uphill) travel way is one-way to the west with a 3'-wide drain pan on the north side that flows south to an underground storm culvert at Maple Street. The south (downhill) travel way is one-way to the east and also has a 3'-wide drain pan on the north side. Neither drain pan functions well. The green strip dividing the travel ways is made of a boulder wall with interspersed native vegetation that prevents excessive slope erosion. Two concrete-filled pipe bollards at the west end of this green strip, and a single concrete filled pipe bollard defines the east end of the division. There are no sidewalks or curb and gutter. No parking is allowed in the right of way. Curb cuts at the northeast end of the block provide head in parking that may extend into the right of way.

Environment/Drainage/Snow Storage. Due to the light traffic on this block the street surface does not appear to negatively impact air and water quality. Runoff effectively drains east via drain pans along the north side of each of the travel ways. While the storm culvert appears to also be effective, the entire drainage system is partially filled with gravel and should be cleaned. Snow is plowed to the south. During a heavy snow, this practice could decrease the travel way width. This block is consistent with Telluride's snow storage and drainage plans.

Historic Character. As it exists, this block contributes to the "rough and natural" character of Town. There are three non-rated structures on the south side of this block that are oriented toward Columbia Avenue. Any alteration to the right of way should not negatively affect the historic resources on this block.

Interfacing Streets. Maple Street functions well with this block.

Lighting. There is currently no street lighting on this block and none is required.

Pedestrian Concerns. There is low pedestrian traffic on this block, confined mainly to local residents. Due to low vehicular and pedestrian traffic there is no need for a sidewalk. Handicap access is difficult due to the topography.

Parking. Parking is not currently allowed on this block at any time. Due to the unique and restrictive profile, and the lack of residences on the block parking is neither desired nor practical. The existing curb cuts on the northeast end should be maintained. The lots west of these curb cuts are developable and careful consideration should be given as to whether any on-street parking can be accommodated. Off-street diagonal parking is likely best for this area.

Street Furniture and Public Art. No street furniture or amenities currently exist and none are recommended.

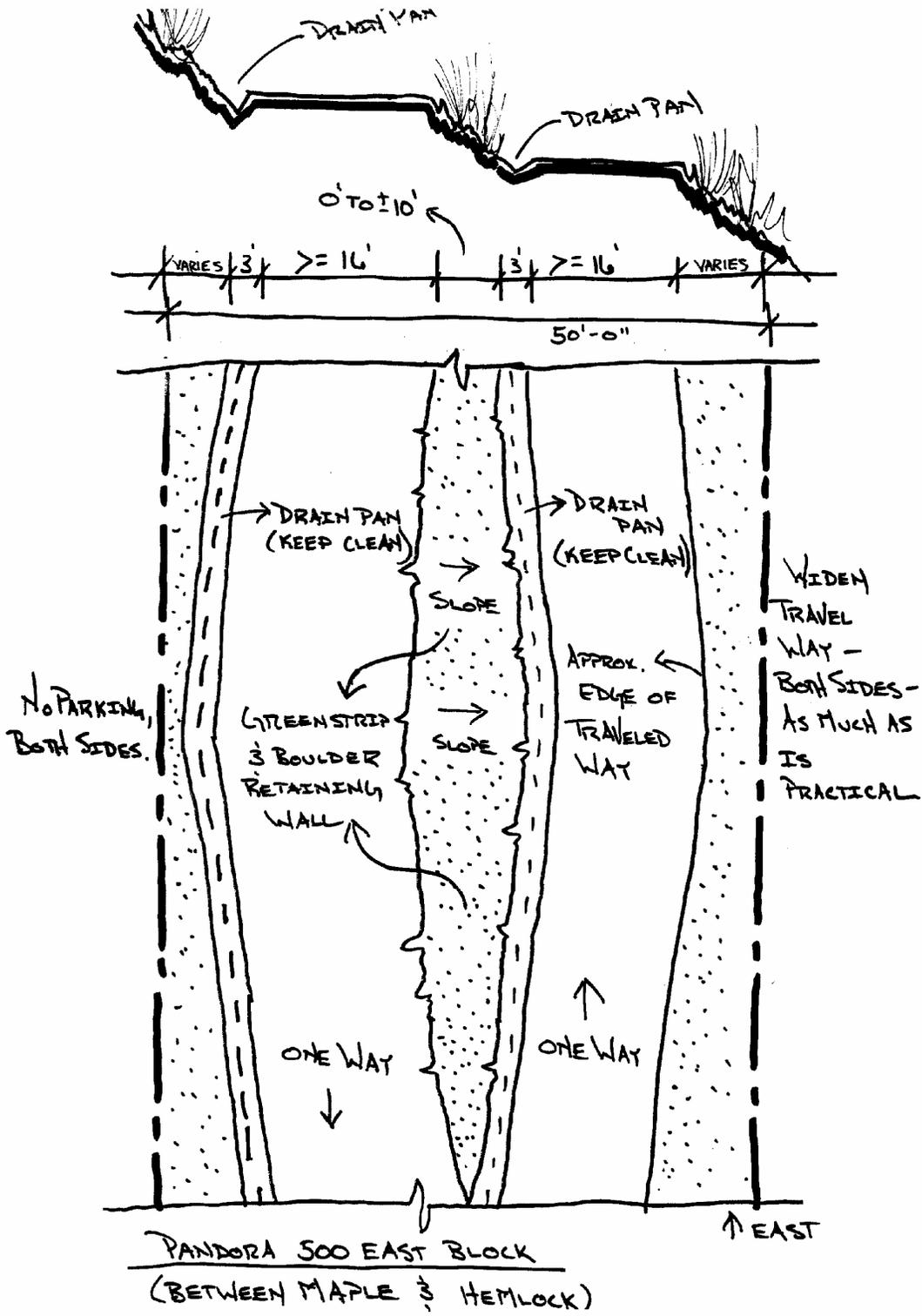
Traffic Circulation. The divided street allows for traffic to flow one-way east on the south and one-way west on the north side. Each of these street sections is narrow, but since they only handle one-way traffic, no on-street parking is allowed, and vehicular traffic is light, the current configuration appears adequate for intensity of use. However, this block may not be adequate for emergency vehicles, which should be considered when engineering proposed improvements. If street widening is undertaken, priority should be given to the south section of the block.

Utilities. There is no mention in meeting notes of utility concerns, though minimizing or eliminating above ground utilities is desired. If possible, electricity transformer boxes should be located on private property. If they are located within the right of way, they should be as inconspicuous and unobtrusive as possible.

RECOMMENDED IMPROVEMENTS

1. Clean existing drain pans of debris so they function efficiently.
2. Engineered plans should seek to maximize the width of both travel ways. If it is impractical to widen both travel ways, priority should be given to the south, one-way east travel way.

Refer to Streetscape East Telluride District, 2006, Engineered Street Designs and adjacent street profiles for additional information.



EAST PANDORA AVENUE – 600 BLOCK

*50' Right of Way
Neighborhood Meetings: July 15, 1996*

BACKGROUND

Existing Conditions. This west sloping block has not been formally improved. Nevertheless, it is currently paved with chip seal and has a 3'-wide drain pan along the northwest half of its north side. There are no formal green strips, but vegetation is growing along the edge of the travel way on both sides. The right of way narrows by 10' at this block.

Environment/Drainage/Snow Storage. This block appears to effectively handle runoff, although a good deal of sediment ends up on the travel way. Drain pans along the entire north and south sides would help define the travel way and better manage runoff in a manner consistent with Telluride's drainage plan. Winter snow is stored along both sides of this block, consistent with Telluride's snow storage plan.

Historic Character. As it exists, this block contributes to the "rough and natural" character of Town. There are several non-contributing historic structures.

Interfacing Streets. All interfacing streets appear to function well with this block.

Lighting. There is currently no street lighting on this block, which is consistent with the Streetscape Lighting Plan.

Parking. Parking demand on this block is light. Currently there is little or no parking allowed in the right of way due to the narrow travel way and steep slopes both north and south of that travel way. While maximizing use of existing off-street parking in this vicinity is therefore very important, some additional head in parking may be possible on the east end of this block adjacent to the head in parking at the west end of the 700 Block.

Pedestrian Concerns. The low pedestrian volume along this block is confined mainly to local residents. Due to low vehicular and pedestrian traffic there is no need for a sidewalk. Handicap access is difficult due to the topography.

Street Furniture and Public Art. No street furniture or amenities currently exist and none are required.

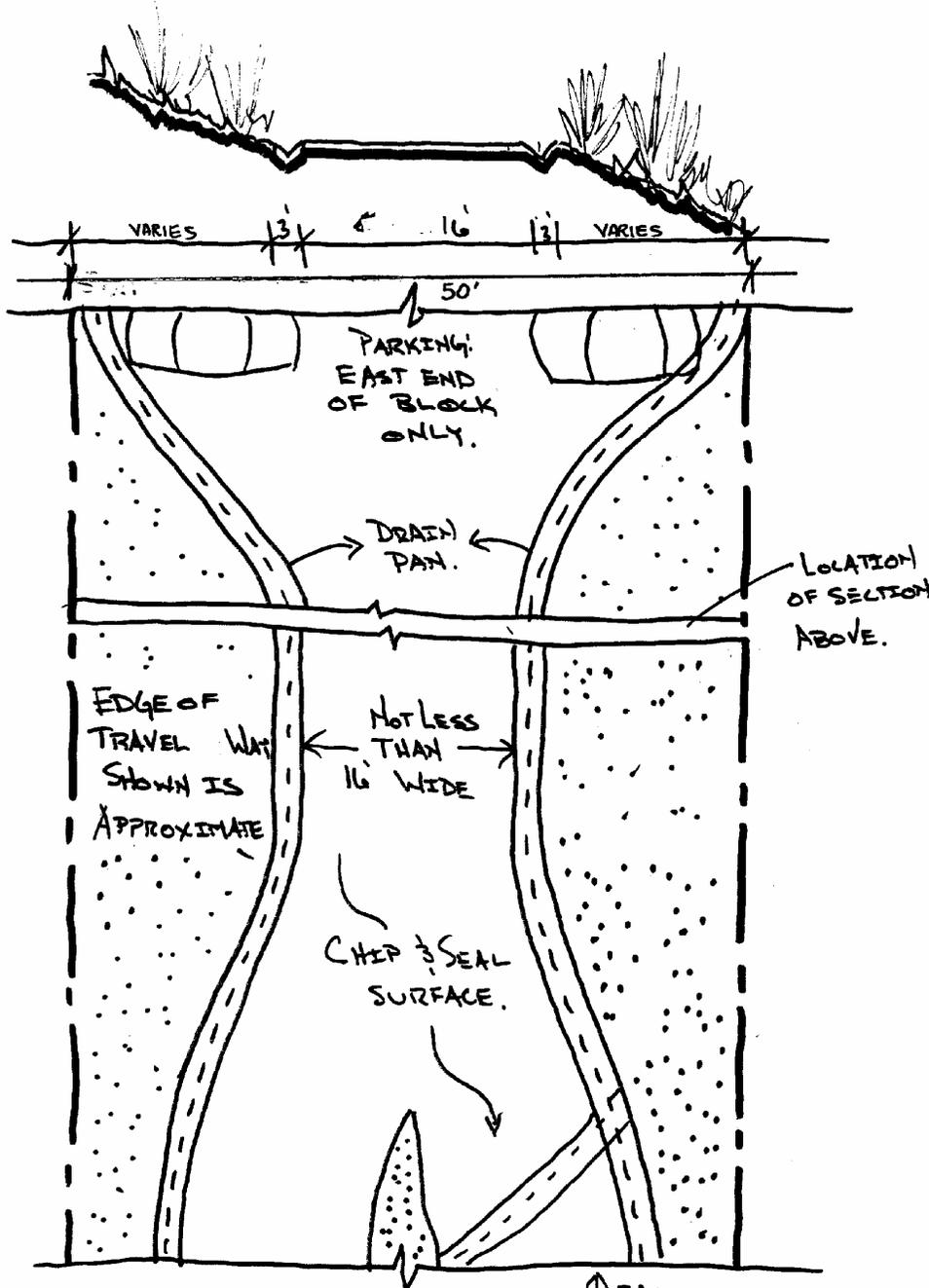
Traffic Circulation. This secondary street has low traffic intensity. Travel way width appears adequate for intensity of use and emergency vehicle access.

Utilities. Currently, there are no utility concerns for this block.

RECOMMENDED IMPROVEMENTS

1. Construct 3'-wide drain pans along the north and south edge of a 16'-wide travel way.
2. Potentially formalize additional head in parking at the east end of this block adjacent to the head in parking that is already in place at the west end of the 700 Block.

Refer to Streetscape East Telluride District, 2006, Engineered Street Designs and adjacent street profiles for additional information.



LOCATION OF SECTION ABOVE.

PANDORA - EAST 600 BLOCK
(BETWEEN HEMLOCK & PINON)

EAST PANDORA AVENUE – 700 BLOCK

60' Right of Way

Neighborhood Meetings: July 16, 1996

BACKGROUND

Existing Conditions. The west $\frac{3}{4}$ of this west sloping block was improved when new residences were constructed. The entire block is paved with asphalt. There is a 5'-wide sidewalk with curb and gutter along the north side and curb and gutter along the south side. All improvements are in good condition.

Environment/Drainage/Snow Storage. Air and water quality impacts from this block appear to be minimal. As well, the drainage system appears to effectively handle runoff volumes. Winter snow is stored along both sides. This block is consistent with Telluride's snow storage and drainage plans.

Historic Character. As it exists, this block neither contributes to nor detracts from the historic character of Town, as there are not historic structures in the vicinity. Its street improvements sharply contrast with all other blocks of Pandora Avenue and it does not contribute at all to the "rough and natural" character of Town.

Interfacing Streets. All interfacing streets appear to function well with this block.

Lighting. There is currently no street lighting on this block, which is consistent with the Streetscape Lighting Plan.

Parking. Parking demand on this block is light. Parallel parking is permitted along both the north and south sides, except in winter. There is a head in parking area at the west end of the north side that can service 4 vehicles. Maximizing use of off-street parking will ensure that sufficient on-street parking remains available.

Pedestrian Concerns. The low pedestrian volume along this block is confined mainly to local residents. Due to low vehicular and pedestrian traffic there is no need for a sidewalk. Nevertheless, the existing 5'-wide sidewalk does provide adequate capacity. There is handicap access.

Street Furniture and Public Art. No street furniture or amenities currently exist and none are required.

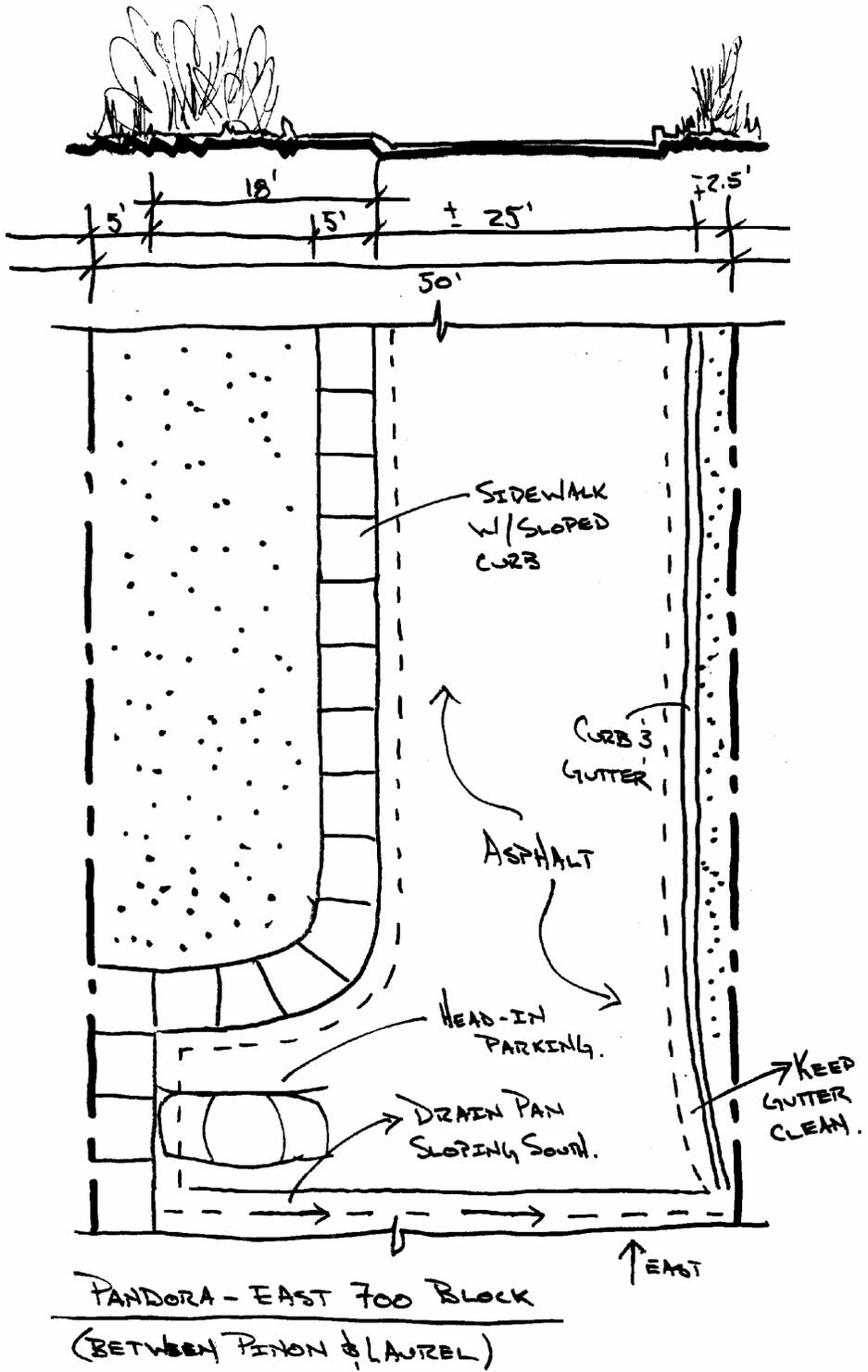
Traffic Circulation. This secondary street has low traffic intensity. Travel way width is definitely adequate for intensity of use and emergency vehicle access.

Utilities. Currently, there are no utility concerns for this block.

RECOMMENDED IMPROVEMENTS

No improvements are recommended at this time, although, when vacant lots develop, they should be required to make improvements that are consistent with the rest of the block.

Refer to adjacent street profiles for additional information. No Engineered Street Designs are available.



NORTH PINE STREET – 100 BLOCK

50' Right of Way

Neighborhood Meetings: July 10, 1996; February 24, 1997

BACKGROUND

Existing Street Conditions. This block was improved as part of Streetscape Phase I 1998 construction. Paved with asphalt, it slopes south. South of the intersecting alley, this block has curb and gutter and a 7.5'-wide sidewalk along its east side and curb and a 5'-wide drain pan and a 7.5'-wide sidewalk along its west side. North of the intersecting alley, there is an curb and gutter on the east side, a 3'-wide drain pan on the west, and 5'-wide sidewalks with 2.5'-wide green strips on the property side along both sides to accommodate encroachments. The alley apron on the west side slopes steeply from the alley to the travel way and is difficult for pedestrians to negotiate, especially in winter. A roof, extending from the building at the southwest section of this block, covers the sidewalk and the Free Box. The wider drain pan adjacent to the curb in this location collects drainage and snow/ice fall from the roof. The sidewalks, curb and gutter, drain pan, and travel way are in good condition.

Environment/Drainage/Snow Storage. This block does not appear to negatively impact air and water quality. The existing drainage system effectively handles runoff volumes. Snow is temporarily stored along the west side. In its current condition, this block is consistent with Telluride's snow storage and drainage plans.

Historic Character. As it exists, this block neither contributes to nor detracts from the historic character of the area.

Interfacing Streets. All interfacing streets appear to work well with this block.

Lighting. The single pole-mounted residential light fixture at the northeast corner of the block should be replaced with a fixture that is consistent with the Streetscape Lighting Plan.

Parking. Parking pressure is high because this block is directly adjacent to the commercial district. Paid parallel parking is permitted on both sides of the block, except along the south half of the west side where there is a free 20-minute limit. In winter, parking is prohibited along the west side to allow snow storage and to protect vehicles from snow and ice falling off of the overhanging roof. Although parking demand is high, the current parking configuration should be retained.

Pedestrian Concerns. Pedestrian volume on this block is moderate to high. The existing sidewalks effectively handle this traffic and allow for handicap access. However, the alley apron on the west side slopes too steeply down from the west to the travel way, creating an icy hazard for pedestrians in winter.

Street Furniture and Public Art. The Free Box mural is located on the building at the southwest corner of the block. This should be retained. Also, the amenity of the Free Box is within the right of way. If used appropriately the Free Box is an asset to the community; however, the Free Box becomes a liability when large furniture or appliances are left so that they block the sidewalk.

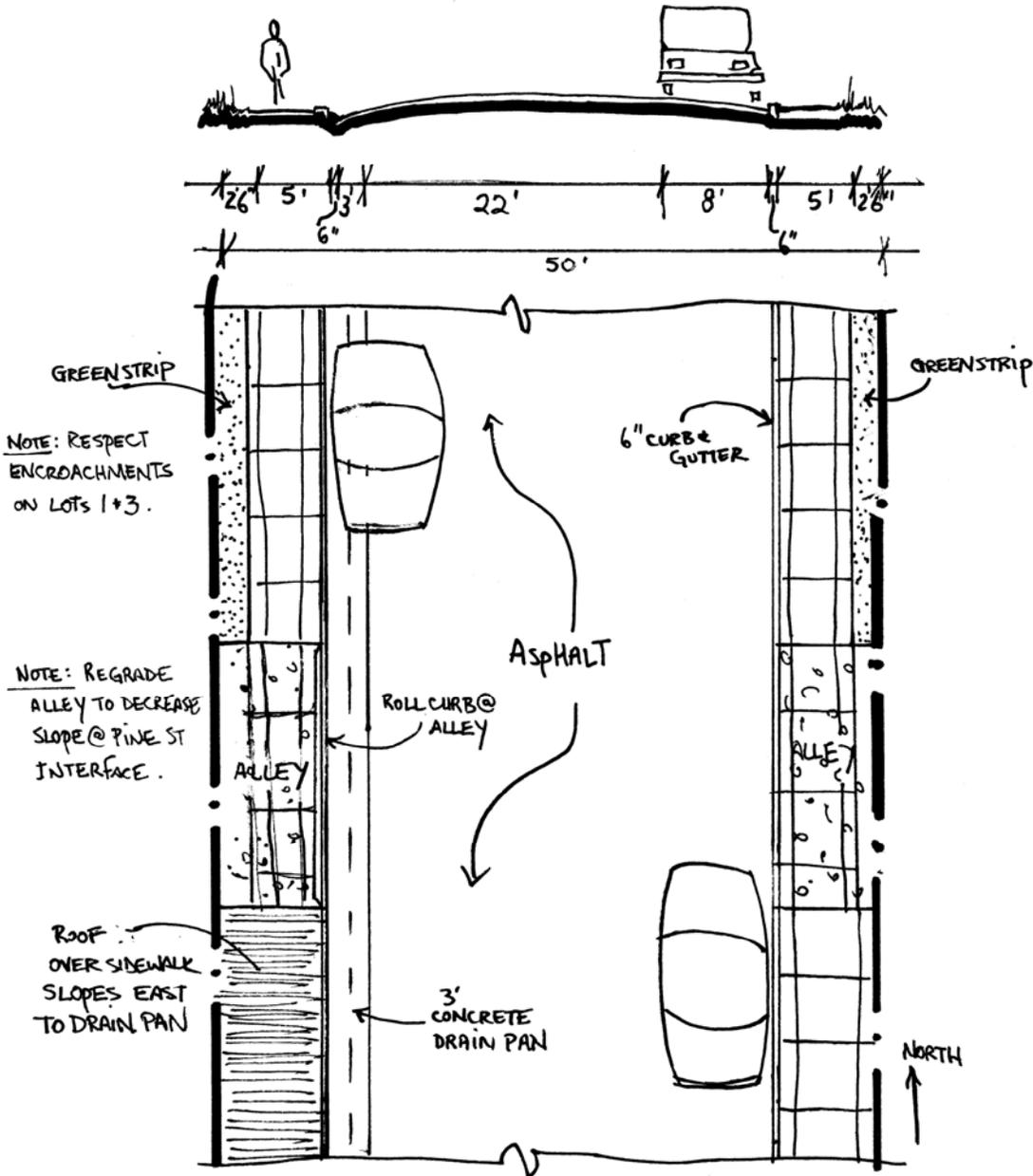
Traffic Circulation. Vehicular traffic on this block is moderate to high. Travel way width appears adequate for intensity of use and emergency vehicle access; although, it is sometimes tight when all of the parking is used.

Utilities. There are no utility concerns for this block.

RECOMMENDED IMPROVEMENTS

1. Improve the west alley intersection by lowering the alley elevation and changing the apron design so that the sidewalk is easier for pedestrians to negotiate.

Refer to Streetscape Phase I, 1998, Engineered Street Designs and adjacent street profiles for additional information.



NORTH PINE STREET - 100 BLOCK
(BETWEEN COLORADO & COLUMBIA AVENUES)

NORTH PINE STREET – 200 BLOCK

50' Right of Way

Neighborhood Meetings: July 10, 1996; February 24, 1997

BACKGROUND

Existing Street Conditions. This south sloping block was improved as part of Streetscape Phase I 1998 construction. Paved with asphalt, there are 5'-wide sidewalks and a 3'-wide green strips between the sidewalk and the curb and gutter along the east and west sides. The west sidewalk is substantially elevated above the travel way so that at 3 locations, concrete steps have been constructed within the green strip to provide safe access from the sidewalk to the travel way. There is a 3'-wide drain pan along the curb on the north 2/3 of the west side. A section of west side sidewalk at the KOTO curb cut and gravel fills the area instead. No alleys intersect this block. Except for the missing section of sidewalk, the sidewalks, curb and gutter, drain pan, green strips and travel way are in good condition.

Environment/Drainage/Snow Storage. Air and water quality impacts from this block are minimal. The existing drainage system effectively handles runoff. Snow is stored along the west side. In its current condition, this block is consistent with Telluride's snow storage and drainage plans.

Historic Character. As it exists this block neither contributes to nor detracts from the historic character of the area. There are contributing structures on the northwest and southwest corners. Any improvements to the right of way should not negatively impact these historic resources.

Interfacing Streets. All interfacing streets appear to function well.

Lighting. There are no streetlights on this block and none are required.

Parking. Intensity of on-street parking is moderate to high and consists of neighborhood residents and visitors to neighboring commercial buildings. Parallel parking is permitted on both sides of this block, except in winter when parking is prohibited on the east to allow snow storage. The current parking configuration should be maintained and use of existing off-street parking should be maximized.

Pedestrian Concerns. Existing sidewalks appear to effectively handle the moderate pedestrian traffic along this block. One problem spot is at KOTO's head in parking where vehicles tend to encroach into the sidewalk partly because no sidewalk is delineated. Construction of a sidewalk at this location could improve this situation. This block has handicap access.

Street Furniture and Public Art. No street furniture or amenities currently exist within the right of way. None are recommended.

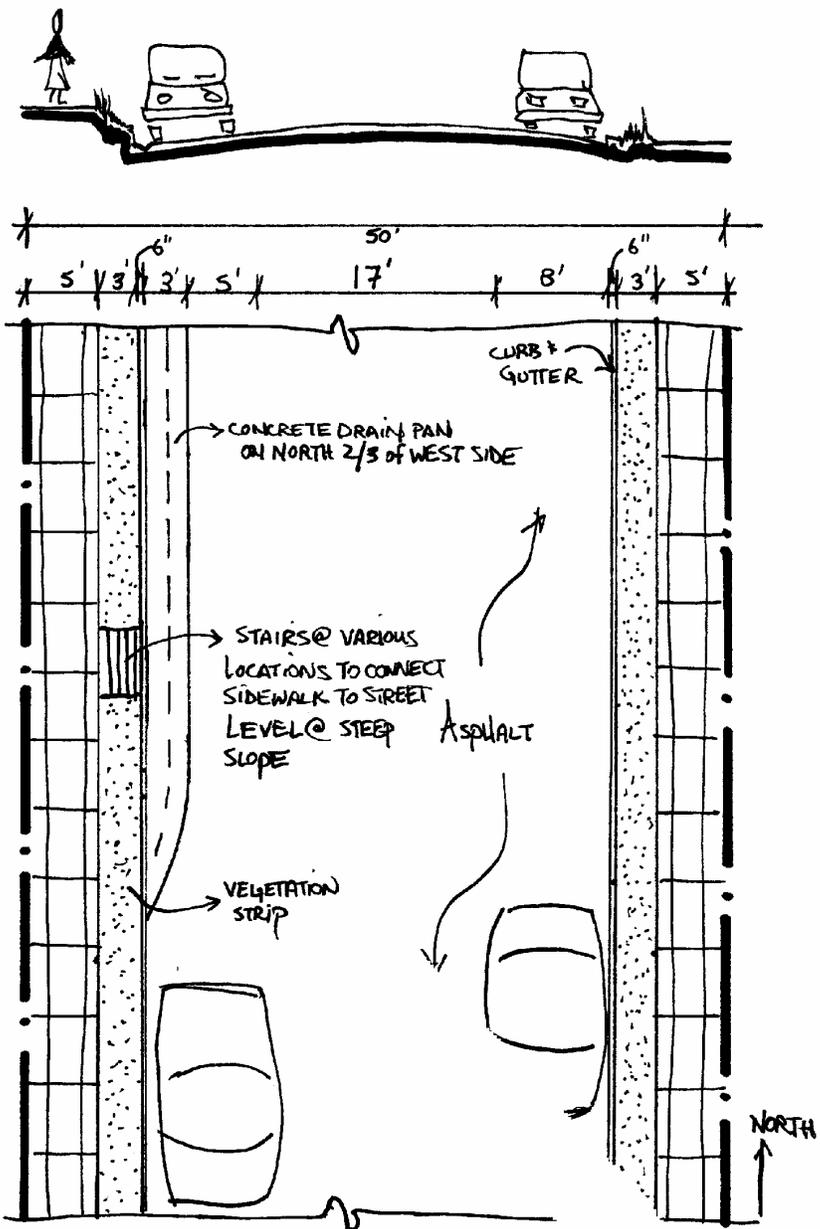
Traffic Circulation. Travel width appears adequate for the moderate amount of vehicle traffic using this block. Emergency vehicle access also appears adequate.

Utilities. There are currently no utility concerns for this block.

RECOMMENDED IMPROVEMENTS

- 1 Construct the missing section of 5'-wide sidewalk at the KOTO curb cut along the southwest quarter to discourage vehicles parked in the KOTO radio station parking area from blocking the sidewalk lane. Due to the steep slope, a roll curb is recommended for the driveway cut.

Refer to Streetscape Phase I, 1998, Engineered Street Designs and adjacent street profiles for additional information.



NORTH PINE STREET - ZOO BLOCK
(BETWEEN COLUMBIA & GALENA)

NORTH PINE STREET – 300 BLOCK

50' Right of Way

Neighborhood Meetings: July 10, 2996; February 24, 1997

BACKGROUND

Existing Street Conditions. This south sloping block was improved as part of Streetscape Phase I 1998 construction. Paved with asphalt, there is a 5'-wide concrete sidewalk with a 3'-wide green strip and curb and gutter along the west side. This sidewalk is in poor condition, except where it is new at the north end. The green strip is also poorly maintained. The curb and gutter, however, are in good condition. A 3'-wide drain pan and a 7.5'-wide green strip border the travel way along the east side. This green strip is used for head in parking for 2 to 3 cars about halfway up the block.

Environment/Drainage/Snow Storage. Impacts to air and water quality are minimal. The existing curb and gutter on the west side of the block effectively handles runoff. However, the east drain pan is sometimes ineffective because vehicle wheels block runoff flow. Improvements, such as graveling or paving 2.5' to the east might encourage parking out of the pan. Snow is stored along the east side of this block. This block is consistent with Telluride's snow storage and drainage plans.

Historic Character. This block neither detracts from nor contributes to the historic character of the area. There is a contributing structure at the far north end at the Gregory Avenue intersection. Any future alteration of the right of way should not negatively impact this historic resource.

Interfacing Streets. All interfacing streets appear to work well with this block.

Lighting. The single pole-mounted residential light fixture at the southwest corner of this block should be replaced with a fixture that is consistent with the Streetscape Lighting Plan.

Parking. Intensity of on-street parking is moderate and consists of neighborhood residents and, to some extent, visitors to neighboring commercial buildings. Parallel parking is permitted on both sides of the block, except in winter when east side parking is prohibited to allow snow storage. Residents at 330 North Pine currently use the east side green strip for head in parking. When this lot redevelops, one parking spot should be relocated onto the private lot and the second spot should revert to on-street parking so that the green strip can be vegetated. Otherwise, maintain the current parking configuration and maximize creation and use of off-street parking.

Pedestrian Concerns. Existing sidewalks cannot adequately handle the moderate to low pedestrian volumes along this block. The west side sidewalk that is in poor condition should be replaced. A sidewalk is neither desirable nor necessary on the east side of the street. Handicap access to this block is inadequate.

Street Furniture and Public Art. No street furniture or amenities currently exist on this block. None are recommended.

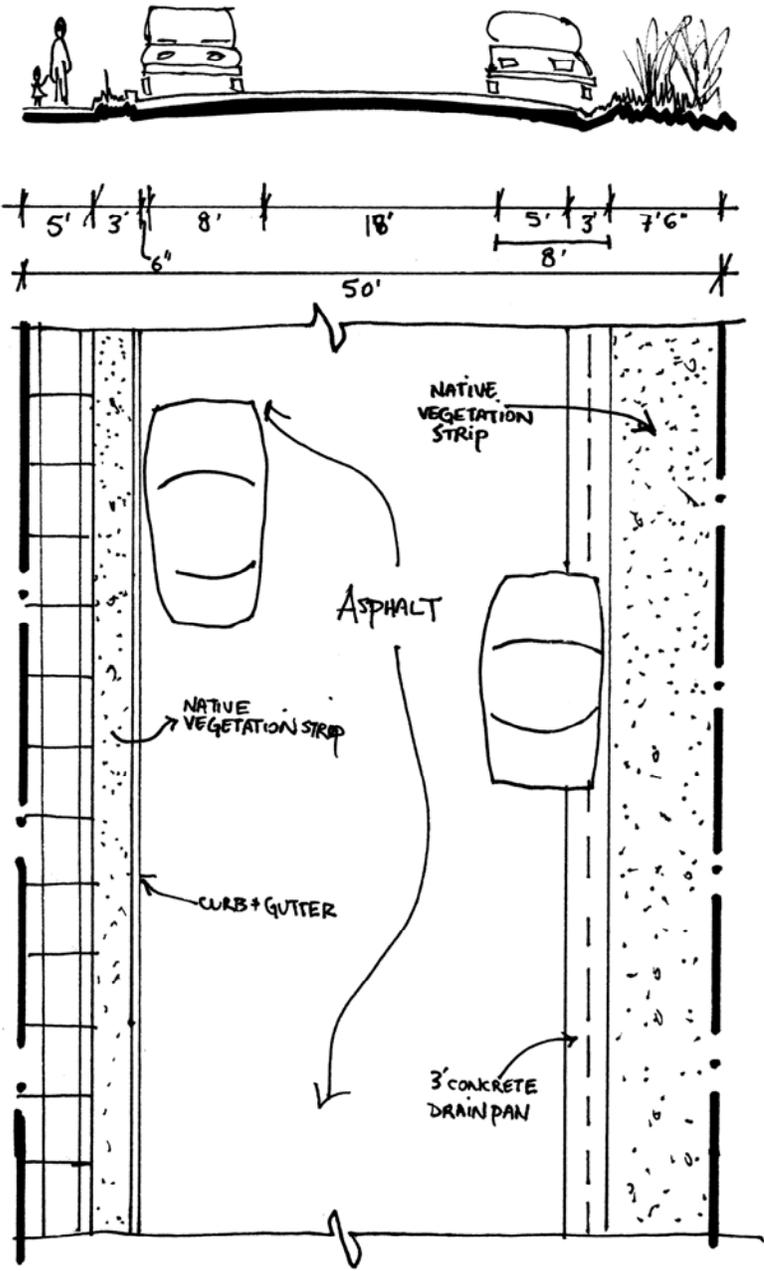
Traffic Circulation. Travel way width appears adequate to handle the low to moderate vehicle traffic on this block. Emergency vehicle access also appears adequate.

Utilities. There are currently no utility concerns for this block.

RECOMMENDED IMPROVEMENTS

1. Repair or replace the existing sidewalks on the west side.
2. Replace the pole-mounted residential light fixture at the southwest corner with a fixture that is consistent with the Streetscape Lighting Plan.

Refer to Streetscape Phase I, 1998, Engineered Street Designs and adjacent street profiles for additional information.



NORTH PINE STREET - 300 BLOCK
(BETWEEN GALENA & GREGORY)

SOUTH PINE STREET – 100 BLOCK

50' Right of Way

NOTE: This block is not part of the Streetscape Project; however it has been improved with asphalt, and sidewalks and curb and gutter on its east and west sides. All infrastructure is in good condition. The Plan and Profile for this block is identical to the South Pine Street 200 block.

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SOUTH PINE STREET – 200 BLOCK

50' Right of Way

Neighborhood Meetings: July 23, 1996; April 1, 1998; May 27, 1998; November 17, 1998

BACKGROUND

Existing Street Conditions. This south sloping block between Pacific and West San Juan avenues, is only partially improved as a result of adjacent properties re(developing). It is paved with asphalt, which is in poor condition along its south half. The Wilkinson Public Library abuts the northwest half of this block, while Smuggler Restaurant abuts the southwest quarter. Each has an 8'-wide sidewalk with curb and gutter that is in good condition. A section of sidewalk is still needed along the undeveloped lot between them. Because of street grades, there is a 3'-wide drain pan that extends away from the west curb and gutter mid-block and rejoins it a short distance to the south. There is a narrow, intermittent sidewalk and intermittent curb and gutter along the east side. There are no green strips; although the Library has created several planting areas as part of its building.

Environment/Drainage/Snow Storage. Impacts to air and water quality from this block are minimal. There are sometimes drainage problems at the West San Juan interface, especially where the curb and gutter and drain pans are heaving along the east side because of sidewalk heating. Snow is stored along the west side of this block, which is consistent with Telluride's snow storage plan. Improvements are needed to be consistent with Telluride's drainage plan.

Historic Character. There are two contributing structures along this block, however, the right of way neither detracts from nor contributes to the historic character of the area. Any alterations to the right of way should not negatively impact these historic resources.

Interfacing Streets. While the Pacific Street interface functions well, drainage problems at the West San Juan interface should be addressed.

Lighting. The standard commercial streetlight at the northwest corner of this block should remain, as it is consistent with the Streetscape Lighting Plan. The commercial streetlight that is stubbed at the east side alley should be activated rather than awaiting development of the adjacent lot.

Parking. Intensity of on-street parking is very heavy, consisting of neighborhood residents and patrons of nearby businesses. Parallel parking is permitted on both sides of the block, except in winter when west side parking is prohibited to allow snow storage. The current parking configuration should be maintained, use existing off-street parking should be maximized, and creation of new off-street parking when vacant lots develop is essential.

Pedestrian Concerns. Existing sidewalks do not adequately handle the moderate pedestrian volumes along this block, which links nearby businesses and residences with the Gondola Plaza, the Post Office, and Town Park. The missing section of sidewalk along the west side should be installed upon site development or sooner, if possible, to discourage pedestrians from walking in the street where conflicts with motor vehicles are likely. Handicap access to this block needs improvement.

Street Furniture and Public Art. No street furniture or amenities currently exist on this block and none are required.

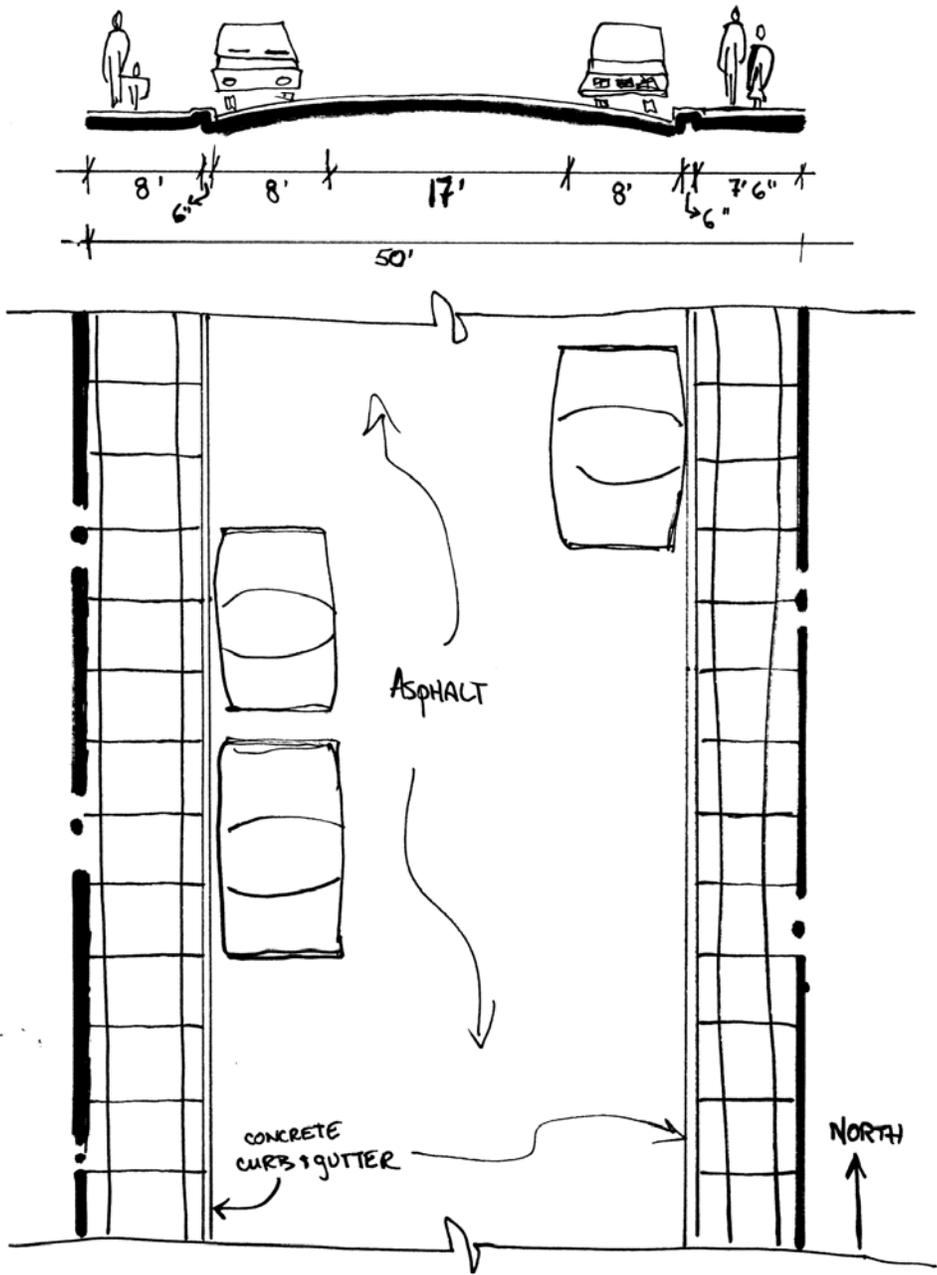
Traffic Circulation. This block makes up one section of the Galloping Goose Town Loop. Travel way width appears just adequate to handle the moderate vehicle traffic on this block. Emergency vehicle access appears adequate.

Utilities. There are currently no utility concerns for this block.

RECOMMENDED IMPROVEMENTS

1. Maintain the alignment of the curb and gutter to the north 100 Block of South Pine.
2. Construct a 7.5'-wide sidewalk along the east side.
3. Construct the missing 8'-wide sidewalk section on the west side.
4. Improve the existing drainage.
5. Crown and re-asphalt the travel way.
6. Try to activate the standard commercial streetlight that is mid-block at the east side alley.

Refer to Streetscape Warehouse District, 2005, Engineered Street Designs and adjacent street profiles for additional information.



SOUTH PINE STREET - 200 BLOCK
(BETWEEN PACIFIC & WEST SAN JUAN AVENUES)

SOUTH PINE STREET – 300 BLOCK

50' Right of Way

Neighborhood Meetings: July 23, 1996; April 1, 1998; May 27, 1998; November 17, 1998

BACKGROUND

Existing Street Conditions. This south sloping block, between West San Juan Avenue and the San Miguel River, is paved with asphalt that is in fair condition. There is a 5'-wide sidewalk with curb and gutter adjacent to the Ice House Lodge, which stops north of Pine Street Bridge. There is a 7.5'-wide sidewalk along the east side that at the curb cut for Ghost Riders Condominiums, north of the River Trail. There are no green strips; although the southwest half of the block abuts the Ice House Pond and the southeast corner abuts the River Park Corridor.

Environment/Drainage/Snow Storage. Impacts to air and water quality from this block are minimal. As this block discharges collected runoff from numerous Telluride streets into the San Miguel River via the Ice House Pond, a pre-treatment system is desirable. Also, runoff from Pine Street Bridge that is currently eroding the slope down to the Pond should be directed, if possible, into the pre-treatment system. Curb and gutter is needed along the southeast section of this block and a drain pan should be installed to connect it to the Bridge. Drainage can be problematic at the West San Juan interface, especially where the curb and gutter and drain pans are heaving along the east side because of sidewalk heating. Snow is stored along the west side of this block, which is consistent with Telluride's snow storage plan. Improvements are needed for this block to be consistent with Telluride's drainage plan.

Historic Character. This block neither detracts from nor contributes to Telluride's historic character. There are no historic structures.

Interfacing Streets. This block's intersection with West San Juan Avenue functions well from a traffic and pedestrian standpoint. Drainage problems at the West San Juan interface should be addressed.

Lighting. There is no street lighting along this block. If necessary a standard commercial streetlight could be installed adjacent to Lot 1A, Block 22. Any street lighting installations should avoid negative impacts to the Ice House Pond and River Corridor habitats.

Parking. Intensity of on-street parking is moderate, consisting mostly of folks that are hiking Bear Creek Trail or the River Trail. Parallel parking is permitted on both sides of the block, except in winter when west side parking is prohibited to allow snow storage. The current parking configuration should be maintained and use existing off-street parking should be maximized.

Pedestrian Concerns. Existing sidewalks do not adequately handle the light to moderate pedestrian traffic along this block, which links the Bear Creek Trail and the River Trail to the commercial core. The east sidewalk should be extended to connect with the sidewalk along the east side of Pine Street Bridge. The west sidewalk should link up with the River Trail via a similar trail. Handicap access is adequate.

Street Furniture and Public Art. No street furniture or amenities currently exist on this block. None are recommended.

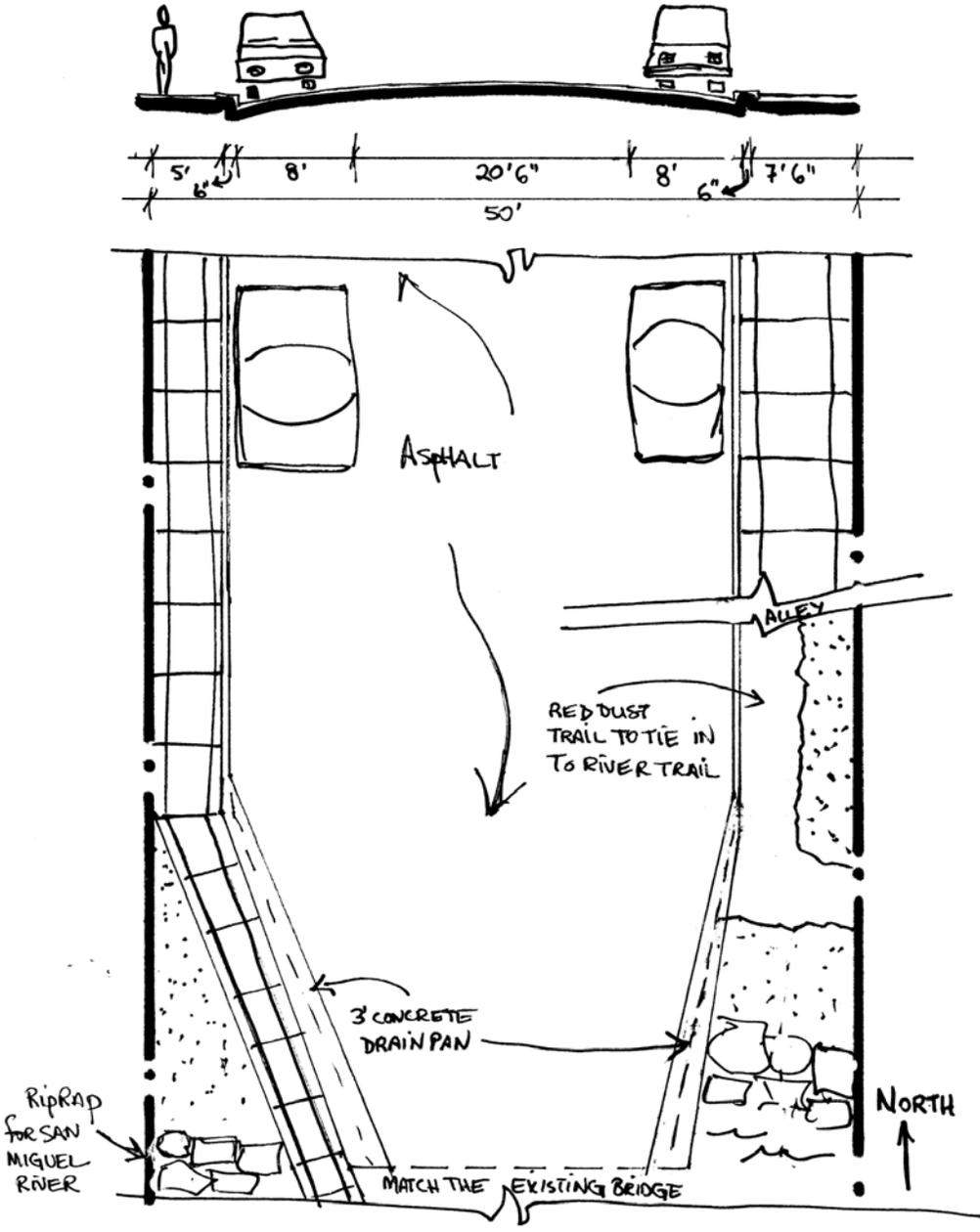
Traffic Circulation. Travel way width appears adequate to handle the moderate vehicle traffic on this block. Emergency vehicle access appears adequate.

Utilities. There are currently no utility concerns for this block.

RECOMMENDED IMPROVEMENTS

1. Maintain the alignment of the curb and gutter to the north 200 Block of South Pine, and extending the curb and gutter to the edge of the River Park Corridor.
2. Construct a 4'-wide sidewalk along the west side to connect the existing sidewalk with the sidewalk on the Pine Street Bridge.
3. Connect the River Trail with the sidewalk on the east side with the simple red gravel path and improved landscaping.
4. Collect runoff from the Pine Street Bridge using 3'-wide concrete drain pans that drain north to the curb and gutter system.
5. Install a Stormwater Treatment System underneath the right of way to pre-treat runoff before it is discharged into the Ice House Pond.
6. Re-grade and re-pave the travel way.

Refer to Streetscape Warehouse District, 2005, Engineered Street Designs and adjacent street profiles for additional information.



SOUTH PINE STREET - 300 BLOCK
(BETWEEN WEST SAN JUAN AVENUE & BEAR CREEK TRAILHEAD)

NORTH PINON STREET – 100 BLOCK

*50' Right of Way
Neighborhood Meetings: July 15, 1996*

BACKGROUND

Existing Street Conditions. Surfaced with chip seal, this block slopes south, except at it's on the southernmost end, which slopes to the north. Shadow Lane intersects the block from the east, and an alley intersects it south of center due to an irregular lot layout on the northwest side of the block. On the north half of the block are gravel and stone swales that carry drainage south. There is curb and gutter along the south half of the block that directs runoff to drop inlets, one on the east and one on the west, just north of the Colorado Avenue intersection. The inlets discharge water to a willow car that borders the southeast quarter of the block. There is a 4.5'-wide sidewalk adjacent to the street south of Shadow Lane on the east with a wetland behind. A cottonwood tree is located in the center of the block, just north of the Colorado Avenue intersection. There is a 9'-wide green strip along the west side behind the curb and gutter and at the edge of the travel way. The east green strip north of Shadow Lane varies in width. All improvements are in good condition.

Environment/Drainage/Snow Storage. Air and water quality appear to be only minimally impacted by this block. The existing drainage system adequately handles runoff. Snow is stored along the west side. As it exists, and as it is proposed this block is consistent with Telluride's snow storage and drainage plans.

Historic Character. This block neither contributes to nor detracts from the historic character of the area.

Interfacing Streets. All interfacing streets appear to function well with this block.

Lighting. There are two standard commercial streetlights on this block. One is on the southeast corner and should remain. The second is located at the southeast intersection with Shadow Lane. This one should be removed to be consistent with the Streetscape Lighting Plan.

Neighborhood Concerns. The residents have expressed a desire to leave this block as it is currently. They have also expressed a strong concern to better manage drainage from the alley that intersects Pinon's west side.

Parking. Parking demand on this block is light, except during Town Park Events. Parallel parking is allowed on both sides of the block, except in winter when parking is prohibited on the west side to allow snow storage. The existing parking configuration should be maintained and use of existing off-street parking should be maximized to ensure on-street parking availability.

Pedestrian Concerns. Pedestrian traffic on this block is low, and attributed mainly to neighborhood residents. As the volume of both pedestrian and vehicle traffic is low, and no sidewalks are needed, it is suggested that the small section of sidewalk that does exist be removed or at least not replaced when it degrades. There is no special handicap access.

Street Furniture and Public Art. Aside from the cottonwood tree, which should remain, no street furniture or other amenities exist. None are recommended.

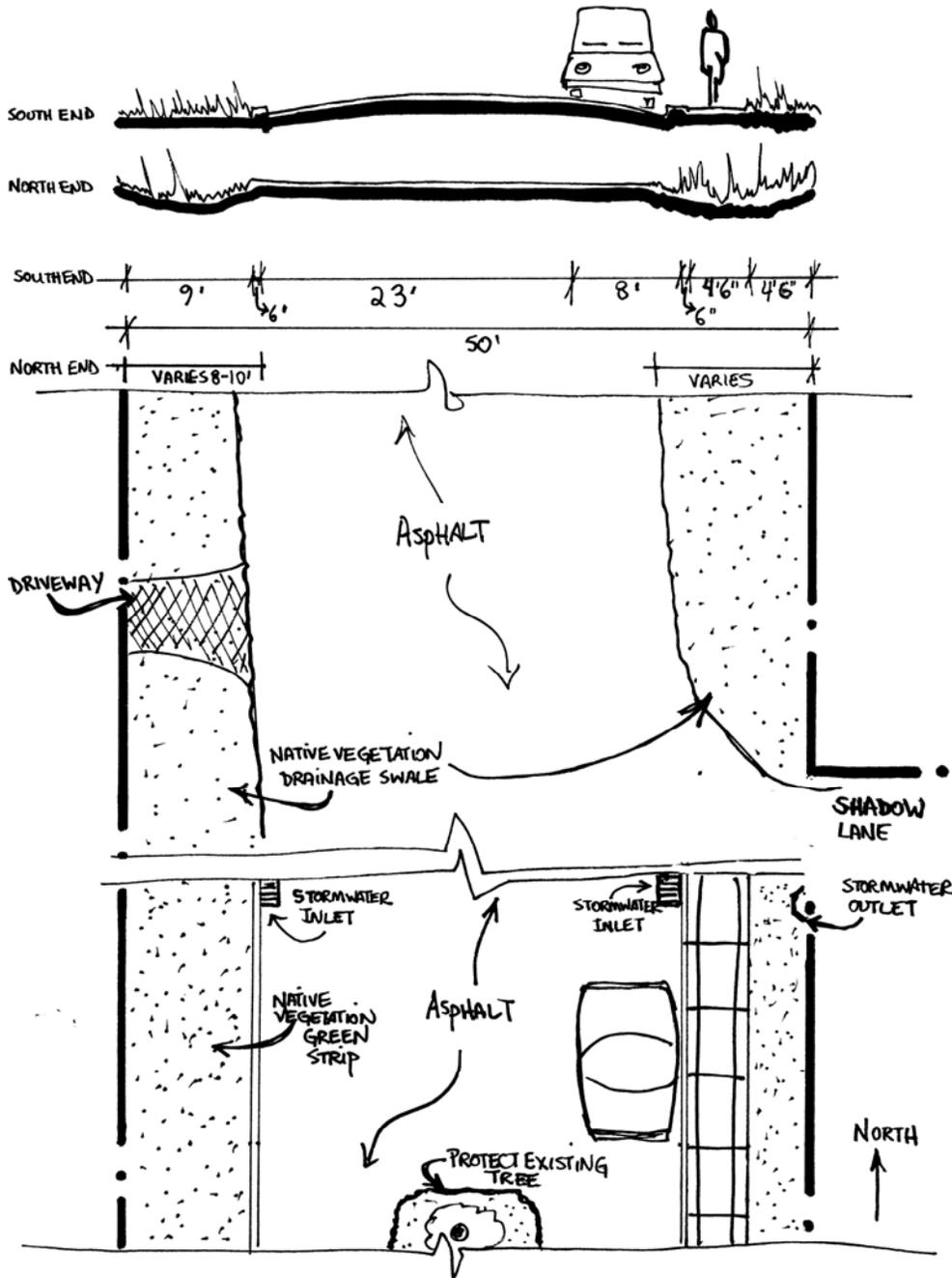
Traffic Circulation. Travel width is adequate along this secondary street with low traffic volumes. Emergency vehicle access is also adequate.

Utilities. There are currently no utility concerns for this block.

RECOMMENDED IMPROVEMENTS

1. The sidewalk section on the southeast half of this block should be removed, or should not be replaced when it degrades. This will give the adjacent wetland more buffer.
2. Remove the standard commercial streetlight that is at Shadow Lane to be consistent with the Streetscape Lighting Plan.
3. Make every reasonable effort to retain the existing Cottonwood tree within the right of way.

Refer to Streetscape East Telluride District, 2006, Engineered Street Designs and adjacent street profiles for additional information.



NORTH PINON STREET - 100 BLOCK
(BETWEEN COLORADO & COLUMBIA AVENUE)

WEST SAN JUAN AVENUE – 100 BLOCK

60' Right of Way

Neighborhood Meetings: July 23, 1996; March 18, 1998; May 27, 1998

BACKGROUND

Existing Street Conditions. This block connects the Gondola Plaza with local businesses, the Post Office, the Library, and Town Park. Heavily traveled, it is paved with asphalt and has a 12'-wide sidewalk with curb and gutter along the north side, and a 5.5'-wide sidewalk with curb and gutter along the south side. There are no green strips. A heavily used alley with a concrete apron bisects the north side. All infrastructure is recent and is in good condition.

Environment/Drainage/Snow Storage. Air and water quality appear to be only minimally impacted by this block. The existing drainage system adequately handles runoff. Snow is stored along the west side. As it exists, and as it is proposed this block is consistent with Telluride's snow storage and drainage plans.

Historic Character. There is one contributing structure on the northeast corner of this block. Otherwise, this block neither contributes to nor detracts from the historic character of the area.

Interfacing Streets. All interfacing streets appear to function well with this block.

Lighting. With four standard commercial streetlights on the north side, there is an overabundance of lighting along this block. There should only be a light at the northwest corner and halfway past the alley that bisects the north side. All others should be removed to be consistent with the Streetscape Lighting Plan.

Neighborhood Concerns. No concerns have been expressed by the neighborhood.

Pedestrian Concerns. Pedestrian volumes are moderate to heavy along this block because of the numerous businesses and its proximity to the River Trail and Gondola Plaza. Existing sidewalks adequately serve pedestrians. There are handicap ramps at each corner.

Parking. Parking demand is heavy on this block; therefore, paid parallel parking is permitting on both sides of the street all year. This parking configuration should be maintained. As well, off-street parking should be maximized.

Street Furniture and Public Art. No street furniture or amenities currently exist on this block and none are required.

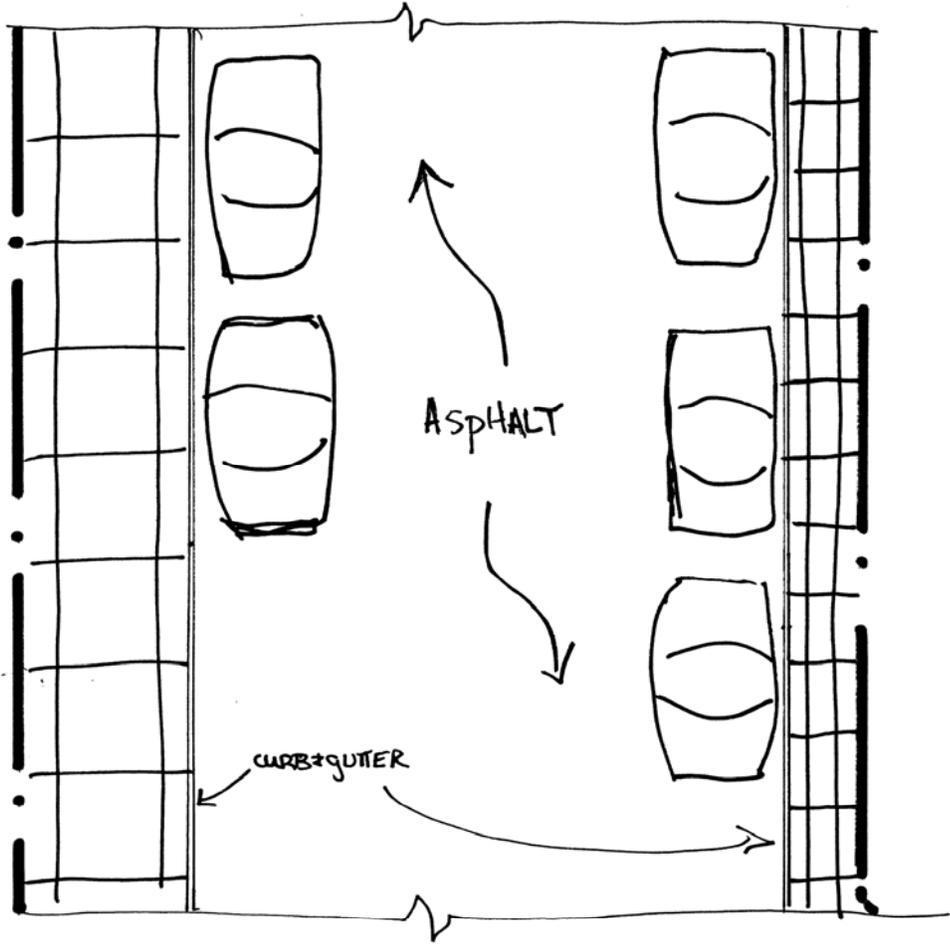
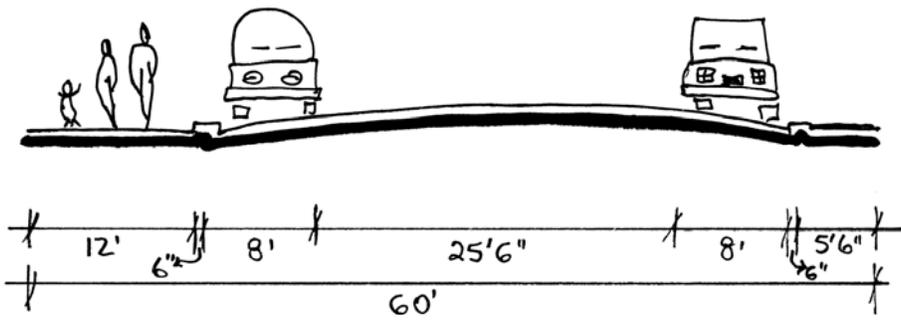
Traffic Circulation. This block is part of the Galloping Goose Town Loop. As well, vehicular traffic can be moderate to heavy. The current travel way width appears to adequately handle the vehicular traffic, business delivery trucks, and emergency vehicle access.

Utilities. There are no utility concerns on this block at this time.

RECOMMENDED IMPROVEMENTS

1. Remove the excess streetlights to be consistent with the Streetscape Lighting Plan.

Refer to Streetscape Warehouse District, 2005, Engineered Street Designs and adjacent street profiles for additional information.



WEST SAN JUAN AVENUE - 100 BLOCK
(BETWEEN FIR & PINE STREETS)

WEST SAN JUAN AVENUE – 200 BLOCK

60' Right of Way

Neighborhood Meetings: July 23, 1996; November 12, 1998

BACKGROUND

Existing Street Conditions. This block connects the Gondola Plaza with local businesses, the Post Office, the Library, and Town Park. It also makes up a section of the Town Bus Loop. A moderately used, paved alley with a concrete apron bisects the north side. This intersecting alley effectively divides two very different street treatments. All infrastructure is recent and is in good condition.

The west half, which has one-way traffic to the east, was improved as part of the Gondola Plaza Project. This west-sloping portion of the block is paved with asphalt, and has an 18.5'-wide sidewalk and curb and gutter on the south side. There is a 3'-wide concrete drain pan on the north side. There are benches and two public bicycle racks on the south side. There is a retaining wall that encroaches 12' into the right of way on the north. This half of the block drains west to a drop inlet south of the 200 block of South Oak Street. This runoff is directed underground and south to a detention basin that is situated just north of the River Trail.

The east half, which has two-way traffic, was improved in summer 2005. Heavily traveled, this block is paved with asphalt and has a 6'-wide sidewalk with curb and gutter along the north, and a 6'-wide sidewalk with curb and gutter along the south. There is a 5' to 6'-wide green strip behind both sidewalks. Most of this half of the block drains east to the 300 block of South Fir Street.

Environment/Drainage/Snow Storage. Air and water quality appear to be only minimally impacted by this block. The existing drainage system, though new, functions well, except at the southeast corner where snowmelt tends to pool and overtop the sidewalk. The detention basin, which serves the west half of this block, functions very well. Snow is stored along the north side, on top of the north sidewalk. As it exists, and as it is proposed this block is consistent with Telluride's snow storage and drainage plans.

Historic Character. The Stronghold Building, an important contributing structure is on the northeast corner of this block. There are several other historic structures along this block. Any future alterations to the right of way should not negatively impact these historic resources.

Interfacing Streets. All interfacing streets appear to function well with this block, except during snowmelt at the southeast corner. Drainage improvements along the 300 Block South Fir may be warranted.

Lighting. There is one standard commercial streetlight at the northeast corner and one at the underground parking entrance south of the alley intersection. This is consistent with the Streetscape Lighting Plan.

Pedestrian Concerns. Pedestrian volumes are moderate to heavy along this block because of the numerous businesses and its proximity to the River Trail and Gondola Plaza. Existing sidewalks adequately serve pedestrians, except during winter when southeast corner icing and north side snow storage make passage difficult. There are handicap ramps at each corner.

Parking. Parking demand is heavy on this block; therefore, paid parallel parking is permitting on both sides of the street, except in winter when snow is stored on the north side. This parking configuration should be maintained. Off-street parking should be maximized, as well.

Street Furniture and Public Art. No street furniture or amenities currently exist on the east half of this block and none are recommended. There are several benches and bicycle racks along the south side of the west half of this block, as well as several amenities at the Gondola Plaza. No additional amenities are recommended.

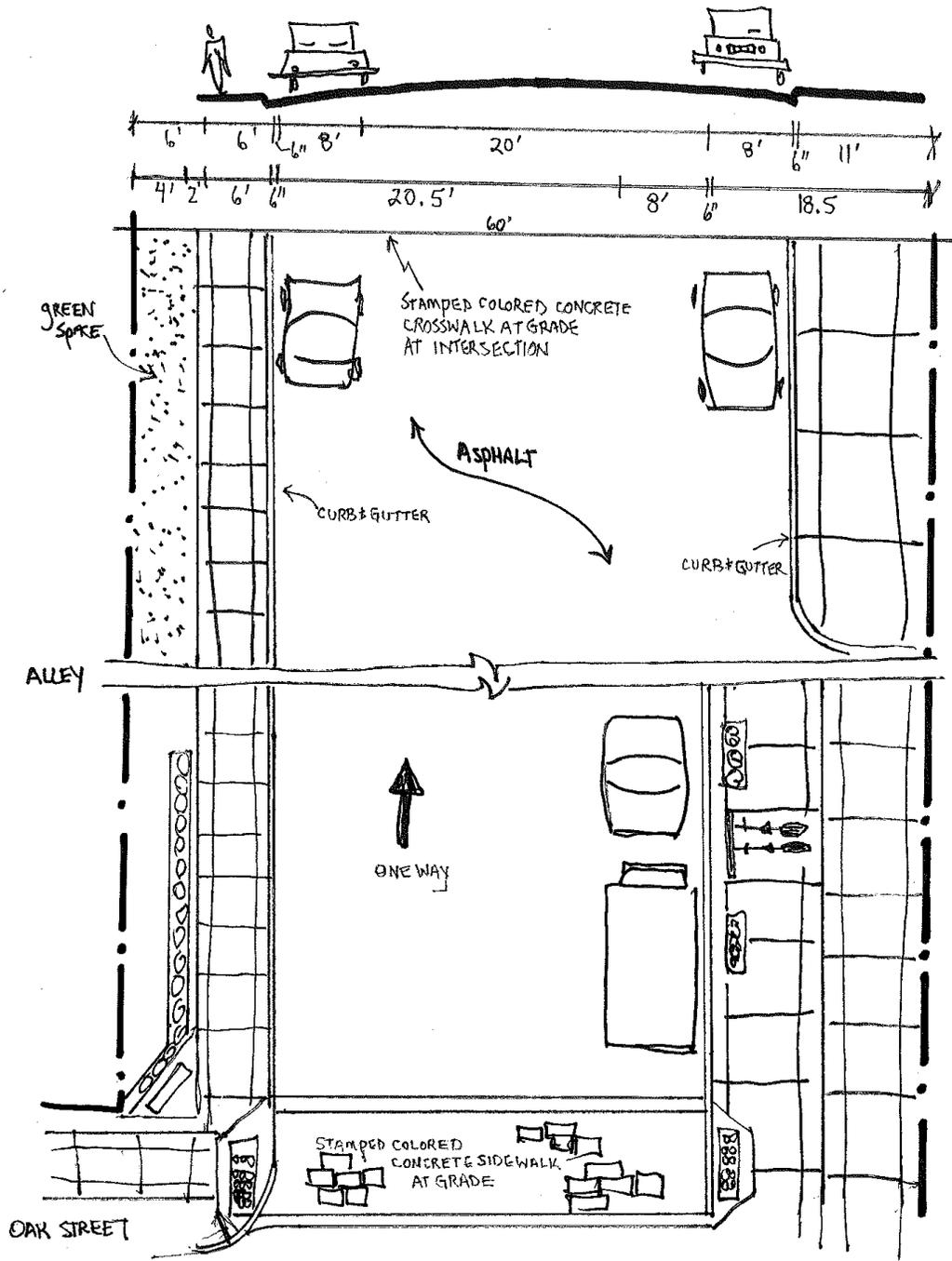
Traffic Circulation. This block is part of the Galloping Goose Town (Bus) Loop. As well, vehicular traffic can be heavy. The current travel way width appears to adequately handle the vehicular traffic and emergency vehicle access.

Utilities. There are no utility concerns on this block at this time.

RECOMMENDED IMPROVEMENTS

1. Drainage at the southeast corner and along the 300 Block South Fir must be improved to ensure a safe pedestrian way.
2. Upon development of lots along the south side, the sidewalk width may need to increase to match Camel's Garden due to the increase in pedestrian volumes.

Refer to Streetscape Warehouse District, 2005, Engineered Street Designs and adjacent street profiles for additional information.



WEST SAN JUAN AVENUE - 200 BLOCK
(BETWEEN S OAK & S FIR)

- REVISED 2016 -

WEST SAN JUAN AVENUE – 300 BLOCK

20' Right of Way

Neighborhood Meetings: None. Added as an amendment to the Streetscape Standards in 2016.

BACKGROUND

Existing Street Conditions. This block was improved when the Gondola Plaza was constructed. Sloping to the west toward South Aspen Street, it is paved with asphalt, has curb and gutter along the north and south sides with drop inlets at the west end. These inlets discharge to the San Miguel River through a 4 inch conduit.

Environmental/Drainage/Snow Storage. Drainage appears to be effective along this block. Snow is traditionally stored in the green space on the south side of this block to the west of the Gondola Station.

Historic Character. This block neither contributes to nor detracts from the historic character of this area. There are no historic structures.

Interfacing Streets. South Oak Street and South Aspen Street appear to function well with this block from a vehicular standpoint.

Lighting. There is a commercial style street light at the southwest corner of the West San Juan Avenue and South Oak Street intersection just east of the Gondola Station. There is a second commercial style street light west of the Gondola Station on the south side approximately at the middle of the street.

Parking. There are two 15-minute loading spaces available for deliveries and drop off to the businesses along this block on the south side near South Aspen Street. Otherwise, the right of way is too narrow to allow for vehicle parking or stopping.

Pedestrian Concerns. This block is a primary pedestrian access point from neighborhoods to the west to reach the Gondola Station to Mountain Village, Chair 8 Lift to the ski area, and the Galloping Goose Bus Stop at Gondola Plaza. It is used heavily by pedestrians, who often walk in the middle of the travel way.

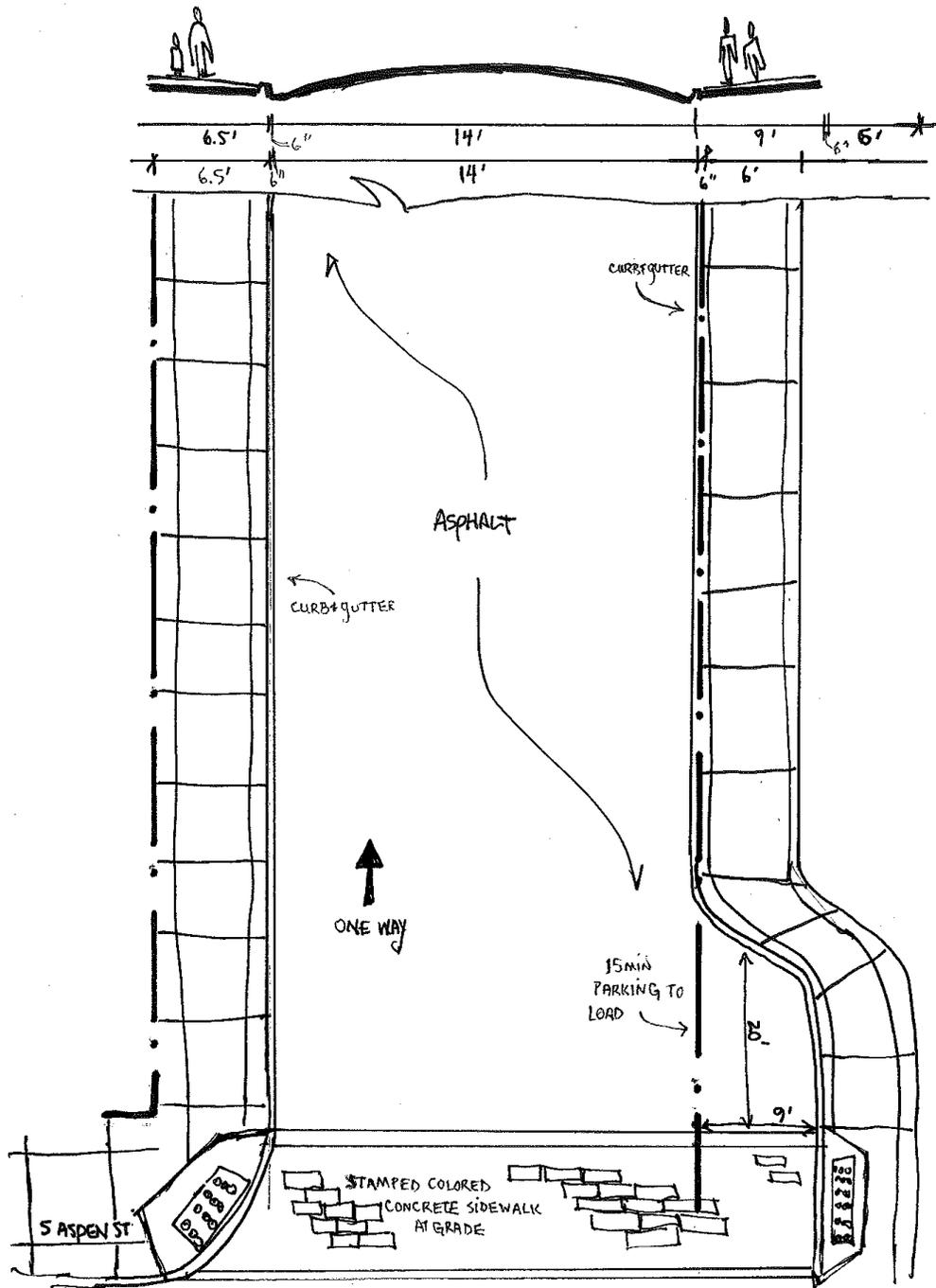
Street Furniture and Public Art. No formal street furniture or amenities currently exist and none are recommended, given the intense use and limited space for this area.

Traffic Circulation. This block provides primary access to the Gondola Plaza, which is the connecting point to Mountain Village from Telluride. Traffic congestion occurs at the Gondola Plaza at the south end of South Oak Street, where the Galloping Goose, private vehicles, and hotel concierge services stop to load and unload passengers.

Utilities. There are no utility concerns for these blocks at this time.

RECOMMENDED IMPROVEMENTS

1. Construct a 6 foot wide sidewalk along the south side of this block to facilitate pedestrian movement.
2. Construct a colored concrete crosswalk at the intersection with South Aspen Street running north-south.
3. Work with Mountain Village to open the northwest corner of the Gondola Station to facilitate pedestrian flow through.



W SAN JUAN AVENUE - 300 BLOCK
(BETWEEN SOAK # S ASPEN)

- PUT INTO STREETSCAPE STANDARDS 2016 -

NORTH SPRUCE STREET – 100 BLOCK

50' Right of Way

Neighborhood Meetings: July 10, 1996; February 24, 1997; March 14, 1998

BACKGROUND

Existing Street Conditions. This south sloping block was improved as part of Streetscape Phase I 1998 construction. The south one-third is a pocket park, which is landscaped with grass and trees and is equipped with benches and a picnic table. There are wide sidewalks on either side that connect through to Colorado Avenue. An unpaved east-west alley bisects the north portion of this block, which is paved with asphalt and has 7'-wide concrete sidewalks with curb and gutter on both sides. Although a section of sidewalk is missing on the east side just north of the park. There are no green strips. All existing infrastructure is in good condition. There is head in parking at the south end of the travel way, where it terminates at the park. There is also a rolled curb along the east and west side nearest the park to facility access to private head in parking. Drainage flows south down the block and east along the curb that separates the park and the travel way. Storm water grates at the southeast and southwest corner of this curb collect flow from this block and direct it underground in a pipe to the south to a grate at the intersection with the Colorado Avenue sidewalk. From that grate there are two pipes that continue to allow drainage to flow south underneath the Colorado sidewalk and daylight at the Colorado gutter.

Environment/Drainage/Snow Storage. Air and water quality appear to be minimally impacted by this block. The existing curb and gutter and associated storm grates effectively handle drainage as long as they are cleaned of sediment regularly. Snow is stored along the east side. In its current condition, this block is consistent with Telluride's snow storage plans and drainage plans.

Historic Character. As it exists this block neither contributes nor detracts from the historic character of the area. There is one contributing structure just northwest of the pocket park. Any future alteration to the right of way should not negatively affect this historic resource.

Interfacing Streets. All interfacing streets appear to function well with this block.

Lighting. There is a single pole-mounted residential light fixture at the northeast corner that should be replaced with a fixture that is consistent with the Streetscape Lighting Plan.

Parking. Parking demand is very heavy on this block because of its proximity to the commercial district. Therefore, parallel parking is permitted on both sides, except in winter when the east side is needed for snow storage. A large curb cut exists at the south end of the travel way, on the east and west sides and on-street parking is therefore not allowed at this location. It may be possible to gain on-street parking by having the east side off-street parking take its access from the alley. There is head-in parking the south end of the travel way where it terminates at the park. Creation of new off-street parking should be a priority. Use of existing off-street parking should be maximized to ensure on-street parking availability.

Pedestrian Concerns. Pedestrian traffic on this block is moderate due to its proximity to the commercial district. Existing sidewalks appear to effectively handle pedestrian traffic. This block has handicap access.

Street Furniture and Public Art. Currently, there are two benches and a picnic table in the pocket park at the south end of this block. These should remain. The existing location of the trash and recycle against the building at the south west corner of the block is efficient and should remain. Additional street furniture and public art could be considered within the park.

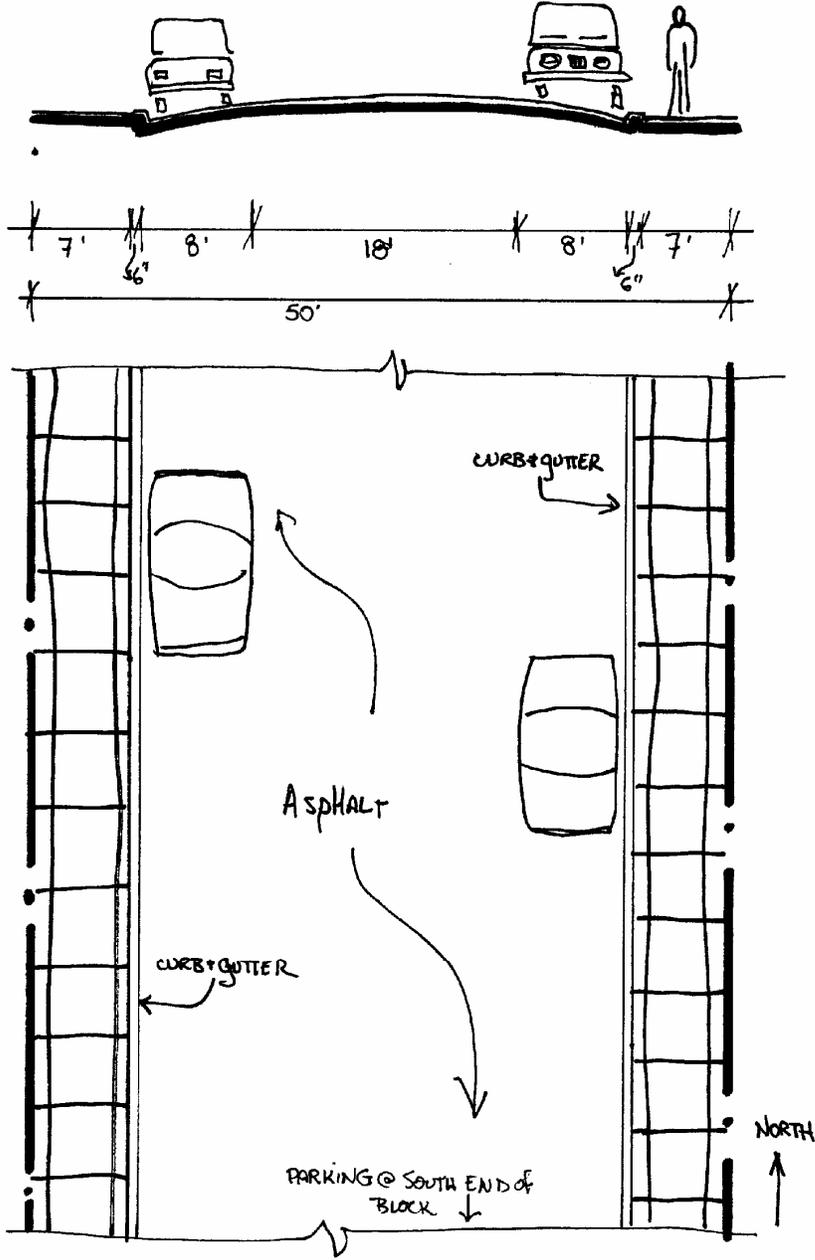
Traffic Circulation. This is a dead end street and traffic coming south down the block must either turn around or turn east or west into the intersecting alley. However, because it is common for drivers to look for parking on this block, traffic intensity is moderate. Travel width appears adequate for intensity of use and emergency vehicle access.

Utilities. There are no utility concerns for this block.

RECOMMENDED IMPROVEMENTS

1. Replace the existing pole-mounted residential light fixture at the northeast corner with a fixture that is consistent with the Streetscape Lighting Plan.

Refer to Streetscape Phase I, 1998, Engineered Street Designs and adjacent street profiles for additional information.



NORTH SPRUCE STREET - 100 BLOCK
(BETWEEN COLORADO + COLUMBIA AVENUES)

NORTH SPRUCE STREET – 200 BLOCK

50' Right of Way

Neighborhood Meetings: July 10, 1996; February 24, 1997; March 14, 1998

BACKGROUND

Existing Street Conditions. This south sloping block was improved as part of Streetscape Phase I 1998 construction. Paved in asphalt, this block has 5'-wide sidewalks, 3'-wide green strips, and curb and gutter along the east and west sides. The green strips are between the back of the curb and gutter and the sidewalk. At the south side of the block, runoff flows tend to jump from the west side to the east side before reaching Columbia Avenue. This sheet flow creates a hazard in winter when it freezes. A drain pan crosses Spruce Street at Columbia Avenue carrying runoff east. Sidewalk is missing at the northeast end of the block, where there is a flagstone path. No alley intersects this block. Except for the missing sections of sidewalk, the sidewalks, curb and gutter, green strips and travel way are in good condition.

Environment/Drainage/Snow Storage. This block does not appear to negatively impact air and water quality. The existing curb and gutter drainage system effectively handles runoff from this block, except where a lack of adequate crown and/or depth of gutter allows runoff to sheet flow diagonally west to east across the travel way at the south end of the block. This situation should be corrected. Snow is stored along the west side. In its current condition, this block is consistent with Telluride's snow storage and drainage plans.

Interfacing Streets. All interfacing streets appear to function well with this block.

Historic Character. As it exists this block neither contributes to nor detracts from the historic character of the area, which is attributed to the many rated structures on this block. Any alteration to the right of way should not negatively affect these historic resources.

Lighting. There are currently no streetlights on this block and none are recommended.

Parking. Intensity of on-street parking is moderate and consists mainly of neighborhood residents. Parallel parking is allowed on both sides of the block, except in winter when parking on the west side is prohibited to allow snow storage. During special events put on by St. Patrick's Church, parking is allowed on both sides of the street on a limited basis. Use of existing off-street parking should be maximized to maintain on-street parking availability.

Pedestrian Concerns. Existing sidewalks appear to effectively handle the moderate pedestrian traffic along this block. There is handicap access.

Street Furniture and Public Art. No street furniture or amenities currently exist on this block. None are recommended.

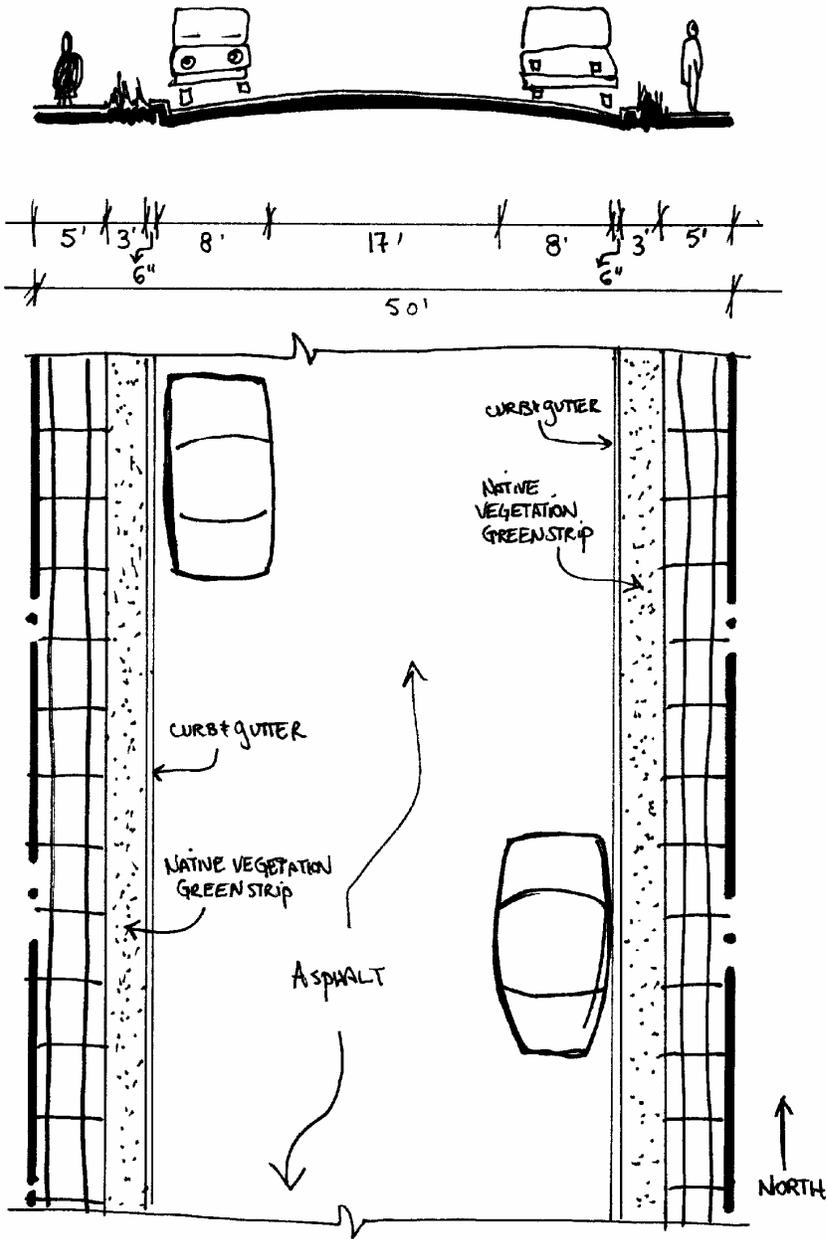
Traffic Circulation. Vehicular traffic on this block is moderate. Travel way width appears adequate for intensity of use and emergency vehicle access.

Utilities. There are no utility concerns at this time.

RECOMMENDED IMPROVEMENTS

1. Improve the crown of the block on the south side and/or the depth of gutter to prevent runoff from sheeting across the street from the west to the east.
2. Construct the missing section of concrete sidewalk on the northeast side.

Refer to Streetscape Phase I, 1998, Engineered Street Designs and adjacent street profiles for additional information.



NORTH SPRUCE STREET - 200 BLOCK
(BETWEEN COLUMBIA & GALENA AVENUES)

NORTH SPRUCE STREET – 300 BLOCK

50' Right of Way

Neighborhood Meetings: July 10, 1996; February 24, 1997; March 14, 1998

BACKGROUND

Existing Street Conditions. This south sloping block was improved as part of Streetscape Phase I 1997 construction. Although the right of way is 50' wide, the asphalted travel way is 18'-wide, as improved. This narrow travel way does not allow for on-street parking. There are no sidewalks, curb and gutter, or concrete drain pans, except for a short section of sidewalk and drain pan at the southwest corner of the block in front of the church. There is a natural cobble and vegetation drainage swale along west side of the block, which directs drainage from this block and the 200 Block East Gregory Avenue, south and onto the 200 Block North Spruce Street. Inefficient drainage at the Gregory Avenue and Willow Street may strain the capacity of this drainage swale at times. There is a wide green strip that is in good condition east side of the travel way.

Environment/Drainage/Snow Storage. Adverse impacts to air and water quality appear to be minimal from this block. The west side drainage swale functions effectively when draining the area for which it was designed. However, inefficient drainage at the Gregory Avenue and Willow Street intersection sometime creates an overflow and strains its capacity. Drainage improvement at Willow and Gregory should alleviate this problem. Snow is stored along both sides of this block. This block is consistent with Telluride's snow storage and drainage plans.

Historic Character. There are numerous rated structures on this block, and in an effort to preserve their context, the "rough and natural character of this block should be retained.

Interfacing Streets. Drainage flowing from beyond the East 200 Block of Gregory Avenue into the west side drainage swale negatively affects this block. Improvements made to the Gregory Avenue and Willow Street intersection should alleviate this problem.

Lighting. The pole-mounted residential light fixtures at the northeast corner should be eliminated. The fixture at the southeast corner should be replaced with a fixture that is consistent with the Streetscape Lighting Plan.

Parking. On-street parking is not permitted on this block at any time due to its steep grade and narrow travel way. All visitors and residents must use off-street parking or park on another block.

Pedestrian Concerns. Pedestrian traffic on this block is low and attributed to neighborhood residents. The lack of sidewalks on this block causes little conflict with vehicular traffic. For these reasons, and in an effort to preserve the "rough and natural" character of the area, no sidewalks are required, except near the front of the church. There is no special handicap access, except into the church.

Street Furniture and Public Art. No street furniture or amenities currently exist on this block and none are required.

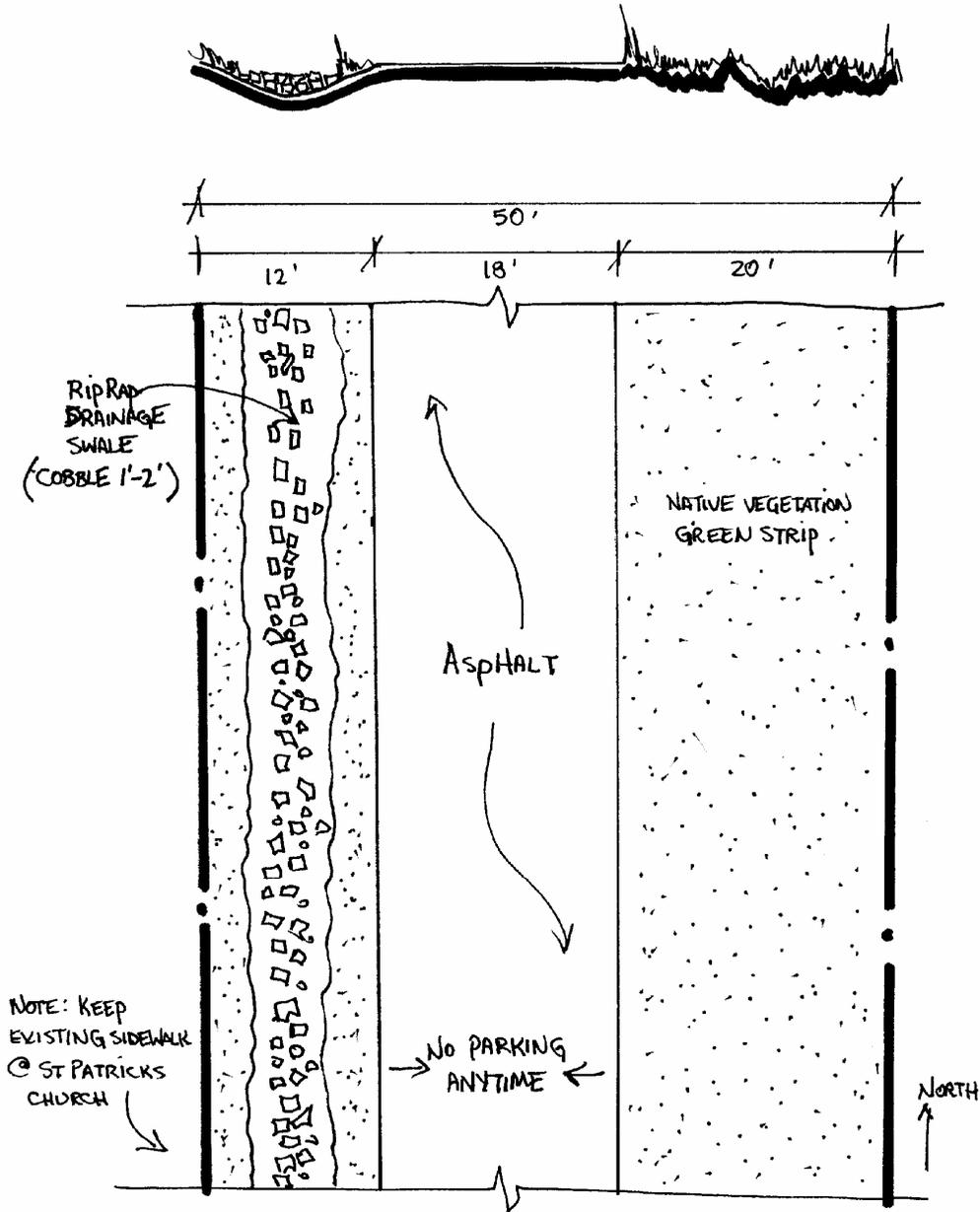
Traffic Circulation. Vehicular traffic on this block is low to moderate, as it carries traffic to Gregory Avenue beyond. The travel width appears adequate for intensity of use and emergency vehicle access.

Utilities. There are currently no utility concerns on this block.

RECOMMENDED IMPROVEMENTS

1. Remove the pole-mounted residential light fixture at the northeast corner of the block and replace the southeast fixture with one that is consistent with the Streetscape Lighting Plan.
2. Re-construct the west side drainage swale to re-establish its original capacity.

Refer to Streetscape Phase I, 1997, Engineered Street Designs and adjacent street profiles for additional information.



NORTH SPRUCE STREET - 300 BLOCK
(BETWEEN GALENA & GREGORY AVENUES)

SOUTH SPRUCE STREET – 100 BLOCK

50' Right of Way

Neighborhood Meetings: July 23, 1996; December 14, 2006

BACKGROUND

Existing Street Conditions. The north third of this south sloping block is a landscaped pocket park with historic interpretive signs, benches, and picnic tables. There are sidewalks along the east and west sides of the park to connect Colorado Avenue to Pacific Avenue. There is head in parking south of the park and north of a heavily used, paved, intersecting (east-west) alley. The head in parking and the travel way that makes up the south portion of this block is paved with chip seal that is in very poor condition. There are intermittent sidewalks and curb and gutter along the east and west sides. Most of this infrastructure is in poor condition.

Environment/Drainage/Snow Storage. This heavily traveled block adversely impacts air and water quality. Proper surfacing and adequate drainage will alleviate most of the problem and will ensure that this block functions in a manner that is consistent with Telluride's drainage plan. Snow is stored along both sides of this block, which is consistent with Telluride's snow storage plan.

Historic Character. There are numerous rated structures on this block that contribute to the history of Telluride, including the Silver Bell Building and the Old Jail. Any alteration to the right of way should not negatively impact these historic resources.

Interfacing Streets. Interfacing streets appear to function well with this block. Maintaining a somewhat wider street width is important to avoid problems with deliveries at the Ace Hardware Store northeast of the alley.

Lighting. The pole-mounted residential light fixtures at the east side at the alley should be eliminated. The fixture at the southeast corner should be replaced with a fixture that is consistent with the Streetscape Lighting Plan.

Parking. Parking demand is very high because this block is in the commercial core. Therefore, paid on-street parking is permitted on both sides throughout the year. There is also head-in parking north of the alley and south of the park. This configuration should be maintained. Creation of new, off-street parking should be a priority. Use of existing off-street parking should be maximized.

Pedestrian Concerns. Continuous sidewalks of sufficient width are needed to accommodate the heavy pedestrian traffic along this block in order to minimize conflicts with vehicles and delivery trucks. Handicap access is also inadequate.

Street Furniture and Public Art. The north end pocket park has benches and picnic tables and historic interpretive signage. Public art or additional amenities in the park would be appropriate, if desired.

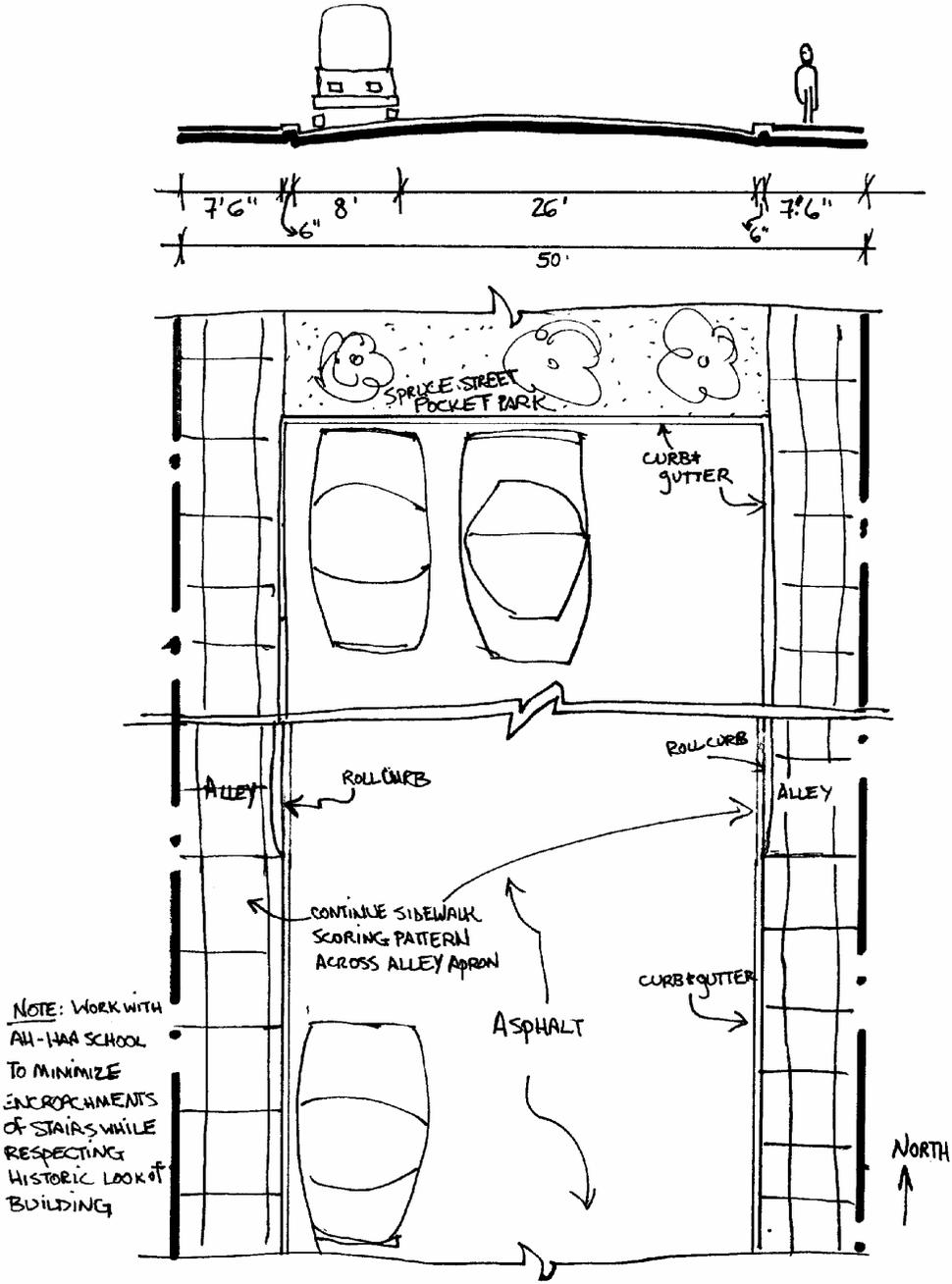
Traffic Circulation. Vehicular traffic on this block and the intersecting alley is moderate. ACE Hardware receives and unloads deliveries from very large trucks, sometimes longer than 50 feet, in this right of way. The travel width appears adequate for intensity of use and emergency vehicle access.

Utilities. There are currently no utility concerns on this block.

RECOMMENDED IMPROVEMENTS

1. Remove the pole-mounted residential light fixture at the east side alley and replace the southeast fixture with one that is consistent with the Streetscape Lighting Plan.
2. Grade and pave the block with asphalt.
3. Construct 7.5'-wide concrete sidewalks with curb and gutter on the east and west sides, accommodating the encroachments of historic structures as needed.

Refer to Streetscape Warehouse District, 2005, Engineered Street Designs and adjacent street profiles for additional information.



SOUTH SPRUCE STREET - 100 BLOCK
(BETWEEN COLORADO & PACIFIC)

SOUTH SPRUCE STREET – 200 BLOCK

50' Right of Way

Neighborhood Meetings: July 23, 1996; December 14, 2006

BACKGROUND

Existing Street Conditions. Bordering on the River Park Corridor, this dead end block is largely unimproved. It slopes very gently south. It is paved with chip seal that is in poor condition. There is a 5.5'-wide section of sidewalk in the middle of the east side and another on the west side. There are no drainage improvements.

Environment/Drainage/Snow Storage. This block adversely impacts air and water quality. Proper surfacing and adequate drainage will alleviate most of the problem and will ensure that this block functions in a manner that is consistent with Telluride's drainage plan. An improved interface at the Hobgood-Drew Wetland is recommended. Consistent with Telluride's snow storage plan, snow is stored along the east side.

Historic Character. This block neither contributes to nor detracts from the historic character of town.

Interfacing Streets. All interfacing streets appear to function well with this block.

Lighting. Consistent with the Streetscape Lighting Plan, there are no light fixtures along this block.

Parking. Parking demand is moderate and created mostly by neighborhood residents and business workers. There is diagonal on-street parking along the west side and parallel parking along portions of the east side, except in winter when the east side is used for snow storage. This configuration should be maintained. Creation of additional off-street parking should be a priority. Use of existing off-street parking should be maximized.

Pedestrian Concerns. Pedestrian traffic along this block is light and due mainly to local residents and workers. Vehicle traffic is also very light and therefore sidewalks are not needed. A 5'-wide sidewalk along the east side could be useful. There is no special handicap access.

Street Furniture and Public Art. No street furniture or amenities currently exist and none are recommended.

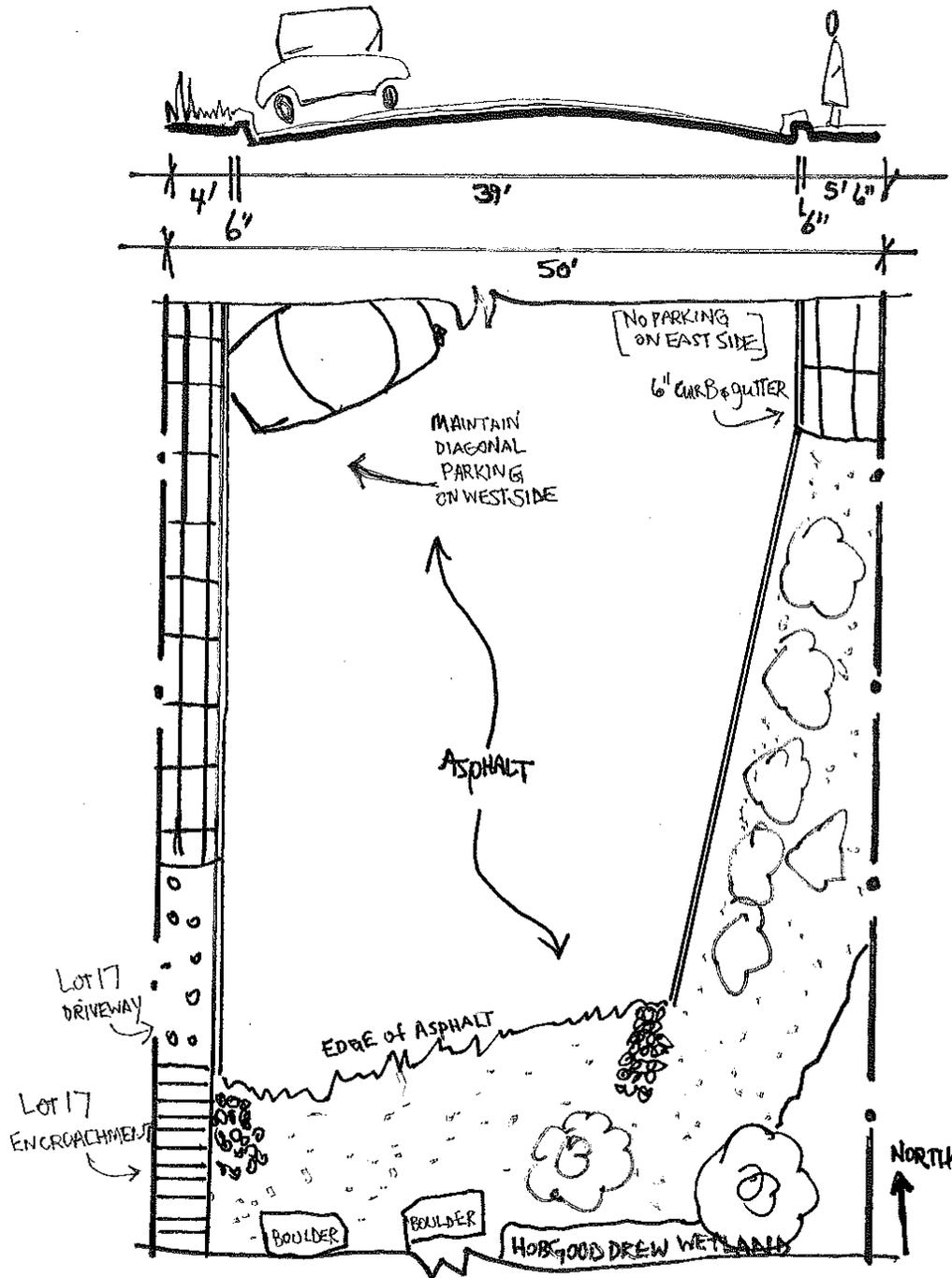
Traffic Circulation. Traffic volume is light on this dead end block. The travel width appears adequate for intensity of use and emergency vehicle access.

Utilities. There are currently no utility concerns on this block.

RECOMMENDED IMPROVEMENTS

1. Construct a traditional curb and gutter along the entire length of the east side. Construct an at-grade ribbon curb along the south side to facilitate snow plowing. Construct a roll curb and gutter along the west side. Align the curb and gutter with the curb and gutter along the 100 Block South Spruce Street.
2. Create a drainage interface that disperses street runoff velocities before they enter the Hobgood-Drew Wetland.
3. Re-establish areas of native vegetation behind the curb on the west side, respecting curb cuts and walkways.
4. Narrow the south portion of the street to provide a larger buffer for the wetland.

Refer to Streetscape Warehouse District, 2005, Engineered Street Designs and adjacent street profiles for additional information.



SOUTH SPRUCE STREET - 200 BLOCK
(BETWEEN PACIFIC AVENUE & HOBGOOD DREW WETLAND)

- REVISED 2016 -

TOMBOY ROAD

Neighborhood Meetings: October 8, 1997; March 25, 1998; September 16, 1998

BACKGROUND

See the following report by David Groeneveld, Chris Hazen and Linda Luther: "Mitigating Runoff from Tomboy Road Above Telluride, Colorado."

The task force conducted extensive review for improvements to this right-of way including various design sessions and numerous site walks. The task force strongly recommends an incremental approach to improvements to enable full examination of the efficacy of "low tech" improvements to this travel way to hiking and biking trails and ATV routes.

Existing Street Conditions. In the fall of 1997 the following improvements were implemented on Tomboy Road between Oak Street and the Liberty Bell Road:

- Re-grading the travel way so that it slopes to the north.
- Placing and compacting new road base on the surface.
- Construction of a defined earthen swale on the north
- Installation of water bars in accordance with recommendations in *Groeneveld et al., 1997*.

Subsequently, the north drainage swale was replaced with a concrete drain pan, which requires constant maintenance.

Environment/Drainage/Snow Storage. This road impacts both air and water quality in town. Runoff carrying significant amounts of sediment from the road travels south down Oak Street and east along Columbia Avenue until it reaches the Runoff Treatment Facility at Alder Street. As there is no clear solution regarding drainage management along this road, continued consideration is required. Consistent with Telluride's snow storage plan, this road is occasionally plowed.

Historic Character. This road characterizes the "rough and natural" feel of old Telluride. Therefore, it should not be paved and no sidewalks should be constructed. There are no historic structures along this road in town.

Interfacing Streets. The interface of this block with Oak Street is problematic. Excess sediment carried by runoff tends to settle out along the drain pans before this intersection and then along the length of Oak Street. At times, runoff volumes are large enough to jump the curb and flow onto the sidewalk and in the green strip on the east side of Oak Street. Decreasing the rate and volume of runoff and improving its quality would help alleviate the problems that are created along Oak Street and beyond. In the meanwhile, regular, intensive maintenance is necessary.

Lighting. Consistent with the Streetscape Lighting Plan, there are no light fixtures along this road and none are recommended.

Neighborhood Concerns. See the section entitled "Interfacing Streets".

Parking. Parking is not permitted in the travel way of this very narrow, steep road. Residences that access from Tomboy Road must provide adequate off-street parking to satisfy the needs of residents and their visitors. Most hikers park along North Oak Street.

Pedestrian Concerns. There is moderate pedestrian traffic along this road, as it provides access to extensive backcountry wilderness and the Jud Wiebe Trail. There are sometimes conflicts with pedestrians and motorized vehicles that are using the road for recreation or travel. Handicap access is very difficult because of the grade and the rough terrain.

Street Furniture and Public Art. No street furniture currently exists and none is required or encouraged. Signage for the Jud Wiebe Trail should remain.

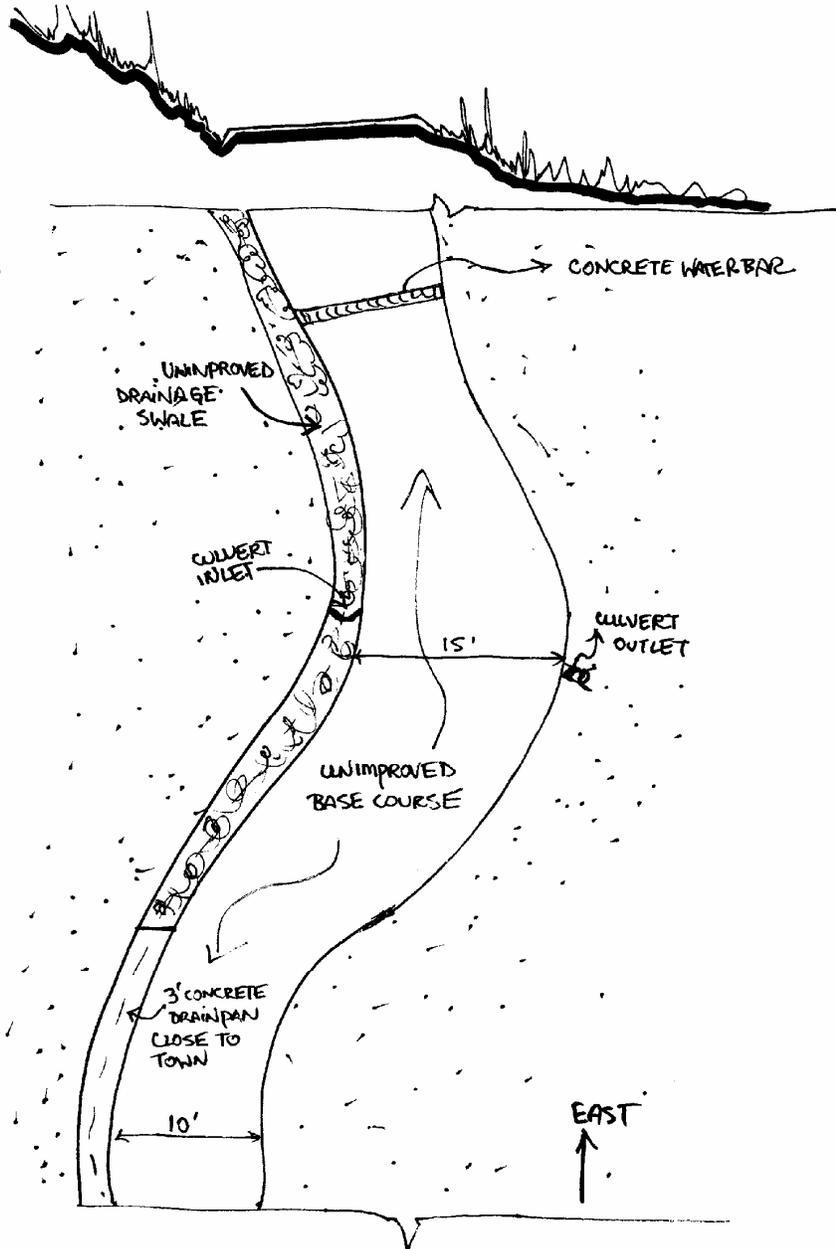
Traffic Circulation. Traffic volume is light to moderate. Summer use of passes for recreation is increasing. Increasing. Residential access is also increasing as lots are developed along the south side of this roadway. The travel width may be adequate in the future for intensity of use. Emergency vehicle access was improved in 2005 and is now adequate.

Utilities. There are currently no utility concerns on this block.

RECOMMENDED IMPROVEMENTS

None are recommended at this time, but several may be expected as use of this road increases.

Refer to Streetscape Phase I, 1997, Engineered Street Designs and adjacent street profiles for additional information.



Tomboy Road

NORTH TOWNSEND STREET – 100 BLOCK

75' Right of Way

Neighborhood Meetings: July 9, 1996; July 24, 1996; January 19, 2000

BACKGROUND

Existing Street Conditions. This south sloping block was improved during Streetscape Phase II, 2000, construction. Townsend Street is 25' wider than typical north-south streets in Telluride. Paved with asphalt there is curb and gutter and a 5'-wide sidewalk on the east and west sides. The asphalt is in good condition. The west sidewalk is in poor condition. The entire 3'-wide green strip that is between the west sidewalk and the travel way has not been properly planted and is also in poor condition. The east side sidewalk and 3'-wide green strip are both in good condition. This is one of the few blocks where trees have been permitted in the green strip.

Environment/Drainage/Snow Storage. Air and water quality appear to be minimally impacted by this block. The existing drainage system effectively handles runoff. Snow is stored along the east side in winter. It is consistent with Telluride's snow storage and drainage plans.

Historic Character. There are numerous rated and non-rated structures on this block. Any alterations to the right of way should not negatively impact these historic resources. In addition, the unusual width of this block is important to the historic character of this area of Telluride. This block was wider than many to accommodate the turning radius of mule teams. Therefore, this aspect of the right of way should be maintained to retain this artifact of history.

Interfacing Streets. All interfacing streets appear to function well with this block.

Lighting. The one pole-mounted residential light fixture at the northeast corner should be replaced with a pole-mounted fixture that is consistent with the Streetscape Lighting Plan. The standard commercial streetlight on the southwest corner at Colorado Avenue should remain.

Neighborhood Concerns. The neighborhood residents are very concerned about the safety of children walking to and from school. Therefore it is important to keep sidewalks in good repair.

Parking. Demand for on street parking on this block is heavy due to proximity of the Elementary School. Parallel parking is permitted on the west side of the street all year. Diagonal parking on the east side, which is possible because of the wider right of way, is prohibited in winter to allow temporary snow storage. The current parking configuration is efficient and should be maintained. As well, use of existing off-street parking should be maximized.

Pedestrian Concerns. Pedestrian traffic, made up mainly of children and parents walking to the Elementary School, can be heavy on this block. Repairing the southwest sidewalk would improve pedestrian flow. There is adequate handicap access to the sidewalks.

Street Furniture and Public Art. No street furniture or amenities currently exist on this block and none are required, as they might impede pedestrian flow.

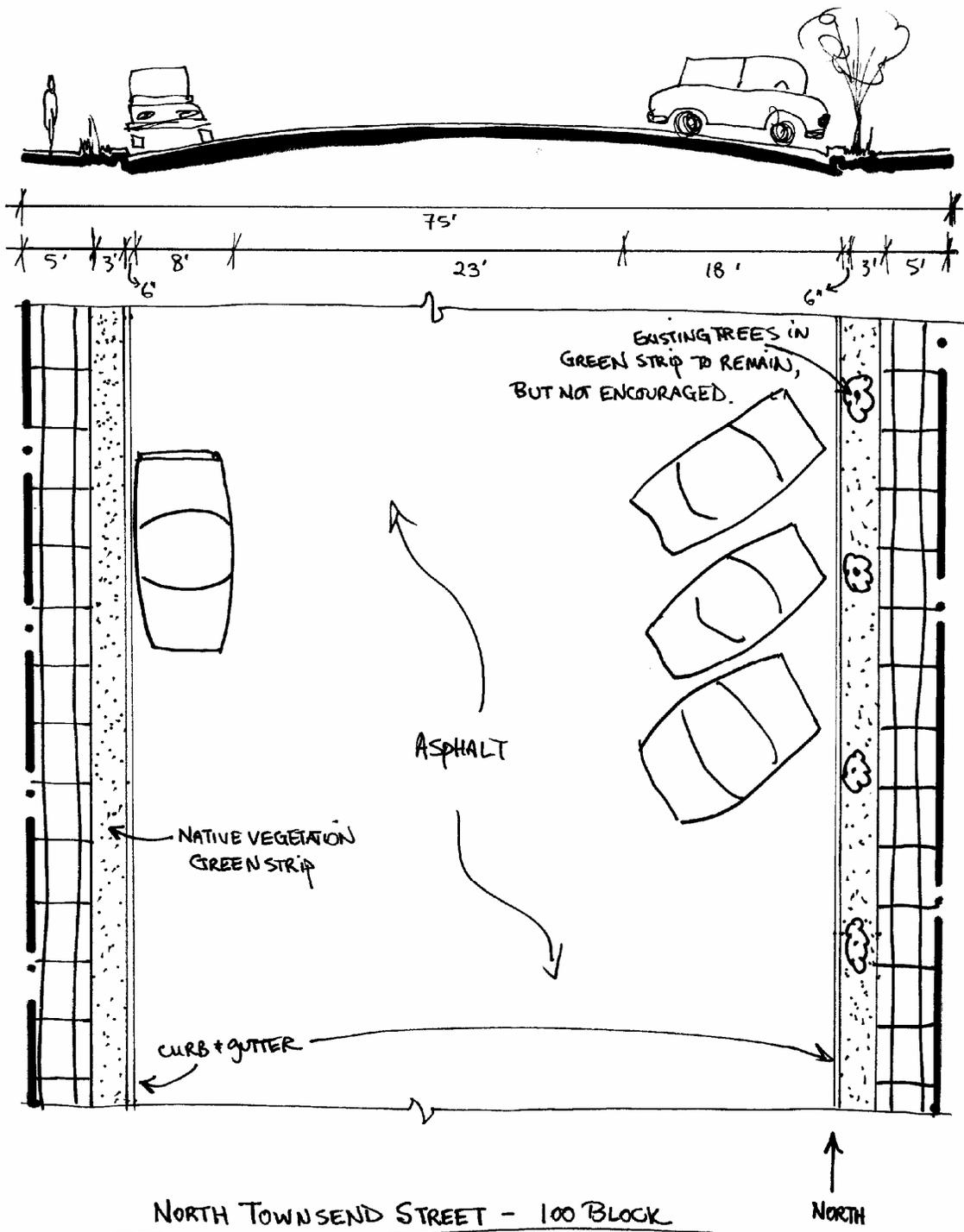
Traffic Circulation. Vehicular traffic on this block is moderate to heavy. Heavy traffic occurs mostly during school mornings when this block feeds traffic to and from the Elementary School. The current travel way width is adequate for intensity of use. Emergency vehicle access is also adequate.

Utilities. Currently, there are no utility concerns along this block.

RECOMMENDED IMPROVEMENTS

1. Repair or replace existing sidewalks that are in poor condition on the west side of the block.
2. Plant the green strips with vegetation that is consistent with the Streetscape Right of Way Landscaping Guidelines.
3. Upon replacement or redevelopment, lower the elevation of the sidewalk along the northeast corner along a portion or the entire length of Lot 21, Block 4. This will improve pedestrian access onto the West Columbia crosswalk to the Elementary School and will allow a handicap ramp. The handicap ramp should be east of the drain pan flow so that runoff does not jump onto the sidewalk on this steep right of way.
4. Replace the pole-mounted light fixture at the northeast corner with a fixture that is consistent with the Streetscape Task Force Lighting Plan.

Refer to Streetscape Phase II, 2000, Engineered Street Designs and adjacent street profiles for additional information.



NORTH TOWNSEND STREET - 100 BLOCK
(BETWEEN COLORADO & COLUMBIA AVENUES)

NORTH TOWNSEND STREET – 200 BLOCK

50' Right of Way

Neighborhood Meetings: July 9, 1996; July 16, 1997; November 3, 1999

BACKGROUND

Existing Street Conditions. This south sloping block was improved during Streetscape Phase II, 2000, construction. It is here that Townsend Street reverts to a normal 50'-wide right of way. The travel way is paved with asphalt. There is curb and gutter and a 5'-wide sidewalk on the east and west sides. There are no green strips in order to maximize travel way width to accommodate traffic around the Elementary School. All infrastructure is in good condition.

Environment/Drainage/Snow Storage. Air and water quality appear to be minimally impacted by this block. The existing drainage system effectively handles runoff, transporting it south onto the 100 Block. Snow is no longer stored on this block. It is consistent with Telluride's snow storage and drainage plans.

Historic Character. There are numerous rated and non-rated structures on this block, including the Elementary School. Any alterations to the right of way should not negatively impact these historic resources. In addition, the unusual width of this block is important to the historic character of this area of Telluride. This block was wider than many to accommodate the turning radius of mule teams. Therefore, this aspect of the right of way should be maintained to retain this artifact of our history.

Interfacing Streets. All interfacing streets appear to function well with this block.

Lighting. There are no streetlights on this block and none are required as there is lighting on adjacent blocks in the immediate vicinity. This is consistent with the Streetscape Lighting Plan.

Neighborhood Concerns. Neighborhood residents, parents, and school personnel are very concerned about the safety of the children walking to and from school and efficient drop off and pick up of children from vehicles during mornings and afternoons. Therefore it is important to keep sidewalks in good repair.

Parking. Demand for on street parking has been heavy due to the Elementary School, but in 2005, parallel parking on the east side was abandoned in favor of dedicating a student loading area along the sidewalk. The current parking configuration appears to be working and should be maintained. Use of existing off-street parking should be maximized.

Pedestrian Concerns. Pedestrian traffic, made up mainly of children and parents walking to the Elementary School, can be heavy on this block. Existing sidewalks are adequate for this volume of pedestrian traffic. There is adequate handicap access to sidewalks.

Street Furniture and Public Art. No street furniture or amenities currently exist on this block and none are encouraged, as they might impede pedestrian flow.

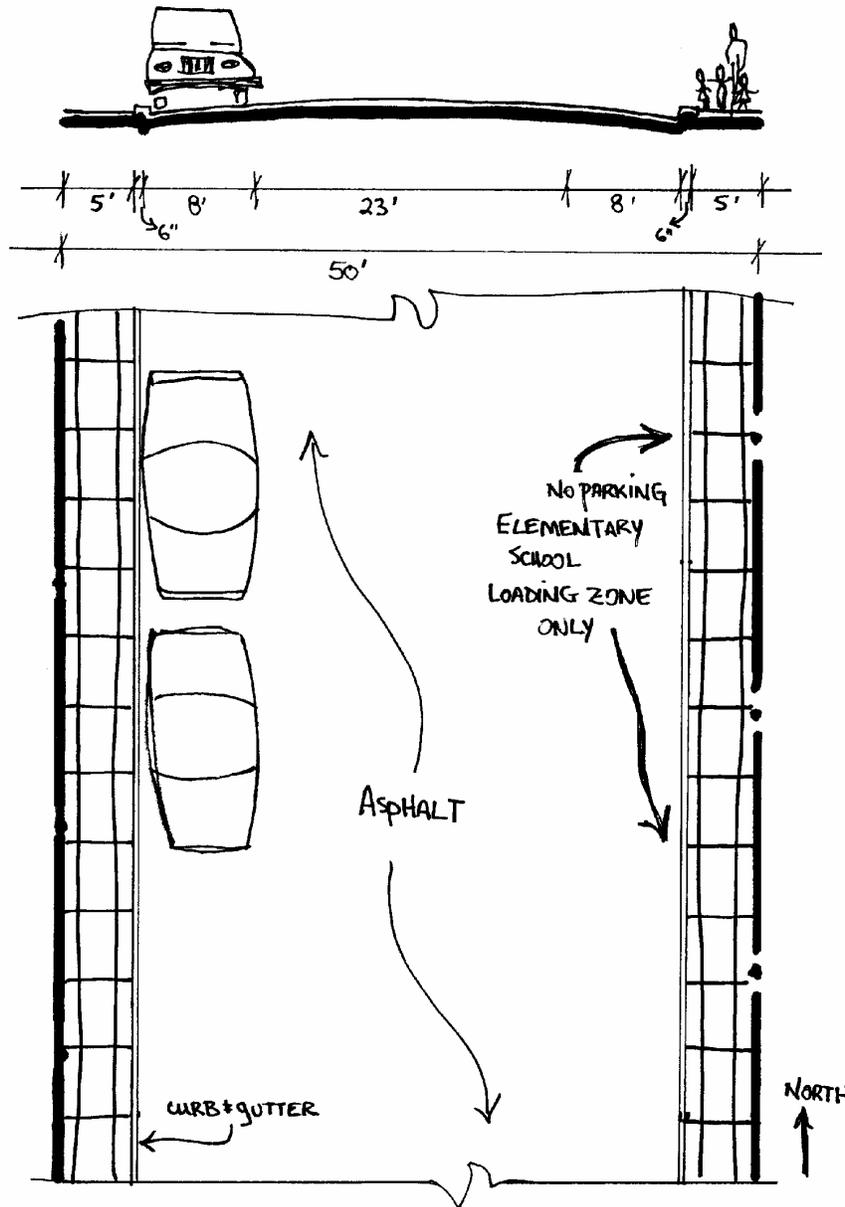
Traffic Circulation. Vehicular traffic on this block is moderate to heavy. Heavy traffic occurs mostly during school mornings when this block feeds traffic to and from the Elementary School and students are dropped off along the east side. The current travel way width is adequate for intensity of use. Emergency vehicle access is also adequate.

Utilities. Currently, there are no utility concerns along this block.

RECOMMENDED IMPROVEMENTS

No improvements are recommended at this time.

Refer to the Streetscape Phase II, 2000, Engineered Street Designs and adjacent street profiles for additional information.



NORTH TOWNSEND STREET - 200 BLOCK
(BETWEEN COLUMBIA + GALENA AVENUES)

NORTH TOWNSEND STREET – 300 BLOCK

*50' Right of Way
Neighborhood Meetings: July 9, 1996*

BACKGROUND

Existing Street Conditions. This block is shorter than a typical street. It begins at the Galena Avenue intersection, rises somewhat steeply to the north (i.e., it slopes southward), crosses Cornet Creek, and continues to the Curtis Drive intersection at a very slight uphill grade to the north. It is paved with chip seal north of the bridge and with asphalt on the bridge and south. The bridge was replaced in 2005. A drain pan on the southwest corner of the block directs runoff south to the 200 Block. There are no sidewalks, curb and gutter, or formal green strips. There is a wide vegetated area west of the travel way. Vegetation is needed east of the travel way.

Environment/Drainage/Snow Storage. Air and water quality appear to be minimally impacted by this block. Drainage from the north side, however, enters Cornet Creek untreated. Draining runoff onto the vegetated areas on either side of the travel way would be an effective and easy treatment method. Snow is stored along the east side. This block is consistent with Telluride's snow storage and drainage plans.

Historic Character. There are two rated structures on this block. Any alterations to the right of way should not negatively affect these historic resources.

Interfacing Streets. All interfacing streets function well with this block.

Lighting. The one pole-mounted residential light fixture on the southeast corner of the block should be replaced with a fixture that is consistent with the Streetscape Lighting Plan.

Parking. On street parking demand is very light, consisting mainly of neighborhood residents and their guests. On street parking is permitted on east and west side of this block, however, space is limited to north of the bridge and at the dead end north of the Curtis Drive intersection. The current parking configuration should be maintained and use of existing off street parking should be maximized.

Pedestrian Concerns. Pedestrian traffic on this block is made up mainly of neighborhood residents. Because this block crosses Cornet Creek and pedestrian traffic is light no sidewalks exist and none are required. There is no special handicap access.

Street Furniture and Public Art. No street furniture or amenities currently exist on this block and none are required.

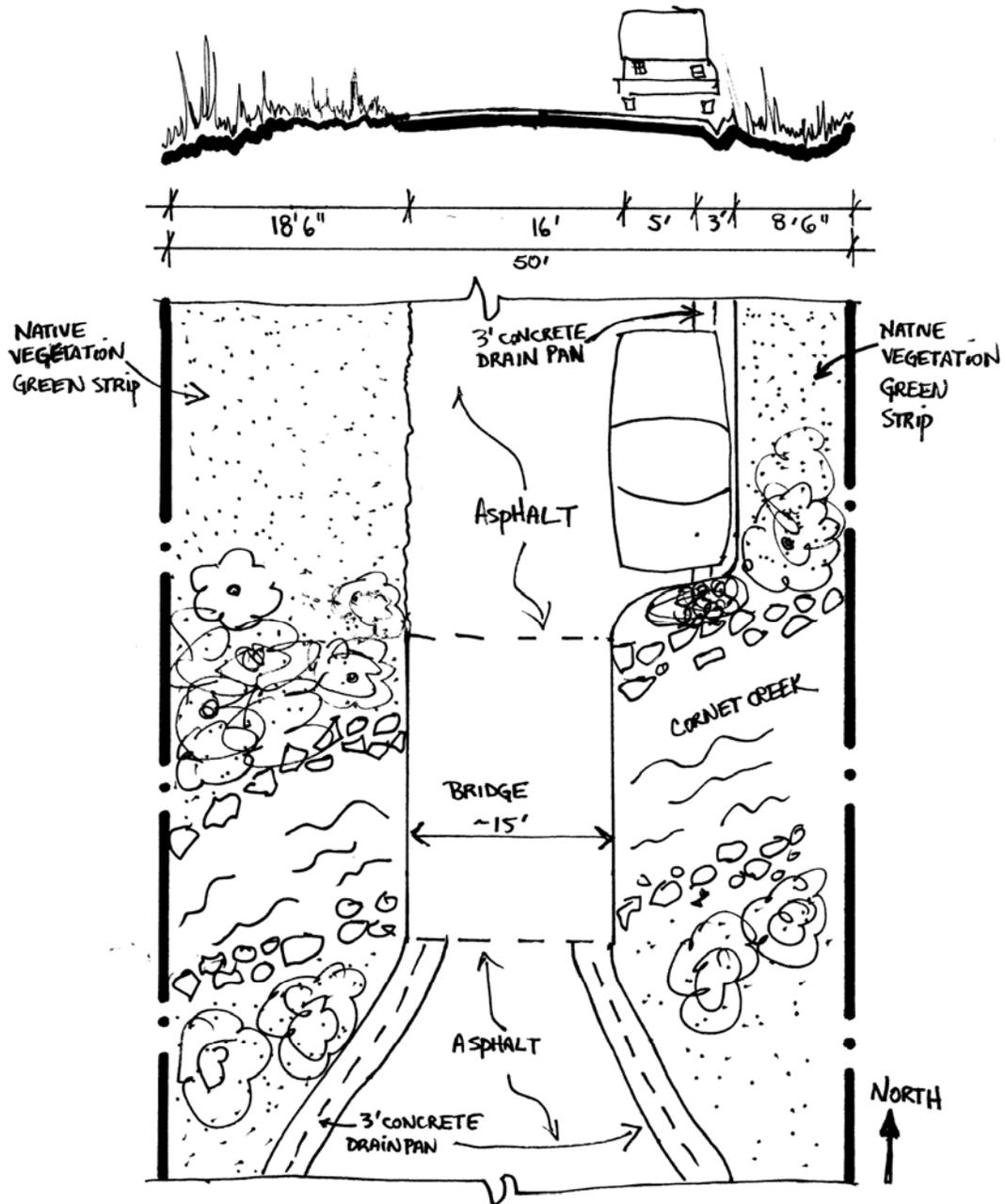
Traffic Circulation. Vehicular traffic on this block is very low, since it only services a small number of homes. This is the only access to Curtis Drive beyond. Travel way width appears adequate for intensity of use, even though the bridge is narrow. Emergency vehicle access also appears adequate.

Utilities. There are no utility concerns at this time.

RECOMMENDED IMPROVEMENTS

1. Surface the block with asphalt.
2. Improve drainage flow onto vegetated swales on either side of the travel way to treat runoff prior to it entering Cornet Creek.
3. Replace the pole-mounted residential light fixture at the southeast corner with a fixture that is consistent with the Streetscape Lighting Plan.

Refer to Streetscape Phase II, 2000, Engineered Street Designs and adjacent Street profiles for additional information.



NORTH TOWNSEND STREET - 300 BLOCK
(BETWEEN GALENA AVENUE + CURTIS DRIVE)

SOUTH TOWNSEND STREET – 100 BLOCK

*75' Right of Way
Neighborhood Meetings: July 24, 1996*

BACKGROUND

Existing Conditions. This 75'-wide, south sloping block is bisected at its center by a moderately used, largely unpaved, east-west alley. It has a dirt drainage swale along the northwest side and a deteriorating chip seal surface on the entire north half. The 5'-wide northwest sidewalk is in poor condition and requires replacement. The severely damaged 5'-wide northeast sidewalk was removed in spring 2006. Replaced temporarily with compacted road base, it is awaiting development of Lot 21 Block 5 (Telluride Addition) for reconstruction of a new 5'-wide sidewalk. There is a 3'-wide green strip between the sidewalk and curb on the southwest and southeast sides. The Task Force identified this block as a priority for construction as the current condition has negative impacts to air and water quality. The task force originally proposed to align the curb and gutter on the west side of the street with the curb and gutter of the 200 Block to the north (only a 50'-wide right of way). This would have reduced the travel way significantly, as well as adding a large portion of green space on the west side—both favorable solutions. However, since the original recommendation was made, the Town authorized diagonal parking on this street requiring more space within the travel way curb-to-curb. As a result, this block remains an important connection for large trucks that must move from Colorado Avenue to Pacific Avenue.

Environment/Drainage/Snow Storage. Sediment from erosion along the unimproved northwest drainage is often on the travel way and adversely impacts air and water quality. Snow is stored along the east side. This block is consistent with Telluride's snow storage and drainage plans.

Historic Character. The unusual width of this block is important to the historic character of this area of Telluride. This block was wider than many to accommodate the turning radius of mule teams. Therefore, this aspect of the right of way should be maintained to retain this artifact of our history.

Interfacing Streets. All interfacing streets appear to function well with this block.

Lighting. The standard commercial streetlight on the northeast corner and the streetlight on the southwest corner should remain. This is consistent with the Streetscape Lighting Plan.

Neighborhood Concerns. The neighborhood has expressed a strong desire to improve street drainage and the area around the utility boxes on the east side halfway down the block.

Parking. On street parking demand is high on this block, consisting of neighborhood residents and day-use commuters and visitors. Therefore, year-round parallel parking is permitted along the west side and diagonal parking is permitted along the east side. The only curb cuts are for the alley intersections mid-block on the west and east. Maximizing use of existing off street parking is essential for maintaining on-street parking availability. Additional curb cuts are not recommended.

Pedestrian Concerns. Pedestrian traffic is light to moderate. Northwest sidewalks require replacement. A sidewalk is needed on the northeast half, as well. There is handicap access at all four corners.

Street Furniture and Public Art. There is currently no street furniture or amenities. None is recommended.

Traffic Circulation. Capacity is adequate for the moderate traffic intensity on this block. Maintaining the wide travel way is important for delivery trucks attempting to connect between West Colorado and West Pacific and enter the alley. Emergency vehicle access is also adequate.

Utilities. It is desirable to minimize or eliminate above ground utilities. If possible, locate electric transformer boxes on private property. If they must be in the right of way, concrete bollards are required to protect them from vehicle impact.

RECOMMENDED IMPROVEMENTS

1. Replace the northwest sidewalk with a new 5' wide concrete sidewalk that aligns with sidewalk on the south half of the block.
2. Construct a new 5'-wide sidewalk and a new handicap ramp on the northeast half of the street.
3. Create a 3'-wide green strip between the back of curb and the sidewalk on the east and west side.
4. Install new curb and gutter along the northwest side.
5. Regrade and asphalt the north half of this block, north of the alley.

Refer to Streetscape Warehouse District, 2005, Engineered Street Designs and adjacent street profiles for additional information.

SOUTH TOWNSEND STREET – NORTH ½ of 200 BLOCK

*75' Right of Way
Neighborhood Meetings: July 24, 1996*

BACKGROUND

Existing Street Conditions. This street profile addresses only the north half of the 200 Block of South Townsend Street, because the south half is part of the Depot PUD and is drastically different, in size, function, and requirements. This portion of this block of Townsend Street is 25' wider than typical north-south streets in Telluride. It is paved with chip and seal and slopes downward to the south. The block contains a curb and gutter on both sides of the block. Both the west and east sides contain 5'-wide sidewalks that are in fair condition. Both sides of the block contain green strips between the sidewalk and the travel way. These are generally in good condition and contain trees.

Environment/Drainage/Snow Storage. The existing curb and gutter drainage system effectively handles runoff from this block and this block does not negatively affect air and water quality. It is consistent with snow storage plans and the overall Town Drainage Master Plan.

Historic Character. There are two contributing structures on this block. Any alterations to the right of way should not negatively impact these historic resources. Also, the unusual width of this block is important to the historic character of this area of Telluride. This block was wider than many to accommodate the turning radius of mule teams. Therefore, this aspect of the right of way should be maintained to retain this artifact of our history.

Interfacing Streets. The interfacing streets function well with this block.

Lighting. There is currently one standard light fixture on this block, located at the northeast corner. This fixture should be retained.

Parking. Parallel parking is permitted on the east side of the street. Diagonal parking is permitted on the west side of the street due to the wider right of way. On street parking demand on this block is high due to the nearby Medical Center, the Elks Lodge, and other Depot businesses. Although snow is stored on the east side, there is no winter parking restriction. The current parking configuration is efficient and should be maintained. Maximize use of existing off street parking.

Pedestrian Concerns. Pedestrian traffic on this block is light and is made up mainly of residents and pedestrians accessing nearby businesses. The sidewalks are in good condition and adequately handle pedestrian flow. There is handicap access.

Street Furniture and Public Art. No street furniture or amenities currently exist in the right of way. None are recommended.

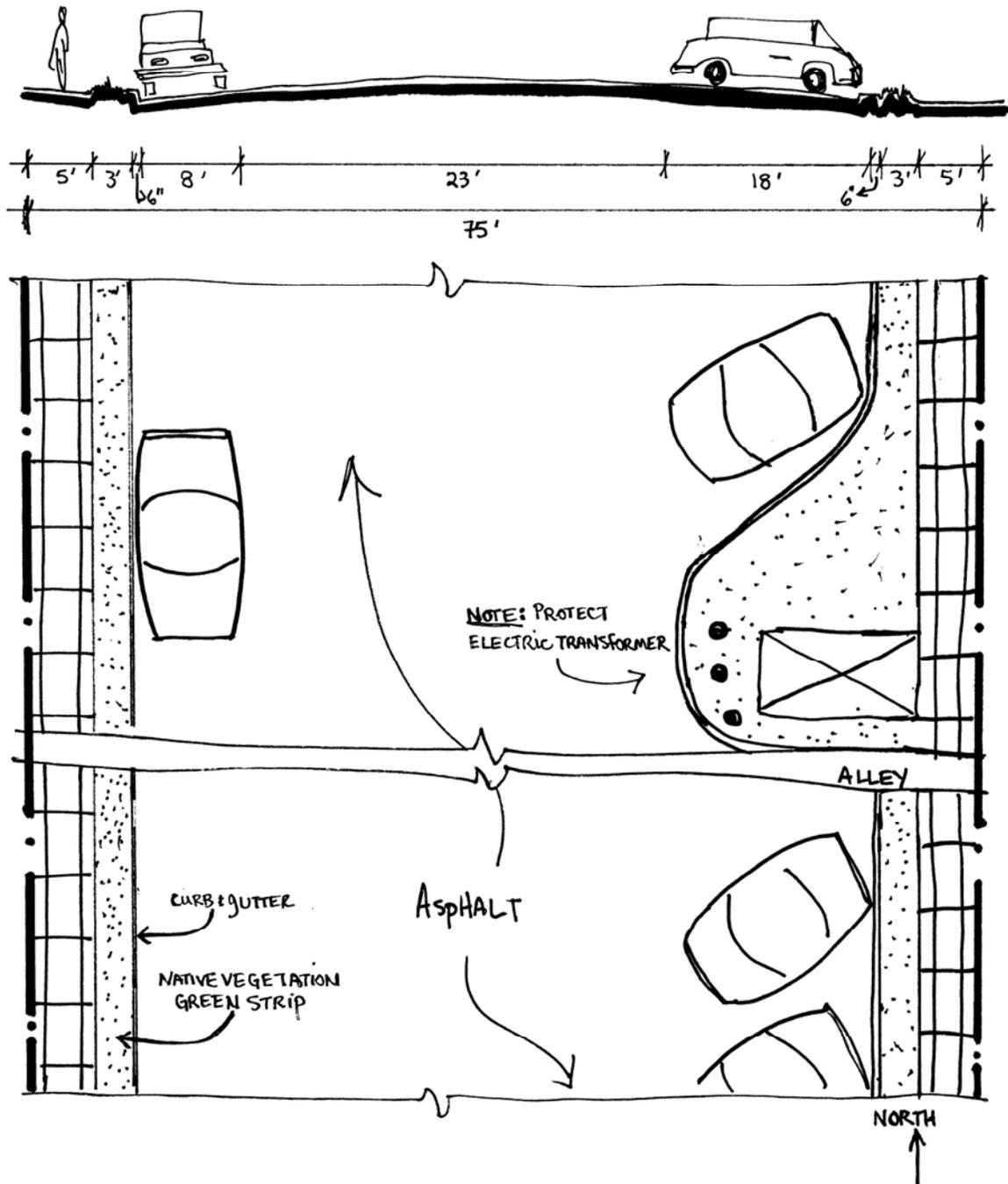
Traffic Circulation. Vehicular traffic on this block is moderate, attributed mainly to the Medical Center, the Elks' Lodge, and commercial businesses in the Depot area. The travel way width appears adequate intensity of use and emergency vehicle access.

Utilities. There are no utility concerns at this time.

RECOMMENDED IMPROVEMENTS

No improvements are recommended at this time.

Refer to Streetscape Warehouse District, 2005, Engineered Street Designs and adjacent street profiles for additional information.



SOUTH TOWNSEND STREET - 100 BLOCK + 1/2 200 BLOCK
(BETWEEN COLORADO + PACIFIC AVENUES + 200 BLOCK ALLEY)

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NORTH WILLOW STREET – 100 BLOCK

50' Right of Way

Neighborhood Meetings: July 10, 1996; February 24, 1997

BACKGROUND

Existing Street Conditions. This south sloping block was improved during Streetscape Phase I 1998 construction. It is paved with asphalt and is intersected mid-block by an east-west alley. There are 5.5'-wide concrete sidewalks immediately adjacent to the curb along the east and west sides. There are no green strips. All infrastructure is in good condition, except for the sidewalk along the northeast section.

Environment/Drainage/Snow Storage. Air and water quality are minimally impacted. Drainage is effective. Snow is stored along the west side. This block is consistent with Telluride's snow storage and drainage plans.

Historic Character. As it exists, the right of way itself neither contributes to nor detracts from the historic character of this area. There are several rated structures along this block, however. Any future alterations to the right of way should not negatively impact these historic resources.

Interfacing Streets. Since improvements have been made to this block the main interfacing streets pose no negative impacts.

Lighting. There are currently two standard type light fixtures on this block. One is on the east side of the block near the intersection with the alley and should remain. The second is on the east side of the block between the alley and Colorado Avenue. This fixture should be removed.

Parking. There is moderate to heavy parking pressure on this block. Parallel parking is permitted on both sides of this block. This on-street parking configuration appears adequate and should be maintained. Existing off-street parking should be maintained to the greatest extent possible in order to maintain the availability of on-street parking.

Pedestrian Concerns. Pedestrian use of this block is moderate and the existing sidewalks effectively handle this traffic. The sidewalk on the north end, east side should be replaced due to its poor condition. There is adequate handicap access.

Street Furniture and Public Art. There is no street furniture or amenities and none is required.

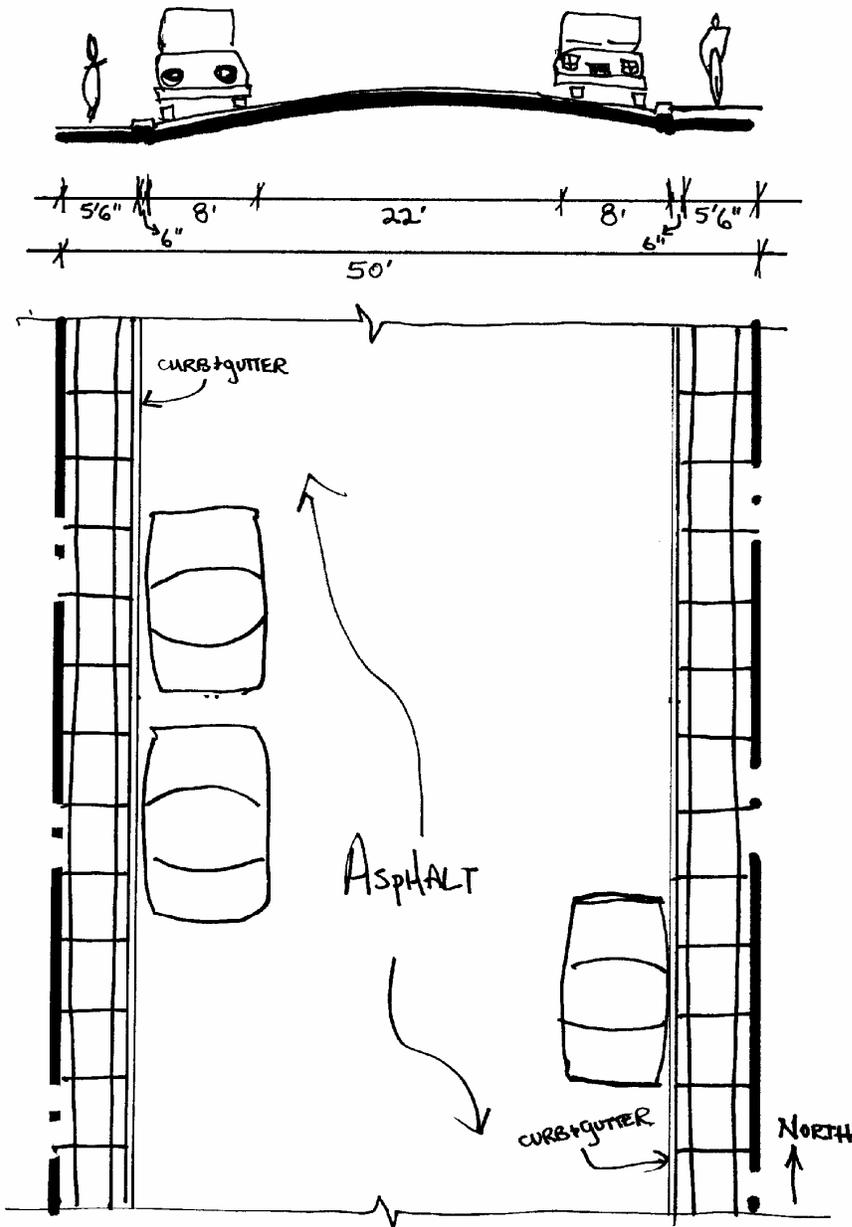
Traffic Circulation. Travel way width appears to be adequate for the moderate traffic volume along this block. Emergency vehicle access also appears adequate.

Utilities. There are no utility concerns at this time.

RECOMMENDED IMPROVEMENTS

1. Existing sidewalks in deteriorated condition should be repaired. If the condition of the sidewalks is poor enough to require replacement, replacement should be done in kind.

Refer to Streetscape Phase I, 1998, Engineered Street Designs and adjacent street profiles for additional information



NORTH WILLOW STREET - 100 BLOCK
(BETWEEN COLORADO + COLUMBIA AVENUES)

NORTH WILLOW STREET – 200 BLOCK

50' Right of Way

Neighborhood Meetings: July 10, 1996; February 24, 1997

BACKGROUND

Existing Street Conditions. This south sloping block was improved during Streetscape Phase I construction. Paved with asphalt, the travel way is 21' wide and there are 3'-wide drain pans on the east and west sides. An east-west drain pan at the south end of the block parallels East Columbia Avenue. North of the Galena Avenue intersection Willow Street narrows. There are remnant sidewalks along the west side. The 14'-wide green strip on the west and the 9'-wide green strip on the east are in good condition. No alleys intersect this block.

Environment/Drainage/Snow Storage. This block does not appear to adversely impact air and water quality. The existing drain pans effectively handle runoff flowing onto this block primarily from the 300 Block of North Willow Street. Snow is stored along the west side. This block is consistent with Telluride's snow storage and drainage plans.

Historic Character. As it exists, this right of way neither contributes to nor detracts from the historic character of the area, although it is bordered by numerous contributing structures. Any future alterations to the right of way should not negatively impact these historic resources.

Interfacing Streets. Galena Avenue east of Willow is surfaced with chip and seal and there is a steep grade from Willow Street to this block of Galena Avenue. This causes vehicles to spin out as they enter Galena from the south, depositing gravel onto Willow Street and into the east drain pan. This negatively impacts this portion of Willow Street and therefore the west end of this block of Galena to the east should be paved with asphalt.

Lighting. The pole-mounted residential light fixture on the southeast corner of this block should be replaced with fixture that is consistent with the Streetscape Lighting Plan.

Parking. On street parking demand is moderate. Therefore, parallel parking is permitted on the east and west sides of the block. In winter, the west side is used to store snow and parking is prohibited. The current parking configuration appears to be efficient and should be maintained. Use of existing off street parking should be maximized.

Pedestrian Concerns. Pedestrian access to this block is light, attributed mainly to neighborhood residents. Because vehicular and pedestrian traffic are low, sidewalks are unnecessary on this block. No special handicap access is needed.

Street Furniture and Public Art. No street furniture or public amenities currently exist and none are required.

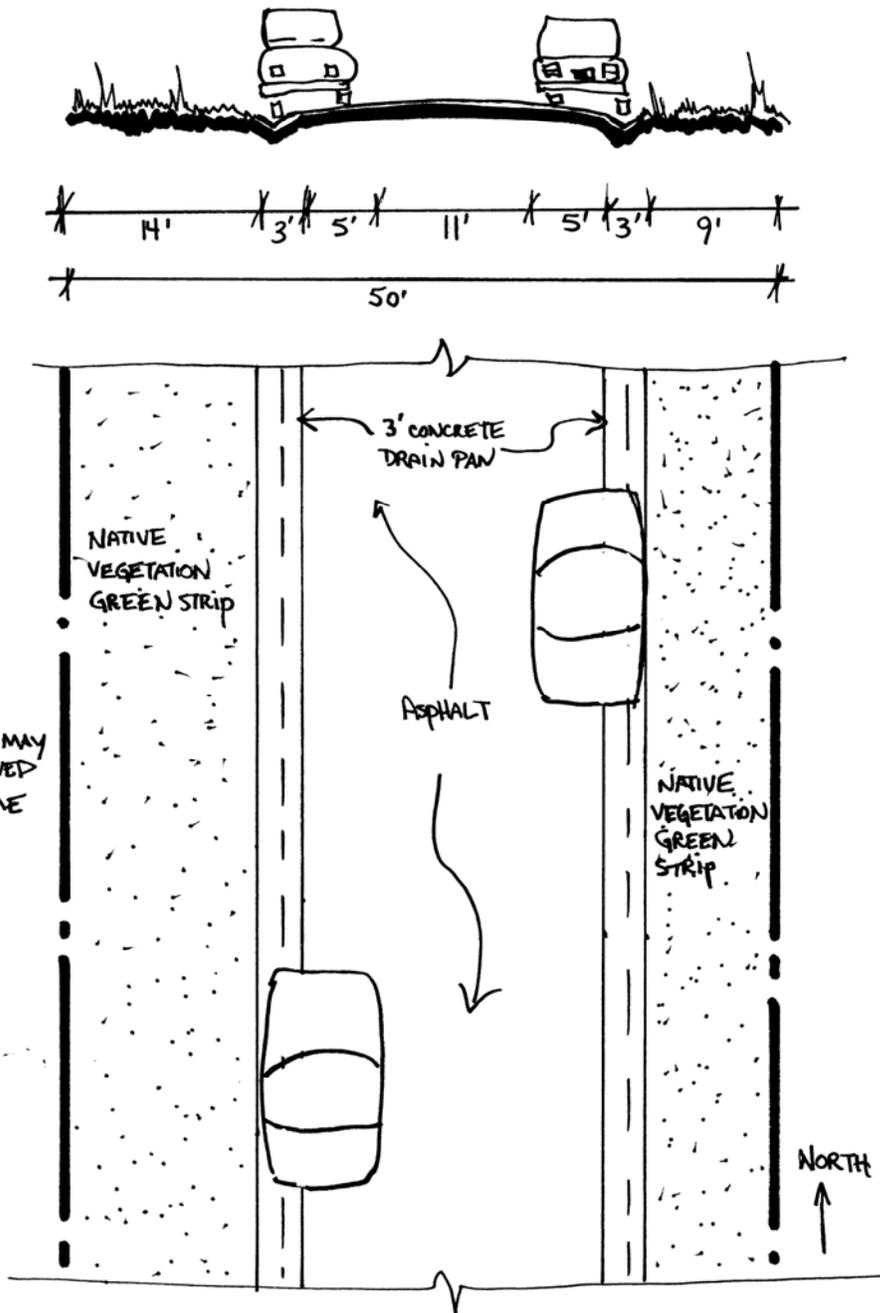
Traffic Circulation. Vehicular traffic on this block is light and attributed mainly to neighborhood residents. Travel way width appears adequate for intensity of use and emergency vehicle access.

Utilities. There are no utility concerns at this time.

RECOMMENDED IMPROVEMENTS

1. Replace the pole-mounted residential light fixture on the southeast corner with a fixture that is consistent with the Streetscape Lighting Plan.
2. Monitor the adequacy of the drain pan to carry runoff volumes from the to be developed North and East Subdivision.

Refer to Streetscape Phase I, 1998, Engineered Street Designs and adjacent street profiles for additional information.



NORTH WILLOW STREET - 200 BLOCK
(BETWEEN COLUMBIA & GALENA AVENUES)

NORTH WILLOW STREET – 300 BLOCK

50' Right of Way

Neighborhood Meetings: July 10, 1996; February 24, 1997

BACKGROUND

Existing Street Conditions. Improved during Streetscape Phase I, 1997, construction, this block slopes very steeply south. The 10'-wide travel way is surfaced with grooved concrete to improve traction. There are 3'-wide concrete drain pans on the east and west sides of the travel way. Both cross East Gregory Avenue to carry runoff onto the 200 Block of North Willow Street. There is no sidewalk, curb and gutter, or formal green strips; although there is a well-vegetated 17'-wide area inside the east and west side of the right of way. All infrastructure is in good condition.

Environment/Drainage/Snow Storage. This impact of this block on air and water quality is minimal. Drainage appears to be effective; although there were historic problems. Snow is plowed only occasionally along this block because of the hazard involved with using large equipment on a steep, icy roadway. The roadway is design to facilitate snowmelt by solar gain, but gravel is often placed on the icy roadway for traction. This gravel can become a problem downhill in drain pans or when ice clears and vehicles lose traction because of the gravel. This block is consistent with Telluride's snow storage and drainage plans.

Historic Character. As it exists, the right of way itself neither contributes to nor detracts from the historic character of the area. There is one contributing structure. Any future alterations to the right of way should not negatively impact these historic resources.

Interfacing Streets. All interfacing streets appear to function well with this block, considering its very steep grade.

Lighting. The pole-mounted residential light fixture at the southeast corner should be replaced with a fixture that is consistent with the Streetscape Lighting Plan. A second pole-mounted residential light fixture at the northeast corner should be removed.

Parking. No on street parking is permitted on this block at any time due to the steep grade and narrow travel way width. All residents must use off street parking, or park on another block. Driveways accessing off street parking from this block should be paved within the right of way to prevent vehicles from depositing gravel in the drain pans. The current parking configuration appears to be working and should be maintained. Use of existing off street parking should be maximized.

Pedestrian Concerns. Attributed mainly to neighborhood residents, pedestrian traffic on this block is light. However, due to the steep grade, and the potential for pedestrian and vehicular accidents it may be advisable to locate a sidewalk, pathway, or staircase within the right of way. The railings that are present to help pedestrians are in need of replacement. Handicap access is limited due to the steep grade.

Street Furniture and Public Art. No street furniture or amenities currently exist. None are recommended.

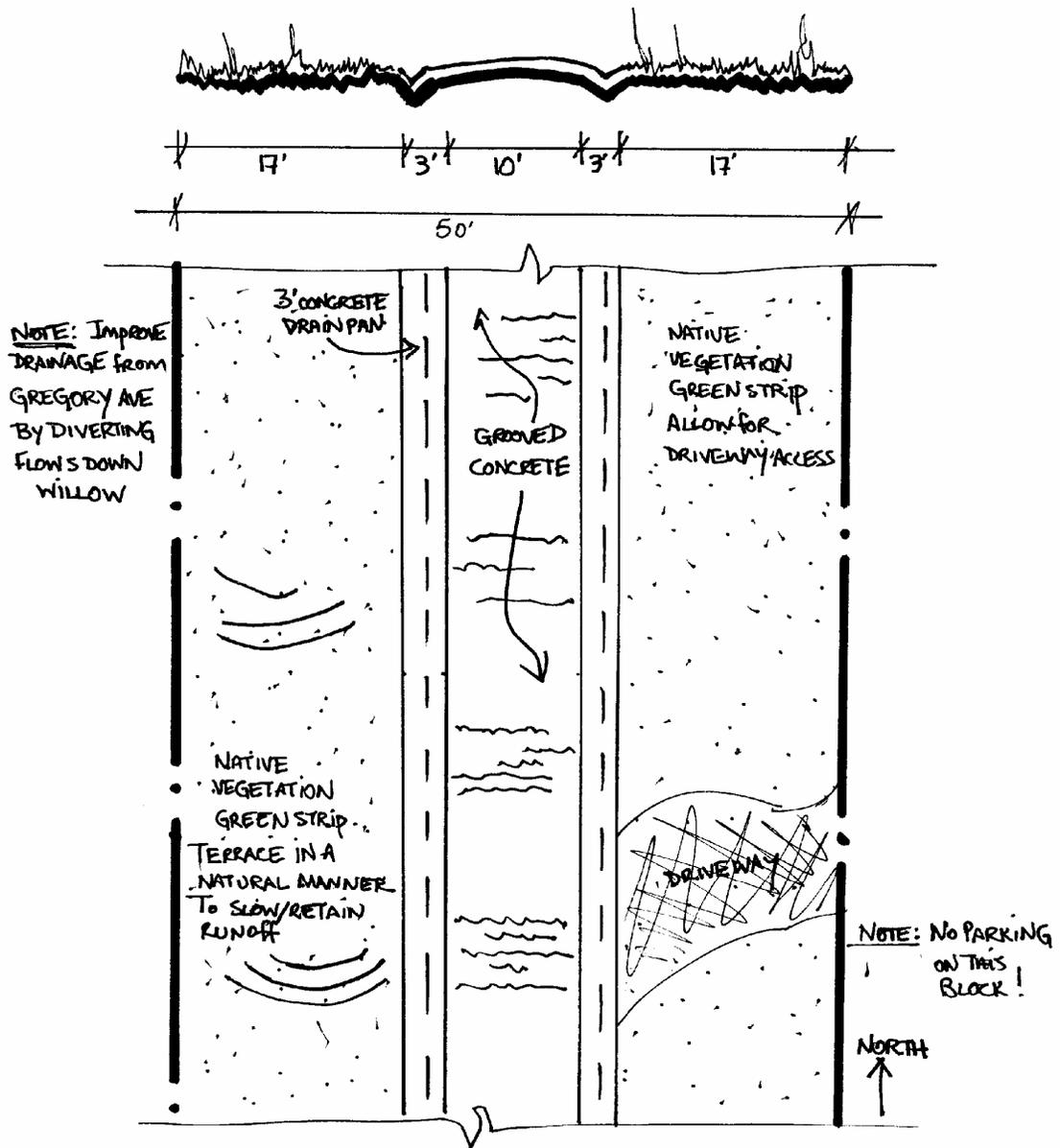
Traffic Circulation. Officially, this block is one-way south, due to the steep grade. However, some residents choose to access their driveways by entering from the south. Although driveway access can be taken from the north, residents' practice of entering from the south appears to create no circulation problems. For the safety of drivers and neighborhood residents, the policy of actively discouraging northbound travel on this block by non-residents should be maintained. After a storm, the block is not usually plowed because of safety issues; therefore, during snow, drivers must make a personal decision to travel along this block. Emergency vehicle access is minimally adequate.

Utilities. There are no utility concerns or issues associated with this block at this time.

RECOMMENDED IMPROVEMENTS

1. Replace the pole-mounted residential light fixture at the southeast corner with a fixture that is consistent with the Streetscape Lighting Plan. Remove the second pole-mounted residential light fixture at the northeast corner.
2. Monitor the adequacy of the drain pan to convey runoff volumes from the soon to be development North and East Subdivision.
3. Re-install the one-way only signage.
4. Remove the gravel on the roadway regularly. Re-stain the concrete to improve the solar gain.

Refer to Streetscape Phase I, 1997, Engineered Street Designs and adjacent street profiles for additional information.



NORTH WILLOW STREET - 300 BLOCK
(BETWEEN GALENA + GREGORY AVENUES)

SOUTH WILLOW STREET – 100 BLOCK

50' Right of Way

Neighborhood Meetings: July 23, 1996

BACKGROUND

Existing Street Conditions. This south sloping block is surfaced with chip and seal. There is a 6.6'-wide concrete sidewalk with curb and gutter along the west side. There is a 7.5'-wide concrete sidewalk with curb and gutter along the southeast half (in front of the Post Office). Existing infrastructure is in good condition. A paved east-west alley intersects the block at its center. The northeast section of the block has no drainage and no sidewalk, except for a small portion of curb and gutter at the northeast corner. Drainage from this block is good until the Pacific Avenue intersection. There are no green strips.

Environment/Drainage/Snow Storage. This block does not appear to negatively impact air and water quality, except when Postal Patrons leave their cars idling while they run into the Post Office on an errand. The existing drainage system is effective. The missing northeast section will improve this. Where drainage is ineffective is at the Pacific Street intersection. This should be improved. Snow is stored on the northeast half of the block, which is consistent with Town snow storage plan.

Historic Character. There is a supporting structure on the block. Improvements to the right of way should not negatively impact this structure.

Interfacing Streets. While the Colorado Avenue intersection works well, there are drainage problems at the Pacific Avenue intersection.

Lighting. There are three standard commercial light fixtures on this block. Two are on the northwest half of the block. These two fixtures are redundant and the northernmost should be removed. The third fixture is at the southeast corner of the block and should remain.

Parking. Demand for on street parking is very high due proximity of the Post Office and the commercial district. Paid parallel parking is permitted along the northwest and northeast sides. Free 15-minute parallel parking is permitted on both sides of the south half to accommodate Postal Patrons. The current parking configuration is efficient and should be maintained. Use of existing off street parking should be maximized.

Pedestrian Concerns. Pedestrian traffic is also heavy—attributed to pedestrian trips to the post office, and access to the River Trail, the Town Park, and nearby businesses. Existing sidewalks effectively handle this traffic, but pedestrian flow is impeded at the missing section of sidewalk on the northeast side. There is adequate handicap access.

Street Furniture and Public Art. Benches and a bike rack are located in front of the post office. These are appropriate and should remain. Public art could be considered in front of the post office if it does not interrupt the flow of pedestrian or vehicular traffic.

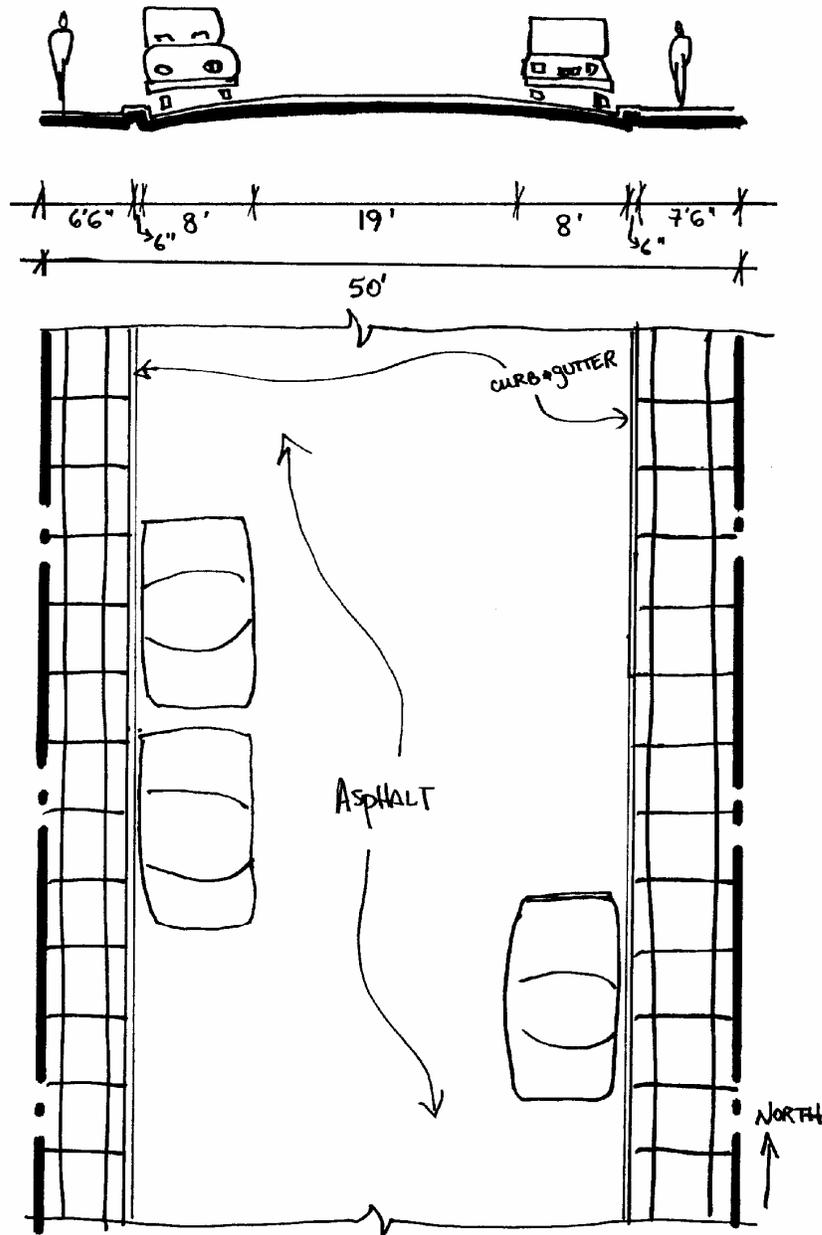
Traffic Circulation. Vehicular traffic on this block is heavy—attributed to vehicle trips to the Post Office. Travel way width appears adequate for the intensity of use and emergency vehicle access.

Utilities. There are utility concerns or issues associated with this block at this time. There is no mention in meeting notes of utility concerns, though minimization or elimination of above ground utilities is desirable. If possible, electricity transformer boxes should be located on private property. If they are located within the right of way, they should be as inconspicuous and unobtrusive as possible.

RECOMMENDED IMPROVEMENTS

1. Construct the missing 7.5'-wide sidewalk and curb and gutter on the northeast half of the block.
2. Remove the northernmost standard light fixture on the west side of the block.
3. Re-engineer or simply re-construct the intersection at East Pacific Avenue to improve drainage.
4. Eliminate the east-west drain pans in this East Pacific Avenue intersection.

Refer to Streetscape Warehouse District, 2005, Engineered Street Designs and adjacent street profiles for additional information.



SOUTH WILLOW STREET - 100 BLOCK
(BETWEEN COLORADO + PACIFIC AVENUES)

SOUTH WILLOW STREET – 200 BLOCK

*50' Right of Way
Neighborhood Meetings: July 23, 1996*

BACKGROUND

Existing Street Conditions. This is a half block that extends from Pacific Avenue south to the River Park, where it terminates. This block slopes slightly south and is surfaced with chip seal. There is a curb and gutter on the east and west side of the block. Along the east side, behind the curb, are intermittent patches of sidewalk separated by landscaped areas that have cottonwood trees. The sidewalks and curb and gutter are in good condition. The chip seal is in poor condition, largely because of poor drainage. There is also a curb cut on the east side. The end of the gutter on the west side tends to fill with sediment. Its terminus should be engineered so that flow into the Hobgood-Drew Wetland is more efficient. The southwest edge of the block borders the Hobgood-Drew Wetland. The Willow Street Wetland is at the south end of the travel way. This block provides an access point to the River Trail.

Environment/Drainage/Snow Storage. In its current condition, this block adversely impacts air and water quality. Because this block drains toward the San Miguel River riparian corridor, it is important that drainage is properly managed. A 3'-wide drain pan should be constructed along the existing curb on the east side of the block. This drain pan should discharge runoff into the Willow Street Wetland so that it is treated prior to entering the river. On the west side, the curb and gutter should be extended south and east to provide a wider buffer for the Hobgood-Drew Wetland, separate the travel way from the River Trail connection and guide runoff that is currently discharging onto the River Trail into the Willow Street Wetland. It is proposed that snow be stored at the south end of this block at the north edge of the Willow Street Wetland. The landscaped area south of the dead end has been bermed to allow for this. These improvements will make this block consistent with Town snow storage plans and the Town Drainage Master Plan.

Interfacing Streets. The poor drainage across East Pacific Avenue should be addressed.

Historic Character. This block neither contributes to, nor detracts from the historic character of the area.

Lighting. There is a standard commercial streetlight on the west side, near the center of the block, which is consistent with the Streetscape Lighting Plan. This fixture should remain.

Neighborhood Concerns. No major neighborhood concerns have been expressed.

Parking. Parking demand is moderate on this block. Therefore, parallel parking is permitted on the east and west sides. Residents, however, have been parking in the River Corridor beyond the travel way. This must be discouraged. Use of existing off-street parking should be maximized.

Pedestrian Concerns. Pedestrian traffic on this block is moderate and attributed to pedestrians accessing the River Trail and going to and from the Post Office. The existing sidewalk on the west side of the block could effectively handle this traffic if it was connected to the River Trail. There is adequate handicap access.

Street Furniture and Public Art. No street furniture or amenities currently exist on this block. Street furniture could be considered at the interface this block with the River Trail.

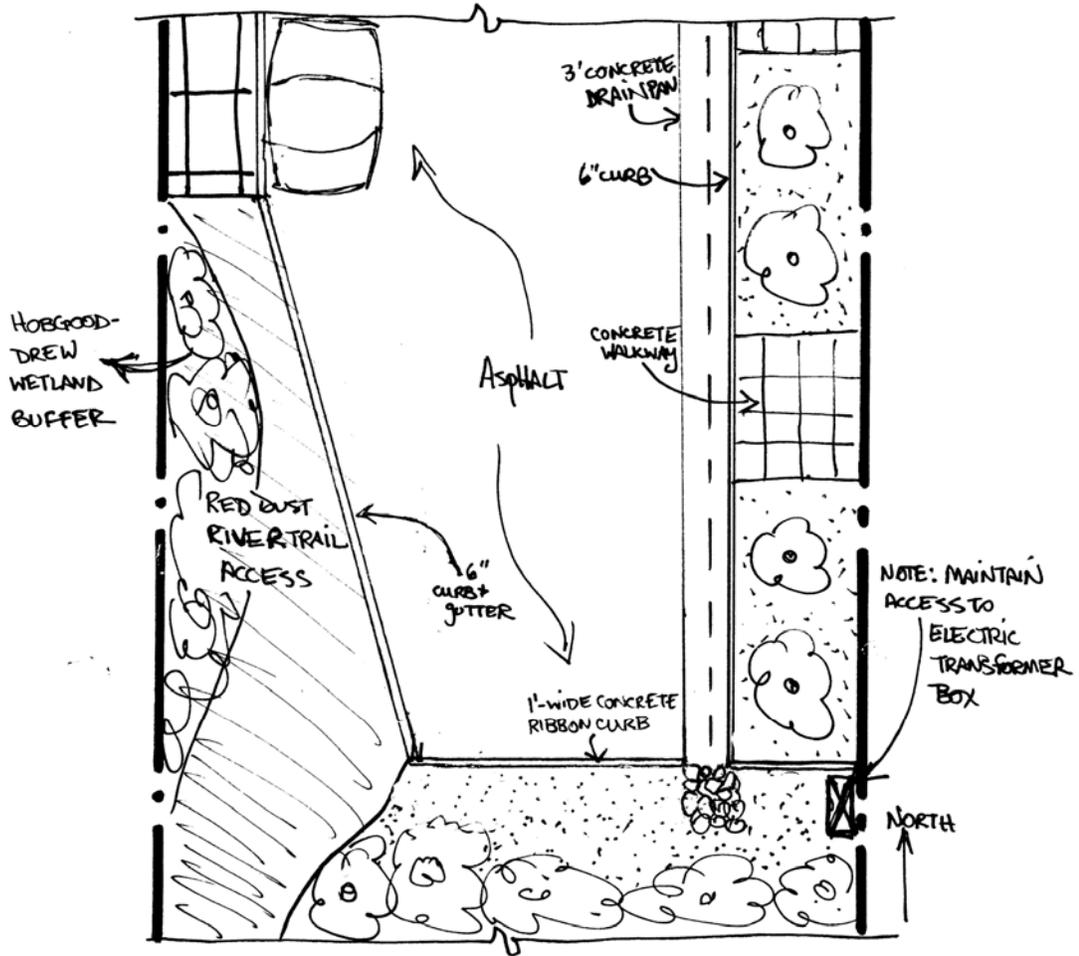
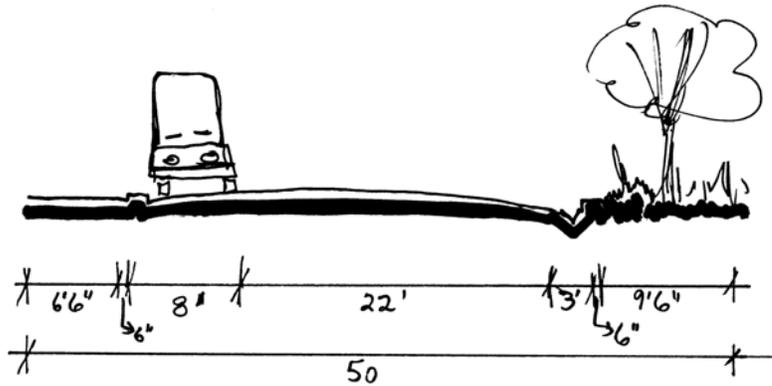
Traffic Circulation. This is a dead end; therefore vehicle traffic on this block is light, and confined mainly to local residents. The travel way width is adequate for intensity of use and emergency vehicle access.

Utilities. There are utility concerns or issues associated with this block at this time.

RECOMMENDED IMPROVEMENTS

1. Rework the River Park Corridor interface at the south end of the block to better manage drainage and to more effectively move pedestrians from the west side sidewalk onto the River Trail.

Refer to Streetscape Warehouse District, 2005, Engineered Street Designs and adjacent street profiles for additional information.



SOUTH WILLOW STREET - 200 BLOCK
(BETWEEN PACIFIC AVENUE + RIVER PARK)

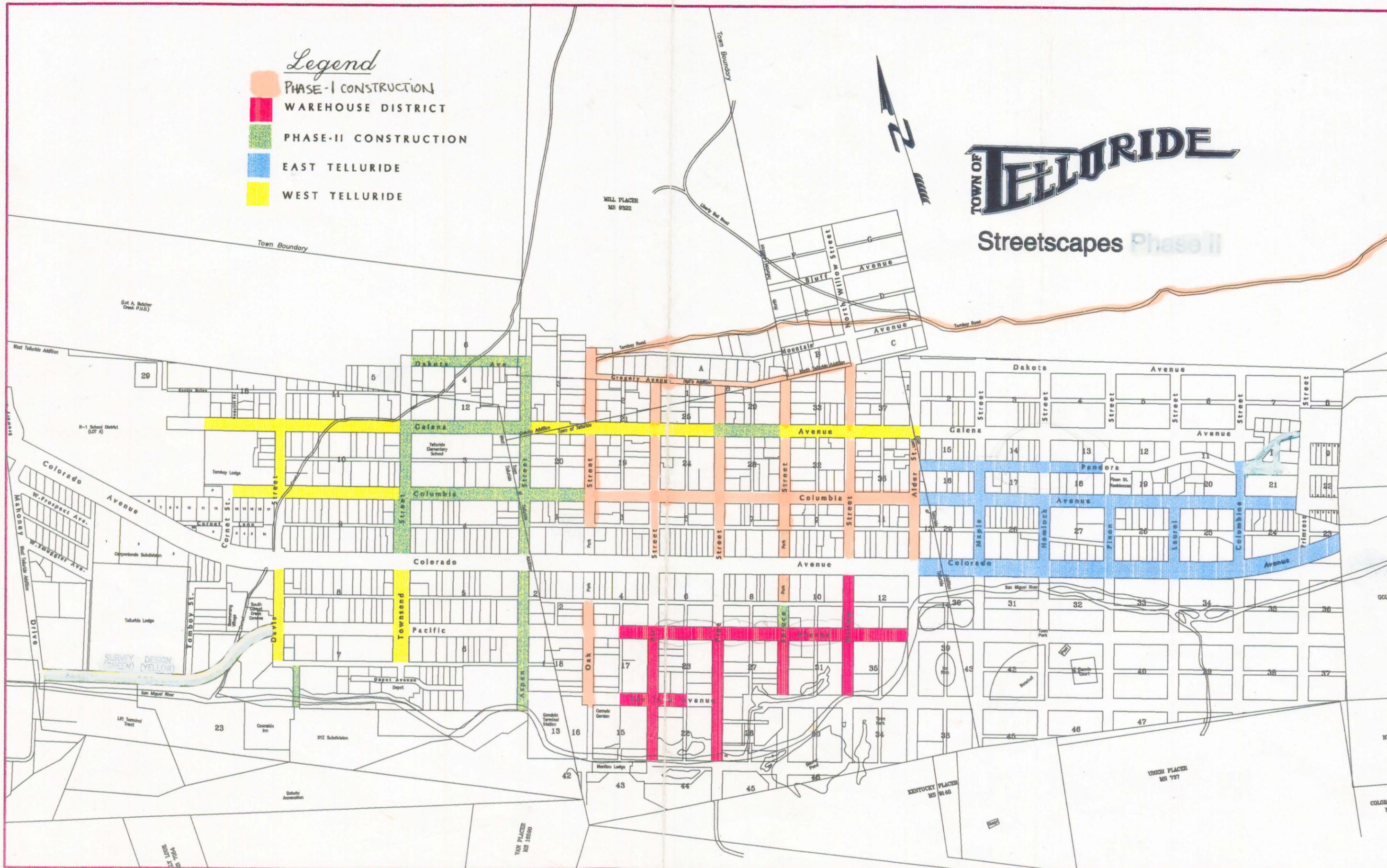
Engineered Street Designs
(by Neighborhood)

Legend

-  PHASE-I CONSTRUCTION
-  WAREHOUSE DISTRICT
-  PHASE-II CONSTRUCTION
-  EAST TELLURIDE
-  WEST TELLURIDE

TOWN OF TELLURIDE

Streetscapes Phase II



TELLURIDE STREETSAPES CONSTRUCTION PROJECT PHASE 1, 1997

SOUTH OAK STREET/NORTH OAK STREET
300 BLOCK NORTH SPRUCE STREET
300 BLOCK NORTH WILLOW STREET
GREGORY STREET\BETWEEN PINE AND WILLOW
TOMBOY ROAD

TOWN OF TELLURIDE SAN MIGUEL COUNTY, COLORADO

PROJECT TEAM

CLIENT: TOWN OF TELLURIDE
P.O. BOX 397
TELLURIDE, COLORADO 81435
PHONE: (970)728-3071

CONTACT: WILLIAM FROWNFELTER OR BOB BISHOP
PUBLIC WORKS DEPARTMENT
300 BLACK BEAR ROAD
TELLURIDE, COLORADO 81435
PHONE: (970)728-2177

ENGINEER: FOLEY AND ASSOCIATES, INC.
P.O. BOX 1385
TELLURIDE, COLORADO 81435
PHONE: (970)728-6153

ENGINEER: JACK GARDNER, P.E.
PROJECT MANAGER: KEVIN J. SMITH

SURVEYOR: FOLEY AND ASSOCIATES, INC.
P.O. BOX 1385
TELLURIDE, COLORADO 81435
PHONE: (970)728-6153

PROJECT SURVEYOR: J. DAVID FOLEY

PROJECT BENCHMARK: BRASS CAP MONUMENT AT THE
CENTERLINE OF COLORADO AVENUE
AND OAK STREET, ELEVATION 8788.55



SHEET INDEX

1	COVER SHEET
2	INDEX SHEET
3	REMOVAL, RELOCATION, AND UTILITY 100-200 BLOCK SOUTH OAK STREET
4-5	PLAN AND PROFILE 100-200 BLOCK SOUTH OAK STREET
6-7	CROSS-SECTIONS 100-200 BLOCK SOUTH OAK STREET
8-9	REMOVAL, RELOCATION, AND UTILITY 100-300 BLOCK NORTH OAK STREET
10-12	PLAN AND PROFILE 100-300 BLOCK NORTH OAK STREET
13-15	CROSS-SECTIONS 100-300 BLOCK NORTH OAK STREET
16	REMOVAL, RELOCATION, AND UTILITY GREGORY STREET BETWEEN PINE AND WILLOW
17-18	PLAN AND PROFILE GREGORY STREET BETWEEN PINE AND WILLOW
19-20	CROSS-SECTIONS GREGORY STREET BETWEEN PINE AND WILLOW
21	REMOVAL, RELOCATION, AND UTILITY 300 BLOCK NORTH SPRUCE STREET
22	PLAN AND PROFILE 300 BLOCK NORTH SPRUCE STREET
23	CROSS-SECTIONS 300 BLOCK NORTH SPRUCE STREET
24	REMOVAL, RELOCATION, AND UTILITY 300 BLOCK NORTH WILLOW STREET
25-26	PLAN AND PROFILE 300 BLOCK NORTH WILLOW STREET
27	CROSS-SECTIONS 300 BLOCK NORTH WILLOW STREET
28-31	PROJECT DETAILS
32-41	PLAN AND PROJECT DETAILS TOMBOY ROAD

DESIGN NOTES

1.) Elevations and dimensions of adjacent facilities and obstructions shall have precedence over elevations and dimensions shown on the drawings. When a conflict arises between elevations and dimensions shown on the drawings, the Contractor shall notify the owner's representative, in writing within 24 hours from the time the conflict is noticed.

2.) The designs herein are for the exclusive use of Owner on the project only. No change or adaptation by and third party shall be allowed without Foley Associates, Inc. written approval. Should a change or adaptation be permitted by the Owner, Foley Associates, Inc. shall have no further responsibility for the contents herein.

3.) The Contractor must coordinate their needs for traffic control with the traffic control contractor. The traffic control contractor must submit traffic control plans and coordinate all traffic control with the Project Manager. The traffic control plans must be submitted and approved 72 hours prior to starting construction and before making each modification. The Traffic Control Plan for the intersection of S. Oak St. and Pacific and N. Oak St. and Columbia shall show (1) drive lane each direction opened at all times for east and west bound traffic. Any modification to these mentioned specifics, must be approved by Project Manager and/or Owner.

4.) The Contractor must work with the adjacent residents to provide and maintain sufficient access during the duration of the project. It will be the Contractor's responsibility to coordinate and communicate with the residents during construction.

5.) The Contractor must maintain access to all businesses during their business hours unless the Contractor is able to make other arrangements with the business. It will be the Contractor's responsibility to coordinate and communicate with the businesses during construction.

6.) The Contractor shall maintain a safe work area and protect area residents, motorists, bicyclists, pedestrians, and children from work area hazards. The Contractor shall provide all work area safety control devices, such as barricades and safety fence around all excavations and drop-offs. Work area safety control devices will not be paid for separately. Cost for this equipment should be included in the unit costs for the work items which will require work area safety control devices.

7.) The Town of Telluride will provide all of the testing required for the construction of this project. The Contractor shall pay for all failing tests at rates based upon the cost of securing such tests from an independent testing laboratory.

8.) The Streets adjacent to the construction area shall be cleaned of debris generated by the project by the Contractor at the earliest opportunity, or immediately when deemed necessary by the Owner for the safety of traffic or pedestrians, but in no case shall the street be left uncleaned after the completion of the day's work. It shall be the Contractor's responsibility to provide the necessary equipment and materials to satisfactorily clean the roadway at no additional cost to the project. The Town Street Department will sweep the street at an hourly rate of \$50.00. The minimum is two hours (\$100.00). The contractor must sign a contract with the Town Public Works Department prior to requesting street sweeping. If the streets are not cleaned by the Contractor after 24 hour notice from the Project Manager, the Project Manager will have the Public Works Department sweep the streets at a cost of \$50.00 per hour. The minimum charge is four (4) hours (\$200.00). The Contractor will be back charged for the expenses incurred.

9.) The Contractor is responsible for dust control, and it shall be the Contractor's responsibility to provide the necessary equipment and materials to satisfactorily control dust at all times. Water shall be used as the dust palative where required. The cost for dust control should be included in unit costs for excavation and backfill.

10.) The Contractor is responsible for protecting existing street improvements, signs, utilities, landscaping, and private property form being disturbed or destroyed during construction. Following the completion of construction the Project Manager will inspect all existing improvements for damage. All damage caused by the Contractor will be corrected and paid for by the Contractor.

11.) All construction shall conform to these plans, and the specifications as identified below:

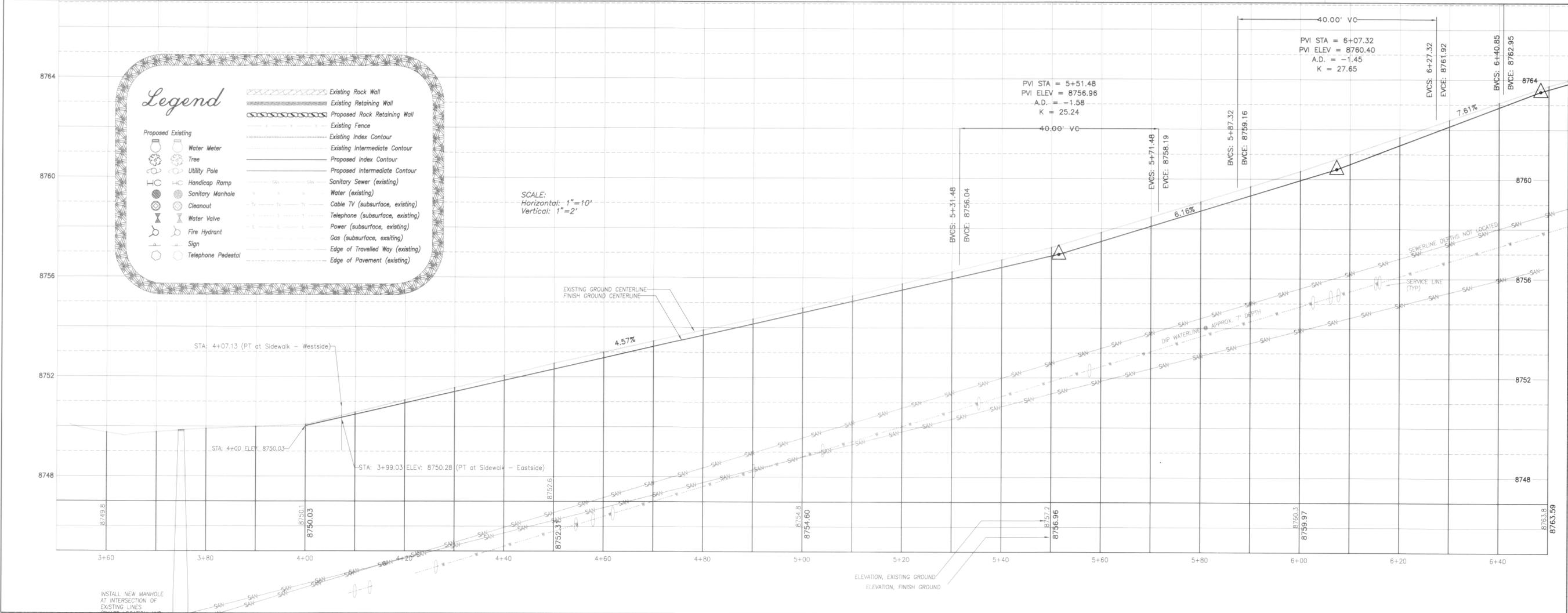
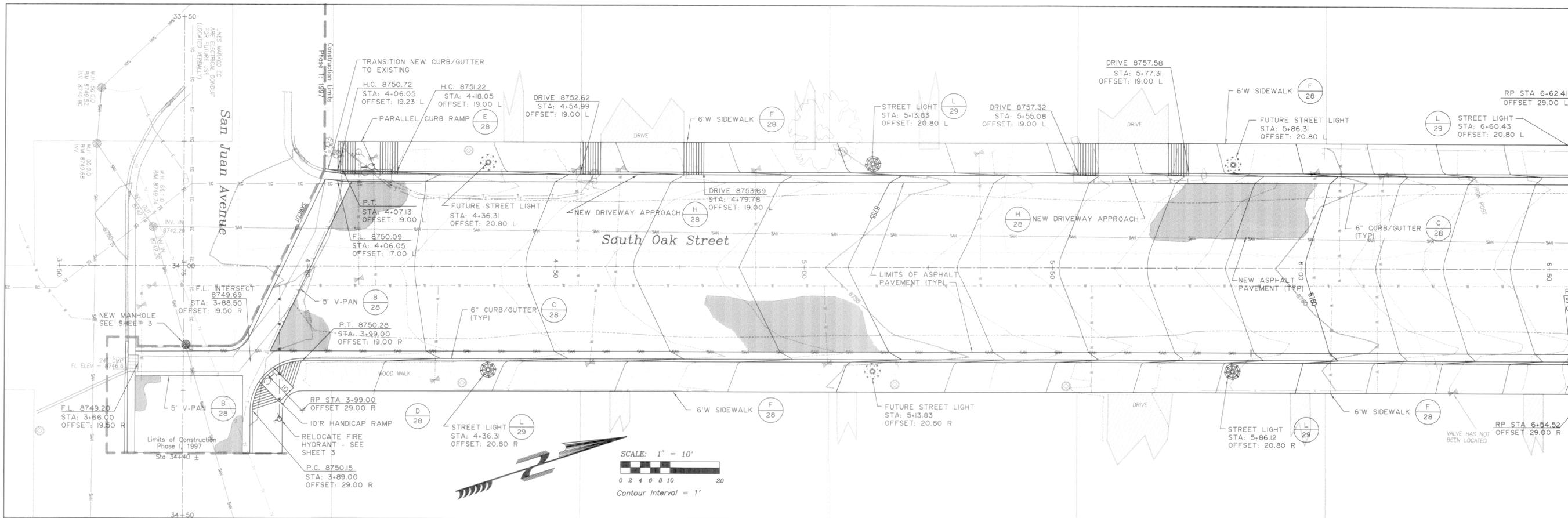
12.) Street Construction shall conform to the State of Colorado Division of Highways "Standard Specifications for Road and Bridge Construction" (1991), except as revised in the Project Specifications which are included with the contract documents. The Town of Telluride Standards for Streets shall also serve as a minimum standard.

13.) Water and Sewer Service Lines shall be relocated or lowered in conformance with the Town of Telluride Standard Construction Specifications.

14.) Landscaping and Irrigation Systems shall conform to the landscape and irrigation specifications contained in the contract documents.

15.) The Contractor is responsible for installing erosion control measures in a timely manner. The Contractor is responsible for cleaning sediment from all new or existing storm sewer facilities which is caused by the construction of this project.

16.) The Contractor is responsible for adjusting all manholes and valve boxes to grade which will be paid for on a unit cost basis.



Telluride Streetscapes (South Oak Street)
PROPOSED PLAN AND PROFILE

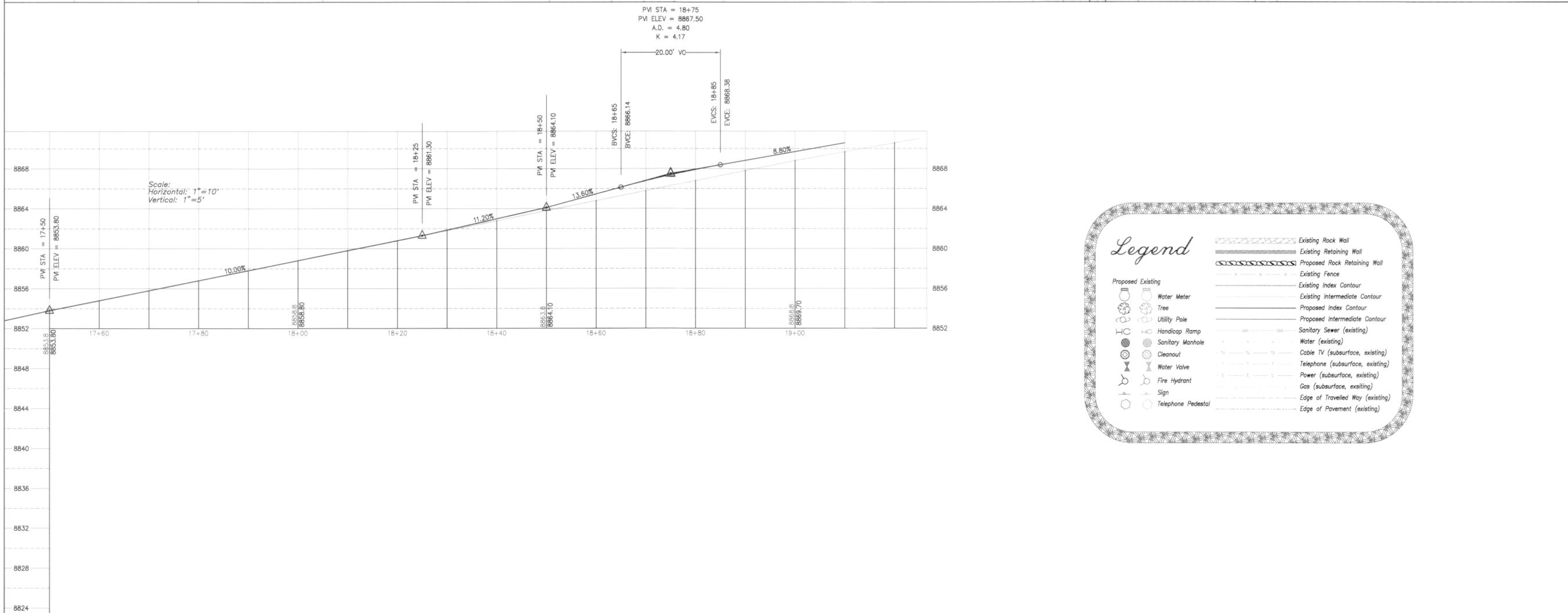
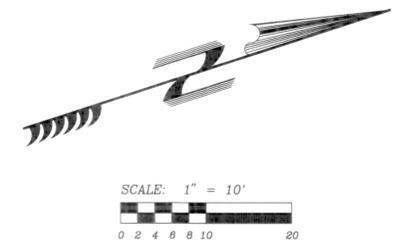
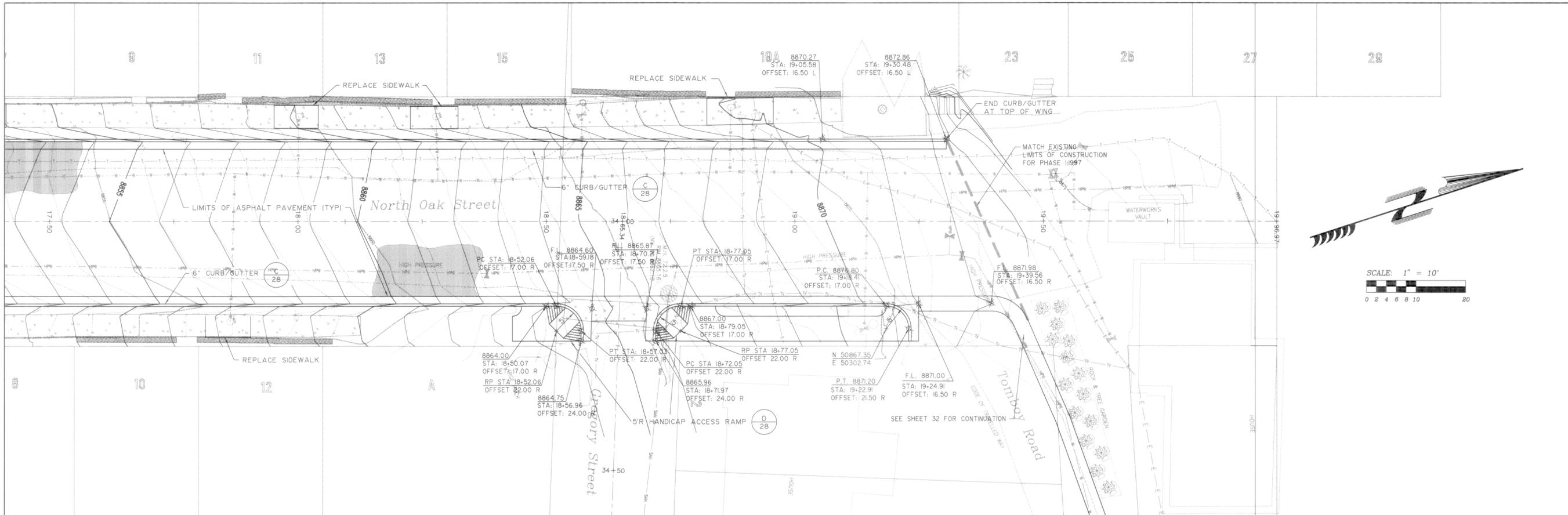
FOLEY ASSOCIATES, INC.
ENGINEERING - PLANNING - SURVEYING
PO Box 1865 125 W. Pacific Ave Ste. B-1
Telluride, Colorado, 81435
970-728-6133 fax 970-728-6050
e-mail to mail@foleyassoc.com

Client:
Town of Telluride
PO Box 397
Telluride
Colorado, 81435
Phone: 970-728-3071
e-mail:

Project Mgr:	J. GARDNER/K.J. SMITH
Drawn by:	MJ
Checked by:	DF
Start date:	5/7/97
Dwg path:	96122\SOAK-CAL
Sheet No.	4 of 41
Project #:	96122

Contact:
William Fox
300 Blue Ridge Road
Telluride
Colorado, 81435
Phone: 970-728-2177
e-mail:

Engineer's stamp and signature



Legend

	Existing Rock Wall
	Existing Retaining Wall
	Proposed Rock Retaining Wall
	Existing Fence
	Existing Index Contour
	Existing Intermediate Contour
	Proposed Index Contour
	Proposed Intermediate Contour
	Sanitary Sewer (existing)
	Water (existing)
	Cable TV (subsurface, existing)
	Telephone (subsurface, existing)
	Gas (subsurface, existing)
	Edge of Travelled Way (existing)
	Edge of Pavement (existing)

Proposed Existing

	Water Meter
	Tree
	Utility Pole
	Handicap Ramp
	Sanitary Manhole
	Cleanout
	Water Valve
	Fire Hydrant
	Sign
	Telephone Pedestal

Telluride Streetscapes (North Oak Street)
 PLAN AND PROFILE, STA 17+4.0 TO 19+96.97

Client:
 Town of Telluride
 PO Box 397
 Telluride
 Colorado, 81435
 Phone: 970-728-3071
 Fax: 970-728-0548
 e-mail: info@townof Telluride.com

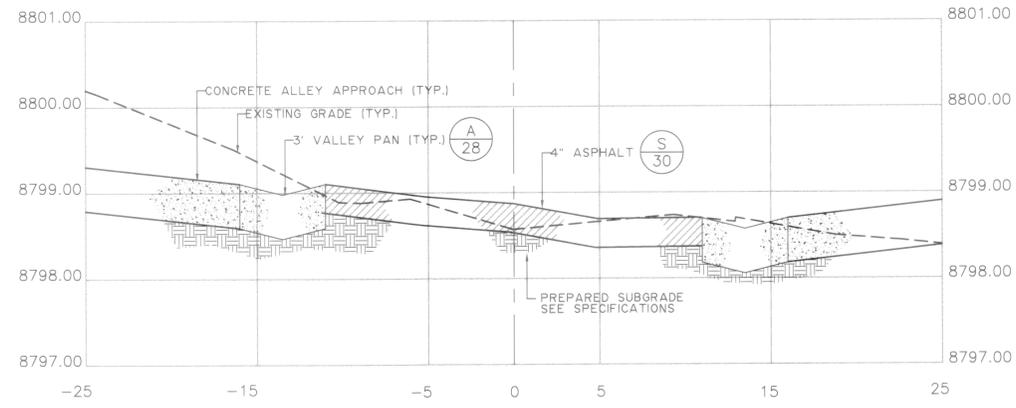
Contact:
 William F. Foleys, Inc.
 300 Black Box Road
 Telluride
 Colorado, 81435
 Phone: 970-728-2177
 Fax: 970-728-0548
 e-mail: info@wffoleys.com

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 Telluride, Colorado, 81435
 970-728-6133 fax 970-728-6050
 e-mail to mail@foleyassoc.com

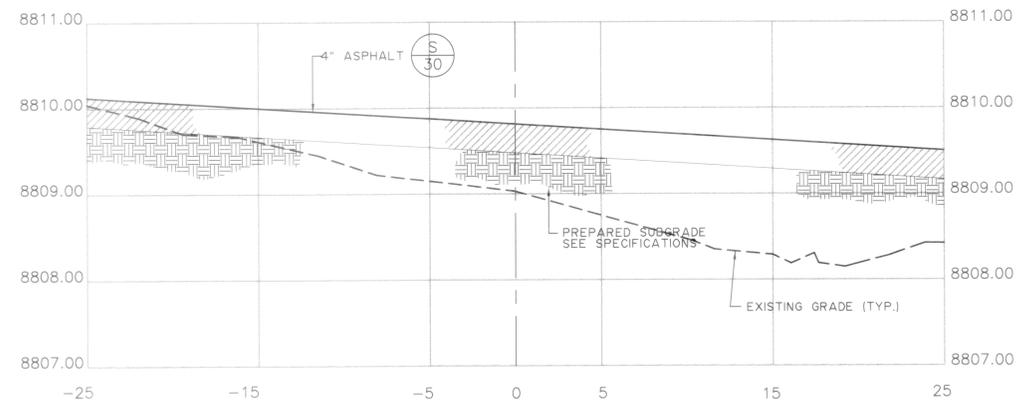
Engineer's stamp and signature

by	
date	
description	
Rev.	
Project Mgr:	JG
Drawn by:	MJ
Checked by:	
Start date:	5/1/97
Dwg path:	96122\NOAK
Sheet No.	12 of 41
Project #:	96122

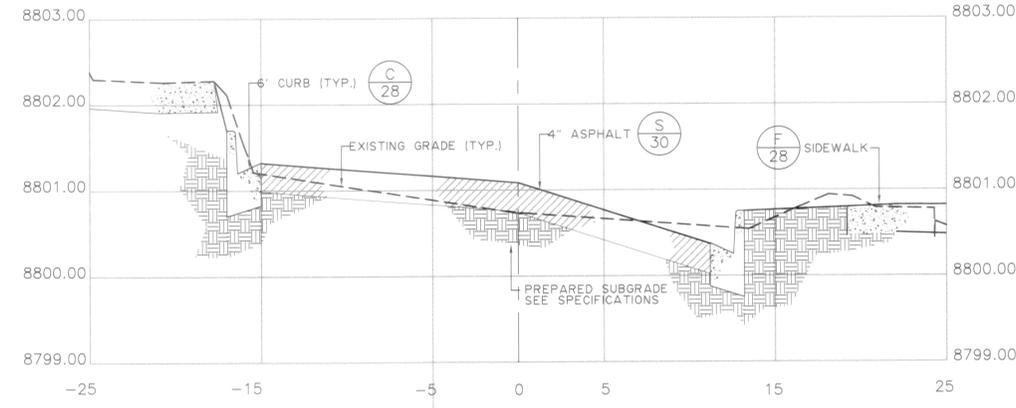
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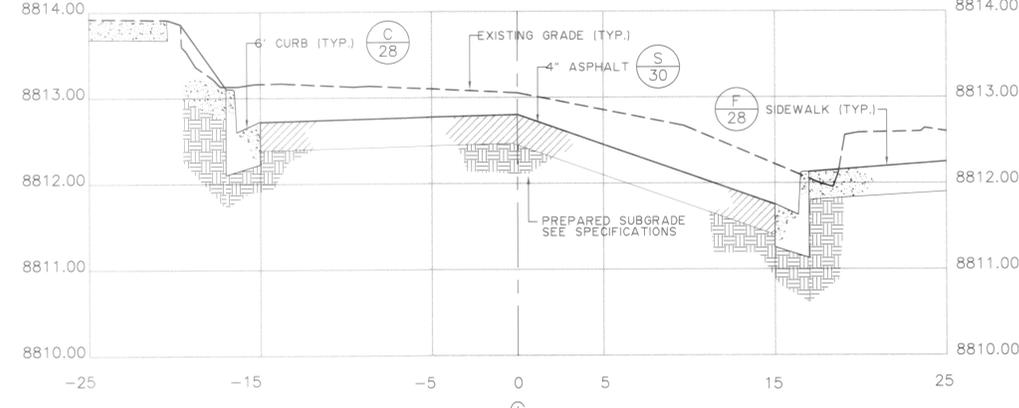
Section 11+75
N.Oak Plan 25



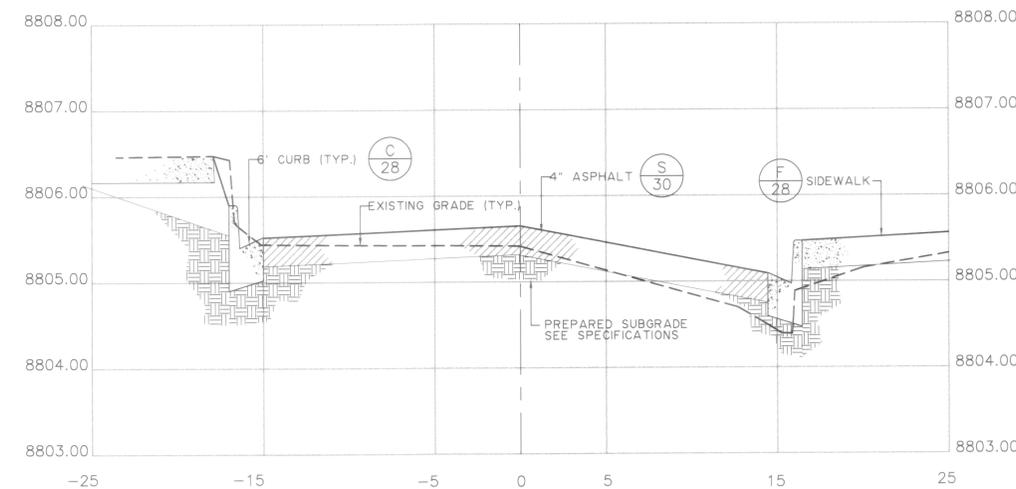
Section 13+00
N.Oak Plan 25



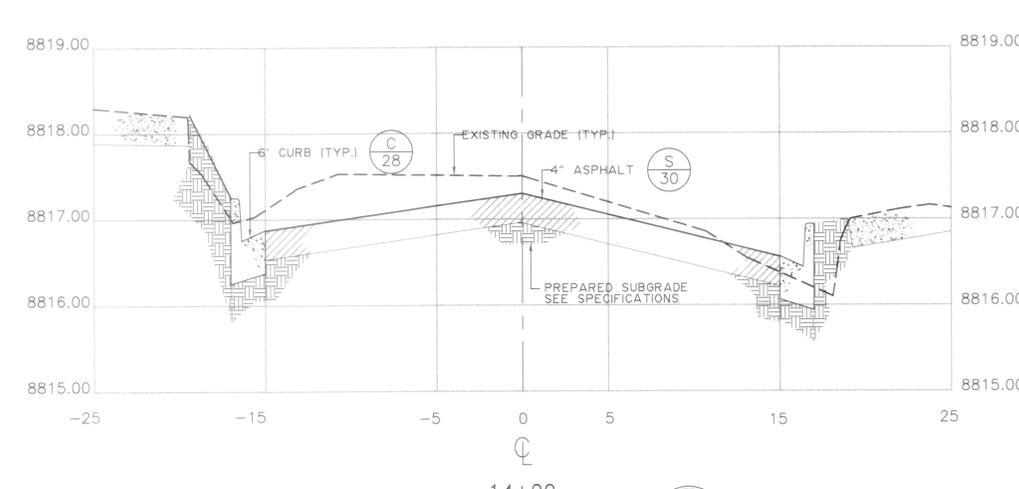
Section 12+00
N.Oak Plan 25



Section 13+50
N.Oak Plan 00



Section 12+50
N.Oak Plan 25



Section 14+00
N.Oak Plan 00

SCALE:
Horizontal: 1" = 5'
Vertical: 1" = 1'

Finish Ground —————
Existing Ground - - - - -

Telluride Streetscapes (North Oak Street)

SECTIONS STA 11+75 TO 14+00

Contact:
William Frowmeyer or Rob Bishop
300 Blair Road
Telluride, Colorado, 81435

Client:
Town of Telluride
PO Box 397
Telluride, Colorado, 81435

Phone: 970-728-2177
e-mail: wfrowmeyer@townof Telluride.com

Phone: 970-728-3071
e-mail: rob@foleyassoc.com

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PO Box 1385, 125 W. Pacific Ave. Ste. B-1
Telluride, Colorado, 81435
970-728-6153 Fax 970-728-6060
e-mail to matt@foleyassoc.com

Engineer's stamp and signature

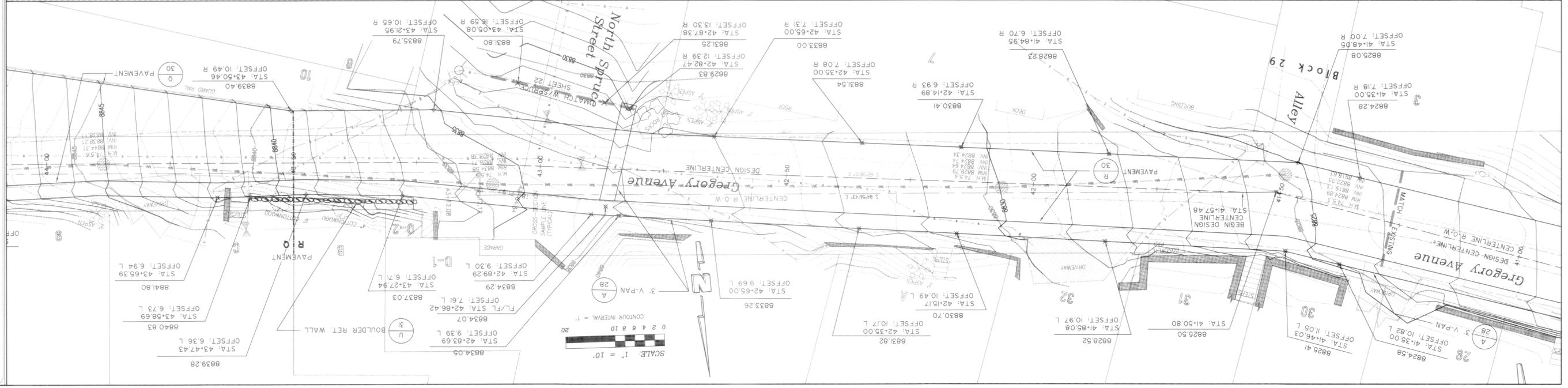
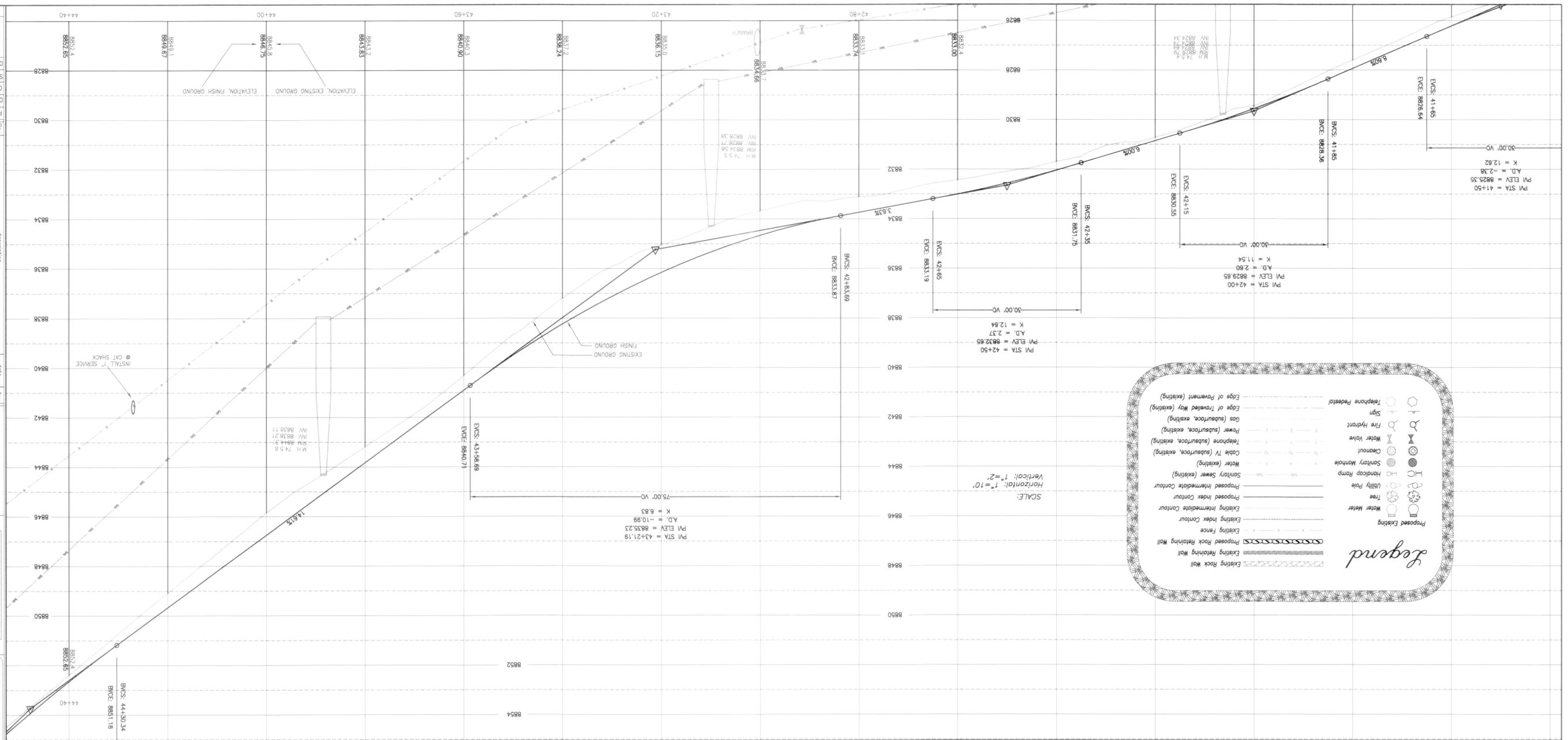
Rev.	description	date	by

Project Mgr: JG
Drawn by: MJ/MFS/KJS
Checked by: DF
Start date: 4/28/97
Dwg path: 96122\NOAKSECT
Sheet No.
13 of **41**
Project #: 96122

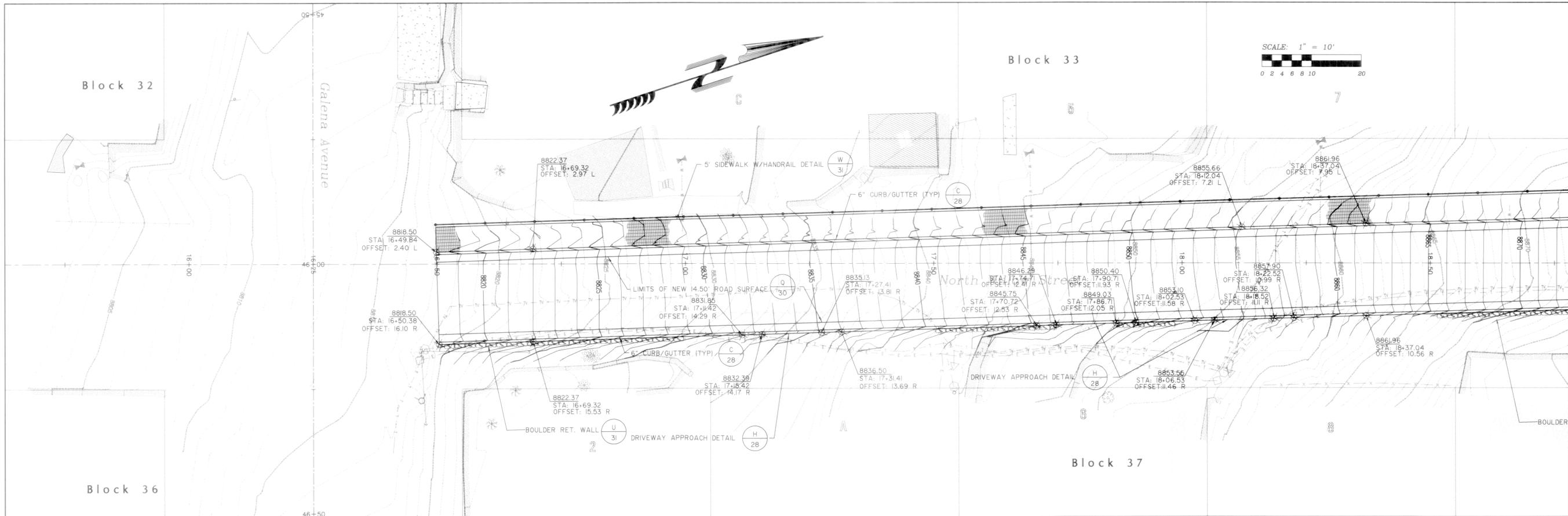
Scale: 1" = 3'(PS)
Drawing File: DRAWINGS\96122\NOAKSECT.DWG (Michael)

Legend

	Proposed Existing
	Water Meter
	Tree
	Utility Pole
	Handicap Ramp
	Sanitary Manhole
	Water (existing)
	Sanitary Sewer (existing)
	Proposed Intermediate Contour
	Existing Intermediate Contour
	Proposed Index Contour
	Existing Index Contour
	Existing Fence
	Proposed Rock Retaining Wall
	Existing Rock Retaining Wall
	Telephone Pedestal
	Sign
	Fire Hydrant
	Water Valve
	Cenotuf
	Cable TV (subsurface, existing)
	Telephone (subsurface, existing)
	Power (subsurface, existing)
	Gas (subsurface, existing)
	Edge of Travelled Way (existing)
	Edge of Pavement (existing)



<p>Project #: 96122</p> <p>Sheet No. 17 of 41</p> <p>Dwg path: 96122\grev</p> <p>Start date: 4/28/97</p> <p>Checked by: MLJ</p> <p>Drawn by: MFS</p> <p>Project Mgr: JC</p>	<p>Revised: _____</p> <p>description: _____</p> <p>date: _____</p> <p>by: _____</p> <p>Engineer's stamp and signature: _____</p>
<p>FOLEY ASSOCIATES, INC.</p> <p>ENGINEERING - PLANNING - SURVEYING</p> <p>PO Box 1885 125 W. Pacific Ave. Ste. B-1 Telluride, Colorado 81435 970-728-6133 fax 970-728-6000 e-mail to mail@foleyassoc.com</p>	
<p>Telluride Streetscapes (Gregory Avenue)</p> <p>PLAN AND PROFILE: STA 41+50 TO 44+00</p>	
<p>Client: Town of Telluride PO Telluride Colorado, 81435</p> <p>Phone: 970-728-3071</p>	<p>Contact: William Frowdler or Rob Bishop 300 Bellvue Road Telluride, Colorado, 81435</p> <p>Fax: 970-728-0548</p> <p>Phone: 970-728-2177</p>

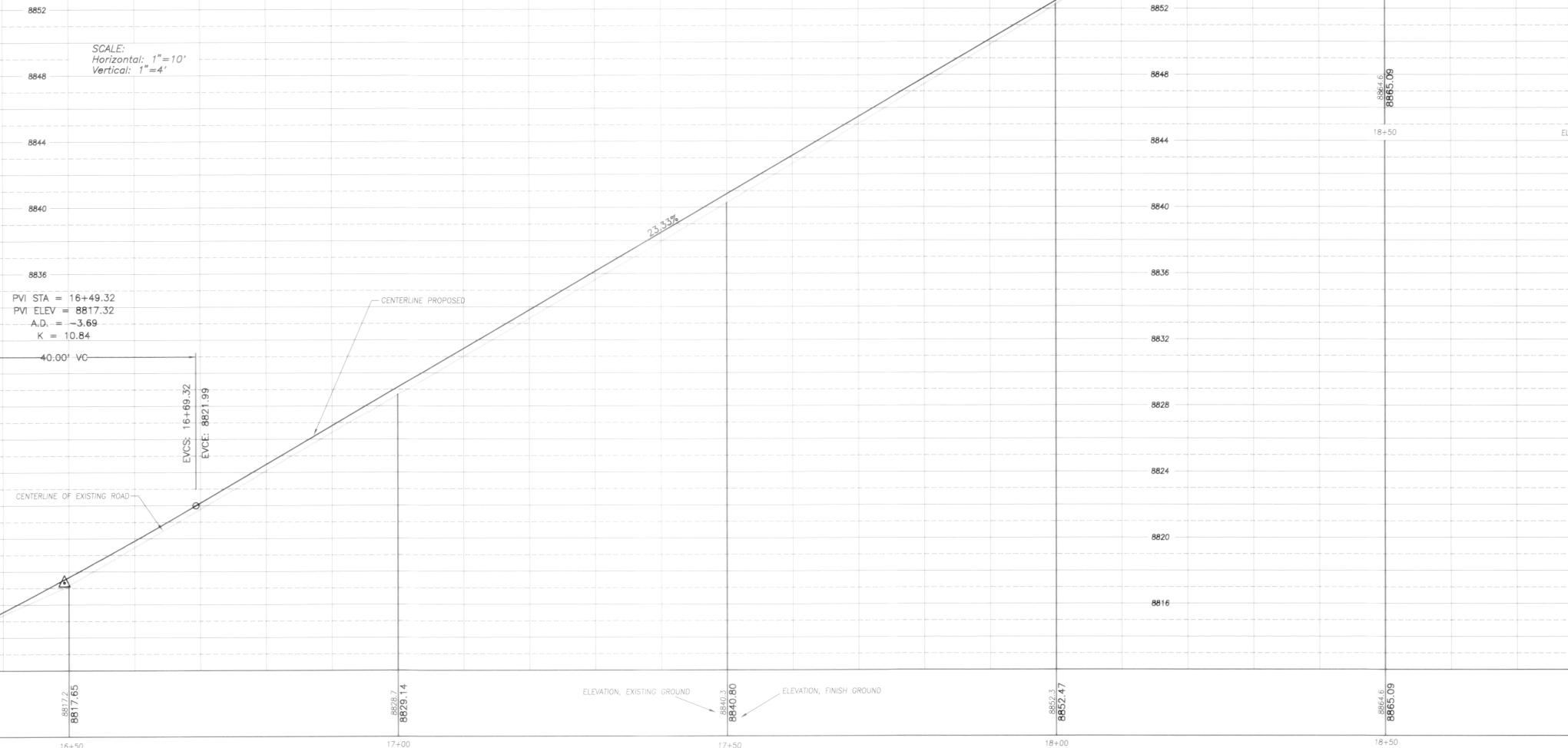


Legend

	Existing Rock Wall
	Existing Retaining Wall
	Proposed Rock Retaining Wall
	Existing Fence
	Existing Index Contour
	Existing Intermediate Contour
	Proposed Index Contour
	Proposed Intermediate Contour
	Sanitary Sewer (existing)
	Water (existing)
	Cable TV (subsurface, existing)
	Telephone (subsurface, existing)
	Power (subsurface, existing)
	Gas (subsurface, existing)
	Edge of Travelled Way (existing)
	Edge of Pavement (existing)

Proposed Existing

- Water Meter
- Tree
- Utility Pole
- Handicap Ramp
- Sanitary Manhole
- Cleanout
- Water Valve
- Fire Hydrant
- Sign
- Telephone Pedestal



Telluride Streetscapes (North Willow St)

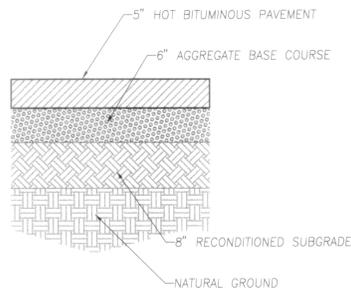
PLAN AND PROFILE, STA 16+50 TO 18+50

Client: Tom & Barbara Boudrie
 1700 South 33rd
 Telluride, Colorado, 81435
 Phone: 970-728-3071
 Fax: 970-728-0546
 Email: tom@foleyassoc.com

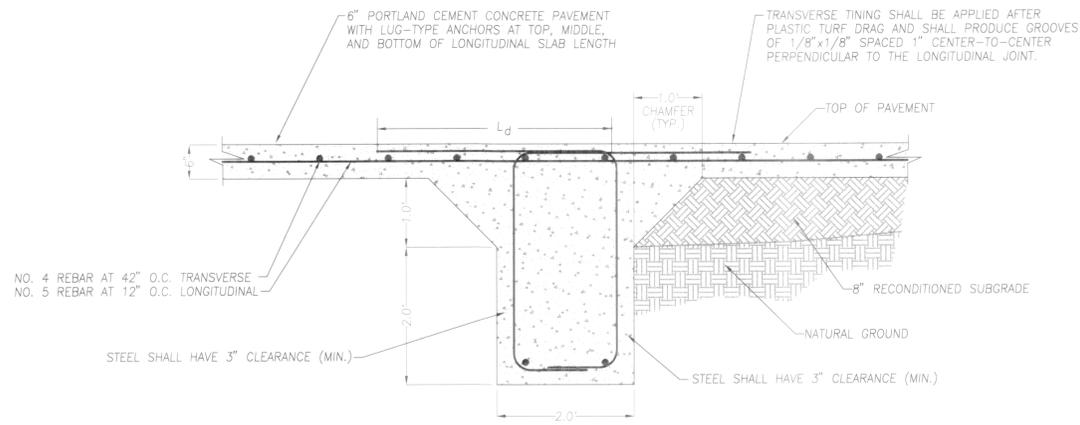
Contact: Rob Bishop
 William E. Fowle, P.E. or
 300 South Main Road
 Telluride, Colorado, 81435
 Phone: 970-728-2177
 Fax: 970-728-0546
 Email: rob@foleyassoc.com

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 Telluride, Colorado, 81435
 970-728-6153 Fax 970-728-6050
 e-mail to: matt@foleyassoc.com

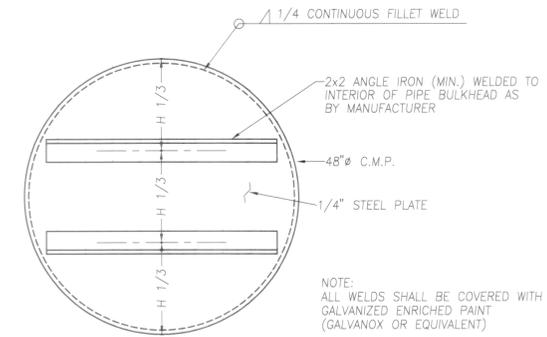
by	
date	
description	
Rev	
Project Mgr:	J GARDNER
Drawn by:	MJ
Checked by:	DF
Start date:	4/28/97
Dwg path:	96122\WILL-FNL
Sheet No.	25 of 41
Project #:	96122



HOT BITUMINOUS PAVEMENT
N.T.S.



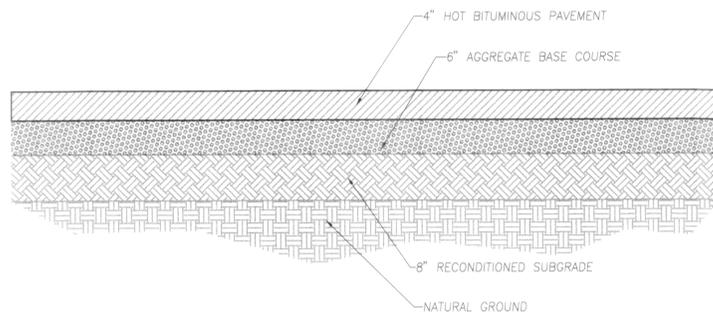
PORTLAND CEMENT CONCRETE PAVEMENT
WITH TRANSVERSE LUG-TYPE ANCHOR
N.T.S.



BULKHEAD DETAIL
N.T.S.

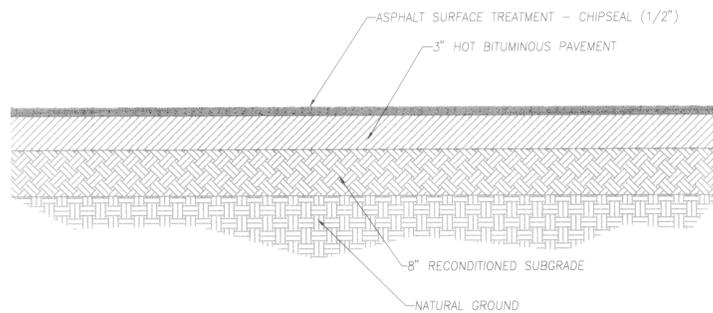
TYPE III PAVEMENT - ALTERNATIVES
Asphalt or Portland Cement Concrete Pavement
N.T.S.

Q
Plan



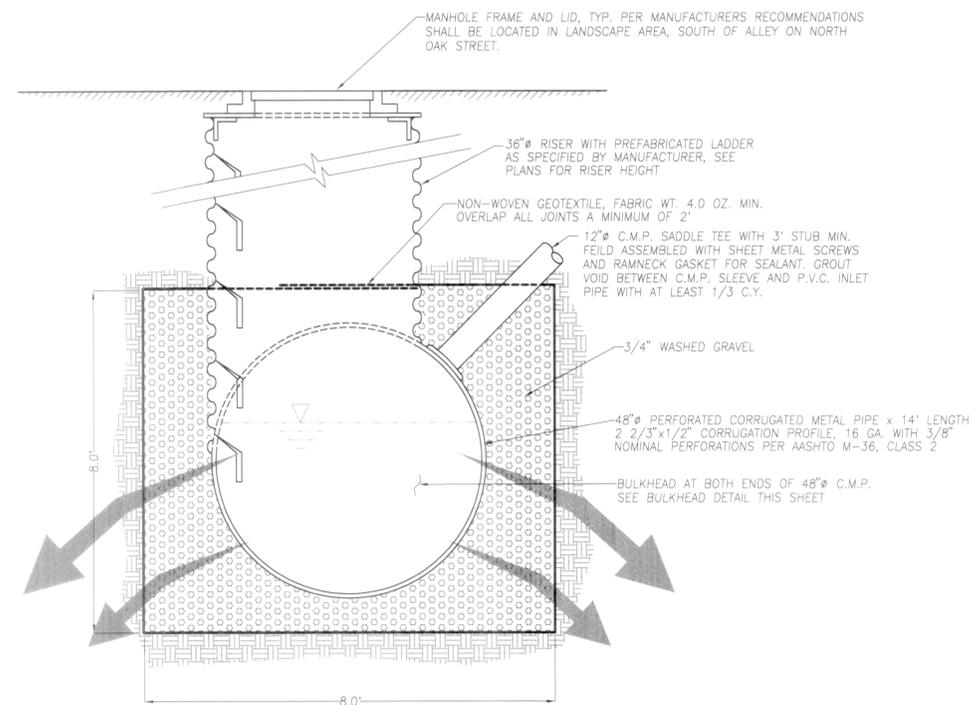
TYPE II PAVEMENT
HOT BITUMINOUS PAVEMENT
N.T.S.

R
Plan



TYPE I PAVEMENT
Asphalt Surface Treatment - Chip Seal
N.T.S.

S
Plan



STORM WATER DETENTION/RECHARGE SYSTEM
N.T.S.

T
10

Telluride Streetscapes
PROJECT DETAILS

Contact:
William Frank Bishop
300 Black Bear Road
Telluride
Colorado, 81435
Phone: 970-728-2177
E-mail: g-m@fley.com

Client:
Telluride
PO Box 397
Telluride
Colorado, 81435
Phone: 970-728-3071
E-mail: g-m@fley.com

FOLEY ASSOCIATES, INC.
ENGINEERING - PLANNING - SURVEYING
PO Box 1885 125 W. Pacific Ave. Ste. B-1
Telluride, Colorado, 81435
970-728-6153 fax 970-728-6050
e-mail to mail@foleyassoc.com

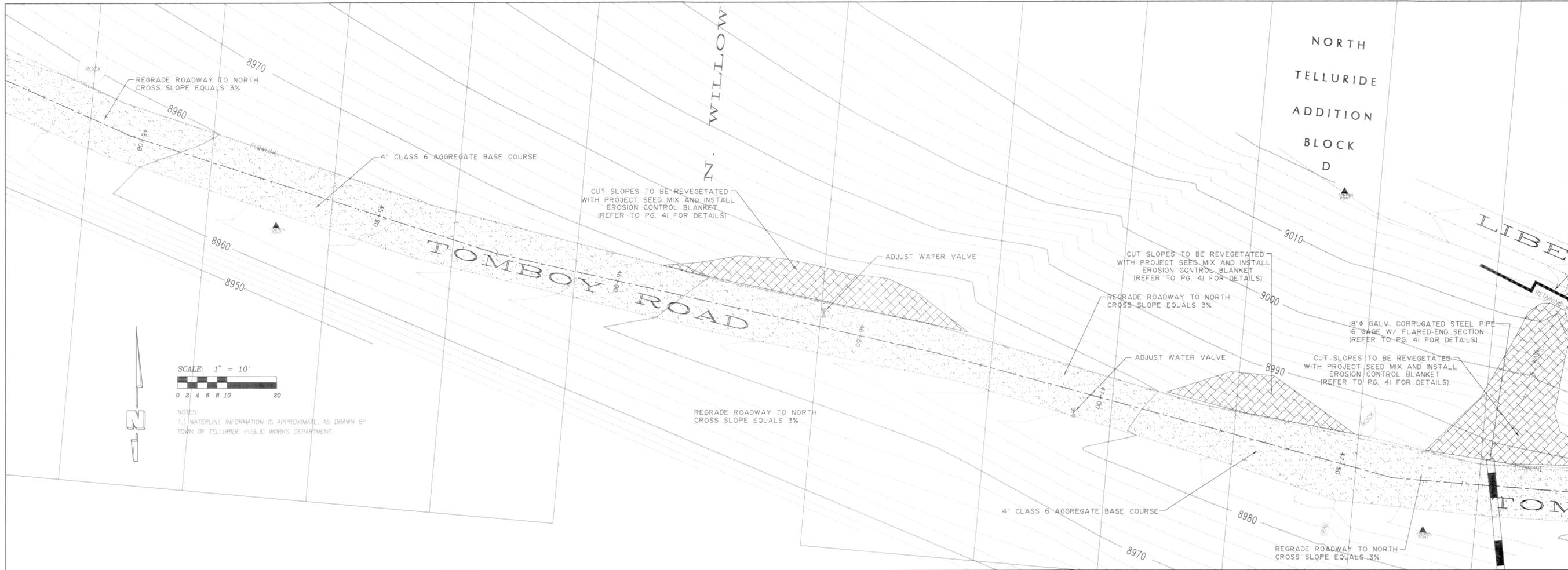
Engineer's stamp and signature

by
date

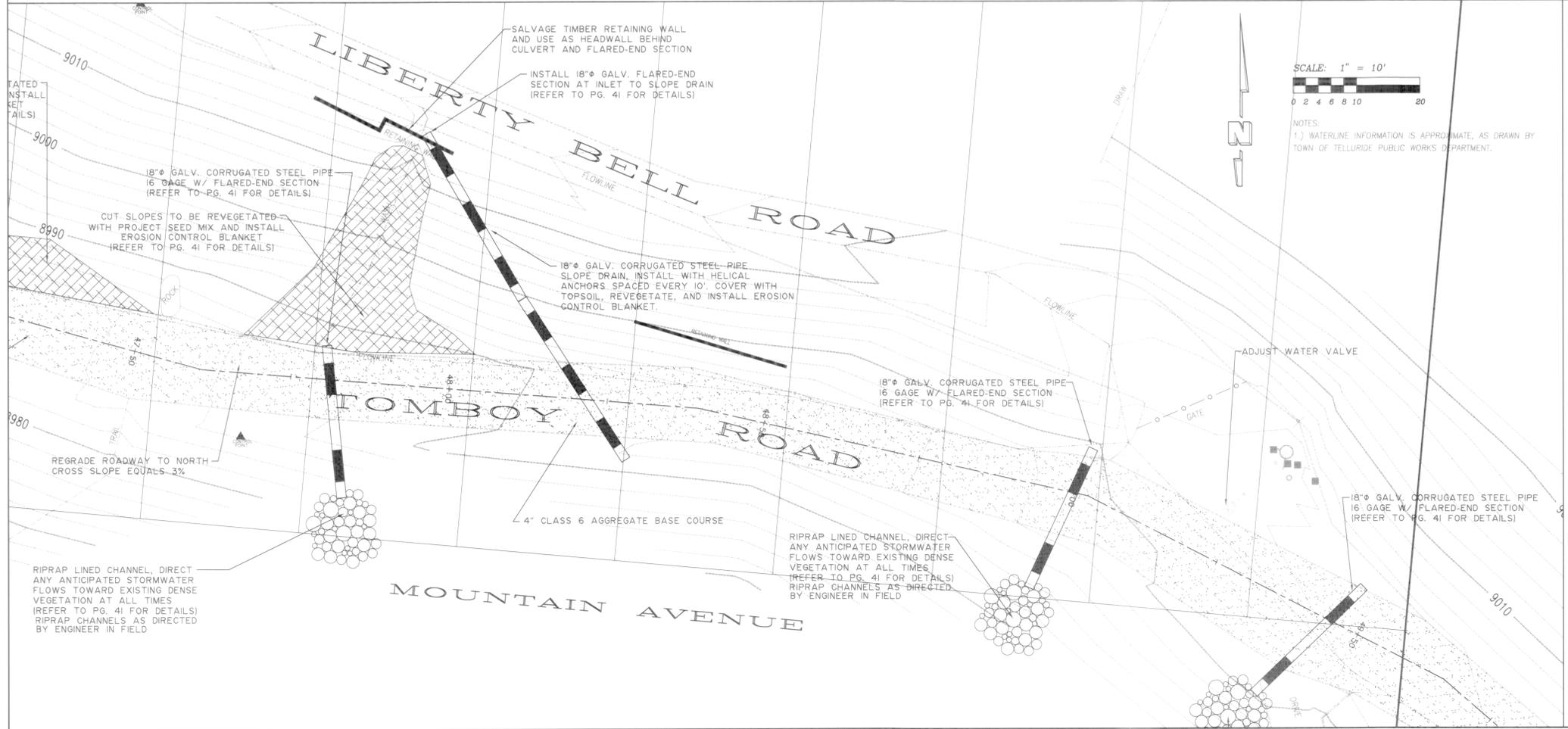
description

Rev.

Project Mgr: J.GARDNER/K.J.SMITH
Drawn by: MFS
Checked by: DF
Start date: 5-20-97
Dwg path: 96122\DETAIL
Sheet No.
30 of 41
Project #: 96122



NOTES:
1.) WATERLINE INFORMATION IS APPROXIMATE, AS DRAWN BY TOWN OF TELLURIDE PUBLIC WORKS DEPARTMENT.



NOTES:
1.) WATERLINE INFORMATION IS APPROXIMATE, AS DRAWN BY TOWN OF TELLURIDE PUBLIC WORKS DEPARTMENT.

Legend

	Existing Rock Wall
	Existing Retaining Wall
	Proposed Rock Retaining Wall
	Existing Fence
	Existing Index Contour
	Existing Intermediate Contour
	Proposed Index Contour
	Proposed Intermediate Contour
	Sanitary Sewer (existing)
	Water (existing)
	Cable TV (subsurface, existing)
	Telephone (subsurface, existing)
	Power (subsurface, existing)
	Gas (subsurface, existing)
	Edge of Travelled Way (existing)
	Edge of Pavement (existing)

Telluride Streetscapes

TOMBOY ROAD FINAL PLAN

Client:
Town of Telluride
PO Box 397
Telluride, Colorado, 81435

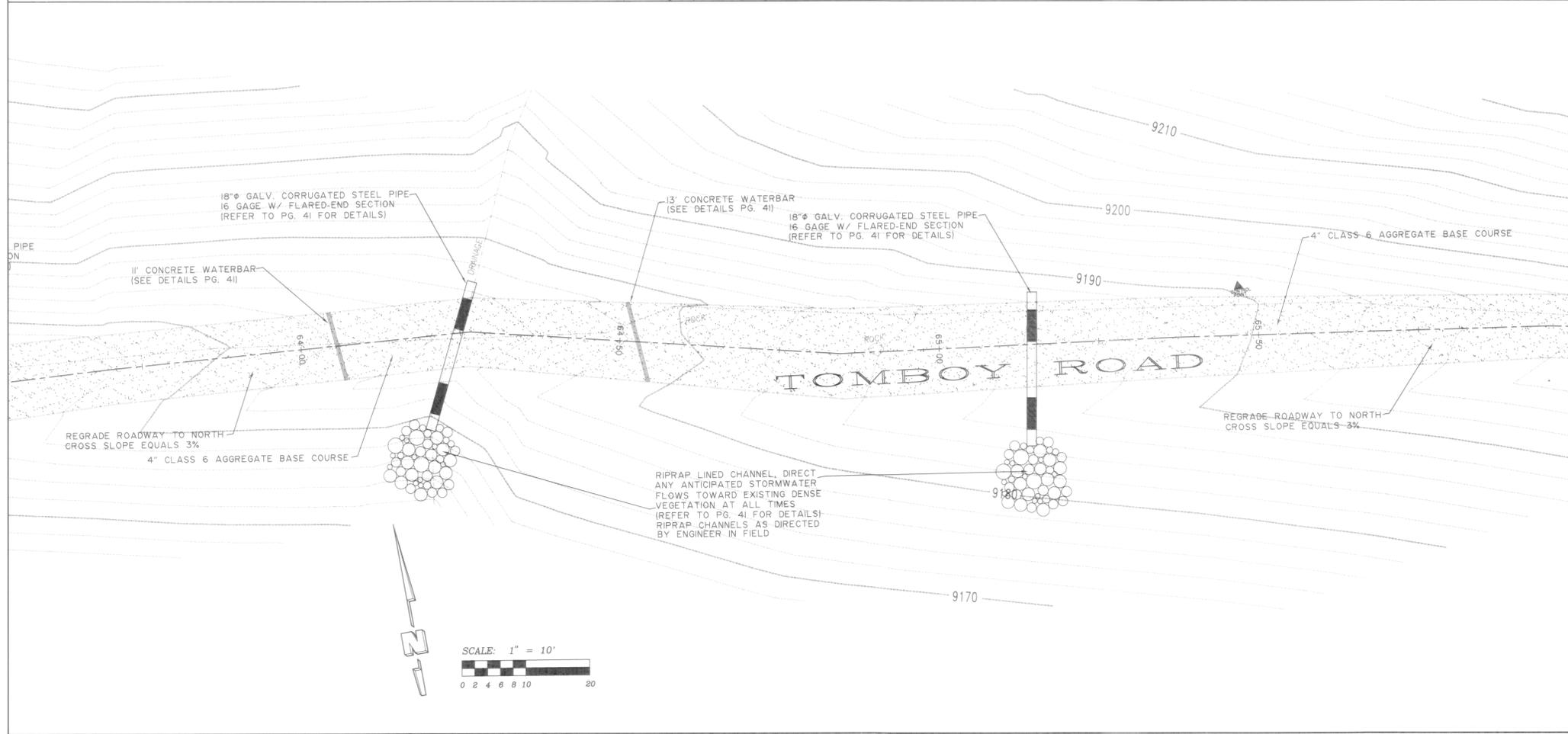
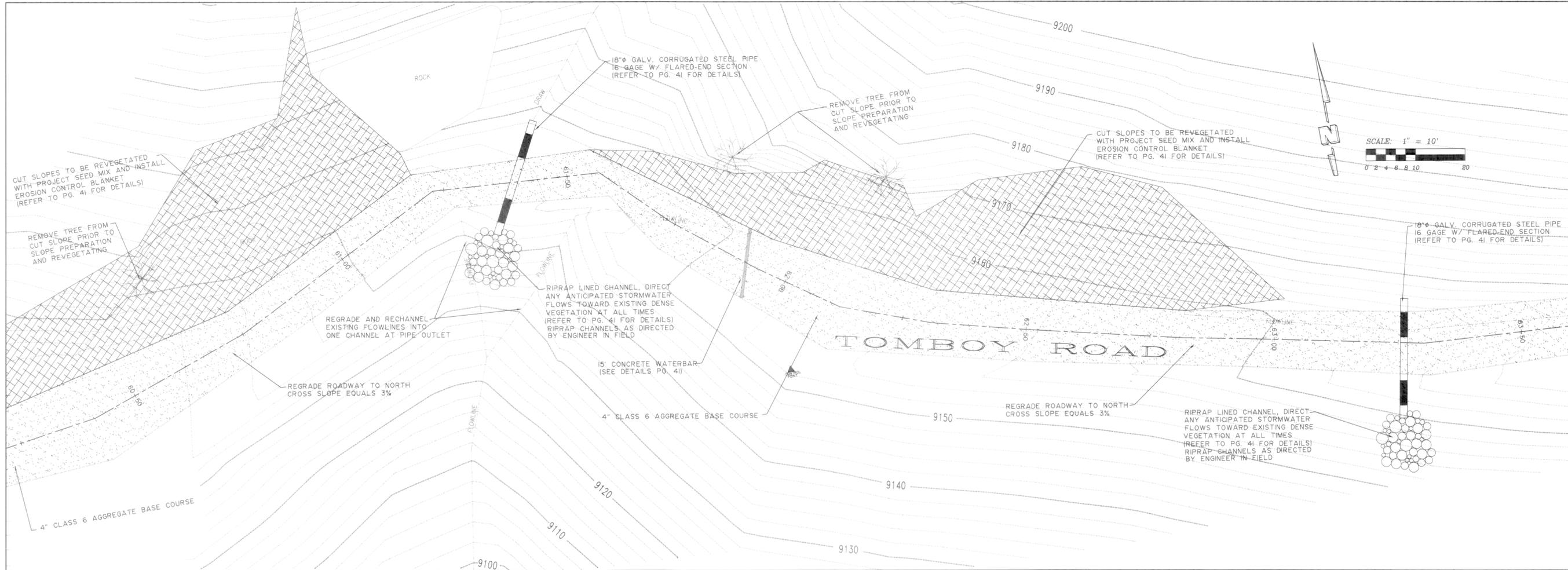
Phone: 970-728-3071
Fax: 970-728-0546
Website: www.townoftelluride.com

Contact:
William E. Bishop
300 Black Bear Road
Telluride, Colorado, 81435

Phone: 970-728-2177
Fax: 970-728-0546
Website: www.foleyassociates.com

Project Mgr: J.GARDNER/K.J.SMITH
Drawn by: MFS
Checked by: DF
Start date: 4/28/97
Dwg path: 96122/TBOY

Sheet No.
34 of **41**
Project #: 96122



Legend

	Existing Rock Wall
	Existing Retaining Wall
	Proposed Rock Retaining Wall
	Existing Fence
	Existing Index Contour
	Existing Intermediate Contour
	Proposed Index Contour
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	Sanitary Sewer (existing)
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	Cable TV (subsurface, existing)
	Telephone (subsurface, existing)
	Power (subsurface, existing)
	Gas (subsurface, existing)
	Edge of Travelled Way (existing)
	Edge of Pavement (existing)
	Proposed Existing Water Meter
	Proposed Existing Tree
	Proposed Existing Utility Pole
	Proposed Existing Handicap Ramp
	Proposed Existing Sanitary Manhole
	Proposed Existing Cleanout
	Proposed Existing Water Valve
	Proposed Existing Fire Hydrant
	Proposed Existing Sign
	Proposed Existing Telephone Pedestal

Telluride Streetscapes
TOMBOY ROAD FINAL PLAN

Contact: William, Francis, Reg, Bishop
 300 Black Bear Road
 Telluride, Colorado, 81435
 Phone: 970-728-2177
 Fax: 970-728-0548
 e-mail: wfrancis@telluridestscapes.com

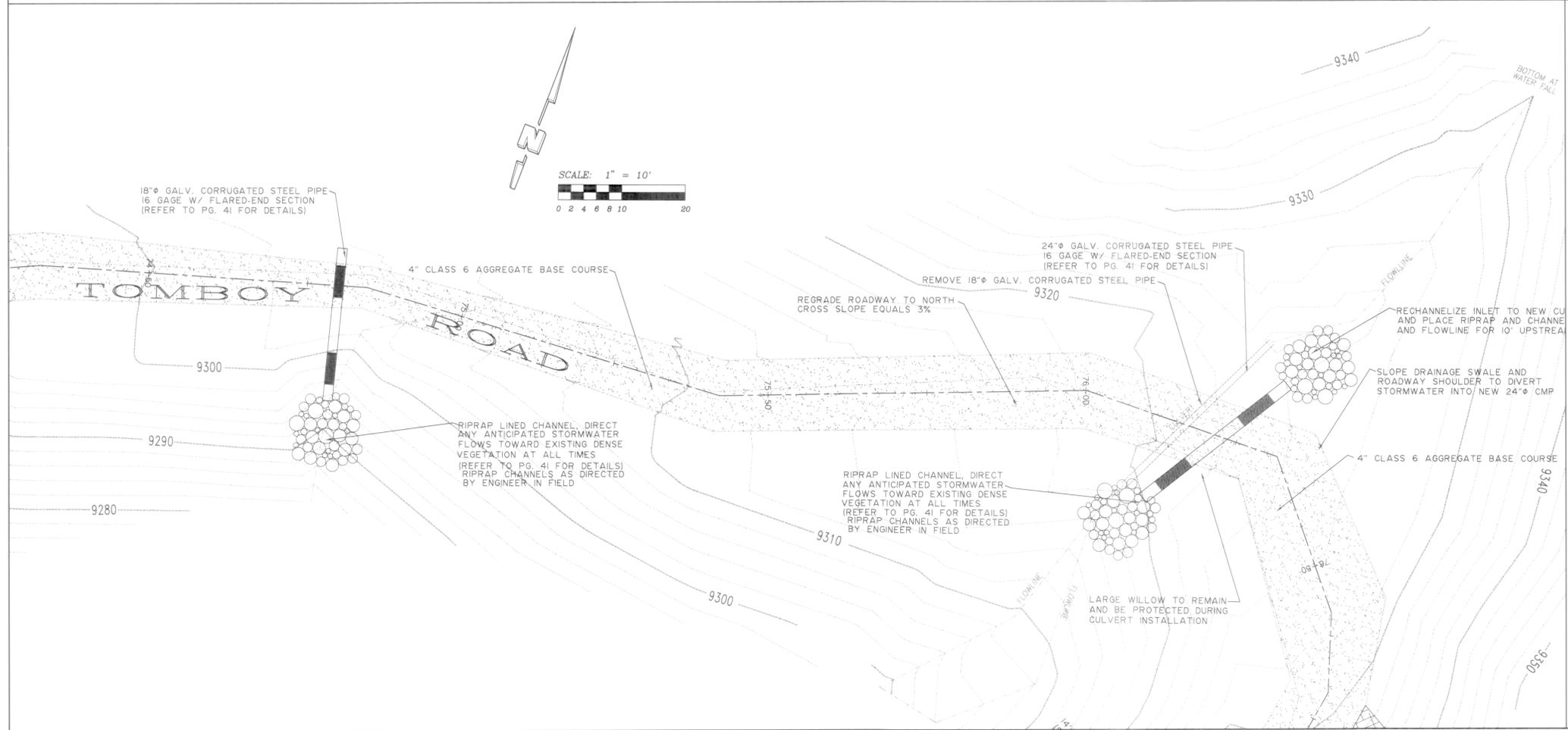
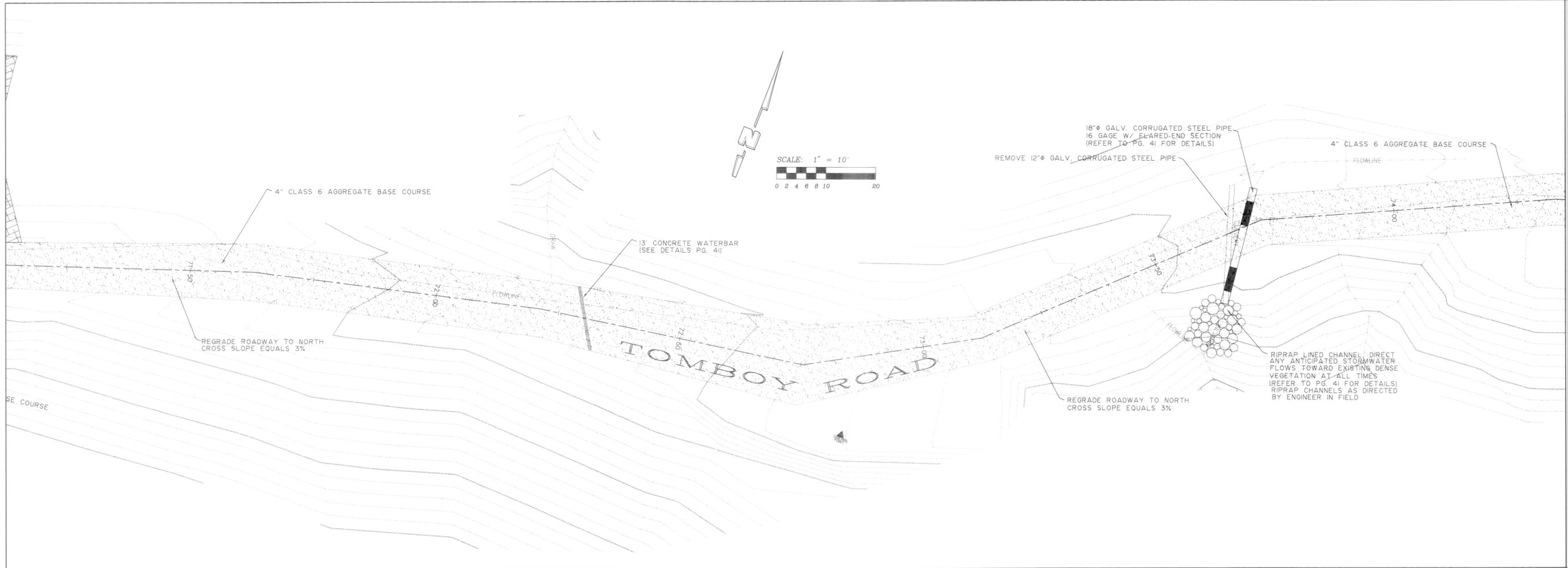
Client: Town of Telluride
 PO Box 397
 Telluride, Colorado, 81435
 Phone: 970-728-3071
 Fax: 970-728-0548
 e-mail: malf@townoftelluride.com

FOLEY ASSOCIATES, INC.
 ENGINEERING • PLANNING • SURVEYING
 PO Box 1885 128 W. Pacific Ave. Ste. B-1
 Telluride, Colorado, 81435
 970-728-6153 fax 970-728-6050
 e-mail to malf@foleyassoc.com

Engineer's stamp and signature

by	
date	
description	
Rev.	
Project Mgr:	J.GARDNER/K.J.SMITH
Drawn by:	MFS
Checked by:	DF
Start date:	4/28/97
Dwg path:	96122\TBOY
Sheet No.	37 of 41
Project #:	96122

Time: 13:28:54
 Date: 8/12/1997
 Drawing File: D:\DRAWINGS\96122\TOM-FNL.DWG (Mch98)
 XREF: LEGEND



Legend

	Existing Rock Wall
	Existing Retaining Wall
	Proposed Rock Retaining Wall
	Existing Fence
	Existing Index Contour
	Existing Intermediate Contour
	Proposed Index Contour
	Proposed Intermediate Contour
	Sanitary Sewer (existing)
	Water (existing)
	Cable TV (subsurface, existing)
	Telephone (subsurface, existing)
	Power (subsurface, existing)
	Gas (subsurface, existing)
	Edge of Travelled Way (existing)
	Edge of Pavement (existing)
	Water Meter
	Tree
	Utility Pole
	Handicap Ramp
	Sanitary Manhole
	Cleanout
	Water Valve
	Fire Hydrant
	Sign
	Telephone Pedestal

Telluride Streetscapes
TOMBOY ROAD FINAL PLAN

Client:
Town of Telluride
PO Box 1885
Telluride, Colorado, 81435
Phone: 970-728-3071
e-mail: townoftelluride@townoftelluride.com

Contact:
William Frowdler or Rob Bishop
300 Broadway Road
Telluride, Colorado, 81435
Phone: 970-728-2177
e-mail: william.frowdler@telluridestreetscapes.com

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Telluride, Colorado, 81435
970-728-6153 Fax 970-728-6050
e-mail: info@foleyassoc.com

Engineer's stamp and signature

by	
date	
description	
Rev	
Project Mgr:	J.GARDNER/K.J.SMITH
Drawn by:	MFS
Checked by:	DF
Start date:	4/28/97
Dwg path:	96122\TBOY
Sheet No.	39 of 41
Project #:	96122

DATE: 4/27/97
DRAWN BY: MFS
CHECKED BY: DF
SCALE: AS SHOWN
XREF: LEGEND

TELLURIDE STREETSCAPES CONSTRUCTION PROJECT PHASE 1, 1998

TOWN OF TELLURIDE SAN MIGUEL COUNTY, COLORADO

PROJECT TEAM

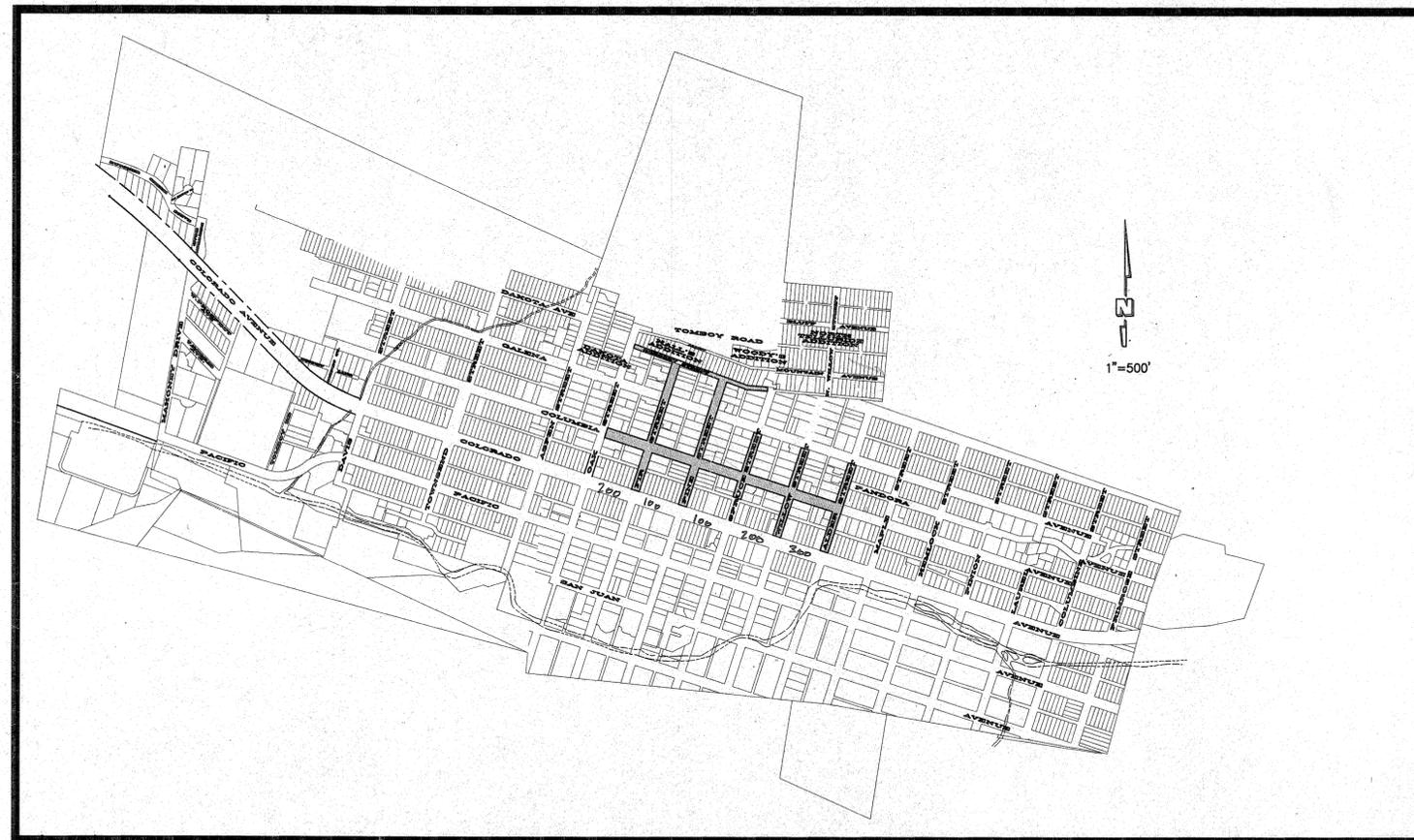
CLIENT: TOWN OF TELLURIDE
P.O. BOX 397
TELLURIDE, COLORADO 81435
PHONE: (970)728-3071

CONTACT: WILLIAM FROWNFELTER OR ROB BISHOP
PUBLIC WORKS DEPARTMENT
300 BLACK BEAR ROAD
TELLURIDE, COLORADO 81435
PHONE: (970)728-2177

ENGINEER: FOLEY AND ASSOCIATES, INC.
P.O. BOX 1385
TELLURIDE, COLORADO 81435
PHONE: (970)728-6153
PROJECT MANAGER: JACK GARDNER, P.E.

SURVEYOR: FOLEY AND ASSOCIATES, INC.
P.O. BOX 1385
TELLURIDE, COLORADO 81435
PHONE: (970)728-6153
PROJECT SURVEYOR: J. DAVID FOLEY, P.L.S.

PROJECT BENCHMARK: BRASS CAP MONUMENT AT THE
CENTERLINE OF COLORADO AVENUE
AND OAK STREET, ELEVATION 8788.55



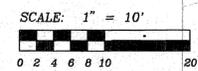
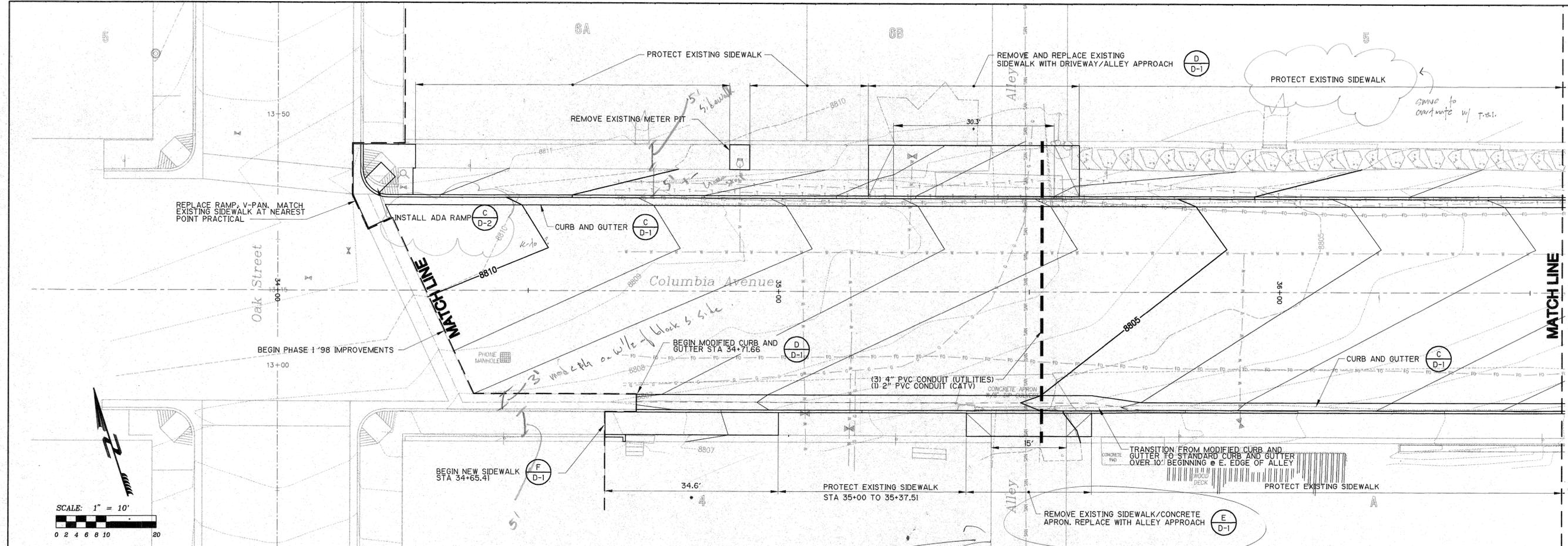
SHEET INDEX

1	COVER SHEET
2	INDEX SHEET
C-1 TO C-5	PLAN AND PROFILE 100 WEST - 400 EAST COLUMBIA AVENUE
C-6 TO C-10	CROSS SECTIONS 100 WEST - 400 EAST COLUMBIA AVENUE
F-1 TO F-3	PLAN AND PROFILE 100 - 300 NORTH FIR STREET
F-4 TO F-6	CROSS SECTIONS 100 - 300 NORTH FIR STREET
P-1 TO P-3	PLAN AND PROFILE 100 - 300 NORTH PINE STREET
P-4 TO P-6	CROSS-SECTIONS 100 - 300 NORTH PINE STREET
S-1 TO S-2	PLAN AND PROFILE 100 - 200 NORTH SPRUCE STREET
S-3 TO S-4	CROSS SECTIONS 100 - 200 NORTH SPRUCE STREET
W-1 TO W-3	PLAN AND PROFILE 100 - 300 NORTH WILLOW STREET
W-4 TO W-6	CROSS SECTIONS 100 - 300 NORTH WILLOW STREET
A-1 TO A-2	PLAN AND PROFILE 100 - 200 NORTH ALDER STREET
A-3 TO A-4	CROSS-SECTIONS 100 - 200 NORTH ALDER STREET
A-5 TO A-	WETLANDS SHEETS 100 SOUTH ALDER STREET
G-1 TO G-3	PLAN AND PROFILE 100 - 300 GREGORY AVENUE
G-4 TO G-6	CROSS SECTIONS 100 - 300 GREGORY AVENUE
D-1 TO D-4	PROJECT DETAILS

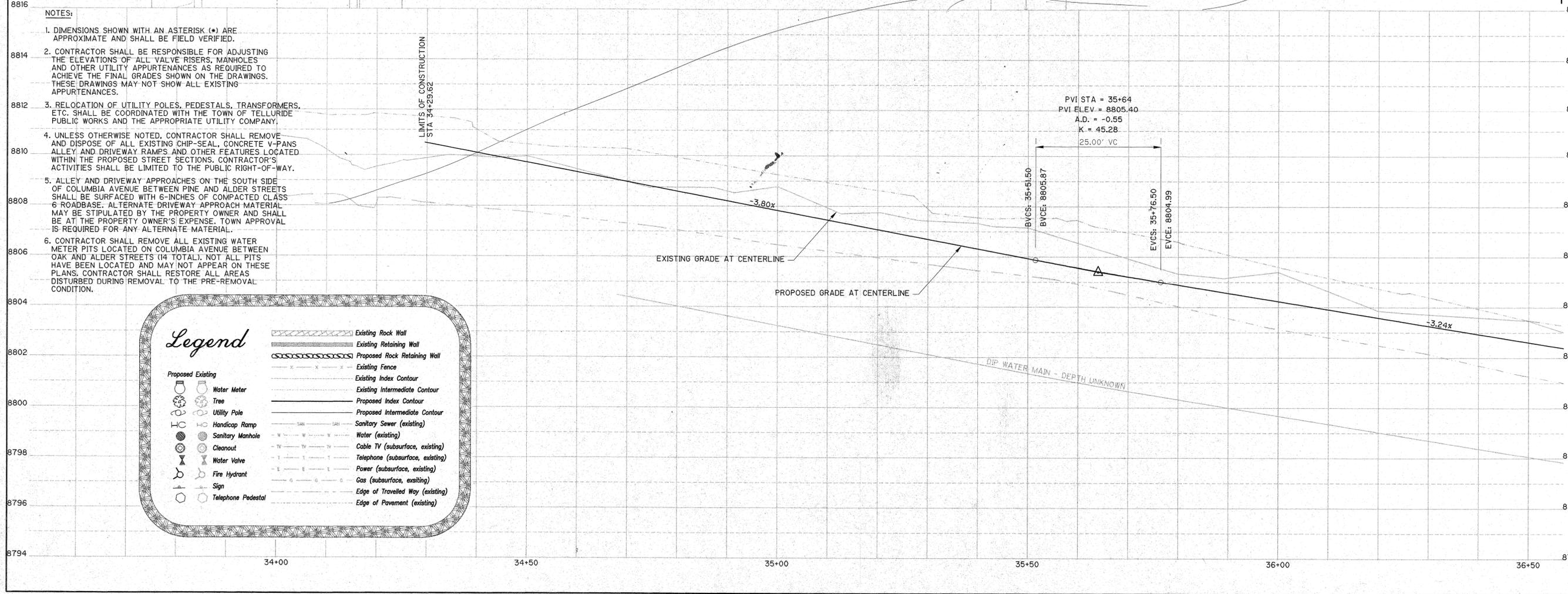
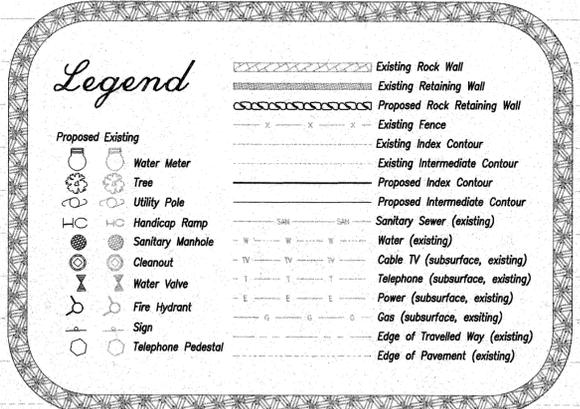
GENERAL NOTES:

- Elevations and dimensions of adjacent facilities and obstructions shall have precedence over elevations and dimensions shown on the drawings. When a conflict arises between elevations and dimensions shown on the drawings, the CONTRACTOR shall notify the OWNERS REPRESENTATIVE in writing within 4 hours from the time the conflict is noticed.
- The designs herein are for the exclusive use of the Owner. Alterations, revisions or adaptation by others shall not be permitted without Foley Associates, Inc. written permission. Foley Associates, Inc. assumes no responsibility for their use other than their originally intended purpose.
- The Contractor must coordinate traffic control with both the Town of Telluride Planning and Public Works Departments. Traffic Control Plans must be submitted to the Town at least 72 hours prior to their intended use.
- The Contractor must coordinate all work such that access is maintained to each residential or commercial establishment during the course of the project.
- The Contractor shall maintain a safe work area which includes securing the project area to prevent unauthorized entry by the public, during and after the hours of construction.
- The OWNER will provide the QA/QC testing required during the construction of this project. OWNER shall not be responsible for the expense of any failing tests. Such costs shall be deducted from the CONTRACT amount.
- The Project area shall be maintained in a neat and orderly condition at all times. CONTRACTOR SHALL NOT ALLOW litter, debris, or other construction derived material to accumulate on the project site, nor near the project site. Contractor shall secure all project litter and debris at all times. See Project Specifications for additional requirements.
- The Contractor is responsible for dust control for the duration of construction. Contractor shall provide all materials, equipment and labor necessary to control fugitive dust emission from the project dust at all times. Water shall be used as the dust palliative site.
- The CONTRACTOR shall protect existing improvements, including signs, utilities, landscaping and all private property during construction. Damage caused by the CONTRACTOR, or results from the CONTRACTORS operations shall be repaired at the CONTRACTORS expense.
- All construction shall conform to these plans, and the specifications as identified below:
- Street Construction shall conform to the State of Colorado Division of Highways "Standard Specifications for Road and Bridge Construction" (1991), except as revised in the Project Specifications which are included with the contract documents. The Town of Telluride Standards for Streets shall also serve as a minimum standard.
- Water and Sewer Service relocation shall conform to the Town of Telluride Standards.
- Landscaping and Irrigation Systems shall conform to the landscape and irrigation specifications contained in the contract documents.
- The CONTRACTOR is responsible for erosion control at all times during construction and through final acceptance by the OWNER. CONTRACTOR shall repair any area which sustains damage as a result of erosion in a timely manner at CONTRACTORS expense.
- The Contractor shall adjust all manholes, valve risers, valve boxes and utility pedestals to final grade.

STREETSCAPES PHASE I - 1998



- NOTES:**
1. DIMENSIONS SHOWN WITH AN ASTERISK (*) ARE APPROXIMATE AND SHALL BE FIELD VERIFIED.
 2. CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING THE ELEVATIONS OF ALL VALVE RISERS, MANHOLES AND OTHER UTILITY APPURTENANCES AS REQUIRED TO ACHIEVE THE FINAL GRADES SHOWN ON THE DRAWINGS. THESE DRAWINGS MAY NOT SHOW ALL EXISTING APPURTENANCES.
 3. RELOCATION OF UTILITY POLES, PEDESTALS, TRANSFORMERS, ETC. SHALL BE COORDINATED WITH THE TOWN OF TELLURIDE PUBLIC WORKS AND THE APPROPRIATE UTILITY COMPANY.
 4. UNLESS OTHERWISE NOTED, CONTRACTOR SHALL REMOVE AND DISPOSE OF ALL EXISTING CHIP-SEAL, CONCRETE V-PANS ALLEY AND DRIVEWAY RAMPS AND OTHER FEATURES LOCATED WITHIN THE PROPOSED STREET SECTIONS. CONTRACTOR'S ACTIVITIES SHALL BE LIMITED TO THE PUBLIC RIGHT-OF-WAY.
 5. ALLEY AND DRIVEWAY APPROACHES ON THE SOUTH SIDE OF COLUMBIA AVENUE BETWEEN PINE AND ALDER STREETS SHALL BE SURFACED WITH 6-INCHES OF COMPACTED CLASS 6 ROADBASE. ALTERNATE DRIVEWAY APPROACH MATERIAL MAY BE STIPULATED BY THE PROPERTY OWNER AND SHALL BE AT THE PROPERTY OWNER'S EXPENSE. TOWN APPROVAL IS REQUIRED FOR ANY ALTERNATE MATERIAL.
 6. CONTRACTOR SHALL REMOVE ALL EXISTING WATER METER PITS LOCATED ON COLUMBIA AVENUE BETWEEN OAK AND ALDER STREETS (14 TOTAL). NOT ALL PITS HAVE BEEN LOCATED AND MAY NOT APPEAR ON THESE PLANS. CONTRACTOR SHALL RESTORE ALL AREAS DISTURBED DURING REMOVAL TO THE PRE-REMOVAL CONDITION.



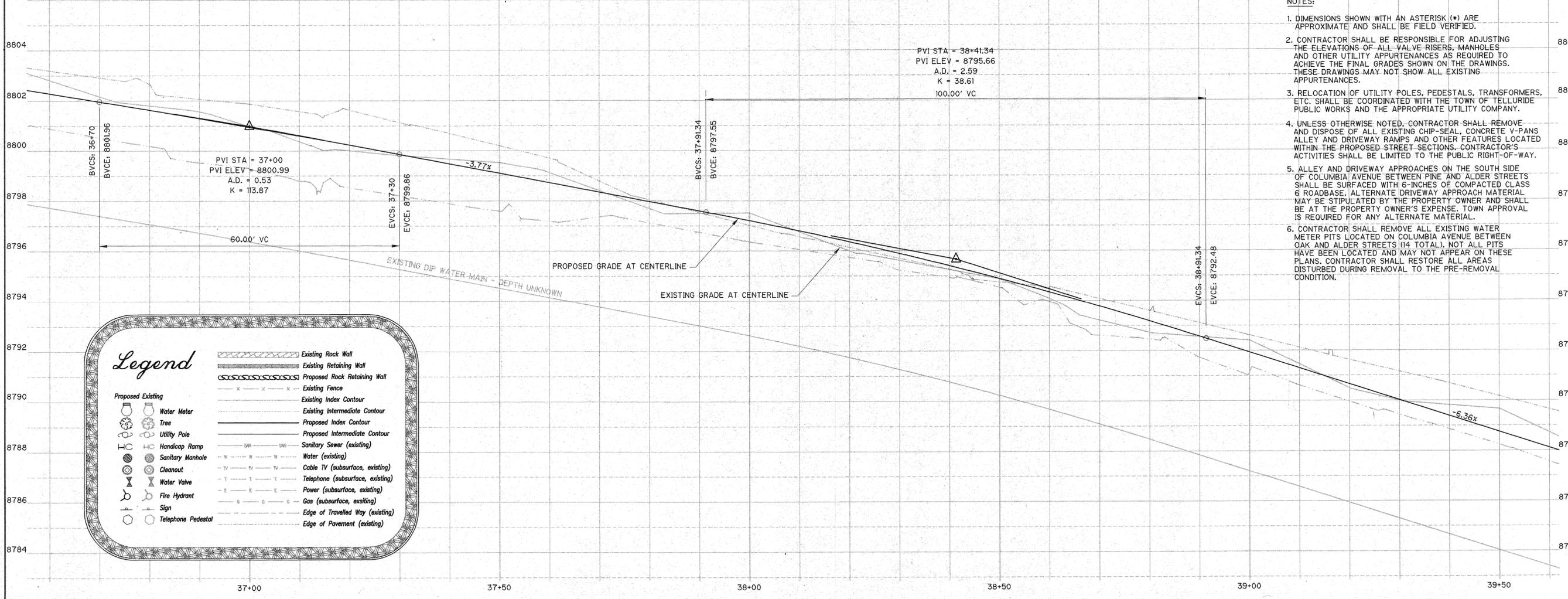
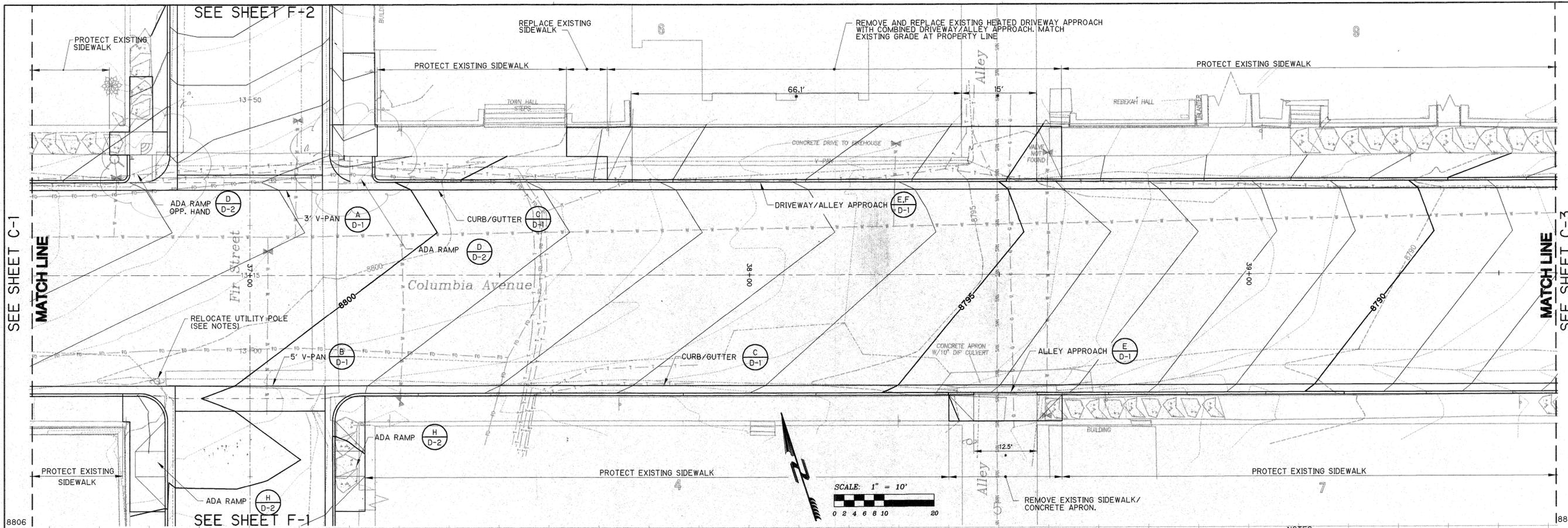
TELLURIDE STREETSCAPES - COLUMBIA AVENUE
PLAN & PROFILE - STA 34+30 TO STA 36+56

FOLEY ASSOCIATES, INC.
 ENGINEERING - PLANNING - SURVEYING
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 Telluride, Colorado, 81455
 970-729-6153 fax: 970-729-9000
 e-mail to smul@foleyassoc.com

Client: Town of Telluride
 PO Box 397
 Telluride
 Colorado, 81455
 Phone: 970-729-3071
 Fax: 970-729-3071
 e-mail: foley@foleyassoc.com

Contact: William Frowder or Rob Bishop
 300 Black Bear Road
 Telluride
 Colorado, 81455
 Phone: 970-728-2177
 Fax: 970-728-2177
 e-mail: frowder@foleyassoc.com

Project Mgr: JG
 Drawn by: MJ/KJS
 Checked by: DF
 Start date: 6-4-97
 Dwg path: 96122\COLUM1a
 Sheet No. **C-1**
 Project #: 96122



Legend

	Existing Rock Wall
	Existing Retaining Wall
	Proposed Rock Retaining Wall
	Existing Fence
	Existing Index Contour
	Existing Intermediate Contour
	Proposed Index Contour
	Proposed Intermediate Contour
	Sanitary Sewer (existing)
	Water (existing)
	Cable TV (subsurface, existing)
	Telephone (subsurface, existing)
	Power (subsurface, existing)
	Gas (subsurface, existing)
	Edge of Travelled Way (existing)
	Edge of Pavement (existing)

- NOTES:**
1. DIMENSIONS SHOWN WITH AN ASTERISK (*) ARE APPROXIMATE AND SHALL BE FIELD VERIFIED.
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TELLURIDE STREETSCAPES - COLUMBIA AVENUE
PLAN & PROFILE STA 36+57 TO STA 39+61

100 W

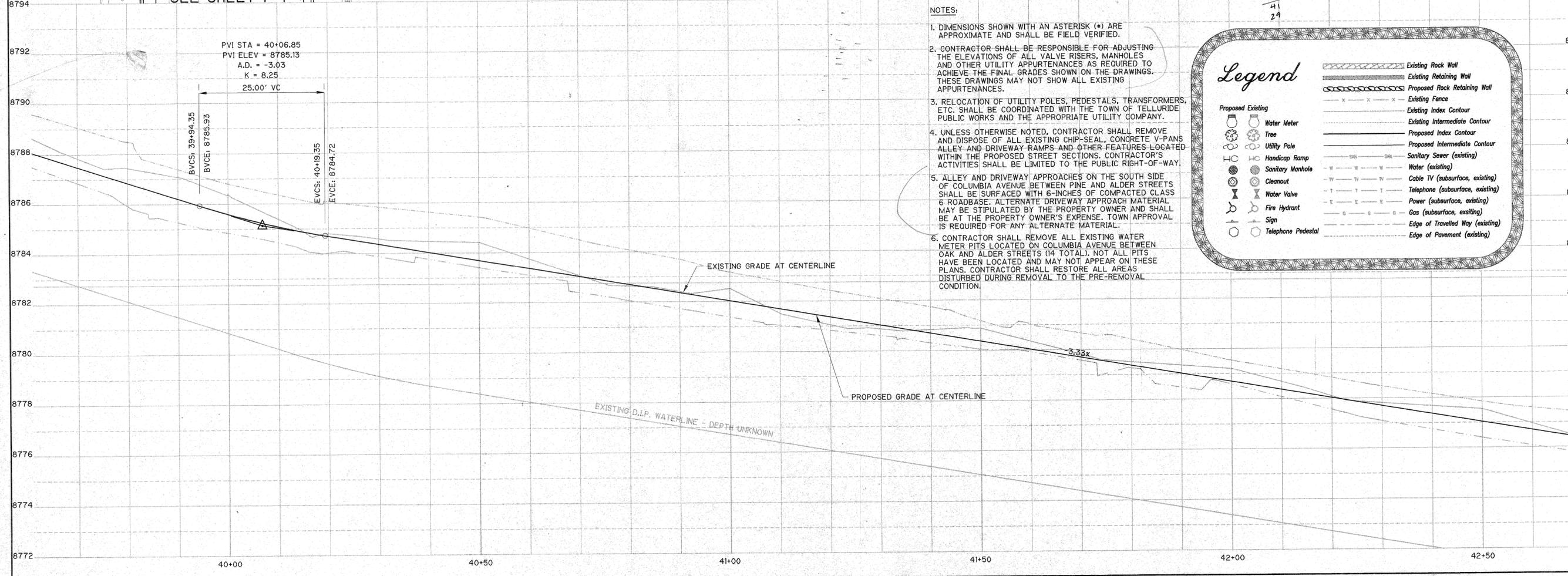
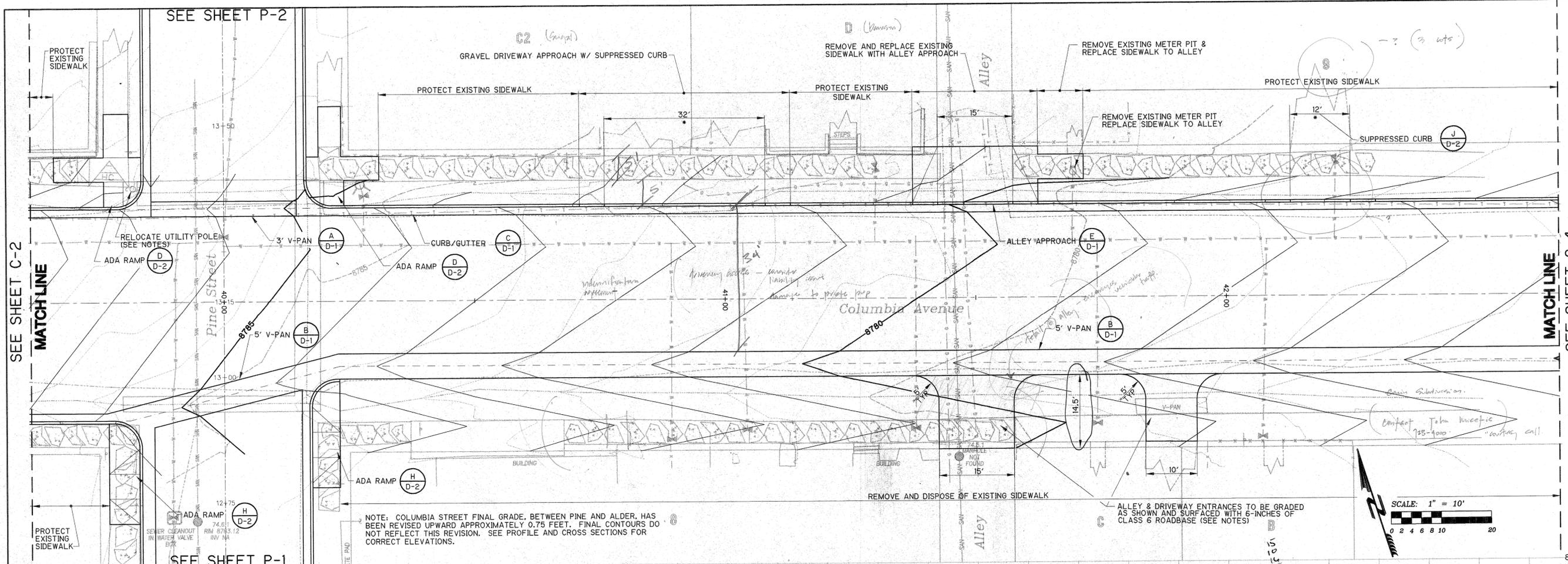
FOLEY ASSOCIATES, INC.
 ENGINEERING - PLANNING - SURVEYING
 PO Box 1885 125 W. Pacific Ave Ste. B-1
 Telluride, Colorado, 81435
 970-728-0155 Fax 970-728-0060
 e-mail to xma@foleyassoc.com

Contact:
 William Frombeter or Rob Bishop
 300 Black Bear Road
 Telluride
 Colorado, 81435
 Phone: 970-728-2177
 Fax: 970-728-0548
 e-mail:

Client:
 Town of Telluride
 PO Box 397
 Telluride
 Colorado, 81435
 Phone: 970-728-3071
 e-mail:

Project Mgr: JG
 Drawn by: MJK/KJS
 Checked by: JG
 Start date: 6-4-97
 Dwg path: 96122\COLUM1A
 Sheet No.
 Project #: 96122

Time: 15:00:18
 Date: 3/2/1998
 Drawing File: STREETS\COLUM1A.DWG (DAX)
 Xrefs: LEGEND



- NOTES:**
- DIMENSIONS SHOWN WITH AN ASTERISK (*) ARE APPROXIMATE AND SHALL BE FIELD VERIFIED.
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Legend

	Existing Rock Wall
	Existing Retaining Wall
	Proposed Rock Retaining Wall
	Existing Fence
	Existing Index Contour
	Existing Intermediate Contour
	Proposed Index Contour
	Proposed Intermediate Contour
	Sanitary Sewer (existing)
	Water (existing)
	Cable TV (subsurface, existing)
	Telephone (subsurface, existing)
	Power (subsurface, existing)
	Gas (subsurface, existing)
	Edge of Travelled Way (existing)
	Edge of Pavement (existing)

SEE SHEET P-2
 SEE SHEET C-2
 MATCH LINE
 SEE SHEET C-4

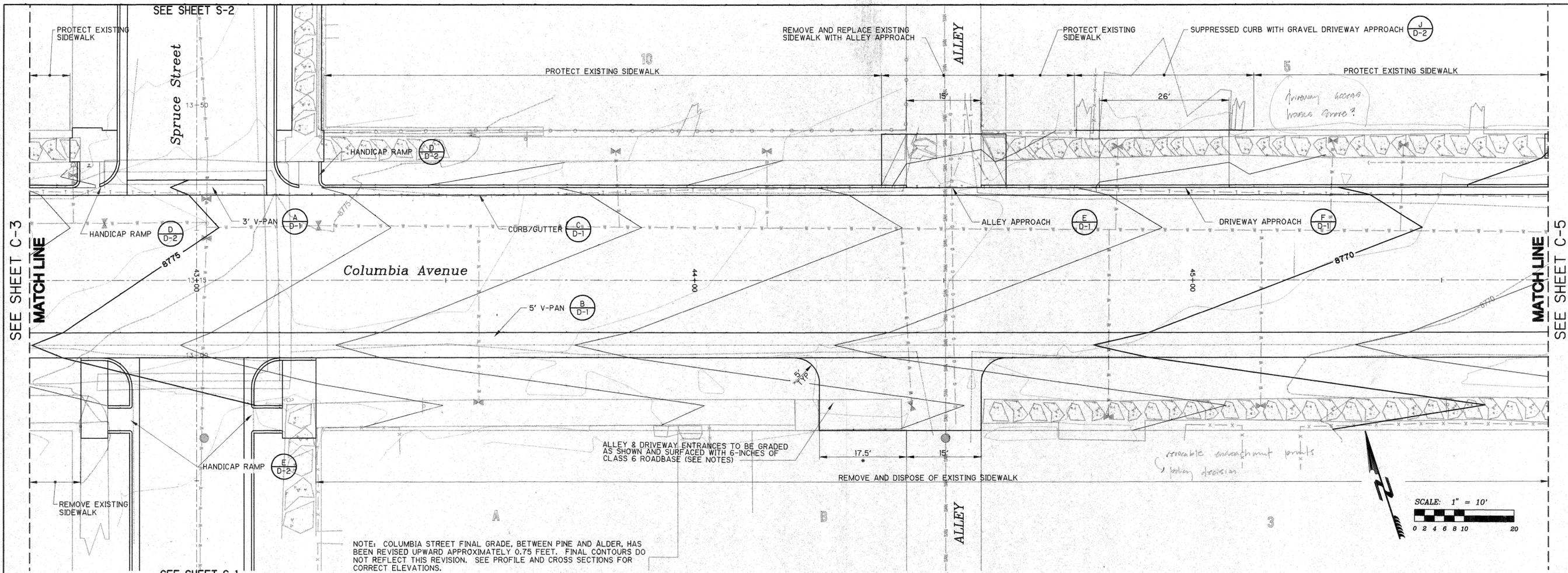
TELLURIDE STREETSCAPES - COLUMBIA AVENUE
 PLAN & PROFILE - STA 39+60 TO 42+65
 100%

FOLEY ASSOCIATES, INC.
 ENGINEERING - PLANNING - SURVEYING
 PO Box 1386 125 W. Pacific Ave Ste. B-1
 Telluride, Colorado, 81465
 970-728-6183 Fax: 970-728-0660
 e-mail to: xmal@foleyassoc.com

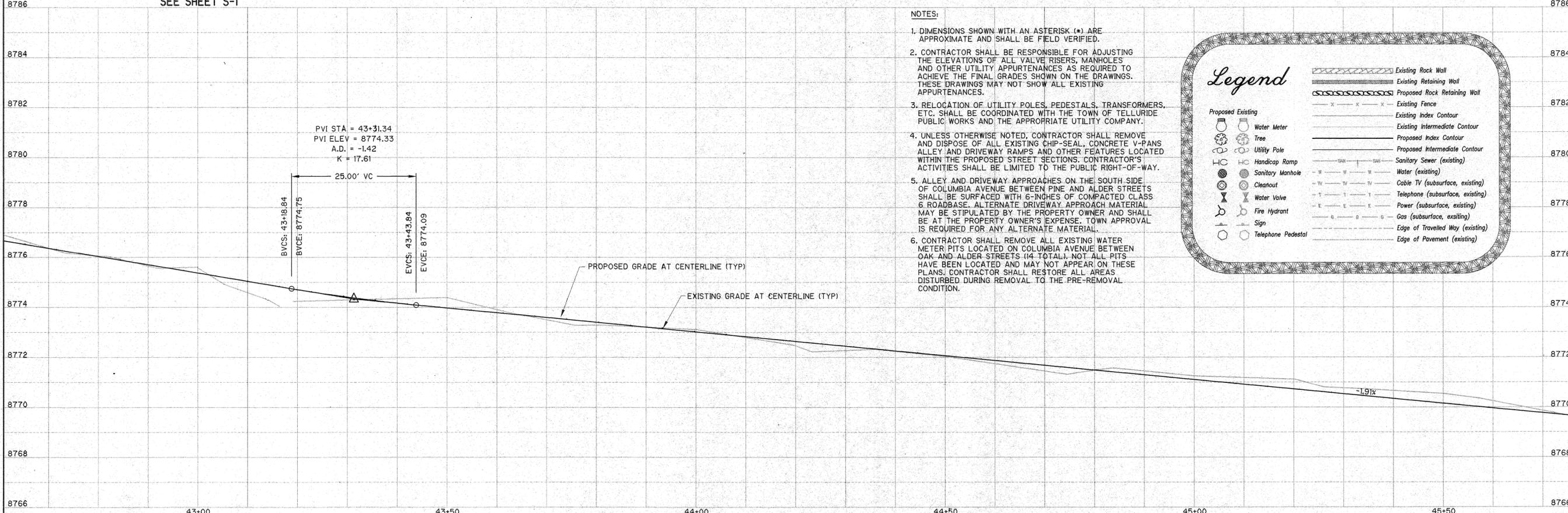
Client: Town of Telluride
 P.O. Box 397
 Telluride, CO 81435
 Phone: (970) 728-3071
 Fax: (970) 728-0548
 e-mail: foley@foleyassoc.com

Contact: William Frowfeller or Rob Bishop
 300 Black Bear Road
 Telluride, CO 81435
 Phone: (970) 728-3177
 Fax: (970) 728-3177

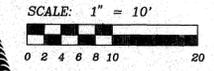
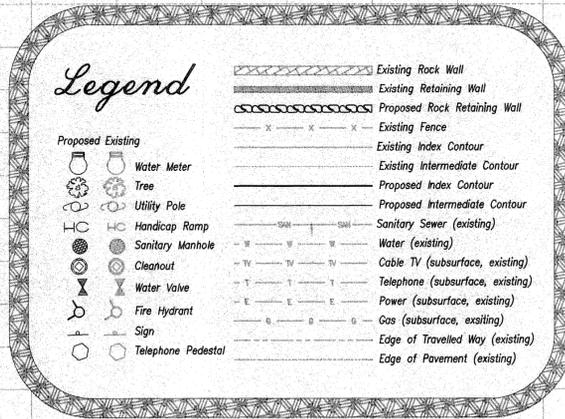
Project Mgr: JG
 Drawn by: BJH
 Checked by:
 Start date: 12/5/97
 Dwg path: STREET\COLUMBIA
 Sheet No. C-3
 Project #: 96122



NOTE: COLUMBIA STREET FINAL GRADE, BETWEEN PINE AND ALDER, HAS BEEN REVISED UPWARD APPROXIMATELY 0.75 FEET. FINAL CONTOURS DO NOT REFLECT THIS REVISION. SEE PROFILE AND CROSS SECTIONS FOR CORRECT ELEVATIONS.



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TELLURIDE STREETSCAPES - COLUMBIA AVENUE
PLAN & PROFILE STA 42+60 TO STA 45+65

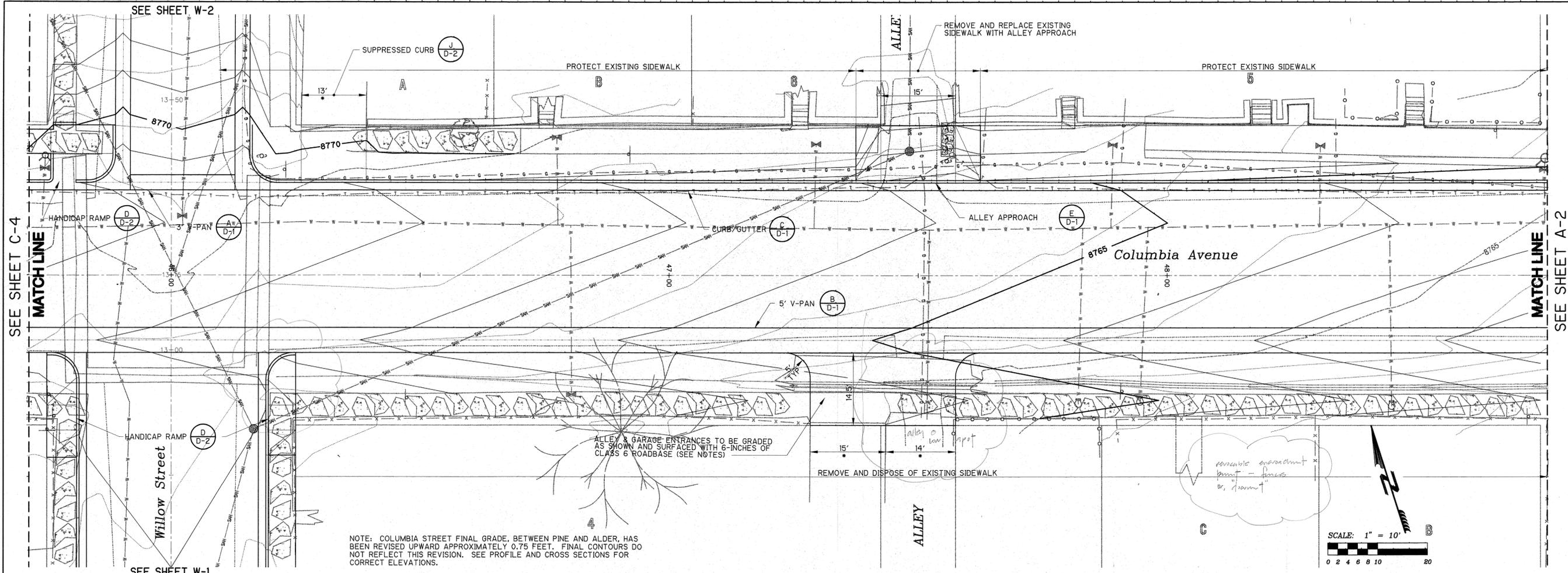
FOLEY ASSOCIATES, INC.
 ENGINEERING - PLANNING - SURVEYING
 PO Box 1885 125 W. Pacific Ave. Ste. B-1
 Telluride, Colorado, 81435
 970-728-6188 fax 970-728-9060
 email to staff@foleyassoc.com

Client: Tom & Nancy Frantelle
 300 W. Highway 160
 Telluride, Colorado, 81435
 Phone: 970-728-3071
 Fax: 970-728-0548

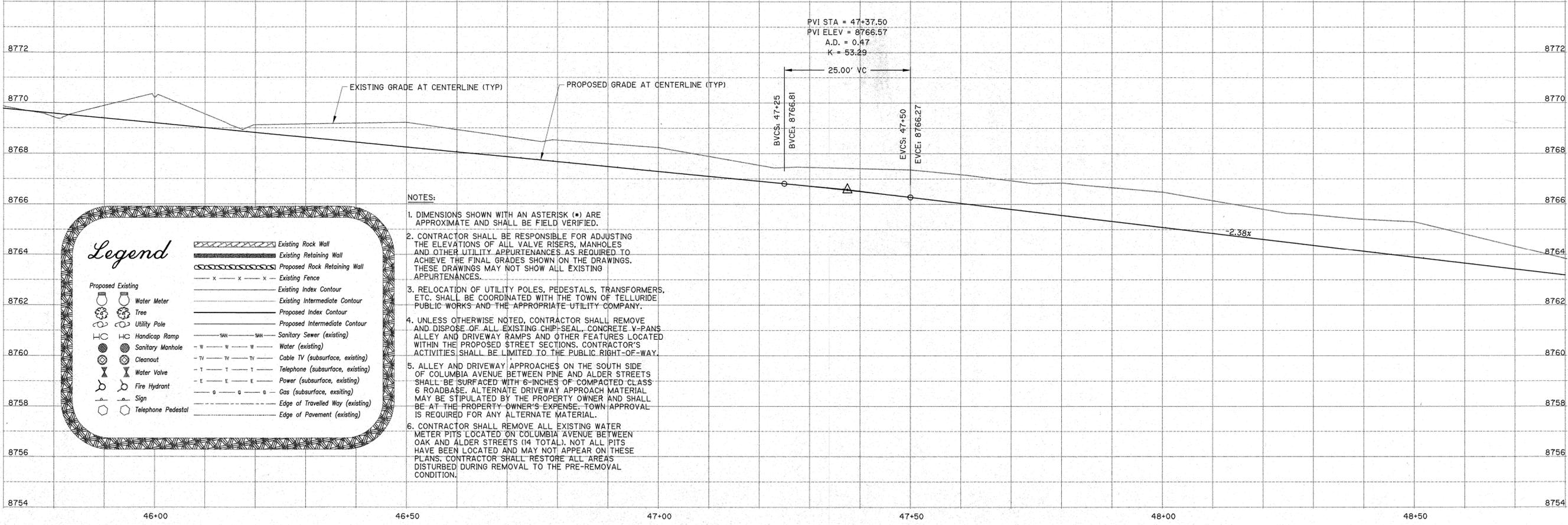
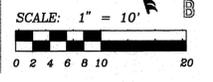
Contact: William Frantelle or Rob Bishop
 300 W. Highway 160
 Telluride, Colorado, 81435
 Phone: 970-728-2177
 Fax: 970-728-0548

Project Mgr: JG
 Drawn by: MJ
 Checked by: JG
 Start date: 6-4-97
 Dwg path: 96122\COLUM1B

Sheet No. **C-4**
 Project #: 96122



NOTE: COLUMBIA STREET FINAL GRADE, BETWEEN PINE AND ALDER, HAS BEEN REVISED UPWARD APPROXIMATELY 0.75 FEET. FINAL CONTOURS DO NOT REFLECT THIS REVISION. SEE PROFILE AND CROSS SECTIONS FOR CORRECT ELEVATIONS.



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Legend

	Existing Rock Wall
	Existing Retaining Wall
	Proposed Rock Retaining Wall
	Existing Fence
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	Existing Intermediate Contour
	Proposed Index Contour
	Proposed Intermediate Contour
	Sanitary Sewer (existing)
	Water (existing)
	Cable TV (subsurface, existing)
	Telephone (subsurface, existing)
	Power (subsurface, existing)
	Gas (subsurface, existing)
	Edge of Travelled Way (existing)
	Edge of Pavement (existing)

Proposed Existing

	Water Meter
	Tree
	Utility Pole
	Handicap Ramp
	Sanitary Manhole
	Cleanout
	Water Valve
	Fire Hydrant
	Sign
	Telephone Pedestal

TELLURIDE STREETSCAPES - COLUMBIA AVENUE
 PLAN & PROFILE STA 45+70 TO STA 48+75

Client: Telluride
 Town of Telluride
 PO Box 397
 Telluride, Colorado, 81435
 Phone: 970-728-0546
 Fax: 970-728-3071

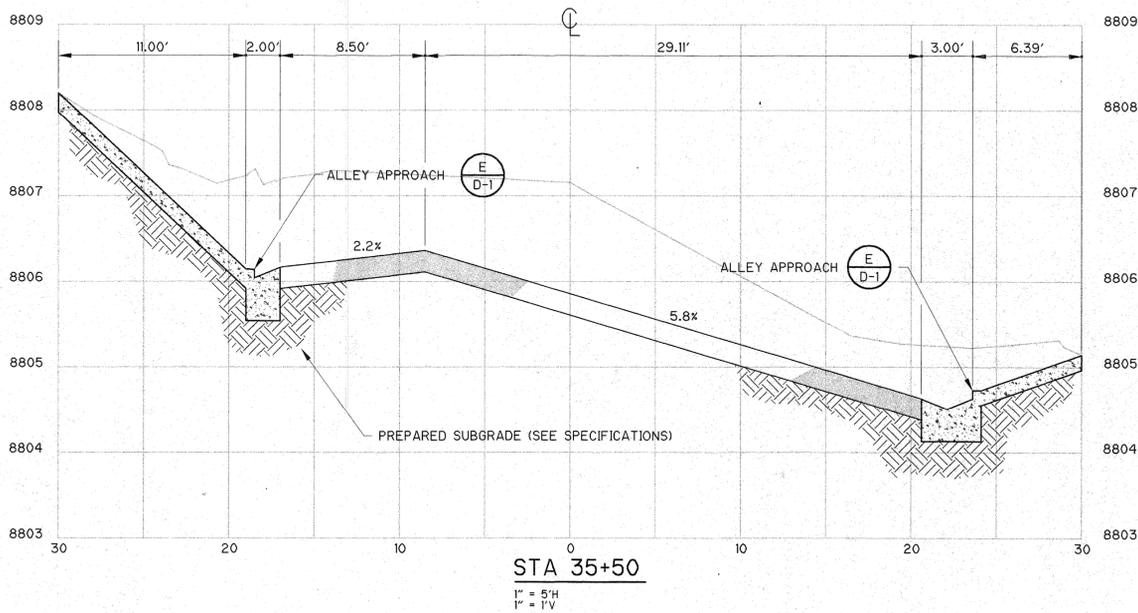
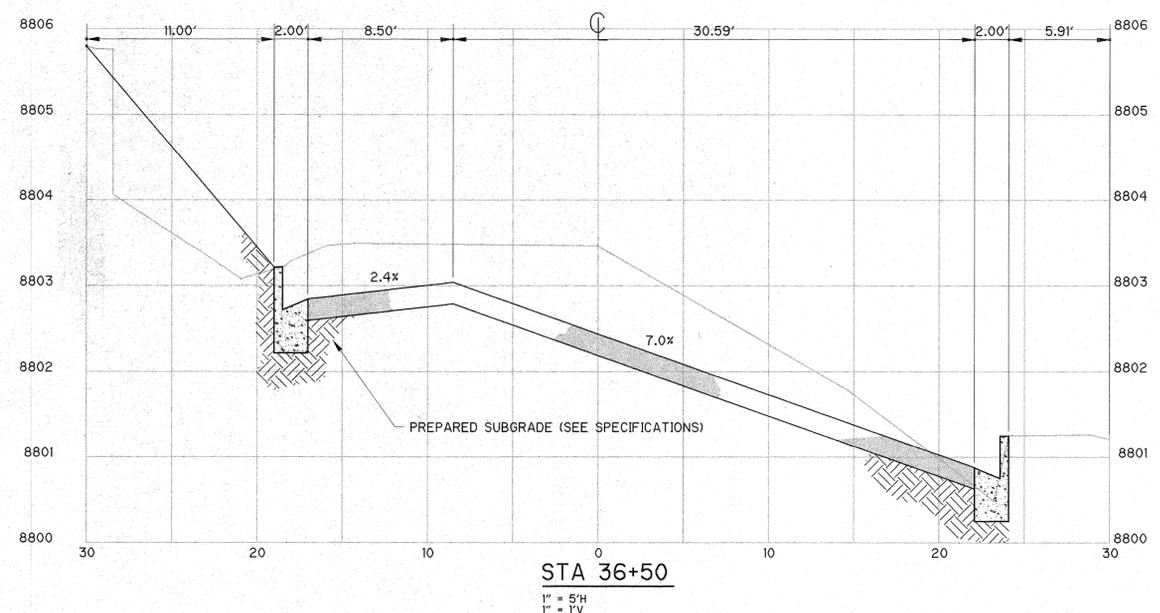
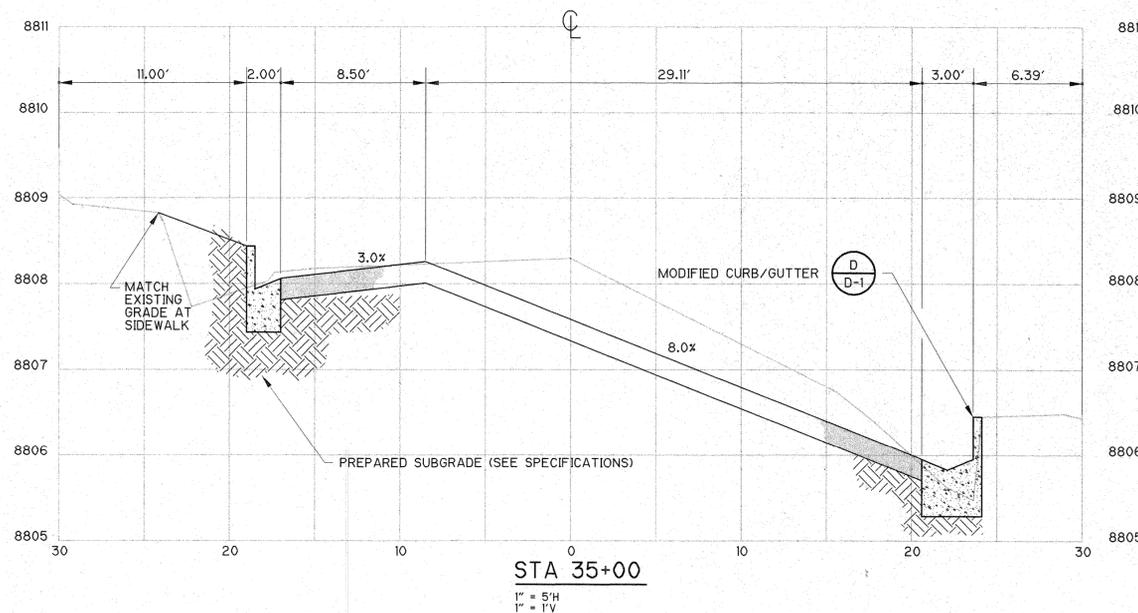
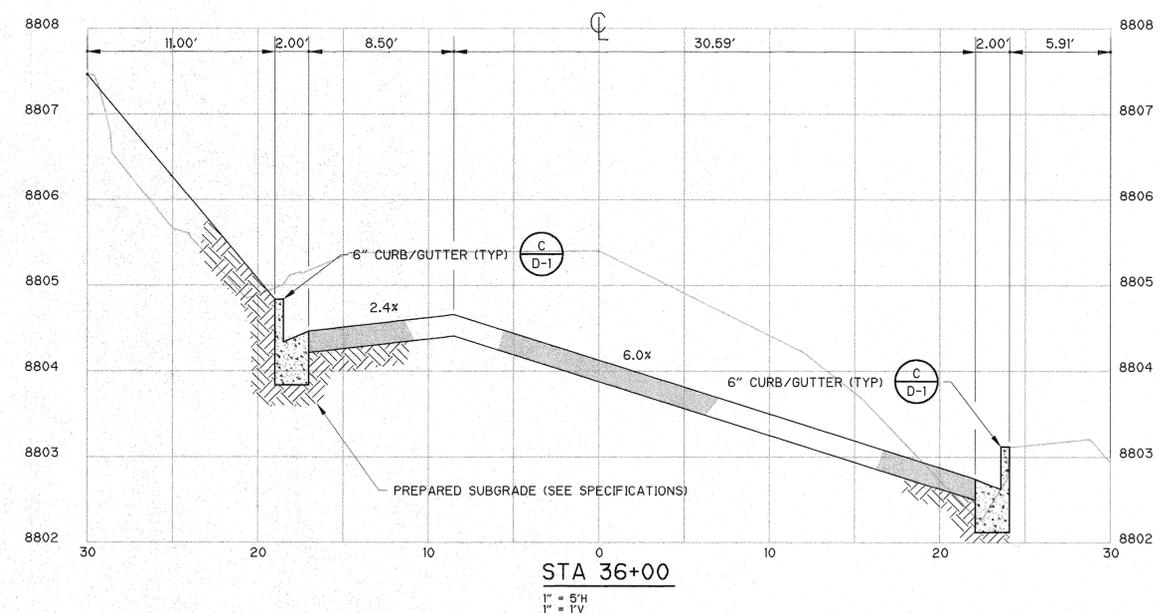
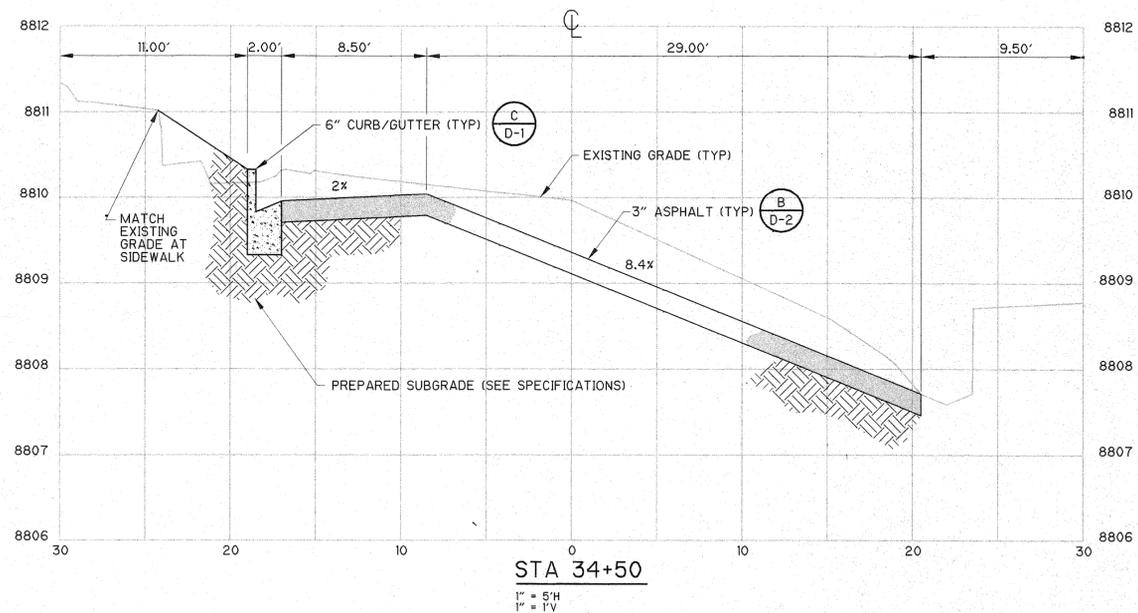
Contact: William F. Foley
 300 Elmwood Road
 Telluride, Colorado, 81435
 Phone: 970-728-2177
 Fax: 970-728-0546

FOLEY ASSOCIATES, INC.
 ENGINEERING - PLANNING - SURVEYING
 PO Box 1885 128 W. Pacific Ave Ste. B-1
 Telluride, Colorado, 81435
 970-728-6158 Fax 970-728-6050
 e-mail: info@foleyassoc.com

Engineer's stamp and signature

Rev.	description	date	by

Project Mgr: JG
 Drawn by: MJ
 Checked by: DF
 Start date: 6-4-97
 Dwg path: 96122\COLUMB1B
 Sheet No. C-5
 Project #: 96122



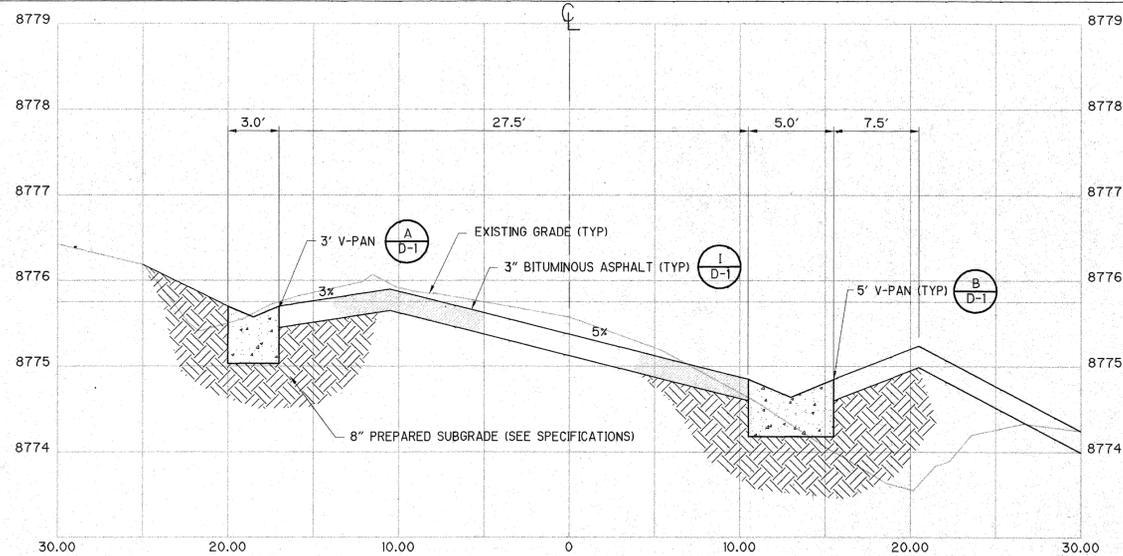
Telluride Streetscapes (Columbia Avenue)
CROSS SECTIONS - STA 34+50 TO STA 36+00

FOLEY ASSOCIATES, INC.
ENGINEERING - PLANNING - SURVEYING
PO Box 186 126 W. Pacific Ave Ste. B-1
Telluride, Colorado, 81435
970-728-6183 fax: 970-728-6900
e-mail to: smail@foleyassoc.com

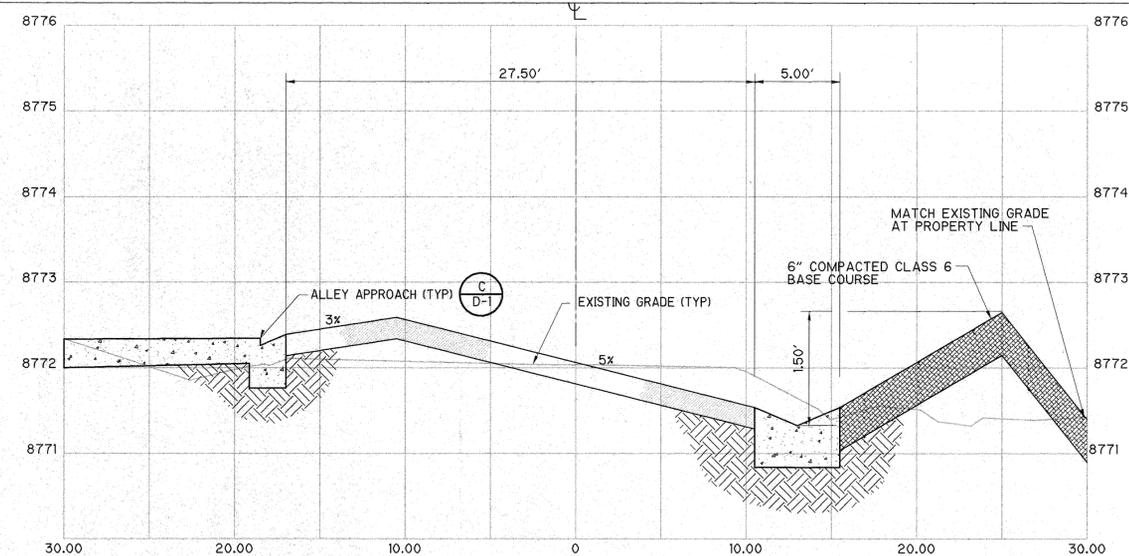
Engineer's stamp and signature

Project Mgr:	JG
Drawn by:	MJ/BJH
Checked by:	DF
Start date:	6-4-97
Dwg path:	96122\COLUMIA
Sheet No.	C-6
Project #:	96122

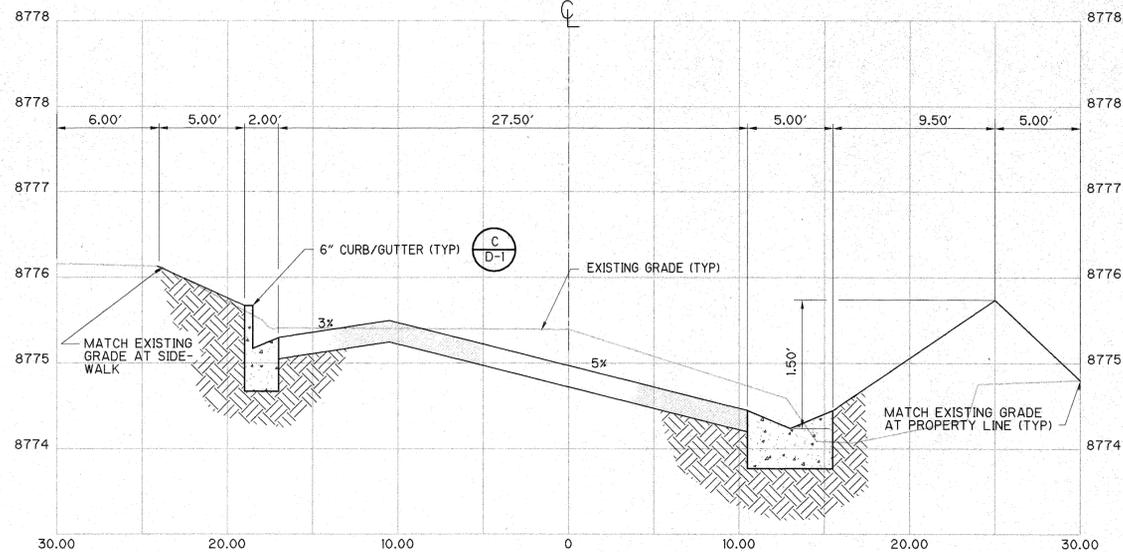
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Scale: 1" = 3'PS
Drawing File: STREETS COLUMIA.DWG (DAX)
Xrefs: LEGEND



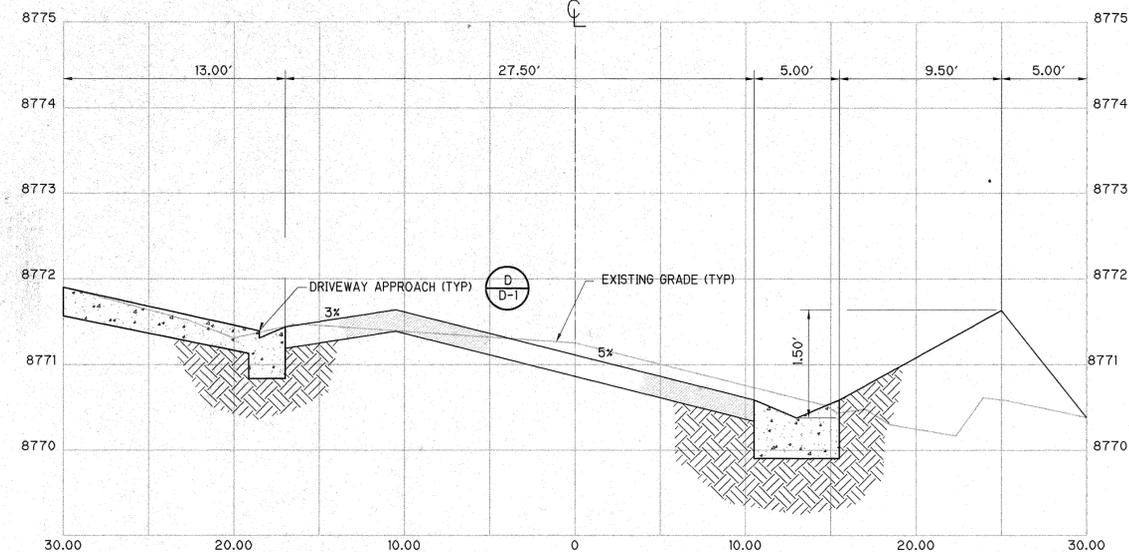
STA 43+00
1" = 5'H
1" = 1'V



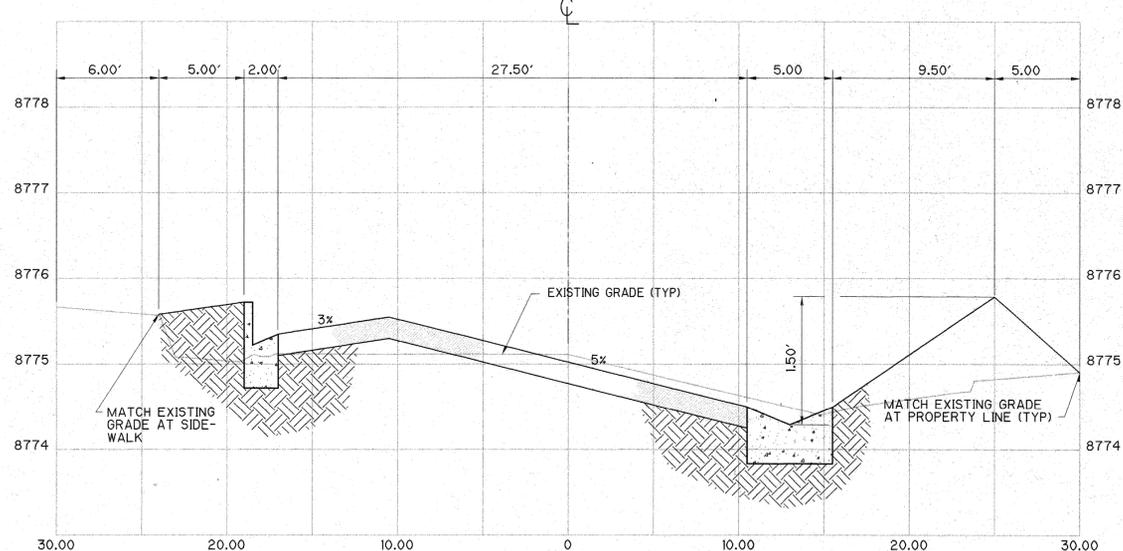
STA 44+50
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1" = 1'V



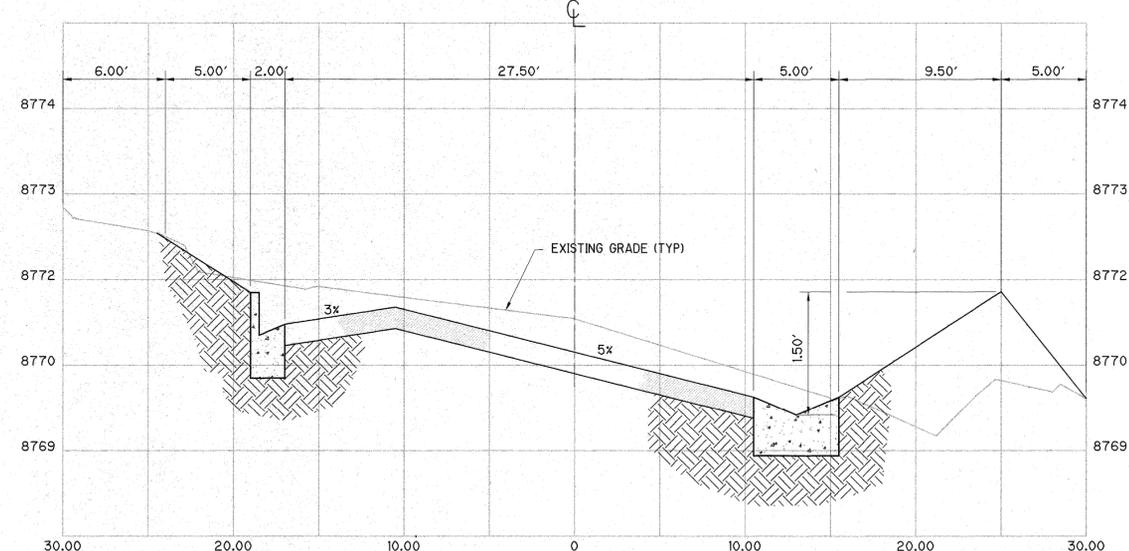
STA 43+50
1" = 5'H
1" = 1'V



STA 45+00
1" = 5'H
1" = 1'V



STA 44+00
1" = 5'H
1" = 1'V



STA 45+50
1" = 5'H
1" = 1'V

Time: 16:41:15
Date: 6/1/98
View: SHEETS
Drawing File: STREETS\COLUMBIA.DWG (DAX)
Xrefs: LEGEND

TELLURIDE STREETSCAPES - COLUMBIA AVENUE
CROSS SECTIONS - STA 43+00 TO 45+50

FOLEY ASSOCIATES INC.
ENGINEERING - PLANNING - SURVEYING
PO Box 1885 125 W. Pacific Ave. Ste. B-1
Telluride, Colorado, 81435
970-728-6153 fax 970-728-0050
e-mail to xmail@foleyassoc.com

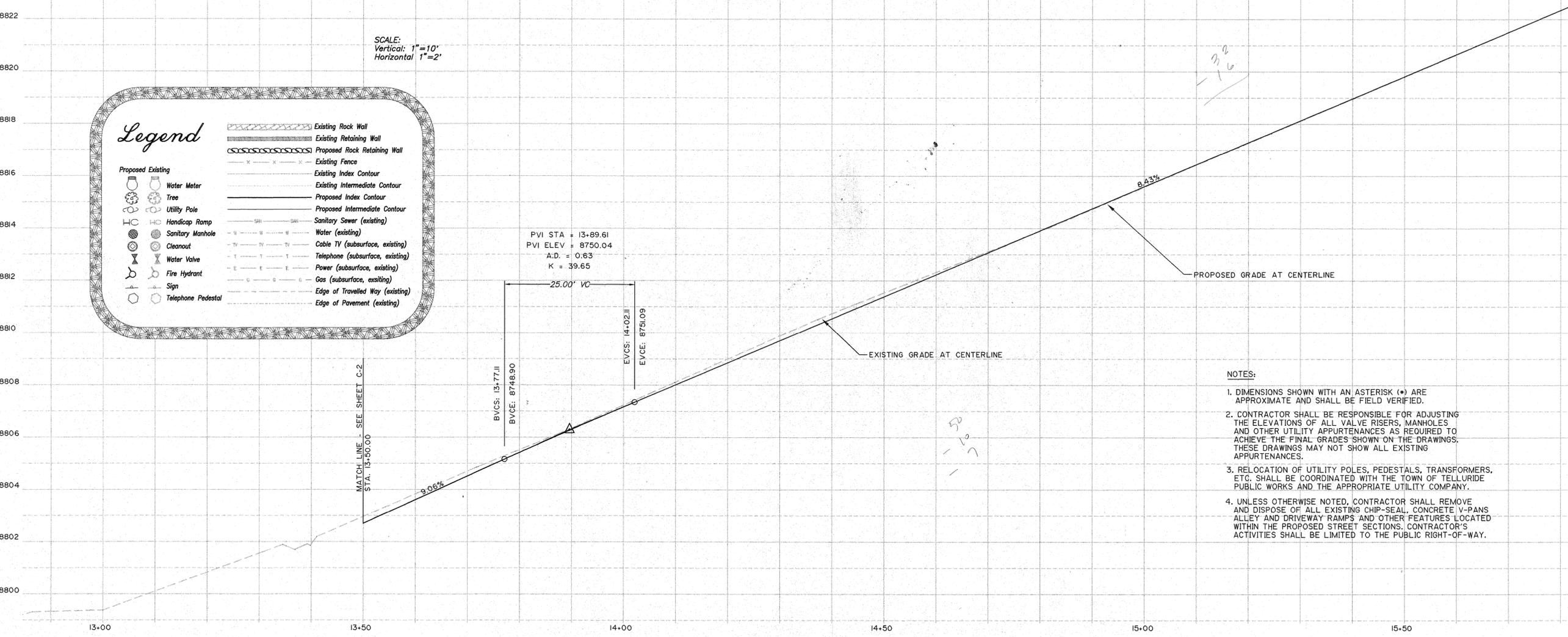
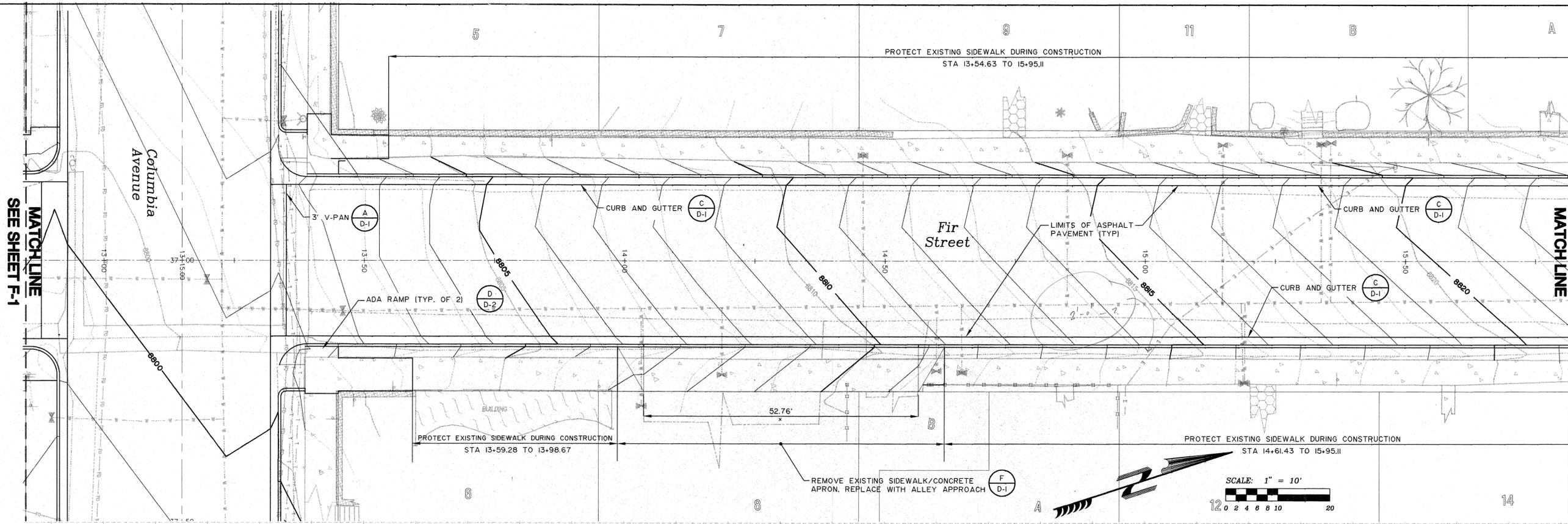
Engineer's stamp and signature

Rev.	date	by	description

Project Mgr: JG
Drawn by: MJK/JS
Checked by: JG
Start date: 6-4-97
Dwg path: 96122\COLUM1B
Sheet No. C-9
Project #: 96122

SEE SHEET F-1
MATCHLINE

SEE SHEET F-3
MATCHLINE



SCALE:
Vertical: 1"=10'
Horizontal 1"=2'

Legend

	Existing Rock Wall
	Existing Retaining Wall
	Proposed Rock Retaining Wall
	Existing Fence
	Existing Index Contour
	Existing Intermediate Contour
	Proposed Index Contour
	Proposed Intermediate Contour
	Sanitary Sewer (existing)
	Water (existing)
	Cable TV (subsurface, existing)
	Telephone (subsurface, existing)
	Power (subsurface, existing)
	Gas (subsurface, existing)
	Edge of Travelled Way (existing)
	Edge of Pavement (existing)

Proposed Existing

	Water Meter
	Tree
	Utility Pole
	Handicap Ramp
	Sanitary Manhole
	Cleanout
	Water Valve
	Fire Hydrant
	Sign
	Telephone Pedestal

- NOTES:**
- DIMENSIONS SHOWN WITH AN ASTERISK (*) ARE APPROXIMATE AND SHALL BE FIELD VERIFIED.
 - CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING THE ELEVATIONS OF ALL VALVE RISERS, MANHOLES AND OTHER UTILITY APPURTENANCES AS REQUIRED TO ACHIEVE THE FINAL GRADES SHOWN ON THE DRAWINGS. THESE DRAWINGS MAY NOT SHOW ALL EXISTING APPURTENANCES.
 - RELOCATION OF UTILITY POLES, PEDESTALS, TRANSFORMERS, ETC. SHALL BE COORDINATED WITH THE TOWN OF TELLURIDE PUBLIC WORKS AND THE APPROPRIATE UTILITY COMPANY.
 - UNLESS OTHERWISE NOTED, CONTRACTOR SHALL REMOVE AND DISPOSE OF ALL EXISTING CHIP-SEAL, CONCRETE V-PANS, ALLEY AND DRIVEWAY RAMPS AND OTHER FEATURES LOCATED WITHIN THE PROPOSED STREET SECTIONS. CONTRACTOR'S ACTIVITIES SHALL BE LIMITED TO THE PUBLIC RIGHT-OF-WAY.

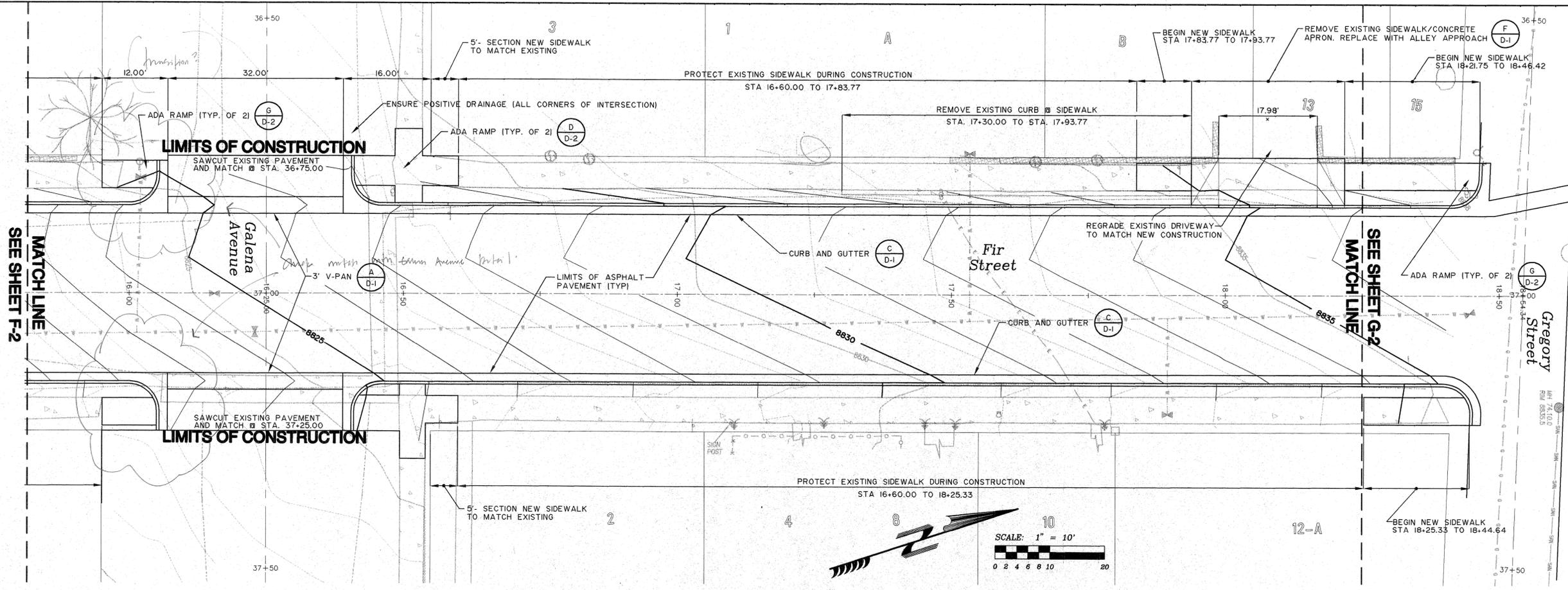
FOLEY ASSOCIATES, INC.
ENGINEERING - PLANNING - SURVEYING
PO Box 1885 125 W. Pacific Ave Ste. B-1
Telluride, Colorado, 81435
970-728-6153 fax 970-728-6050
e-mail to smul@foleyassoc.com

Engineer's stamp and signature

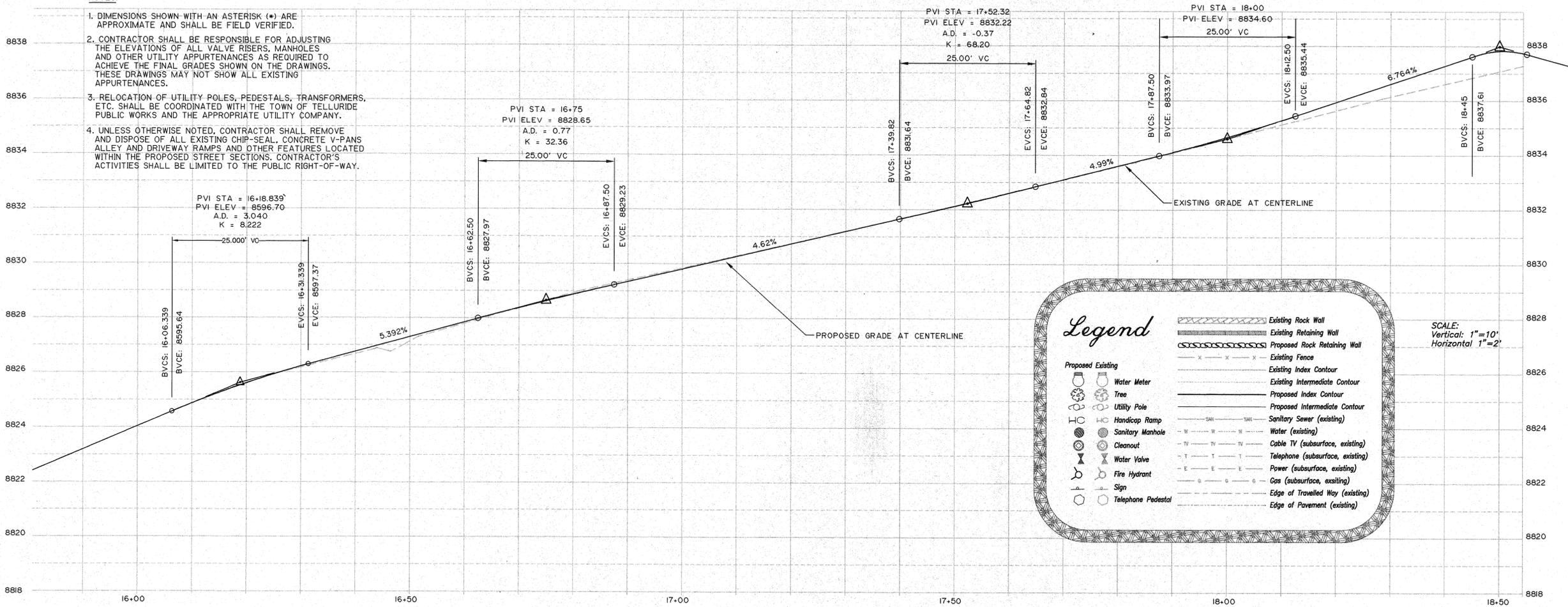
Project Mgr:	JG
Drawn by:	MJ/KJS
Checked by:	
Start date:	12-5-97
Dwg path:	STREETS\FIR-2
Sheet No.	F-2
Project #:	96122

TELLURIDE STREETSCAPES
PLAN AND PROFILE
NORTH FIR STREET - STA 13+50 TO STA 15+80

Client:
Town of Telluride
PO Box 397
Telluride
CO 81435
Phone: 970-728-0548
e-mail:
Contact:
William Fowler, Esq. Bishop
300 Black Bear Road
Telluride
CO 81435
Phone: 970-728-2177
e-mail:
Fax: 970-728-0548



- NOTES:
1. DIMENSIONS SHOWN WITH AN ASTERISK (*) ARE APPROXIMATE AND SHALL BE FIELD VERIFIED.
 2. CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING THE ELEVATIONS OF ALL VALVE RISERS, MANHOLES AND OTHER UTILITY APPURTENANCES AS REQUIRED TO ACHIEVE THE FINAL GRADES SHOWN ON THE DRAWINGS. THESE DRAWINGS MAY NOT SHOW ALL EXISTING APPURTENANCES.
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Legend

	Existing Rock Wall
	Existing Retaining Wall
	Proposed Rock Retaining Wall
	Existing Fence
	Existing Index Contour
	Existing Intermediate Contour
	Proposed Index Contour
	Proposed Intermediate Contour
	Sanitary Sewer (existing)
	Water (existing)
	Cable TV (subsurface, existing)
	Telephone (subsurface, existing)
	Power (subsurface, existing)
	Gas (subsurface, existing)
	Edge of Travelled Way (existing)
	Edge of Pavement (existing)

Proposed Existing:

- Water Meter
- Tree
- Utility Pole
- Handicap Ramp
- Sanitary Manhole
- Cleanout
- Water Valve
- Fire Hydrant
- Sign
- Telephone Pedestal

SCALE:
Vertical: 1"=10'
Horizontal 1"=2'

TELLURIDE STREETSCAPES
PLAN AND PROFILE
NORTH FIR STREET - STA 15+80 TO STA 18+25

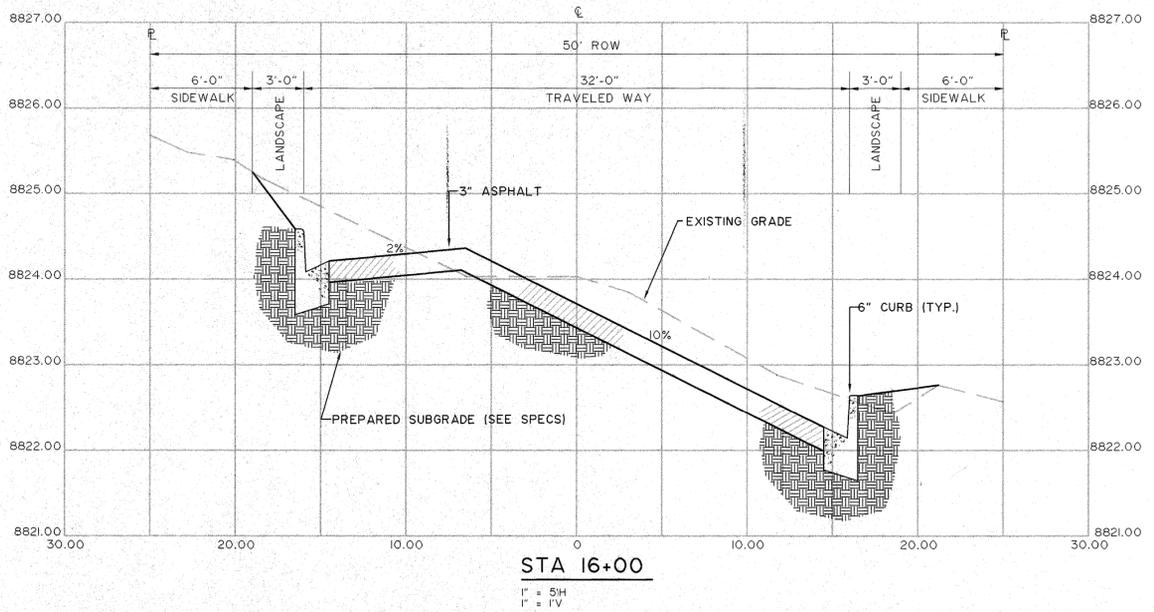
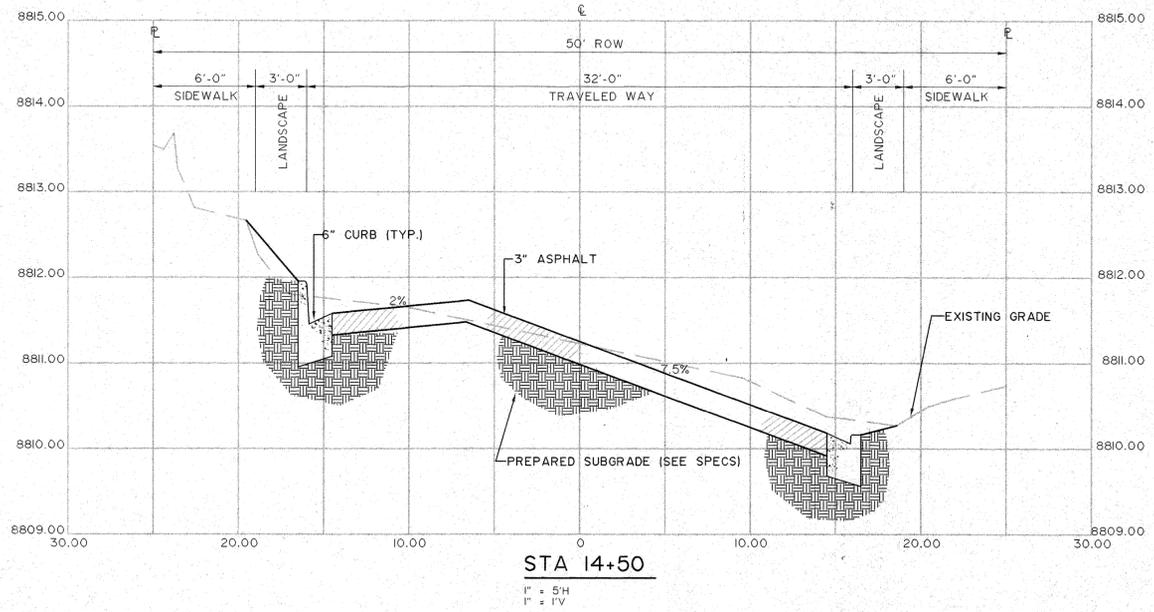
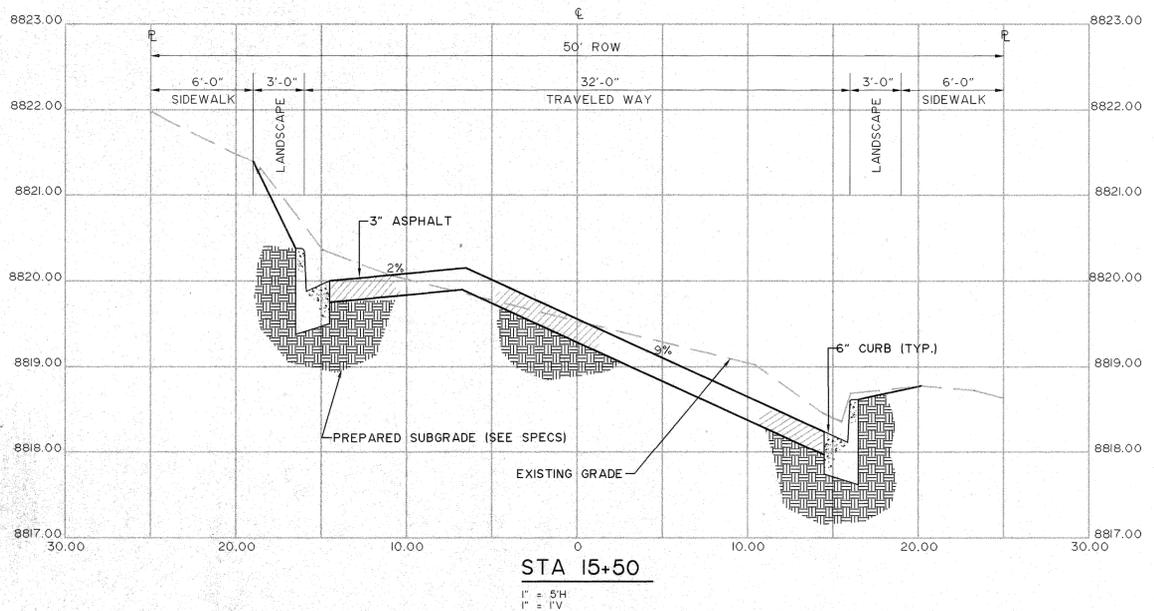
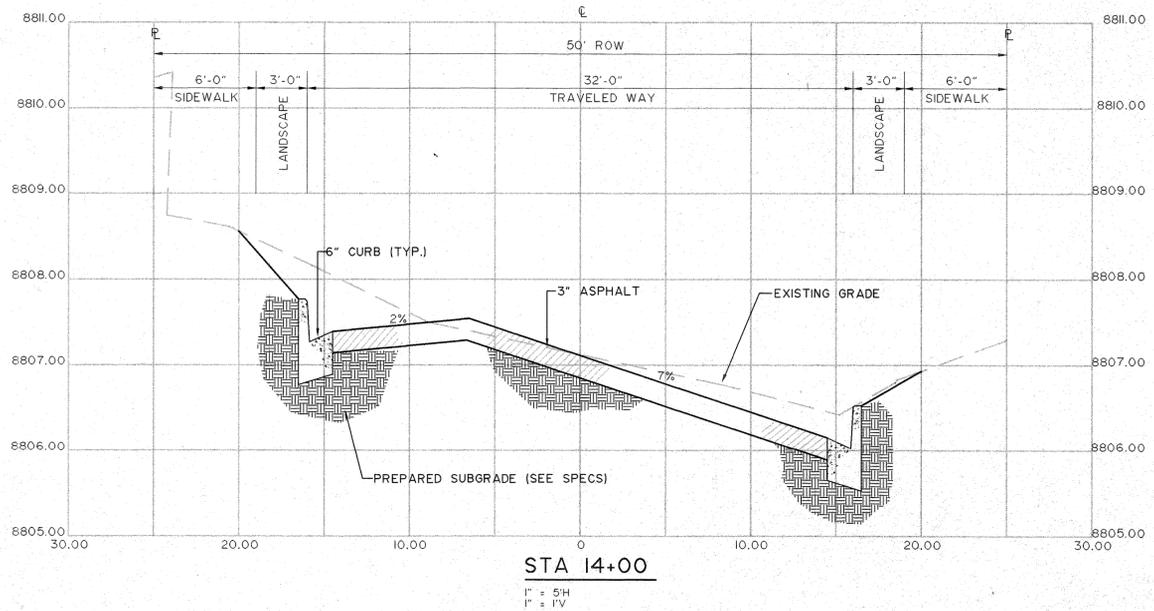
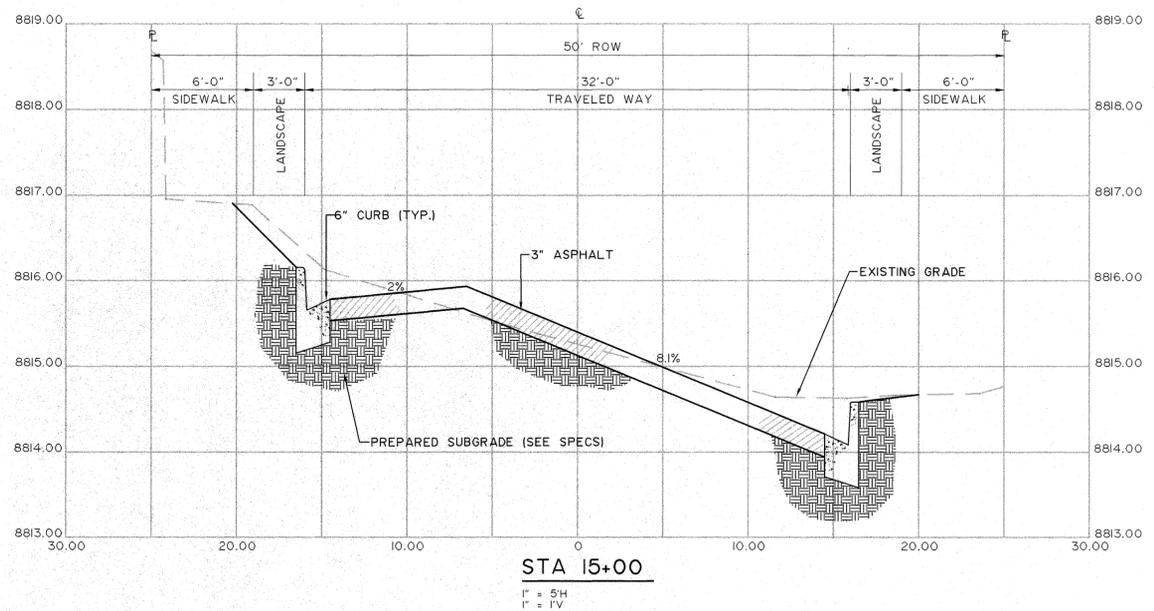
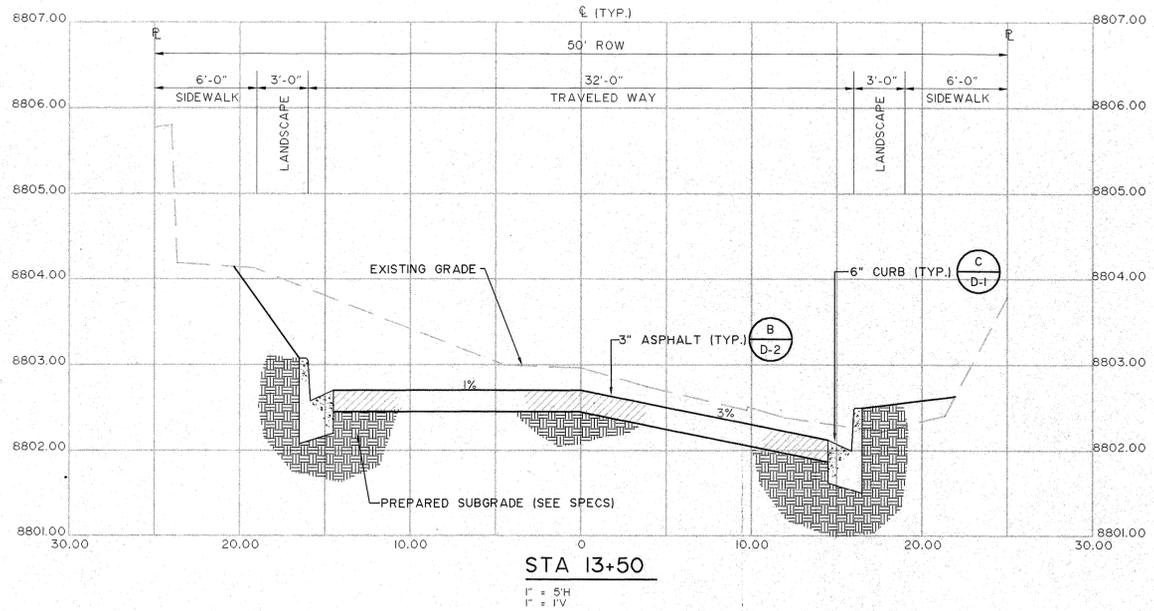
FOLEY ASSOCIATES, INC.
ENGINEERING - PLANNING - SURVEYING
PO Box 1986, 125 W. Pacific Ave. Ste. B-1
Telluride, Colorado, 81435
970-728-6108 fax 970-728-6000
e-mail to smurf@foleyassoc.com

Client:
Town of Telluride
PO Box 397
CO 81435

Contact:
William Frowmfeiler or Rob Bishop
300 Back Bear Road
CO 81435

Phone: 970-728-2177
Fax: 970-728-0548
e-mail:

Project Mgr:	JG
Drawn by:	MJ/KJS
Checked by:	
Start date:	12-5-97
Dwg path:	STREET\FIR-2
Sheet No.	F-3
Project #:	96122



TELLURIDE STREETSCAPES

CROSS SECTIONS
 NORTH FIR STREET - STA 13+50 TO STA 16+00

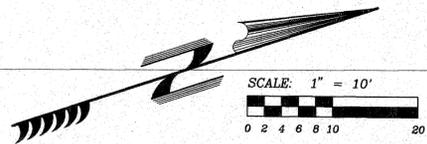
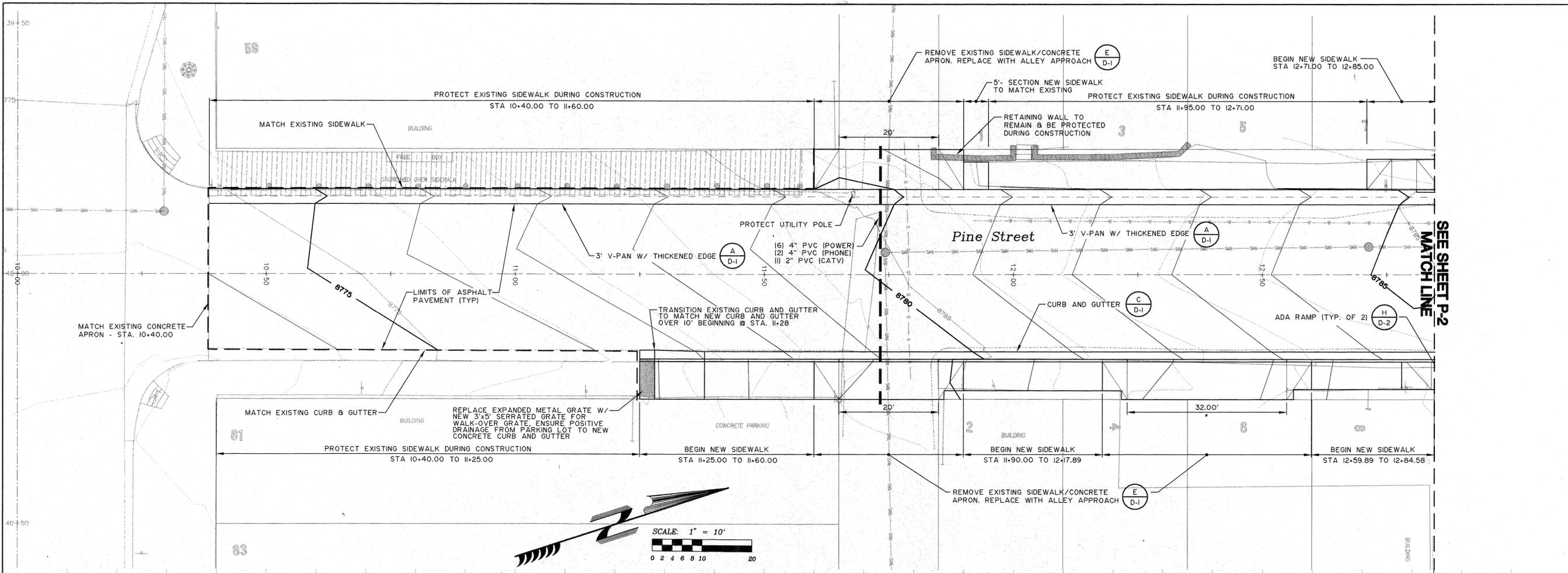
Client: Town of Telluride
 PO Box 397
 Telluride, Colorado, 81652
 Contact: William Frowmiller or Rob Bishop
 300 Black Bear Road
 CO 81435
 Phone: 970-728-2177
 Fax: 970-728-0548
 e-mail: wfrowmiller@townof Telluride.com

FOLEY ASSOCIATES, INC.
 ENGINEERING - PLANNING - SURVEYING
 PO Box 1888 125 W. Pacific Ave Ste. B-1
 Telluride, Colorado, 81652
 970-728-6139 Fax 970-728-0000
 e-mail to info@foleyassoc.com

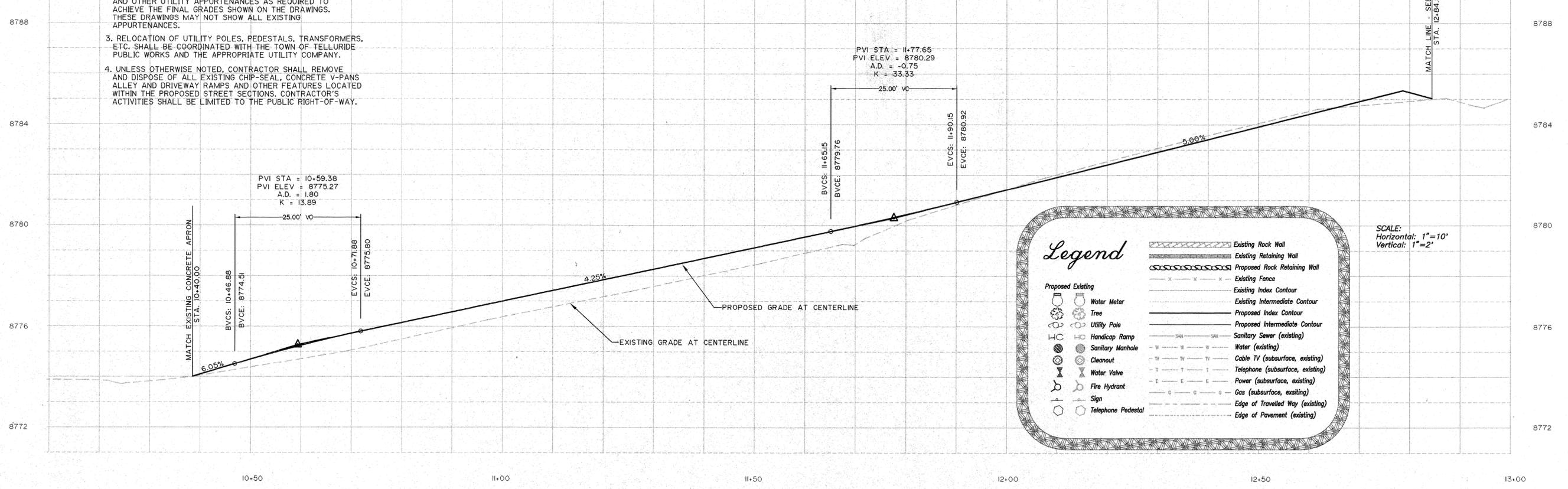
Engineer's stamp and signature

Rev.	description	date	by

Project Mgr: JG
 Drawn by: MJ/KJS
 Checked by:
 Start date: 12-5-97
 Dwg path: STREETS\FIR-2
 Sheet No.



- NOTES:
1. DIMENSIONS SHOWN WITH AN ASTERISK (*) ARE APPROXIMATE AND SHALL BE FIELD VERIFIED.
 2. CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING THE ELEVATIONS OF ALL VALVE RISERS, MANHOLES AND OTHER UTILITY APPURTENANCES AS REQUIRED TO ACHIEVE THE FINAL GRADES SHOWN ON THE DRAWINGS. THESE DRAWINGS MAY NOT SHOW ALL EXISTING APPURTENANCES.
 3. RELOCATION OF UTILITY POLES, PEDESTALS, TRANSFORMERS, ETC. SHALL BE COORDINATED WITH THE TOWN OF TELLURIDE PUBLIC WORKS AND THE APPROPRIATE UTILITY COMPANY.
 4. UNLESS OTHERWISE NOTED, CONTRACTOR SHALL REMOVE AND DISPOSE OF ALL EXISTING CHIP-SEAL, CONCRETE V-PANS ALLEY AND DRIVEWAY RAMPS AND OTHER FEATURES LOCATED WITHIN THE PROPOSED STREET SECTIONS. CONTRACTOR'S ACTIVITIES SHALL BE LIMITED TO THE PUBLIC RIGHT-OF-WAY.



Legend

	Existing Rock Wall
	Existing Retaining Wall
	Proposed Rock Retaining Wall
	Existing Fence
	Existing Index Contour
	Existing Intermediate Contour
	Proposed Index Contour
	Proposed Intermediate Contour
	Sanitary Sewer (existing)
	Water (existing)
	Cable TV (subsurface, existing)
	Telephone (subsurface, existing)
	Power (subsurface, existing)
	Gas (subsurface, existing)
	Edge of Travelled Way (existing)
	Edge of Pavement (existing)

Proposed Existing

	Water Meter
	Tree
	Utility Pole
	Handicap Ramp
	Sanitary Manhole
	Cleanout
	Water Valve
	Fire Hydrant
	Sign
	Telephone Pedestal

SCALE:
Horizontal: 1"=10'
Vertical: 1"=2'

TELLURIDE STREETSCAPES
PLAN AND PROFILE
NORTH PINE STREET - STA 10+00 TO STA 12+80

Client: Town of Telluride
PO Box 307
Telluride, Colorado, 81435
970-728-6153 fax 970-728-6050
e-mail to stamp@fleyassoc.com

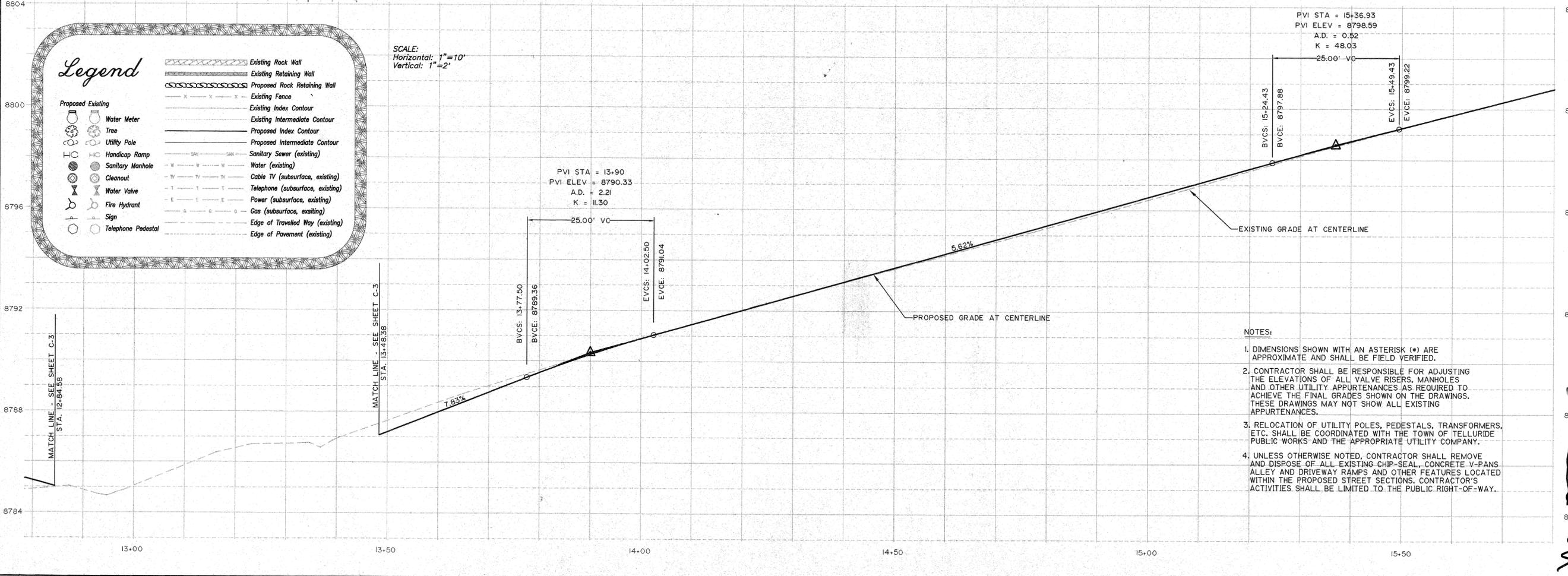
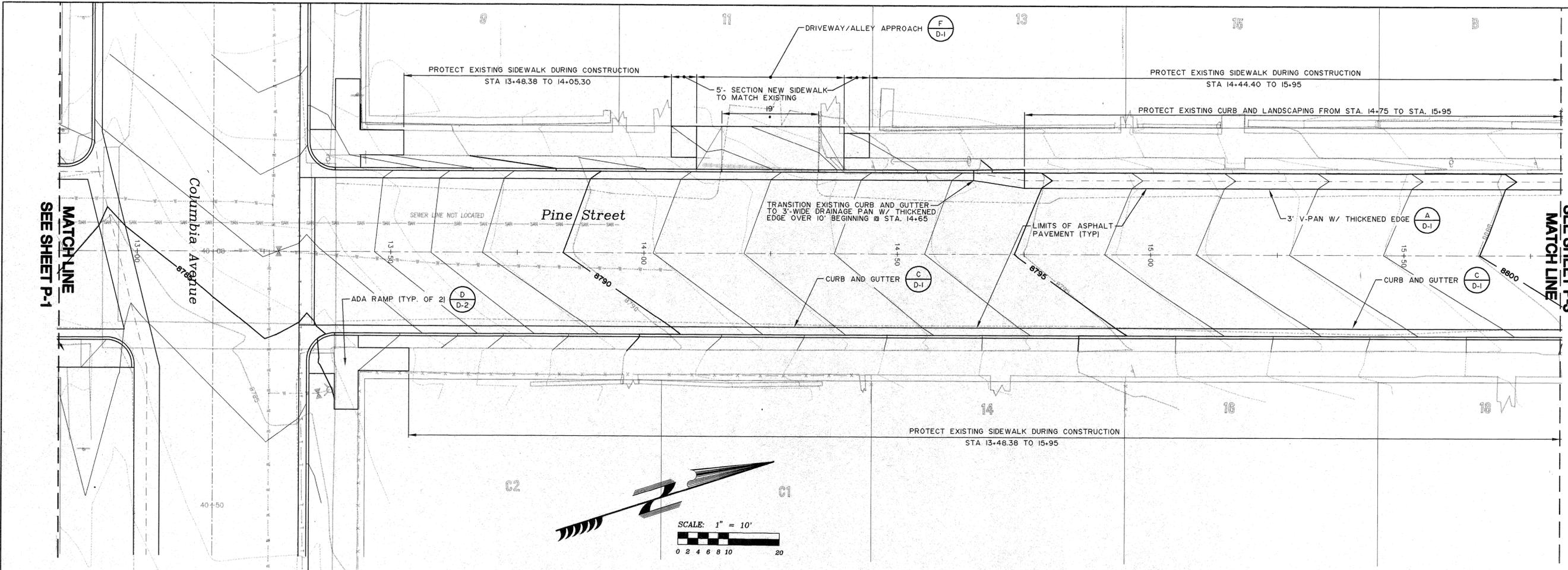
Contact: William F. Bishop
300 Rockwood Road
Telluride, Colorado, 81435
Phone: 970-728-2177
Fax: 970-728-0546

FLEY ASSOCIATES, INC.
ENGINEERING - PLANNING - SURVEYING
PO Box 1885 122 W. Pacific Ave Ste. B-1
Telluride, Colorado, 81435
970-728-6153 fax 970-728-6050
e-mail to stamp@fleyassoc.com

Project Mgr: JG
Drawn by: MJ/KJS
Checked by:
Start date: 7-23-97
Dwg path: STREETS/PINE
Sheet No.
P-1
Project #: 96122

Time: 14:53:60
Date: 4/10/1998
View: X-SEC
Scale: 1"=20'
PLOT: LEGEND, PINE/STC

Time: 14:53:00 Scale: 1"=3'(PS)
 View: X=SEC1-3
 Drawing File: STREETS\PINE.DWG (Red)
 Xrefs: LEGEND, PINEHATC



Legend

	Existing Rock Wall
	Existing Retaining Wall
	Proposed Rock Retaining Wall
	Existing Fence
	Existing Index Contour
	Proposed Index Contour
	Existing Intermediate Contour
	Proposed Intermediate Contour
	Sanitary Sewer (existing)
	Water (existing)
	Cable TV (subsurface, existing)
	Telephone (subsurface, existing)
	Power (subsurface, existing)
	Gas (subsurface, existing)
	Edge of Travelled Way (existing)
	Edge of Pavement (existing)
	Water Meter
	Tree
	Utility Pole
	Handicap Ramp
	Sanitary Manhole
	Cleanout
	Water Valve
	Fire Hydrant
	Sign
	Telephone Pedestal

SCALE:
 Horizontal: 1"=10'
 Vertical: 1"=2'

SCALE: 1" = 10'
 0 2 4 6 8 10 20

- NOTES:**
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 - UNLESS OTHERWISE NOTED, CONTRACTOR SHALL REMOVE AND DISPOSE OF ALL EXISTING CHIP-SEAL, CONCRETE V-PANS ALLEY AND DRIVEWAY RAMPS AND OTHER FEATURES LOCATED WITHIN THE PROPOSED STREET SECTIONS. CONTRACTOR'S ACTIVITIES SHALL BE LIMITED TO THE PUBLIC RIGHT-OF-WAY.

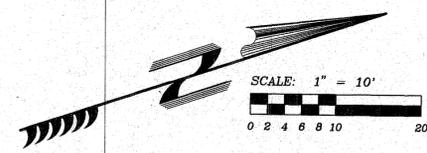
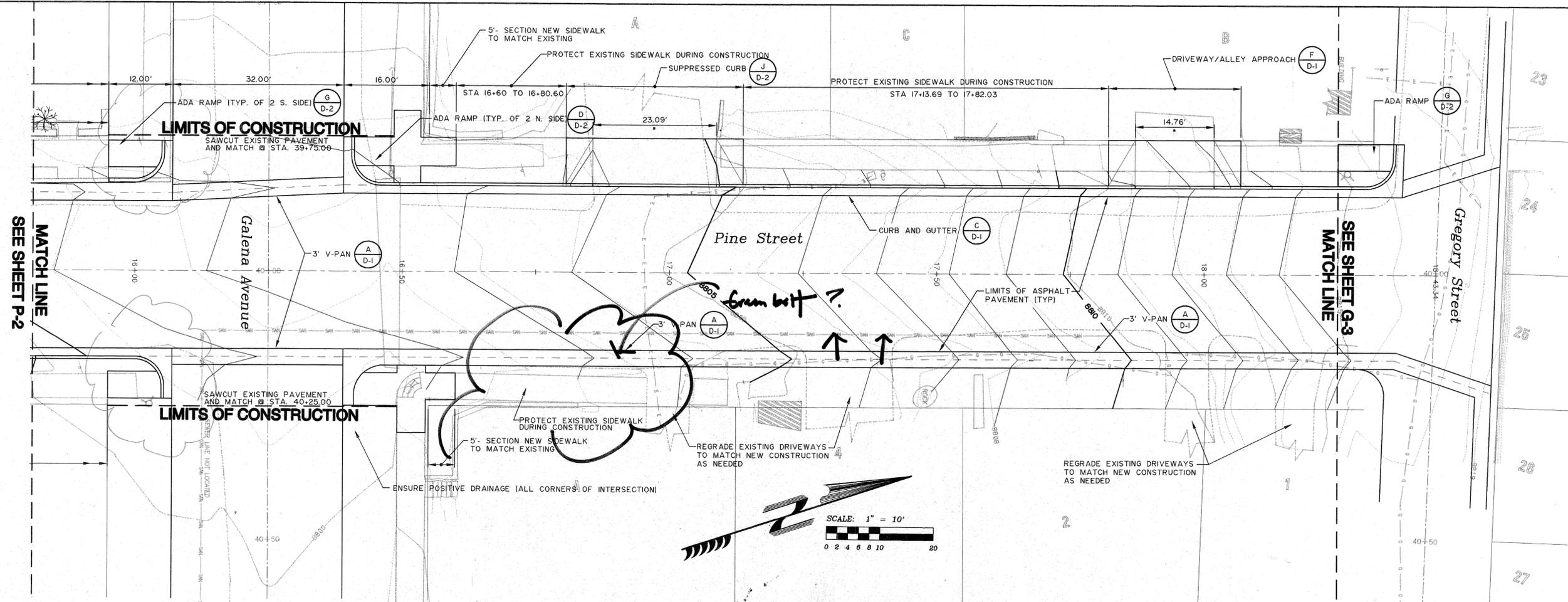
FOLEY ASSOCIATES, INC.
 ENGINEERING • PLANNING • SURVEYING
 125 W. Pacific Ave. Ste. B-1
 Telluride, Colorado, 81485
 970-728-6183 Fax: 970-728-6060
 e-mail to smaller@foleyassoc.com

TELLURIDE STREETSCAPES
 PLAN AND PROFILE
 NORTH PINE STREET - STA 13+50 TO STA 15+80

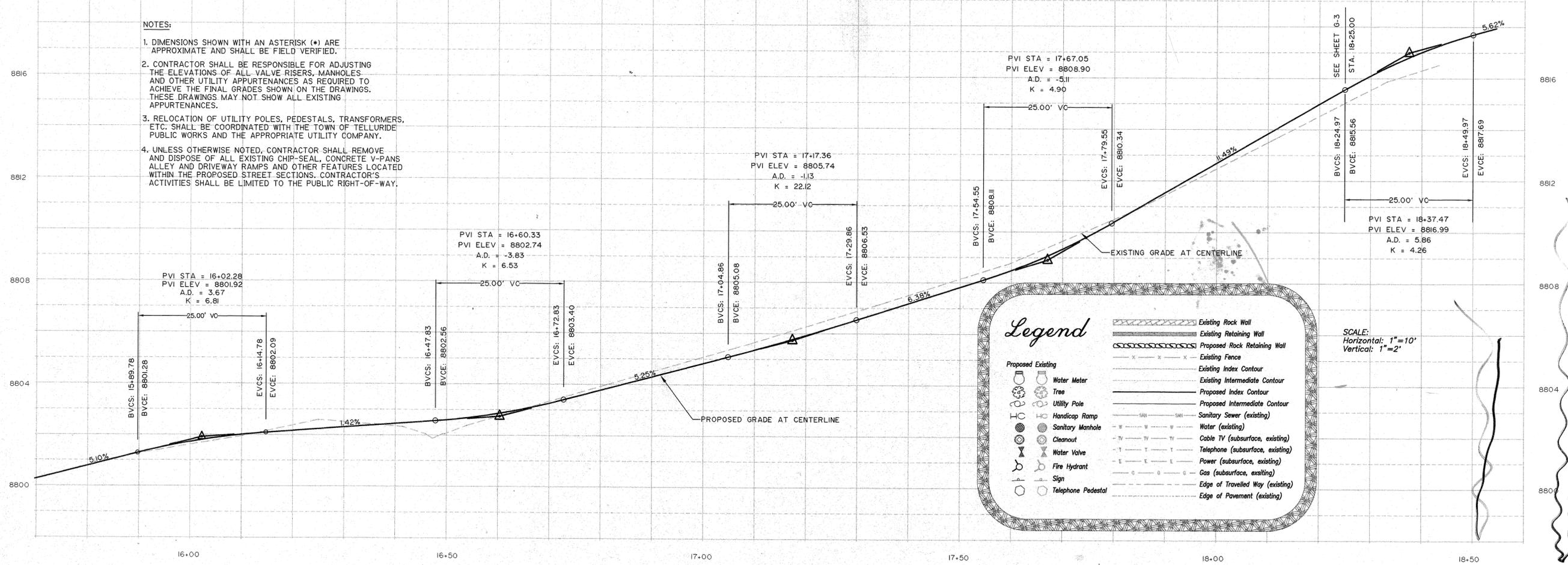
Contact:
 William F. ...
 300 Black Bear Road
 Telluride, Colorado, 81435
 Phone: 970-728-2177
 e-mail: ...

Project Mgr: JG
 Drawn by: M/VJS
 Checked by:
 Start date: 7-23-97
 Dwg path: STREETS\PINE
 Sheet No. **P-2**
 Project #: 96122

Time: 14:53:60
 Date: 4/10/1999
 Scale: 1"=10'
 Drawing File: C:\STREETS\PINE.DWG (Red)
 Xref: LEGEND, PINEHATC



- NOTES:**
- DIMENSIONS SHOWN WITH AN ASTERISK (*) ARE APPROXIMATE AND SHALL BE FIELD VERIFIED.
 - CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING THE ELEVATIONS OF ALL VALVE RISERS, MANHOLES AND OTHER UTILITY APPURTENANCES AS REQUIRED TO ACHIEVE THE FINAL GRADES SHOWN ON THE DRAWINGS. THESE DRAWINGS MAY NOT SHOW ALL EXISTING APPURTENANCES.
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Legend

	Existing Rock Wall
	Existing Retaining Wall
	Proposed Rock Retaining Wall
	Existing Fence
	Existing Index Contour
	Existing Intermediate Contour
	Proposed Index Contour
	Proposed Intermediate Contour
	Sanitary Sewer (existing)
	Water (existing)
	Cable TV (subsurface, existing)
	Telephone (subsurface, existing)
	Power (subsurface, existing)
	Gas (subsurface, existing)
	Edge of Travelled Way (existing)
	Edge of Pavement (existing)

Proposed Existing

	Water Meter
	Tree
	Utility Pole
	Handicap Ramp
	Sanitary Manhole
	Cleanout
	Water Valve
	Fire Hydrant
	Sign
	Telephone Pedestal

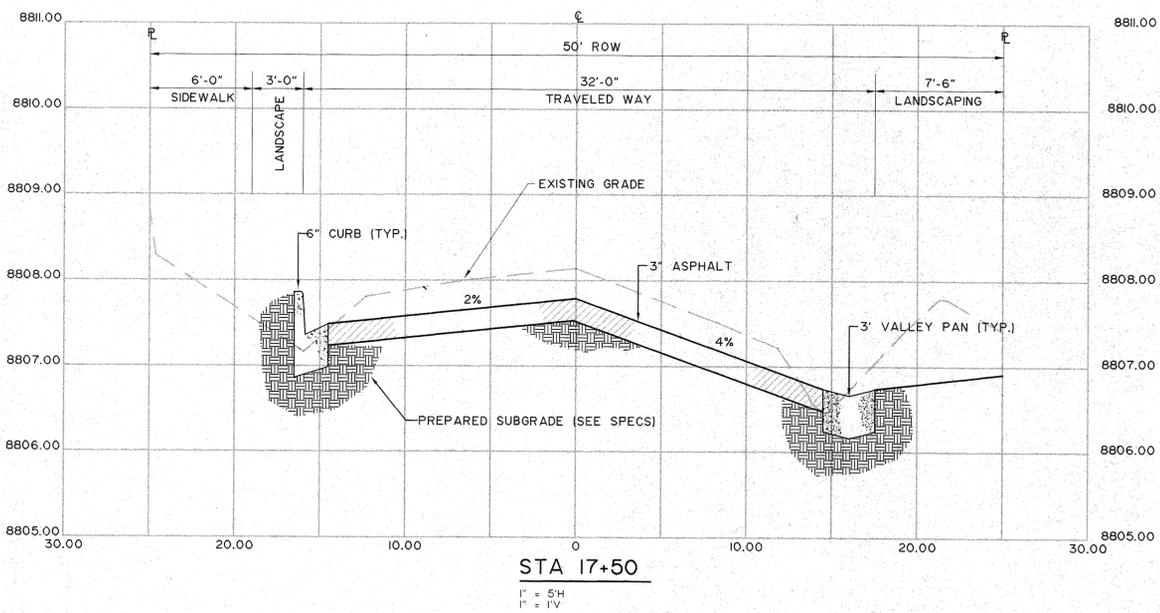
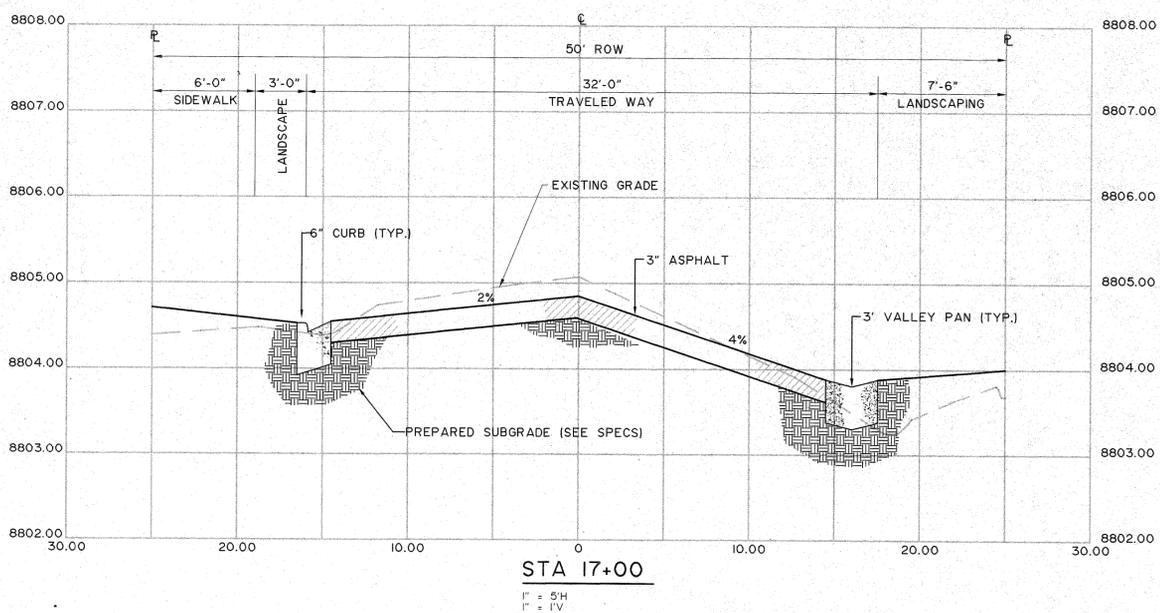
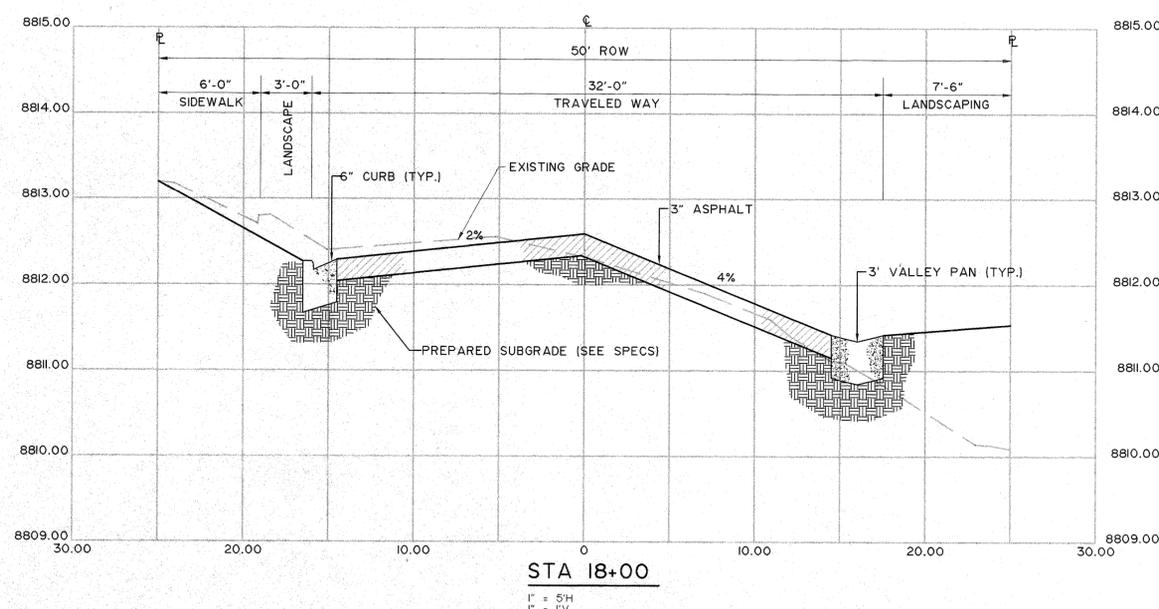
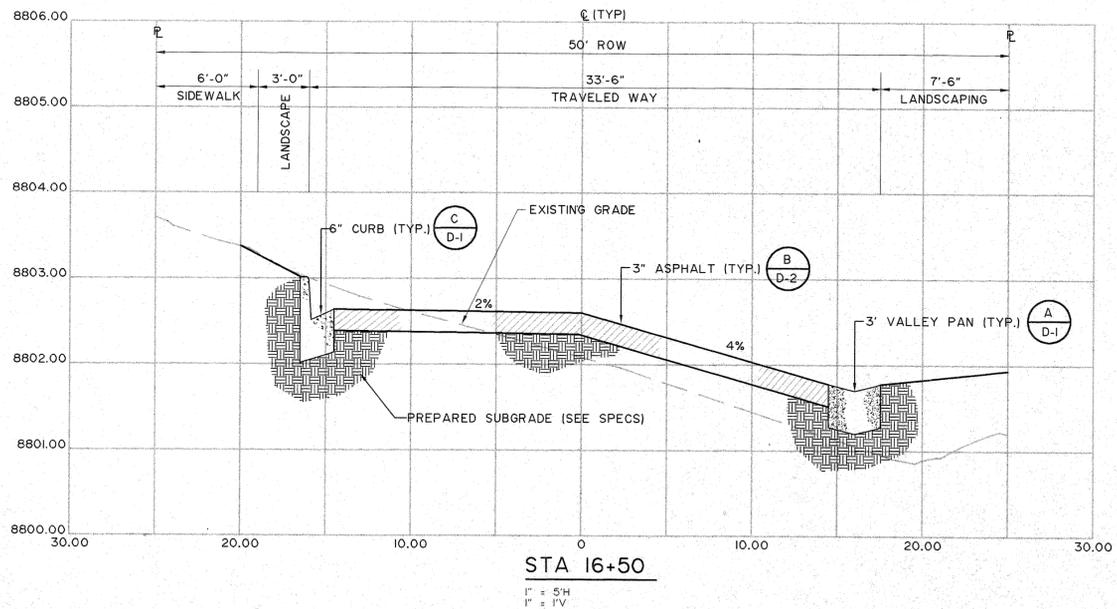
SCALE:
 Horizontal: 1"=10'
 Vertical: 1"=2'

TELLURIDE STREETSCAPES
 PLAN AND PROFILE
 NORTH PINE STREET - STA 15+75 TO STA 18+25

FOLEY ASSOCIATES, INC.
 ENGINEERING - PLANNING - SURVEYING
 PO Box 1385 125 W. Pacific Ave Ste. B-1
 Telluride, Colorado, 81435
 970-728-6153 fax 970-728-6950
 e-mail to staff@foleyassoc.com

Client: Town of Telluride
 Project: 18-037
 Telluride, Colorado, 81435
 Contact: William Frowler or Rob Bishop
 300 Blair Road
 Telluride, Colorado, 81435
 Phone: 970-728-6153
 Fax: 970-728-2177
 e-mail: wfrowler@telluride.com

Project Mgr: JG
 Drawn by: MJK/S
 Checked by:
 Start date: 7-23-97
 Dwg path: STREETS\PINE
 Sheet No.
P-3
 Project #: 96122



Time: 14:53:60
 Date: 4/10/1998
 Scale: 1" = 5'(PS)
 View: X-SECT-3
 Drawing File: STREETS\SPINE.DWG (Red)
 Xrefs: LEGEND; PNEUMATIC

TELLURIDE STREETS CAPES
 CROSS SECTIONS
 NORTH PINE STREET - STA 16+50 TO STA 18+00

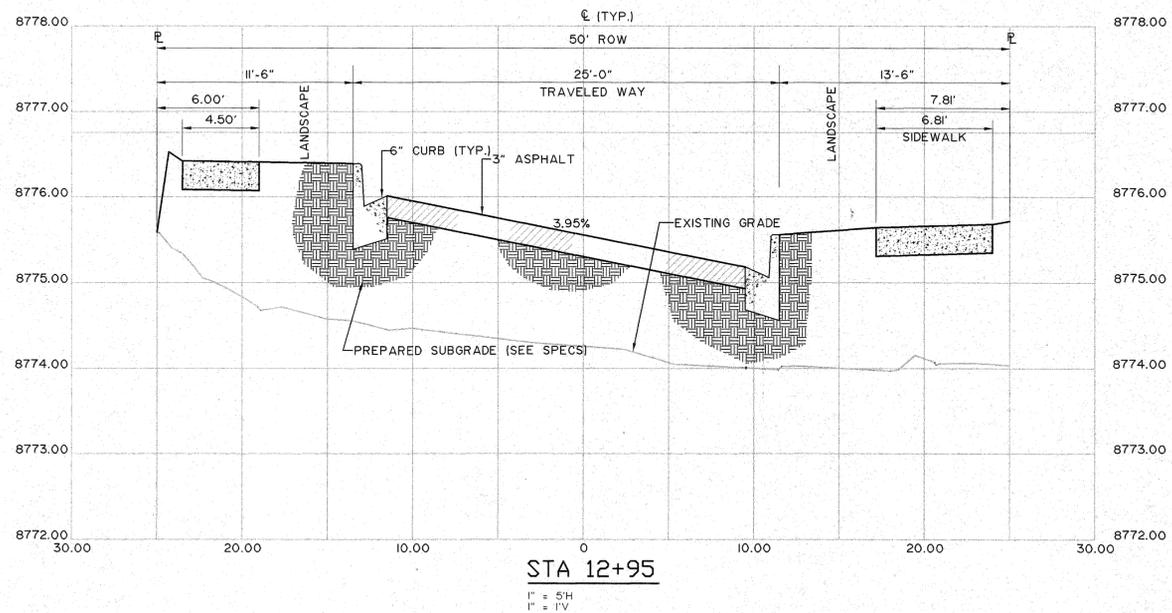
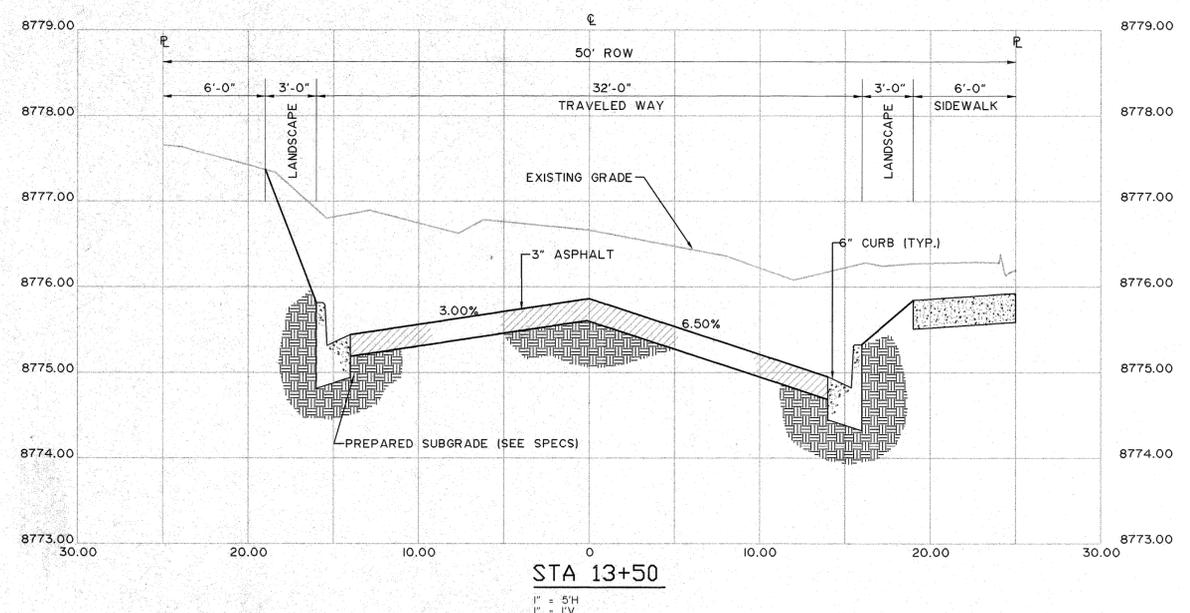
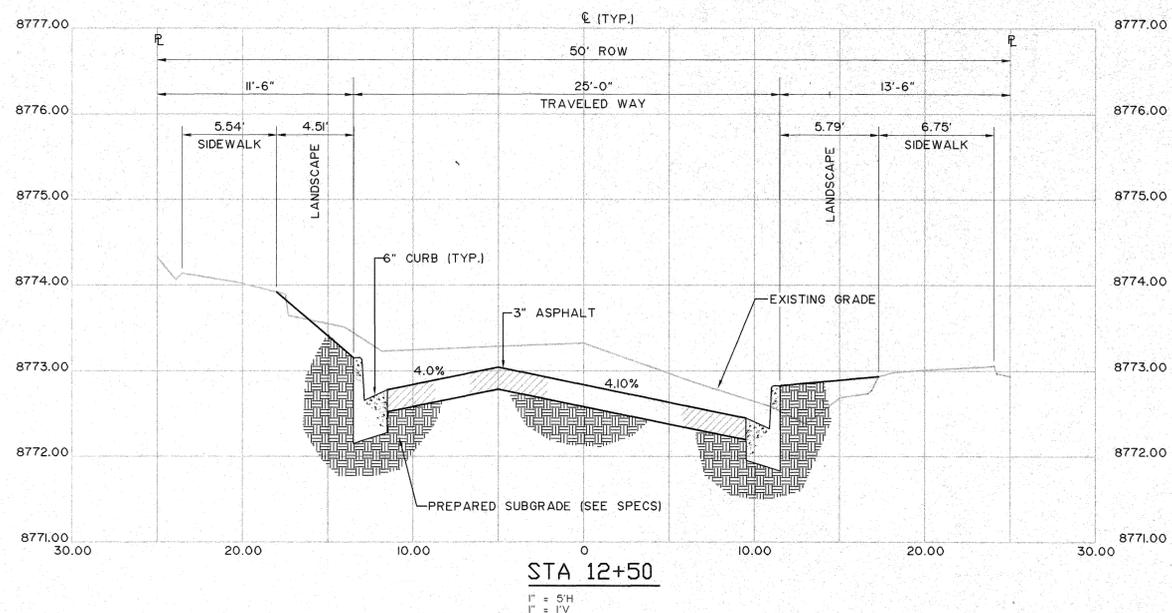
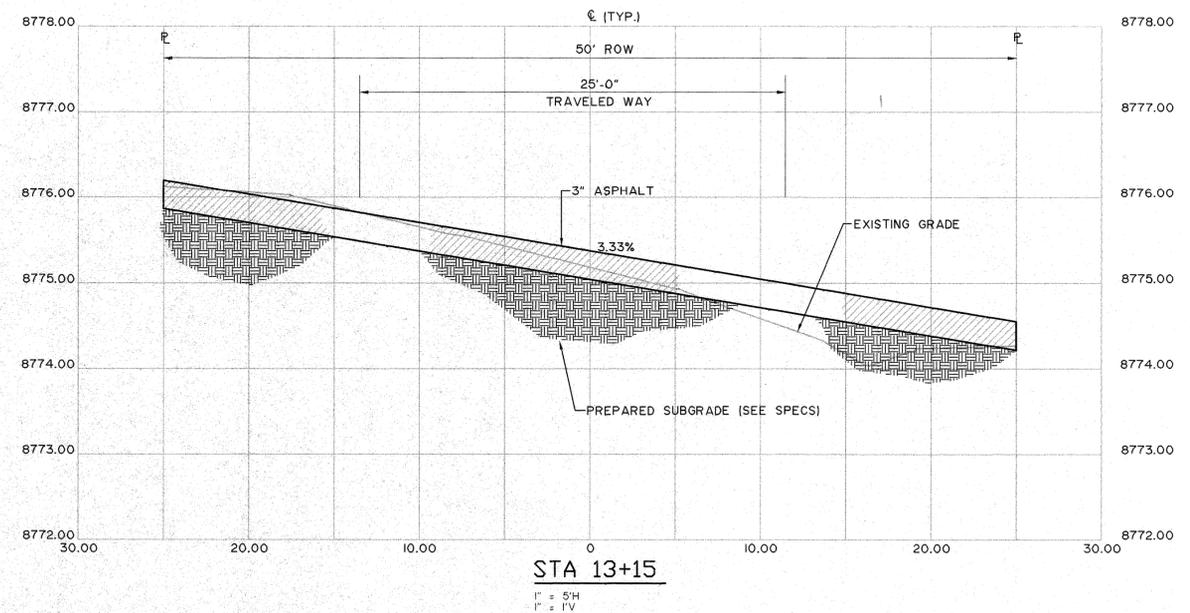
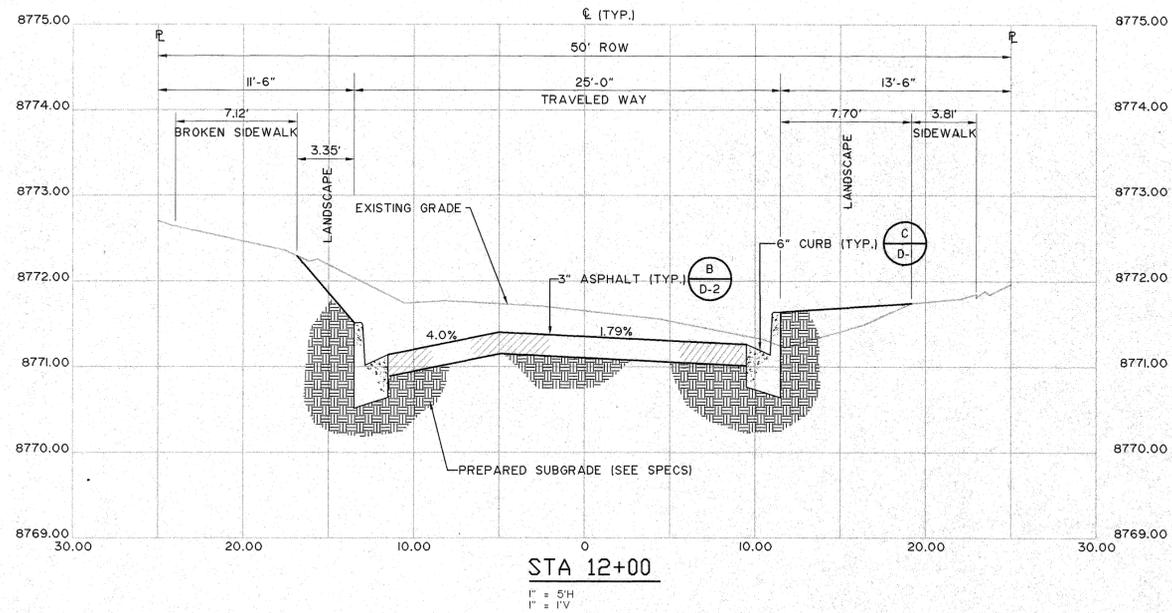
Client:
 Town of Telluride
 PO Box 397
 Telluride
 Colorado, 81435
 Phone: 970-728-6153
 Fax: 970-728-6548
 e-mail:

Contact:
 William Frowlfeiler or Rob Bishop
 300 Black Bear Road
 Telluride
 Colorado, 81435
 Phone: 970-728-3177
 Fax: 970-728-6548
 e-mail:

FOLEY ASSOCIATES, INC.
 ENGINEERING • PLANNING • SURVEYING
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 Telluride, Colorado, 81435
 970-728-6153 Fax: 970-728-0660
 e-mail to xmail@foleyassoc.com

Engineer's stamp and signature

by	
date	
description	
rev	
Project Mgr:	JG
Drawn by:	MA/KJS
Checked by:	
Start date:	7-23-97
Dwg path:	STREETS\SPINE
Sheet No.	P-6
Project #:	96122



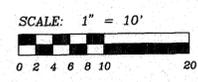
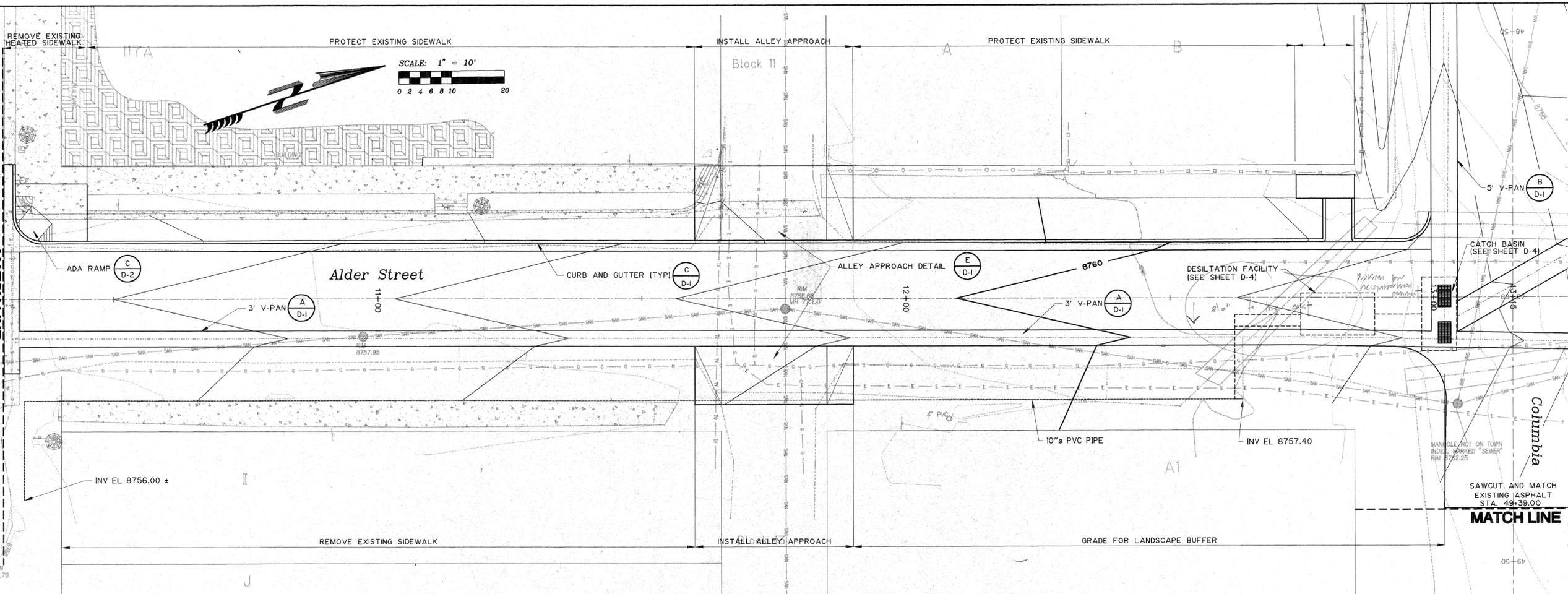
TELLURIDE STREETSCAPES
 CROSS SECTIONS
 NORTH SPRUCE STREET - STA 12+00 TO STA 12+90

Client: Town of Telluride
 PO Box 397
 Telluride, Colorado, 81435
 Contact: William Frowmeyer or Rob Bishop
 300 Back Bear Road
 Telluride, Colorado, 81435
 Phone: 970-728-2177
 Fax: 970-728-0548
 e-mail:

FOLEY ASSOCIATES, INC.
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 PO Box 1886 125 W. Pacific Ave. Ste. B-1
 Telluride, Colorado, 81435
 970-728-6133 fax 970-728-6050
 e-mail to small@foleyassoc.com

Engineer's stamp and signature
 by _____
 date _____
 description _____
 Rev. _____
 Project Mgr: JG
 Drawn by: MLJ/BJH
 Checked by:
 Start date: 7/11/97
 Dwg path: 96122\SPRUCE2
 Sheet No.
S-3
 Project #: 96122

SEE SHEET A-5

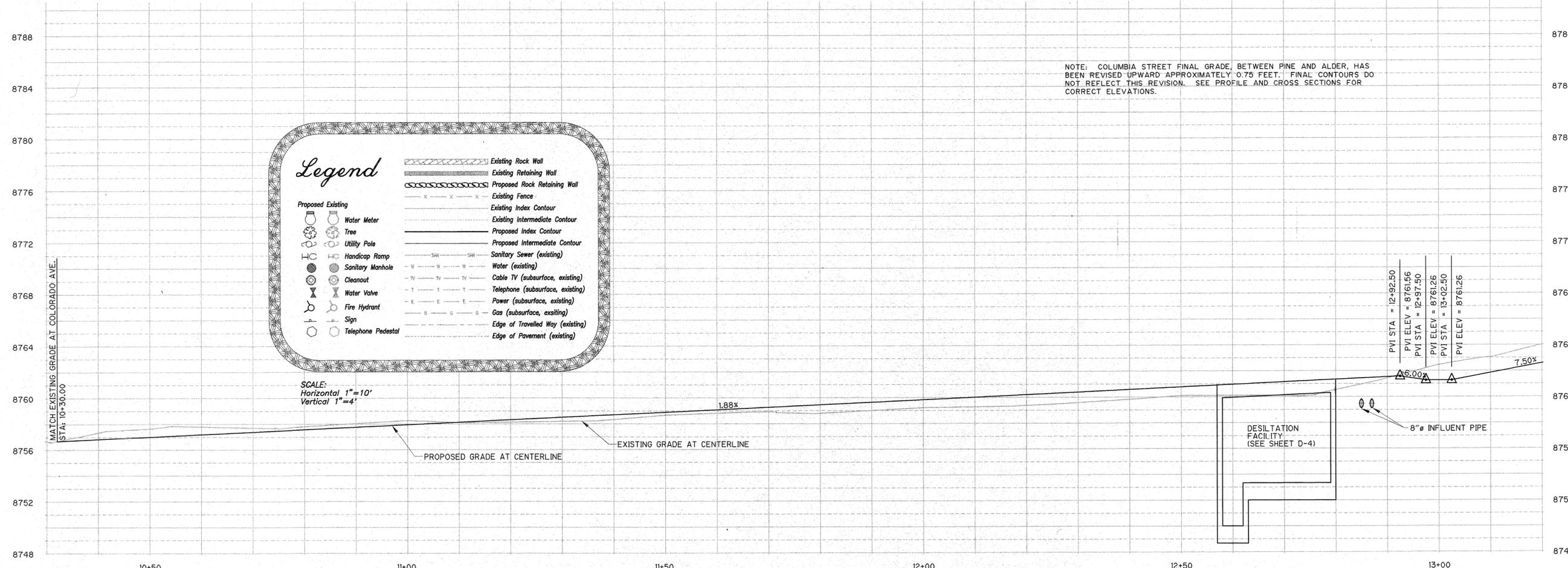


Alder Street

Block 11

Columbia

MATCH LINE



Legend

	Existing Rock Wall
	Existing Retaining Wall
	Proposed Rock Retaining Wall
	Existing Fence
	Existing Index Contour
	Existing Intermediate Contour
	Proposed Index Contour
	Proposed Intermediate Contour
	Sanitary Sewer (existing)
	Water (existing)
	Cable TV (subsurface, existing)
	Telephone (subsurface, existing)
	Power (subsurface, existing)
	Gas (subsurface, existing)
	Edge of Travelled Way (existing)
	Edge of Pavement (existing)

Proposed Existing

	Water Meter
	Tree
	Utility Pole
	Handicap Ramp
	Sanitary Manhole
	Cleanout
	Water Valve
	Fire Hydrant
	Sign
	Telephone Pedestal

SCALE:
Horizontal 1" = 10'
Vertical 1" = 4'

PVI STA = 12+92.50	PVI ELEV = 8761.56
PVI STA = 12+97.50	PVI ELEV = 8761.26
PVI STA = 13+02.50	PVI ELEV = 8761.26

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PO Box 1885 128 W. Pacific Ave Ste. B-1
Telluride, Colorado, 81435
970-728-6188 fax 970-728-6050
e-mail to xmaff@foleyassoc.com

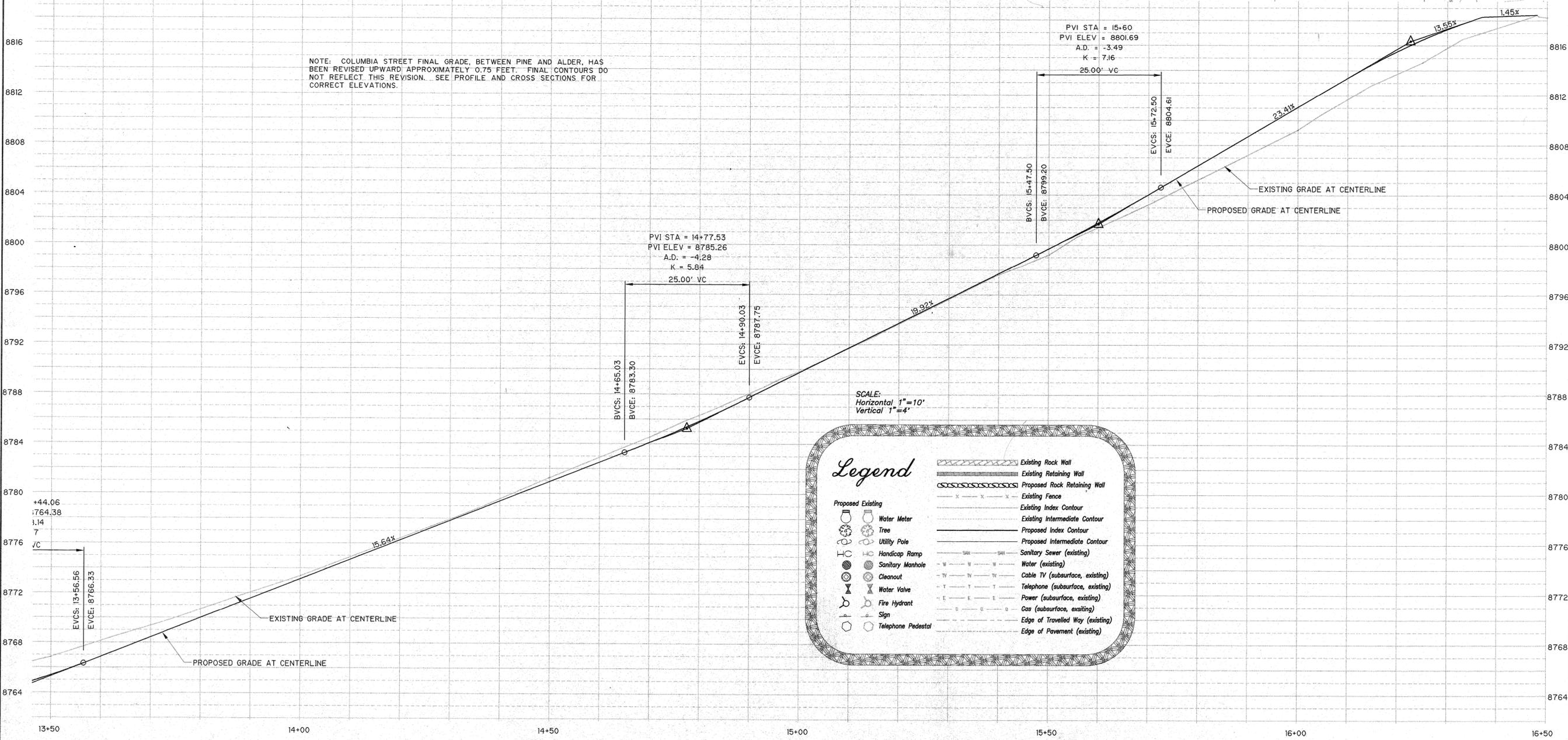
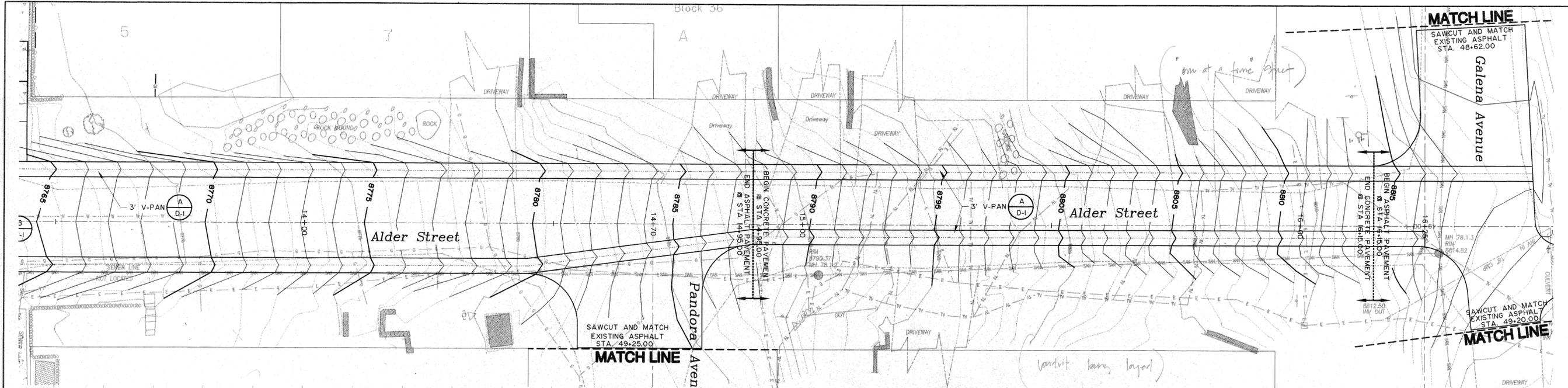
Telluride Streetscapes
PLAN AND PROFILE
ALDER STREET STA 9+55 TO STA 12+00

Contact:
William Foy
300 Black Bear Rd.
Telluride
Colorado, 81435
Phone: 970-728-3177
e-mail:
Client:
Town of Telluride
PO Box 397
Telluride
CO 81435
Phone: 970-728-3071
e-mail:

Engineer's stamp and signature

Rev.	date	description

Project Mgr: JG
Drawn by: MJ/KJS
Checked by: JG
Start date: 10/22/97
Dwg path: 96122\ALDER2
Sheet No.
A-1
Project #: 96122



Legend

	Existing Rock Wall
	Existing Retaining Wall
	Proposed Rock Retaining Wall
	Existing Fence
	Existing Index Contour
	Existing Intermediate Contour
	Proposed Index Contour
	Proposed Intermediate Contour
	Sanitary Sewer (existing)
	Water (existing)
	Cable TV (subsurface, existing)
	Telephone (subsurface, existing)
	Power (subsurface, existing)
	Gas (subsurface, existing)
	Edge of Travelled Way (existing)
	Edge of Pavement (existing)

	Proposed Existing Water Meter
	Proposed Existing Tree
	Proposed Existing Utility Pole
	Proposed Existing Handicap Ramp
	Proposed Existing Sanitary Manhole
	Proposed Existing Cleanout
	Proposed Existing Water Valve
	Proposed Existing Fire Hydrant
	Proposed Existing Sign
	Proposed Existing Telephone Pedestal

TELLURIDE STREETSCAPES - ALDER STREET
 PLAN & PROFILE - STA 13+00 TO STA 16+25
 ALDER STREET STA 12+00 TO STA 14+95

Client: Tom Frowmiller
 1000 S. Main St.
 Telluride, CO 81435

Contact: William Frowmiller or Rob Bishop
 300 S. Main St.
 Telluride, CO 81435

Phone: 970-728-3177
 Fax: 970-728-0546

Project Mgr: JG
 Drawn by: MJ/KJS
 Checked by: JG
 Start date: 10/22/97
 Dwg path: 96122\ALDER2
 Sheet No. **A-2**
 Project #: 96122

TELLURIDE STREETSCAPES - ALDER STREET
 CROSS SECTIONS - STA 10+50 TO STA 13+00

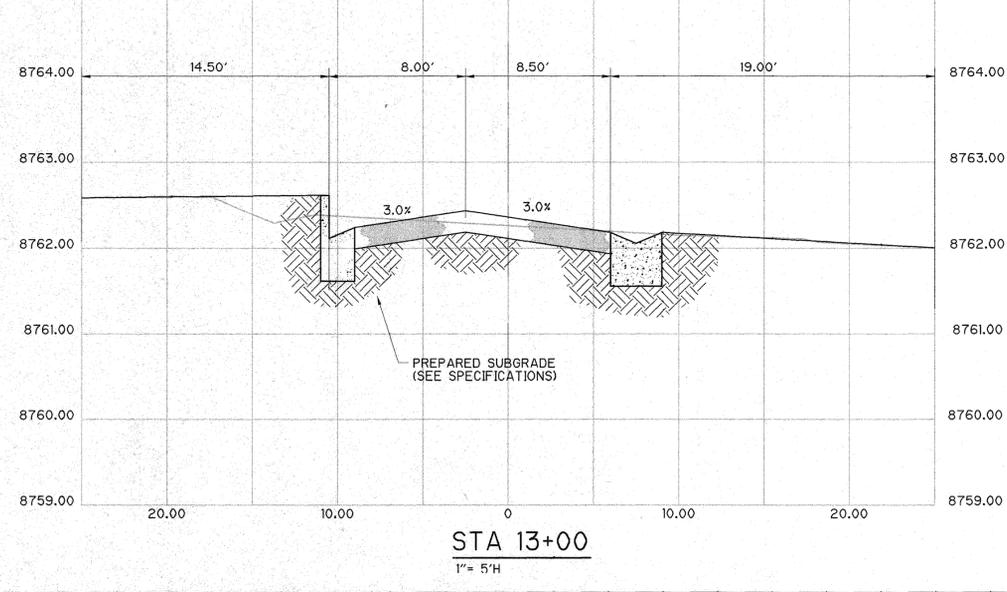
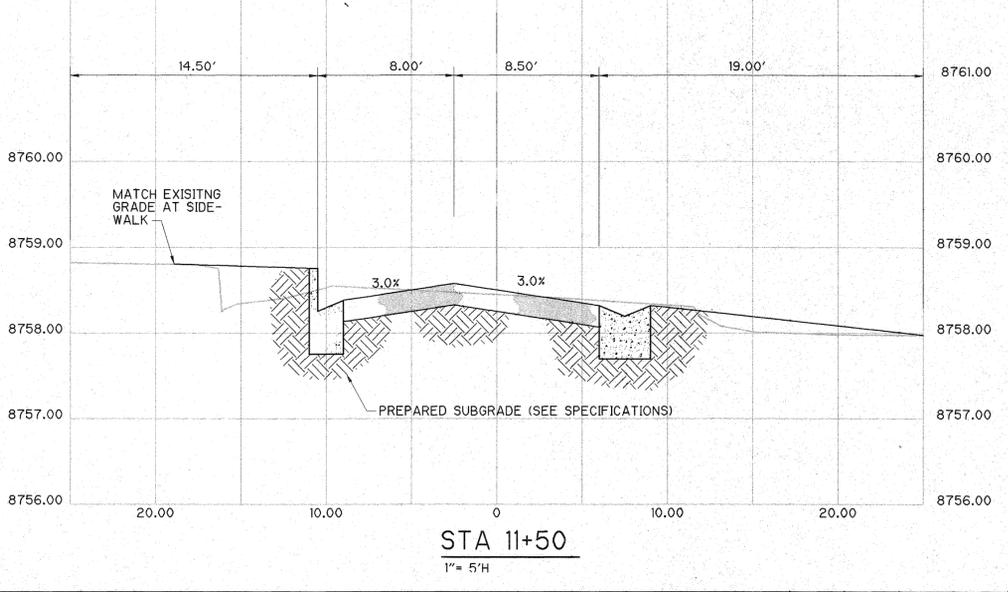
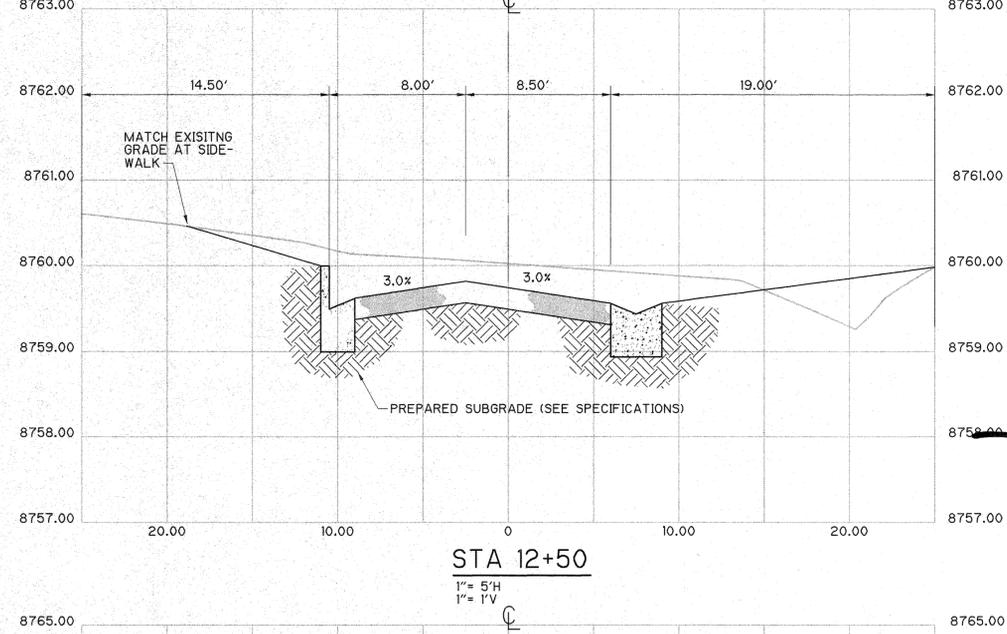
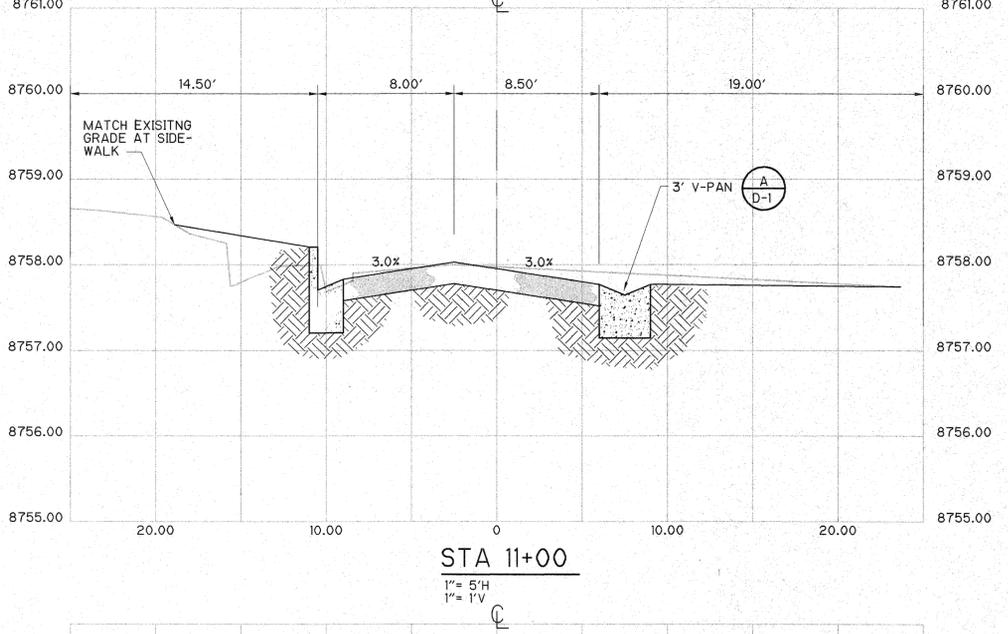
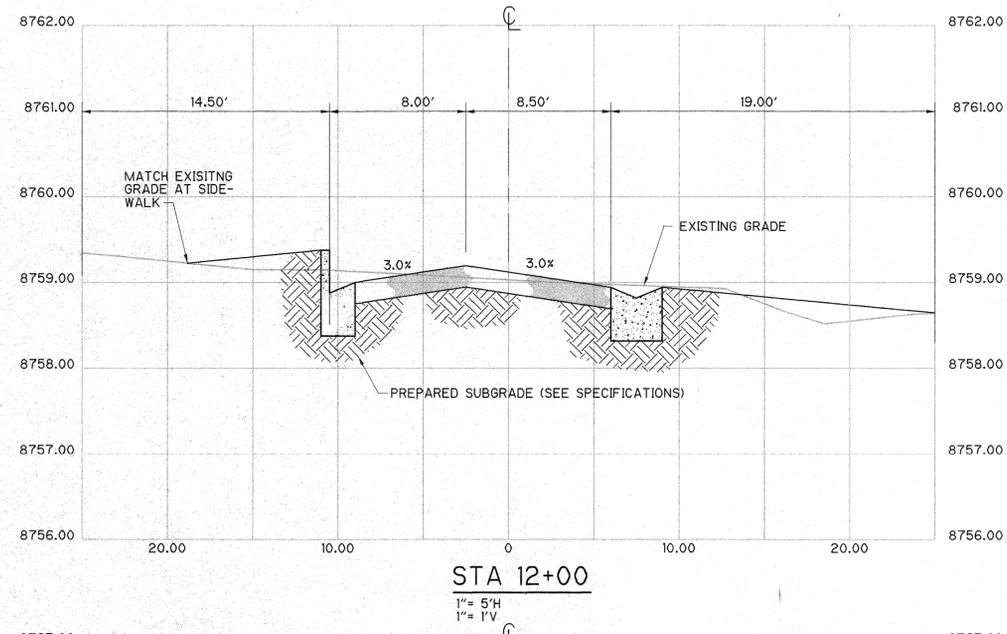
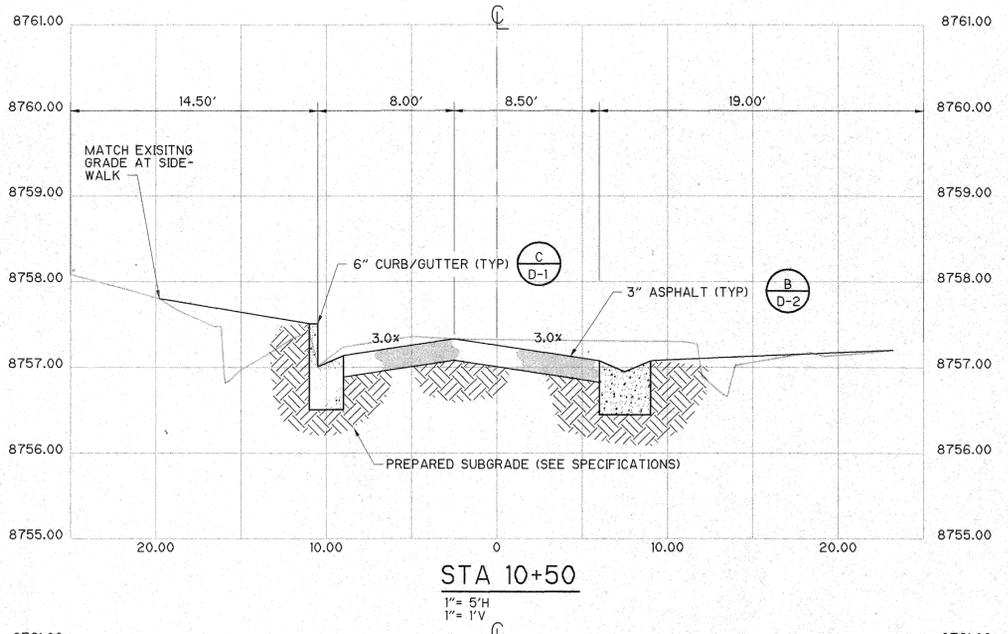
Contact: William Frommelter or Rob Bishop
 300 Black Bear Rd.
 Telluride, Colorado, 81435
 Phone: 970-728-3177
 Fax: 970-728-0546
 e-mail: william@foleyassoc.com

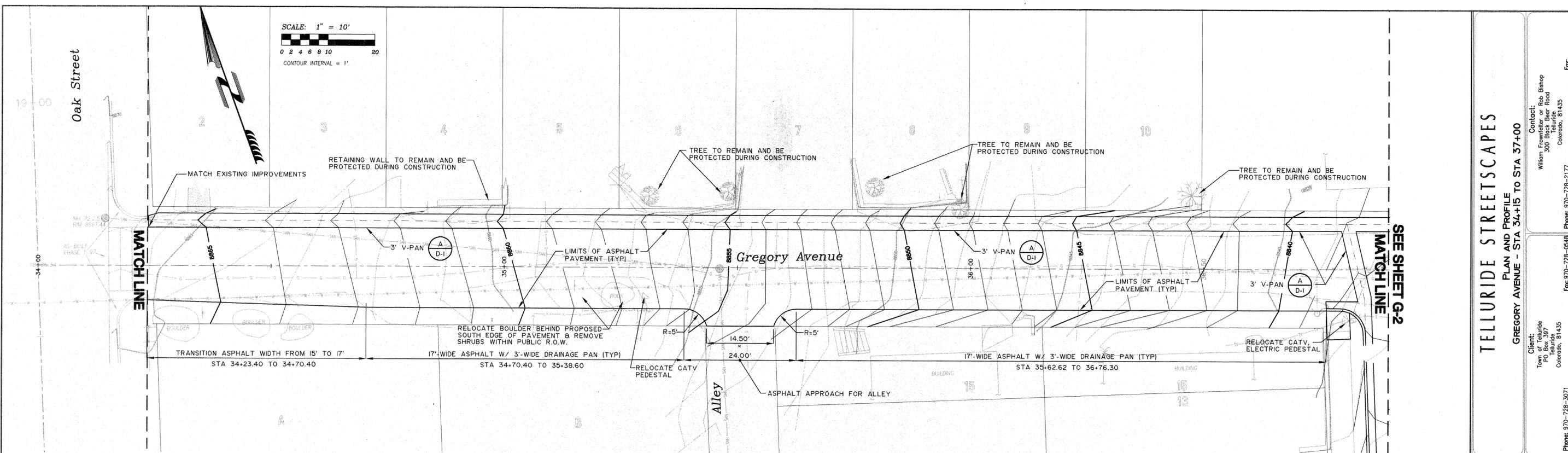
FOLEY ASSOCIATES, INC.
 ENGINEERING • PLANNING • SURVEYING
 PO Box 1885 125 W. Pacific Ave. Ste. B-1
 Telluride, Colorado, 81435
 970-728-6188 fax 970-728-0600
 e-mail to annual@foleyassoc.com

Engineer's stamp and signature

Rev.	date	by	description

Project Mgr: JG
 Drawn by: MJ/KJS
 Checked by: JG
 Start date: 10/22/97
 Dwg path: 96122\ALDER2
 Sheet No. **A-3**
 Project #: 96122

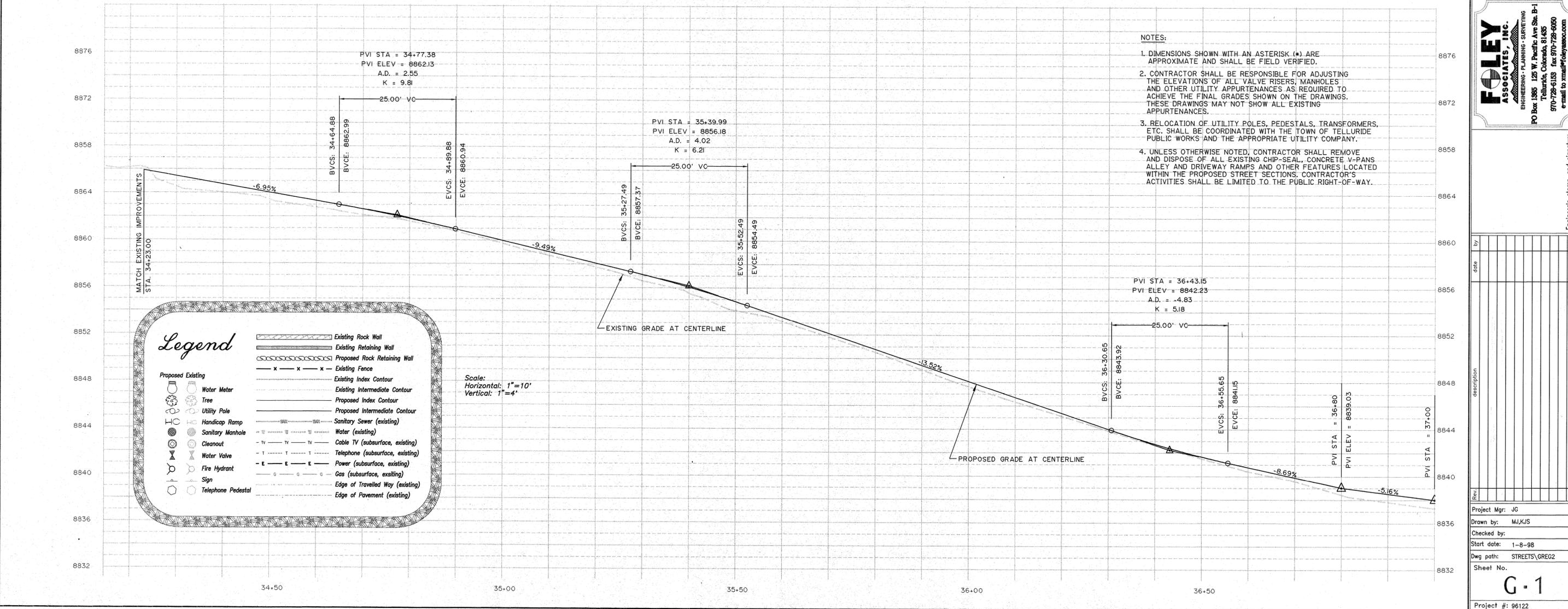




SCALE: 1" = 10'
 0 2 4 6 8 10 20
 CONTOUR INTERVAL = 1'

MATCH LINE

SEE SHEET G-2



Legend

	Existing Rock Wall
	Existing Retaining Wall
	Proposed Rock Retaining Wall
	Existing Fence
	Existing Index Contour
	Existing Intermediate Contour
	Proposed Index Contour
	Proposed Intermediate Contour
	Sanitary Sewer (existing)
	Water (existing)
	Cable TV (subsurface, existing)
	Telephone (subsurface, existing)
	Power (subsurface, existing)
	Gas (subsurface, existing)
	Edge of Travelled Way (existing)
	Edge of Pavement (existing)
	Proposed Existing Water Meter
	Proposed Existing Tree
	Proposed Existing Utility Pole
	Proposed Existing Handicap Ramp
	Proposed Existing Sanitary Manhole
	Proposed Existing Cleanout
	Proposed Existing Water Valve
	Proposed Existing Fire Hydrant
	Proposed Existing Sign
	Proposed Existing Telephone Pedestal

Scale:
 Horizontal: 1" = 10'
 Vertical: 1" = 4'

- NOTES:**
- DIMENSIONS SHOWN WITH AN ASTERISK (*) ARE APPROXIMATE AND SHALL BE FIELD VERIFIED.
 - CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING THE ELEVATIONS OF ALL VALVE RISERS, MANHOLES AND OTHER UTILITY APPURTENANCES AS REQUIRED TO ACHIEVE THE FINAL GRADES SHOWN ON THE DRAWINGS. THESE DRAWINGS MAY NOT SHOW ALL EXISTING APPURTENANCES.
 - RELOCATION OF UTILITY POLES, PEDESTALS, TRANSFORMERS, ETC. SHALL BE COORDINATED WITH THE TOWN OF TELLURIDE PUBLIC WORKS AND THE APPROPRIATE UTILITY COMPANY.
 - UNLESS OTHERWISE NOTED, CONTRACTOR SHALL REMOVE AND DISPOSE OF ALL EXISTING CHIP-SEAL, CONCRETE V-PANS ALLEY AND DRIVEWAY RAMPS AND OTHER FEATURES LOCATED WITHIN THE PROPOSED STREET SECTIONS. CONTRACTOR'S ACTIVITIES SHALL BE LIMITED TO THE PUBLIC RIGHT-OF-WAY.

FOLEY ASSOCIATES, INC.
 ENGINEERING - PLANNING - SURVEYING
 PO Box 186 125 W. Pacific Ave Ste. B-1
 Telluride, Colorado, 81455
 970-728-6183 fax 970-728-6000
 e-mail to mf@foleyass.com

Engineer's stamp and signature

Project Mgr:	JG
Drawn by:	MJKJS
Checked by:	
Start date:	1-8-98
Dwg path:	STREETS\GREG2
Sheet No.	G-1
Project #:	96122

TELLURIDE STREETSCAPES
 PLAN AND PROFILE
 GREGORY AVENUE - STA 34+15 TO STA 37+00

Contact:
 William Foley, P.E. Bishop
 300 Black Bear Road
 Telluride
 Colorado, 81435
 Phone: 970-728-2177
 Fax: 970-728-0548
 e-mail:

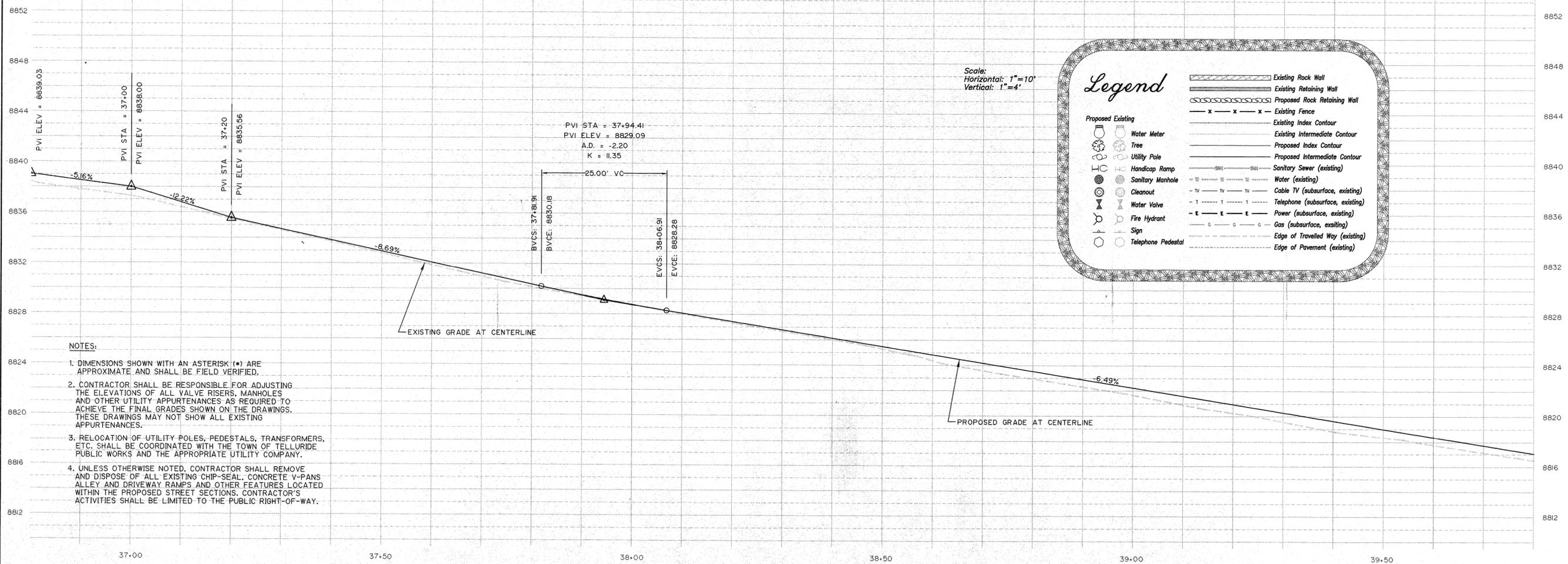
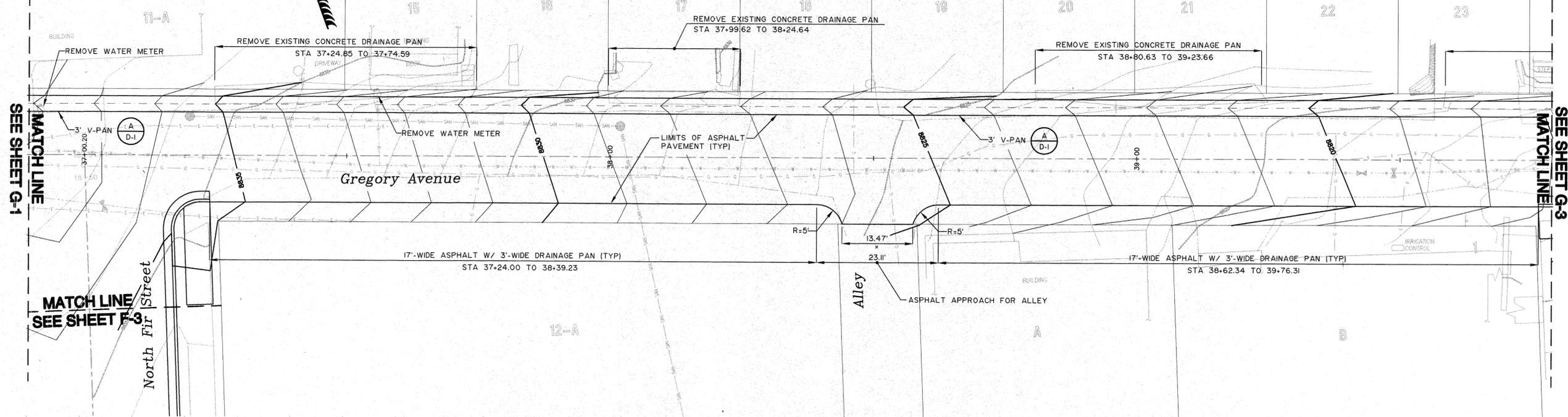
Drawn: 1/27/98
 Checked: 1/27/98
 Date: 1/27/98
 Drawn by: MJKJS
 Checked by: MJKJS
 Scale: 1" = 10'
 Date: 1/27/98

SCALE: 1" = 10'
 0 2 4 6 8 10 20
 CONTOUR INTERVAL = 1'



SEE SHEET G-1

SEE SHEET G-3



- NOTES:
1. DIMENSIONS SHOWN WITH AN ASTERISK (*) ARE APPROXIMATE AND SHALL BE FIELD VERIFIED.
 2. CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING THE ELEVATIONS OF ALL VALVE RISERS, MANHOLES AND OTHER UTILITY APPURTENANCES AS REQUIRED TO ACHIEVE THE FINAL GRADES SHOWN ON THE DRAWINGS. THESE DRAWINGS MAY NOT SHOW ALL EXISTING APPURTENANCES.
 3. RELOCATION OF UTILITY POLES, PEDESTALS, TRANSFORMERS, ETC. SHALL BE COORDINATED WITH THE TOWN OF TELLURIDE PUBLIC WORKS AND THE APPROPRIATE UTILITY COMPANY.
 4. UNLESS OTHERWISE NOTED, CONTRACTOR SHALL REMOVE AND DISPOSE OF ALL EXISTING CHIP-SEAL, CONCRETE V-PANS ALLEY AND DRIVEWAY RAMPS AND OTHER FEATURES LOCATED WITHIN THE PROPOSED STREET SECTIONS. CONTRACTOR'S ACTIVITIES SHALL BE LIMITED TO THE PUBLIC RIGHT-OF-WAY.

Scale:
 Horizontal: 1"=10'
 Vertical: 1"=4'

Legend

	Existing Rock Wall
	Existing Retaining Wall
	Proposed Rock Retaining Wall
	Existing Fence
	Existing Index Contour
	Existing Intermediate Contour
	Proposed Index Contour
	Proposed Intermediate Contour
	Sanitary Sewer (existing)
	Water (existing)
	Cable TV (subsurface, existing)
	Telephone (subsurface, existing)
	Power (subsurface, existing)
	Gas (subsurface, existing)
	Edge of Travelled Way (existing)
	Edge of Pavement (existing)

TELLURIDE STREETSCAPES
 PLAN AND PROFILE
 GREGORY AVENUE - STA 37+00 TO STA 39+80

Client: Tom Frowmeyer or Rob Bishop
 300 Broadway Road
 Telluride, Colorado, 81435

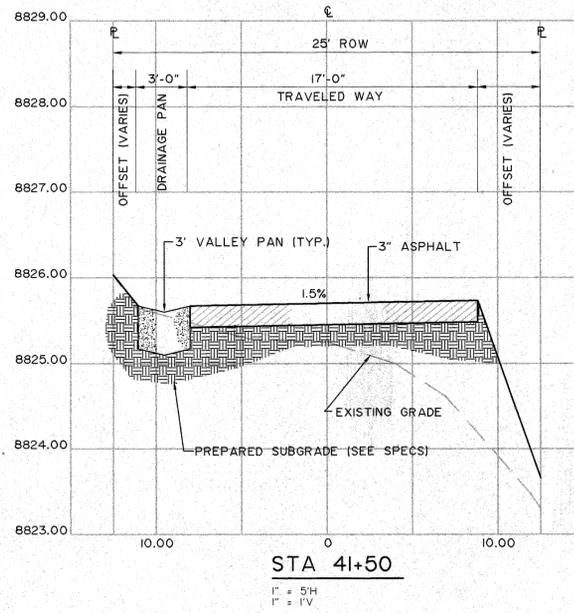
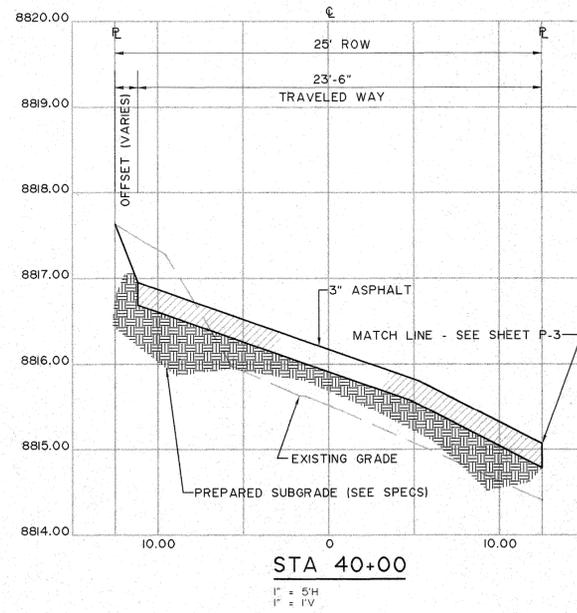
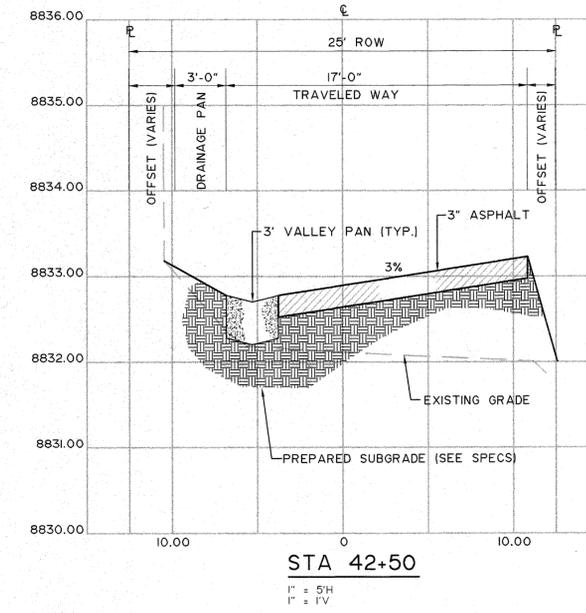
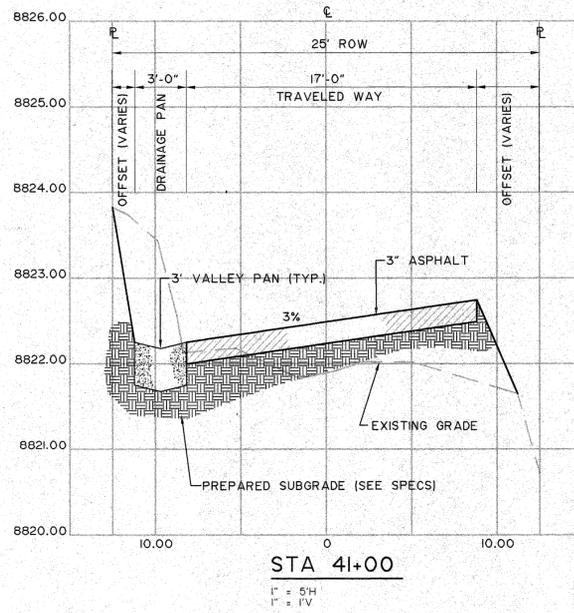
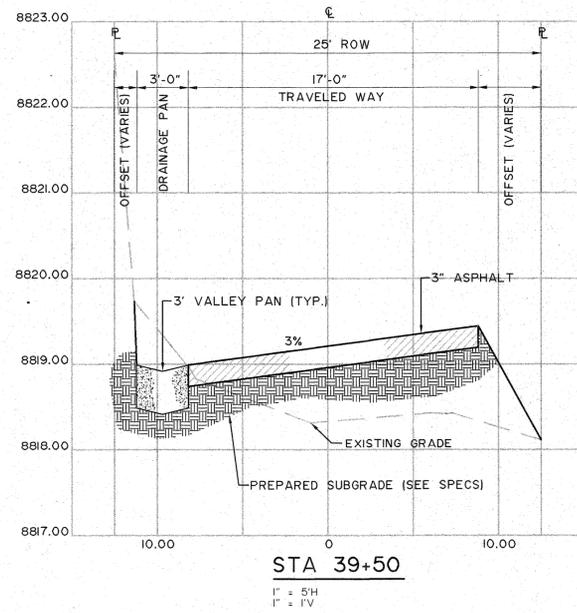
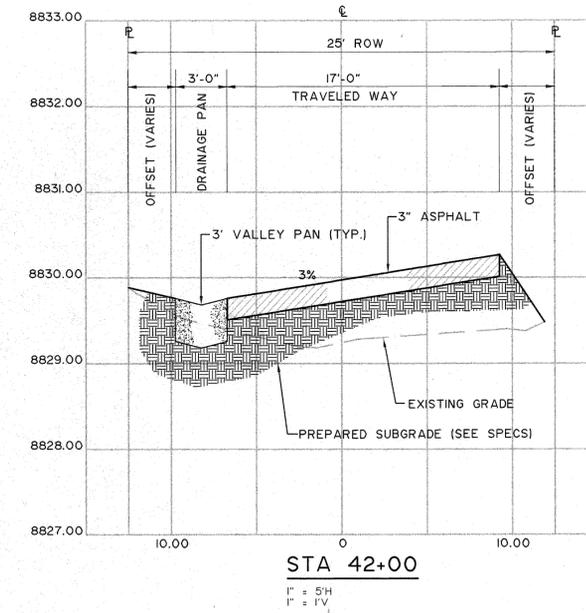
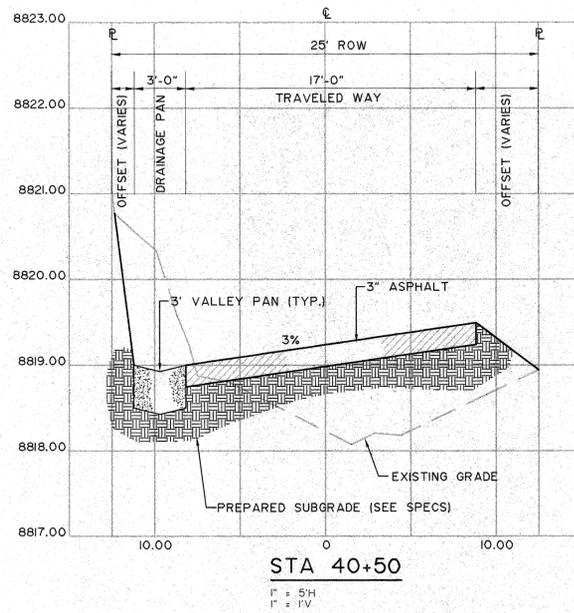
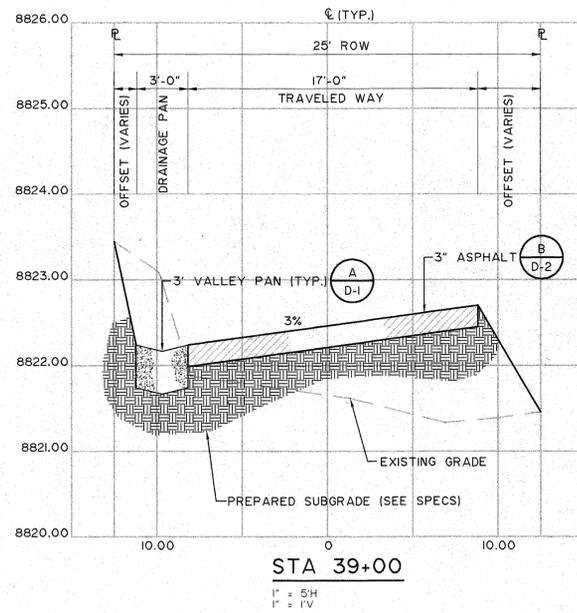
Contact: William Frowmeyer or Rob Bishop
 300 Broadway Road
 Telluride, Colorado, 81435

Phone: 970-728-2177
 Fax: 970-728-0546
 e-mail: wmf@foleyassoc.com

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 Telluride, Colorado, 81435
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 e-mail to: small@foleyassoc.com

Project Mgr: JG
 Drawn by: MJK/S
 Checked by:
 Start date: 1-8-98
 Dwg path: STREETS\GREG2
 Sheet No. **G-2**
 Project #: 96122

Drawn: 1/27/98
 MJK/S
 Checked: 1/27/98
 JG
 Date: 1/27/98
 Title: GREGORY AVENUE

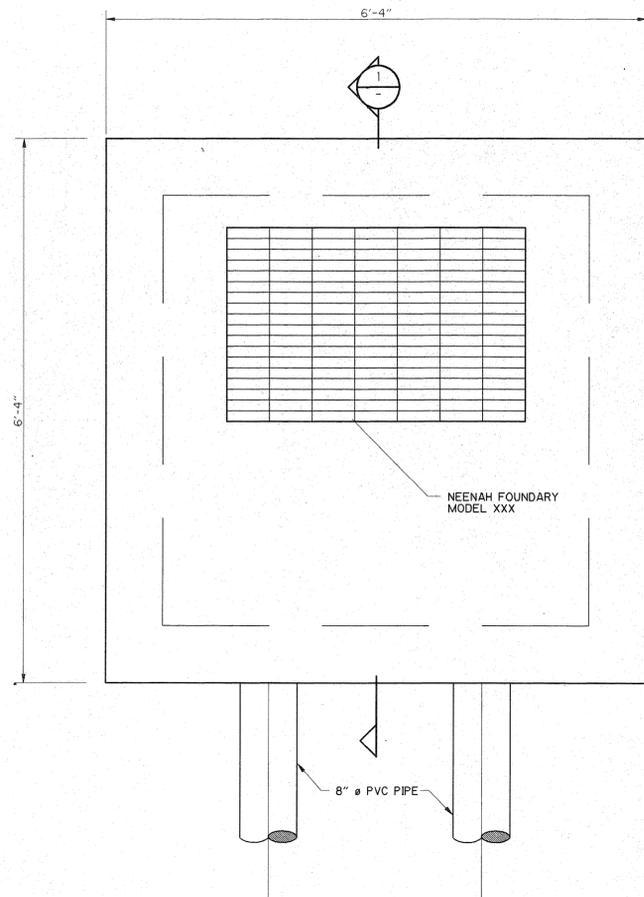


Date: 11/20/06
 Drawn: J.K.J.S.
 Checked: M.J.K.J.S.
 Project: GREGORY AVENUE - STA 39+00 TO STA 43+00

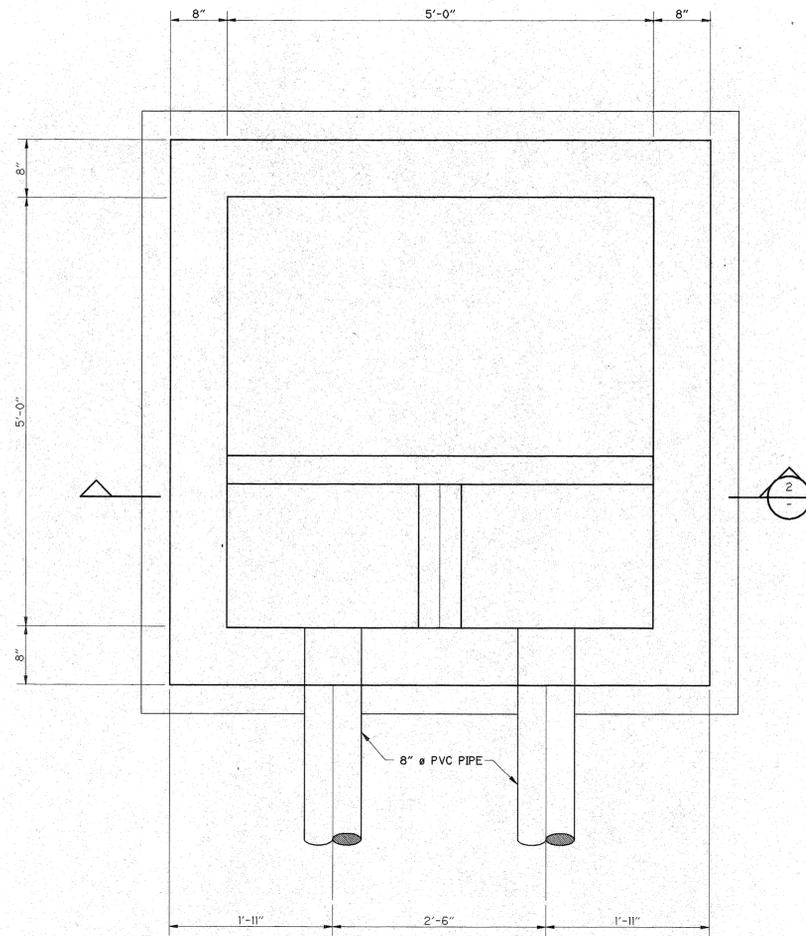
TELLURIDE STREETSCAPES
 CROSS SECTIONS
 GREGORY AVENUE - STA 39+00 TO STA 43+00
 Contact:
 William Frowmiller or Rob Bishop
 300 Big Horn Road
 Telluride, Colorado, 81435
 Phone: 970-728-2177
 Fax: 970-728-0548
 e-mail:

FOLEY ASSOCIATES, INC.
 ENGINEERING - PLANNING - SURVEYING
 PO Box 1885 125 W. Pacific Ave. Ste. B-1
 Telluride, Colorado, 81435
 970-728-6158 fax 970-728-8000
 e-mail:

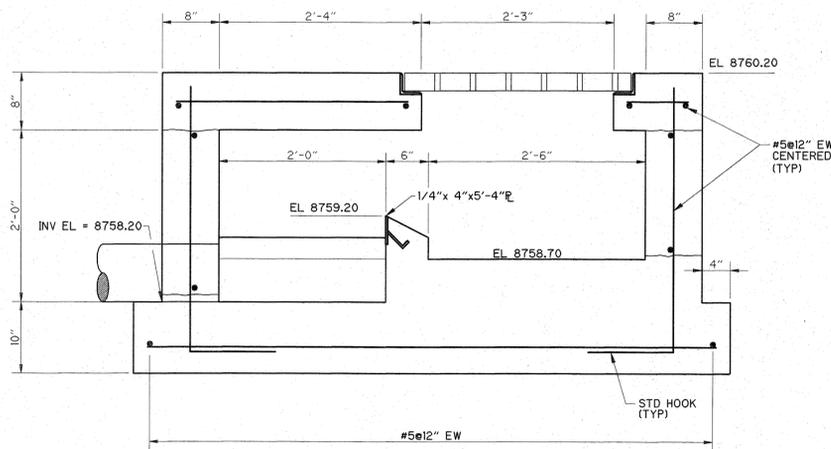
Engineer's stamp and signature
 Project Mgr: JG
 Drawn by: M.J.K.J.S.
 Checked by:
 Start date: 1-8-98
 Dwg path: STREETS\GREG2
 Sheet No.
G-5
 Project #: 96122



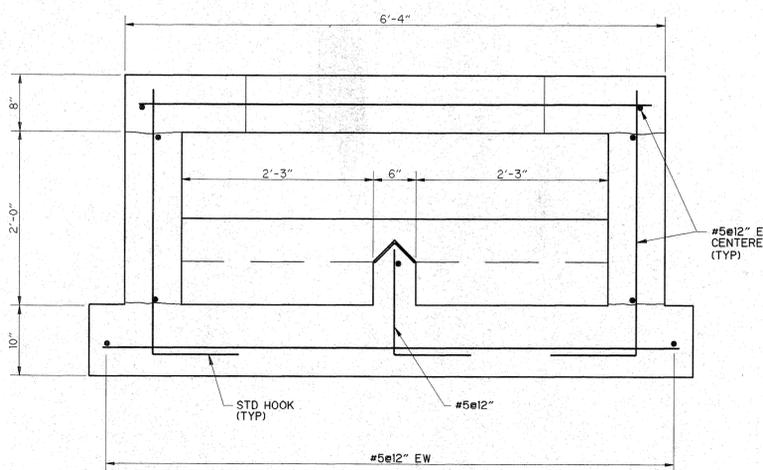
CATCH BASIN
 PLAN AT ELEVATION 8760.20
 1"=1'-0"



CATCH BASIN
 PLAN AT ELEVATION 8759.50
 1"=1'-0"



SECTION 1
 1" = 1'-0"



SECTION 2
 1" = 1'-0"

TELLURIDE STREETSCAPES

PROJECT DETAILS

Contact:
 William Frowntier or Rob Bishop
 300 Black Bear Road
 Colorado, 81435
 Phone: 970-728-2177
 Fax: 970-728-0548
 e-mail: wfrowntier@fley.com

Client:
 Town of Telluride
 PO Box 97
 Telluride, Colorado, 81435
 Phone: 970-728-3071
 Fax: 970-728-3071
 e-mail: info@telluride.com

FLEY ASSOCIATES, INC.
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 PO Box 1385 125 W. Peachy Ave. Ste. B-1
 Telluride, Colorado, 81435
 970-728-3188 fax 970-728-9060
 e-mail to info@fley.com

Engineer's stamp and signature

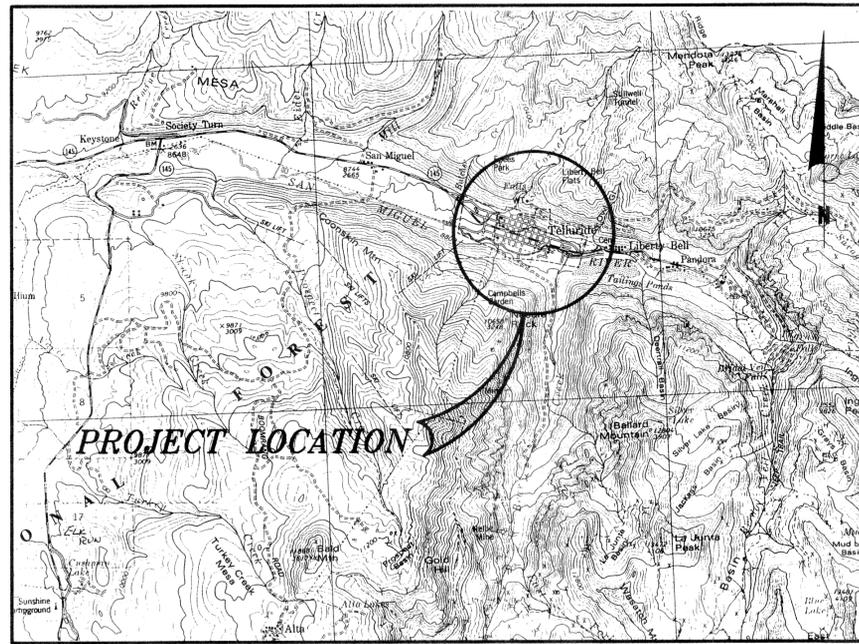
Rev.	description	date	by

Project Mgr: J.GARDNER/K.J.SMITH
 Drawn by: BJH
 Checked by: DF
 Start date: 12-5-98
 Dwg path: 96122\DETAIL98
 Sheet No.
D-3
 Project #: 96122

**TELLURIDE STREETSAPES – PHASE II
STREET IMPROVEMENT CONSTRUCTION PLANS
TOWN OF TELLURIDE
SAN MIGUEL COUNTY, COLORADO
PROJECT # 99102
MARCH 13, 2000**

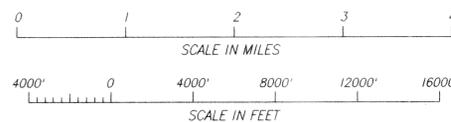
INDEX TO DRAWINGS

- 1 COVER SHEET
- 2 NOTES & LEGEND
- 3 SCHEMATIC TYPICAL STREET CROSS SECTIONS
- 4 SOUTH ASPEN STREET P & P, STA 3+50 TO STA 6+00 (200 BLK)
- 5 SOUTH ASPEN STREET P & P, STA 6+00 TO STA 8+50 (100/200 BLKS)
- 6 SOUTH ASPEN STREET P & P, STA 8+50 TO STA 9+75 (100 BLK)
- 7 NORTH ASPEN STREET P & P, STA 10+00 TO STA 12+00 (100 BLK)
- 8 NORTH ASPEN STREET P & P, STA 12+50 TO STA 15+00 (100/200 BLKS)
- 9 NORTH ASPEN STREET P & P, STA 15+00 TO STA 17+50 (200/300 BLKS)
- 10 NORTH ASPEN STREET P & P, STA 17+50 TO STA 19+50 (300 BLK)
- 11 NORTH ASPEN STREET P & P, STA 0+00 TO STA 2+00 (400 BLK)
- 12 NORTH TOWNSEND STREET P & P, STA 10+00 TO STA 12+50 (100 BLK)
- 13 NORTH TOWNSEND STREET P & P, STA 12+50 TO STA 15+00 (100/200 BLKS)
- 14 NORTH TOWNSEND STREET P & P, STA 15+00 TO STA 17+00 (200 BLK)
- 15 WEST COLUMBIA AVENUE P & P, STA 20+00 TO STA 22+75 (500 BLK)
- 16 WEST COLUMBIA AVENUE P & P, STA 22+75 TO STA 25+50 (500 BLK)
- 17 WEST COLUMBIA AVENUE P & P, STA 26+00 TO STA 27+50 (400 BLK)
- 18 WEST COLUMBIA AVENUE P & P, STA 27+50 TO STA 30+00 (400 BLK)
- 19 WEST COLUMBIA AVENUE P & P, STA 30+00 TO STA 32+50 (300/400 BLKS)
- 20 WEST COLUMBIA AVENUE P & P, STA 32+50 TO STA 34+00 (300 BLK)
- 21 WEST GALENA AVENUE P & P, STA 25+50 TO STA 28+00 (400 BLK)
- 22 WEST GALENA AVENUE P & P, STA 28+00 TO STA 30+50 (400 BLK)
- 23 WEST DAKOTA AVENUE P & P, STA 26+00 TO STA 28+50 (400 BLK)
- 24 WEST DAKOTA AVENUE P & P, STA 28+50 TO STA 31+50 (400 BLK)
- 25 PROJECT DETAILS
- 26 PROJECT DETAILS



SAN MIGUEL COUNTY 1:50000 USGS MAP

VICINITY MAP



APPROVED BY STREETSAPES TASK FORCE :

CHAIR _____ DATE _____

APPROVED BY THE TOWN OF TELLURIDE :

TOWN ENGINEER _____ DATE _____

<p>BUCKHORN GEOTECH</p> <p>Civil, Structural, and Geotechnical Engineers, Inc. 222 South Park Avenue Montrose, Colorado 81401 Phone (970) 249-6628 Fax (970) 249-0945</p>	<p>FOLEY ASSOCIATES, INC.</p> <p>ENGINEERING • PLANNING • SURVEYING</p> <p>PO Box 1385 125 W. Pacific Ave. Suite B1 Telluride, Colorado, 81435 phone 970-728-6153 fax 970-728-6050 e-mail to xmail@foleyassoc.com</p>
--	--

PROJECT ENGINEER

PROJECT MANAGER
AND SURVEYOR

CALL UTILITY NOTIFICATION
CENTER OF COLORADO
1-800-922-1987

CALL 2-BUSINESS DAYS IN ADVANCE
BEFORE YOU DIG, GRADE, OR EXCAVATE
FOR THE MARKING OF UNDERGROUND
MEMBER UTILITIES

Abbreviations

A	-AMP	M.H.	-MANHOLE
A.B.	-ANCHOR BOLT	MATL.	-MATERIAL
A.D.	-ALGEBRAIC DIFFERENCE	MAX.	-MAXIMUM
A.F.F.	-ABOVE FINISHED FLOOR	MFR.	-MANUFACTURER
A.F.G.	-ABOVE FINISHED GRADE	M.S.	-MILD STEEL
ANCH.	-ANCHOR	M.G.D.	-MILLION GALLONS PER DAY
L	-ANGLE	MIN.	-MINIMUM
APA	-AMERICAN PLYWOOD ASSOC.	MISC.	-MISCELLANEOUS
APRD	-APPROVED	M.J.	-MECHANICAL JOINT
APPROX.	-APPROXIMATE	M.P.H.	-MILES PER HOUR
•	-AT	MTL.	-METAL
&	-AND	NEMA	-NATIONAL ELECTRIC MFR ASSOC.
AWWA	-AMERICAN WATER WORKS ASSOC.	NFPA	-NATIONAL FIRE PROTECTION ASSOC.
B.F.	-BLIND FLANGE	N.I.C.	-NOT IN CONTRACT
BFV	-BUTTERFLY VALVE	N.P.T.	-NATIONAL PIPE THREAD
BLDG	-BUILDING	N.R.S.	-NON RISING STEM
BLK.	-BLOCK	N.	-NORTH
B.M.	-BENCH MARK	NOM.	-NOMINAL
B.O.F.	-BOTTOM OF FOOTING	N.S.	-NON-SHRINK
B.O.P.	-BEGINNING OF PROJECT	N.T.S.	-NOT TO SCALE
B.O.W.	-BOTTOM OF WALL	No.	-NUMBER
BP	-BASE PLATE		
BRG.	-BEARING	O.C.	-ON CENTER
BOTT.	-BOTTOM	O.D.	-OVERFLOW DRAIN or OUTSIDE DIAMETER
BTWN.	-BETWEEN	O.F.	-OUTSIDE FACE
B.T.U.	-BRITISH THERMAL UNIT	O.H.	-OPPOSITE HAND or OVERHANG
B.U.	-BELL UP	OPNG.	-OPENING
BVCE	-BEGIN VERTICAL CURVE ELEVATION	OPP.	-OPPOSITE
BVCS	-BEGIN VERTICAL CURVE STATION	O.S. & Y.	-OUTSIDE SCREW AND YOKE
CBX	-CONCRETE BOX CULVERT		
C.D.O.T.	-COLORADO DEPARTMENT OF TRANSPORTATION	P.C.	-POINT OF CURVE
CFM	-CUBIC FEET PER MINUTE	P.C.C.	-POINT OF COMPOUND CURVE
CFS	-CUBIC FEET PER SECOND	P.E.	-POLYETHYLENE
¢	-CENT	PL	-PLATE
C.I.P.	-CAST IN PLACE or CAST IRON PIPE	%	-PERCENT
CJ	-CONSTRUCTION JOINT	PERETR.	-PENETRATION
C.O.	-CLEAN OUT	PERIM.	-PERIMETER
CLR.	-CLEAR	P.I.	-POINT OF INTERSECTION
CMP	-CORRUGATED METAL PIPE	PNT.	-POINT
COL.	-COLUMN	P.O.J.	-PUSH ON JOINT
CONC.	-CONCRETE	P.R.C.	-POINT OF REVERSE CURVE
CONN.	-CONNECTION	PRELIM.	-PRELIMINARY
CONSTR.	-CONSTRUCTION	PROJ.	-PROJECTION
CONT.	-CONTINUOUS	P.S.	-PIPE SUPPORT
CONTR.	-CONTRACTOR	PSI	-POUNDS PER SQUARE INCH
COORD.	-COORDINATE	P.T.	-POINT OF TANGENCY
CONTRD.	-CENTERED	P.V.C.	-POLYVINYL CHLORIDE
CSP	-CORRUGATED STEEL PIPE	P.V.I.	-POINT OF VERTICAL INTERSECTION
C & G	-CURB & GUTTER	QTY.	-QUANTITY
Cu	-COPPER		
C.Y.	-CUBIC YARD	R	-RADIUS
		RCP	-REINFORCED CONCRETE PIPE
DET.	-DETAIL	RD	-ROAD
DC	-DEGREE OF CURVE	RED.	-REDUCER
DIA. (OR Ø)	-DIAMETER	REF.	-REFERENCE (REFER TO)
DIAG.	-DIAGONAL	REINF.	-REINFORCING
DIM.	-DIMENSION	REM	-REMAINDER
D.I.P.	-DUCTILE IRON PIPE	REQD.	-REQUIRED
DIST.	-DISTANCE	REQMT.	-REQUIREMENT
DWG.	-DRAWING	RPM	-REVOLUTIONS PER MINUTE
DWL	-DOWEL	RT.	-RIGHT
E	-EAST	S	-SOUTH
EA.	-EACH	SCHED.	-SCHEDULE
E.F.	-EACH FACE	SECT.	-SECTION
EL. or ELEV.	-ELEVATION	SHT.	-SHEET
ELEC.	-ELECTRICAL	SIM.	-SIMILAR
ELL.	-ELBOW	SJ	-SAWN JOINT (CONTROL JOINT)
ENGR.	-ENGINEER	SP	-SPACE
E.O.A.	-EDGE OF ASPHALT	S.P.D.	-STANDARD PROCTOR DENSITY
E.O.P.	-END OF PROJECT	SPEC.	-SPECIFICATIONS
E.O.R.	-EDGE OF ROAD	SQ.	-SQUARE
EPS	-EXPANDED POLY-STYRENE	S.S.	-STAINLESS STEEL
EQ	-EQUAL	STA.	-STATION
EVCE	-END VERTICAL CURVE ELEVATION	STD.	-STANDARD
EVCS	-END VERTICAL CURVE STATION	STIFF.	-STIFFENER
EXIST.	-EXISTING	STGR.	-STAGGERED
EXP.	-EXPANSION	STL.	-STEEL
EXT.	-EXTERIOR	STR.	-STRUT
E.W.	-EACH WAY	STRUCT.	-STRUCTURAL
FAB.	-FABRICATE	SUPT.	-SUPPORT
FPS	-FEET PER SECOND	S.Y.	-SQUARE YARD
FIN GRD.	-FINISH GRADE	SYMM.	-SYMMETRICAL
F.H.	-FIRE HYDRANT	S/W	-SIDEWALK
FD	-FLOOR DRAIN	T	-TANGENT
FLG.	-FLANGE	TB	-THRUST BLOCK
FES	-FLARED END SECTION	TBM	-TEMPORARY BENCH MARK
FDN.	-FOUNDATION	T & B	-TOP AND BOTTOM
F or F/L	-FLOW LINE	T & G	-TONGUE & GROOVE
FT	-FOOT	TEMP.	-TEMPERATURE
FTG.	-FOOTING	T.H.	-TEST HOLE
		T.O.S.C.	-TOP OF STRUCTURAL CONCRETE
GA.	-GAGE (OR GAUGE)	T.O.C.	-TOP OF CURB
GPM	-GALLONS PER MINUTE	T.O.F.	-TOP OF FOOTING
GALV.	-GALVANIZED	T.O.P.	-TOP OF PIPE
G	-GAS	T.O.S.	-TOP OF STEEL
G.B.	-GRADE BREAK	T.O.W.	-TOP OF WALL
G.L.	-GLULAM	T.P.	-TEST PIT
GR	-GRADE	TRANSV.	-TRANSVERSE
GR.BM.	-GRADE BEAM	TS	-TUBULAR STEEL
GV	-GATE VALVE	TYP.	-TYPICAL
H.C.	-HANDICAP		
HGR.	-HANGER	U.B.C.	-UNIFORM BUILDING CODE
HT	-HEIGHT	U.F.C.	-UNIFORM FIRE CODE
H.P.	-HIGH POINT or HORSE POWER	U.M.C.	-UNIFORM MECHANICAL CODE
H.S.	-HIGH STRENGTH	U.N.O.	-UNLESS NOTED OTHERWISE
H.W.L.	-HIGH WATER LEVEL	U.P.C.	-UNIFORM PLUMBING CODE
HORIZ.	-HORIZONTAL		
H.B.	-HOSE BIB	V	-VELOCITY
		V	-VOLT
IN	-INCH	VAR.	-VARIES
INFO.	-INFORMATION	VERT.	-VERTICAL
I.D.	-INSIDE DIAMETER	V.C.	-VERTICAL CURVE
INV.	-INVERT	V.P.I.	-VERTICAL POINT OF INTERSECTION
IPS	-IRON PIPE SIZE	V.P.T.	-VERTICAL POINT OF TANGENCY
		V.P.	-VALLEY PAN
JST.	-JOIST	W or WTR	-WATER
JT.	-JOINT	W	-WEST
L	-LENGTH	W/	-WITH
L.T.	-LEFT	WD	-WOOD
LAT.	-LATERAL	W.O.G.	-WATER, OIL, GAS
LBS.	-POUNDS	W/O	-WITHOUT
LDGR.	-LEDGER	W.P.	-WORK POINT
LL	-LIVE LOAD	WT.	-WEIGHT
LLH	-LONG LEG HORIZONTAL	W.W.F.	-WELDED WIRE FABRIC
LLV	-LONG LEG VERTICAL		
LONG.	-LONGITUDINAL		
L.P.	-LOW POINT		
L.W.L.	-LOW WATER LEVEL		
L.P.G.	-LIQUID PETROLEUM GAS		

Legend

	Handicap Ramp		Existing Retaining Wall
	Water Valve		Existing Fence
	Tree		Existing Index Contour
	Utility Pole		Existing Intermediate Contour
	Sanitary Manhole		Overhead Utility Line (existing)
	Water Service Valve		Sanitary Sewer (existing)
	Cable Television Pedestal		Water (existing)
	Fire Hydrant		Cable TV (subsurface, existing)
	Sign		Telephone (subsurface, existing)
	Telephone Pedestal		Power (subsurface, existing)
	Transformer		Gas (subsurface, existing)
			Edge of Travelled Way (existing)
			Edge of Pavement (existing)
			Proposed crown of street
			Proposed finished contour, interval noted

87.53 • Proposed Spot Elevation at Flowline of Gutter (Unless noted otherwise)

List of Agencies

TOWN HALL	113 W. COLUMBIA AVE.	(970) 728-3071
TELLURIDE MAINTENANCE FACILITY		(970) 728-3516
GAS		
KN ENERGY		(800) 563-0012
ELECTRIC		
SAN MIGUEL POWER		(970) 728-3825
TELEPHONE		
U S WEST (REPAIR)		(800) 954-1211
SHERIFF'S DEPARTMENT		(970) 728-3081
FIRE DEPARTMENT		(970) 728-3801
MARSHAL'S OFFICE		(970) 728-3818

General Notes

- ALL EXISTING SIGNAGE TO BE RELOCATED BY THE TOWN OF TELLURIDE. SIGNAGE SHALL BE IN ACCORDANCE WITH THE TOWN OF TELLURIDE STANDARDS. ALL CONSTRUCTION TRAFFIC CONTROL SIGNAGE IS THE RESPONSIBILITY OF THE CONTRACTOR, IN ACCORDANCE WITH MUTCD.
- ALL WATER SYSTEMS RELOCATIONS SHALL BE MADE BY TOWN FORCES AS REQUIRED.
- ALL WORK TO BE DONE IN ACCORDANCE WITH THE TOWN OF TELLURIDE, COLORADO STANDARD SPECIFICATIONS, AS CURRENTLY ADOPTED.
- DIMENSIONS SHOWN ON PLANS WITH AN ASTERISK (*) ARE APPROXIMATE AND SHALL BE FIELD VERIFIED.
- CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING THE ELEVATIONS OF ALL VALVE RISERS, MANHOLES, AND OTHER UTILITY APPURTENANCES AS REQUIRED TO ACHIEVE FINISHED GRADES SHOWN ON THE DRAWINGS. THE DRAWINGS MAY NOT SHOW LOCATIONS OF ALL EXISTING APPURTENANCES.
- RELOCATION OF UTILITY POLES, PEDESTALS, TRANSFORMERS, ETC. SHALL BE COORDINATED WITH THE TOWN OF TELLURIDE PUBLIC WORKS AND THE APPROPRIATE UTILITY COMPANY. RELOCATIONS SHALL BE DETERMINED IN THE FIELD.
- UNLESS OTHERWISE NOTED, CONTRACTOR SHALL REMOVE AND DISPOSE OF ALL EXISTING ASPHALT PAVEMENT MATERIALS, CONCRETE VALLEY PANS, CURB AND GUTTER, ALLEY AND DRIVEWAY RAMPS, SIDEWALKS, AND OTHER FEATURES LOCATED WITHIN THE PROPOSED STREET SECTIONS. CONTRACTORS' ACTIVITIES SHALL BE LIMITED TO THE PUBLIC RIGHT-OF-WAY, EXCEPT WHERE NOTED ON THESE DRAWINGS.
- GREEN SPACE AREAS BETWEEN BACK FACE OF CURB AND SEPARATED SIDEWALKS SHALL BE FINISHED WITH EITHER 4" MINIMUM OF NATIVE TOPSOIL MATERIAL OR 4" CONCRETE, AS SHOWN ON THE PLANS.
- CONTRACTOR SHALL PROTECT EXISTING STRUCTURES AND OTHER ENCROACHMENTS INTO THE RIGHT-OF-WAY FROM HARM DURING CONSTRUCTION.
- CONTRACTOR SHALL HAVE ONE SET OF SIGNED APPROVED PLANS ON THE JOB SITE AT ALL TIMES DURING CONSTRUCTION.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE LOCATION OF ALL EXISTING UTILITIES, INCLUDING ANY UTILITIES NOT SHOWN ON THE DRAWINGS. WHEN WORKING NEAR EXISTING UTILITIES, THE CONTRACTOR SHALL EXERCISE SUFFICIENT CARE TO PREVENT DAMAGE TO THE LINES IN THE EVENT THAT THE INFORMATION SHOWN ON THE DRAWINGS DOES NOT REFLECT ACTUAL FIELD CONDITIONS.
- BENCHMARK: HORIZONTAL AND VERTICAL CONTROL BY FOLEY ASSOCIATES.
- ALL CONCRETE AND ASPHALT CUTTING SHALL BE DONE TO CLEAN, NEAT EDGES.
- WHEN SIDEWALK ARE PROPOSED OR ARE EXISTING AT CORNERS OF INTERSECTIONS, CONCRETE RAMPS WILL BE INSTALLED PER THE DIRECTION OF THE OWNER.

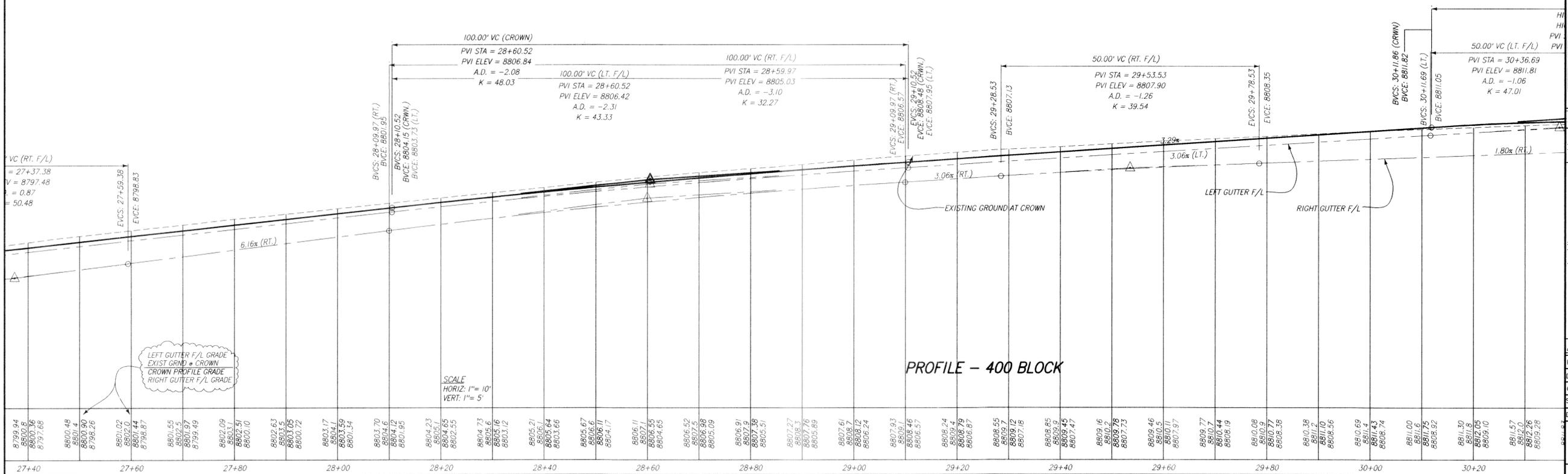
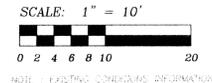
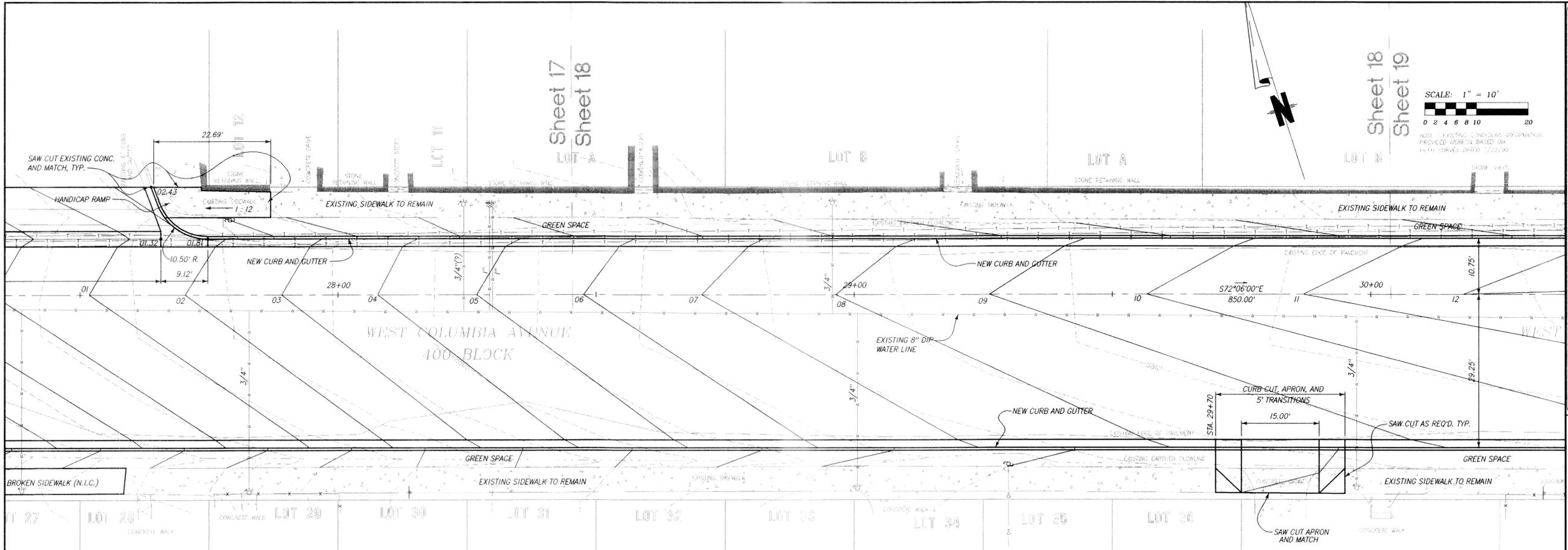
TELLURIDE STREETSAPES - PHASE II
GENERAL NOTES

FOLEY ASSOCIATES, INC.
ENGINEERING • PLANNING • SURVEYING

BUCKHORN GEOTECH
Civil, Structural, and Geotechnical Engineers, Inc.
222 South Park Avenue
Montrose, Colorado 81401
Phone (970) 248-6628 Fax (970) 249-0946

Rev.	1	ADDED NOTE #4
date	7-24-00	DJP
by		
description		
Project Mgr:	JG	
Drawn by:	DJP	
Checked by:	ELK	
Start date:	03-13-00	
Dwg path:	2NOTEPA6.DWG	
Sheet No.	2	
Project #:	99102	

CALL UTILITY NOTIFICATION
CENTER OF COLORADO
1-800-922-1987
CALL 2-BUSINESS DAYS IN ADVANCE
BEFORE YOU DIG, GRADE, OR EXCAVATE
FOR THE MARKING OF UNDERGROUND
MEMBER UTILITIES



VC (RT. F/L)
 $V = 27+37.38$
 $PVI = 8797.48$
 $A.D. = 0.87$
 $L = 50.48$

LEFT GUTTER F/L GRADE
 EXIST GROUND + CROWN
 CROWN PROFILE GRADE
 RIGHT GUTTER F/L GRADE

SCALE
 HORIZ: 1" = 10'
 VERT: 1" = 5'

PROFILE - 400 BLOCK

8799.94	8800.8	8800.36	8797.68	8800.48	8800.90	8800.4	8798.26	8801.02	8802.0	8798.87	8801.55	8802.5	8801.97	8794.49	8802.09	8803.1	8802.51	8800.10	8802.63	8803.6	8803.06	8800.72	8803.17	8804.1	8803.59	8800.34	8803.70	8804.6	8804.12	8801.95	8804.23	8805.2	8804.65	8802.55	8804.73	8805.6	8803.12	8805.21	8806.1	8803.66	8805.67	8806.9	8804.17	8806.11	8807.1	8806.55	8804.65	8806.52	8807.5	8806.98	8805.09	8806.91	8807.8	8805.51	8807.27	8808.3	8807.76	8805.69	8807.61	8808.7	8808.12	8806.24	8807.93	8808.46	8806.57	8808.24	8809.4	8808.79	8806.67	8808.55	8809.7	8809.12	8807.16	8808.85	8809.9	8808.45	8807.47	8809.16	8809.9	8808.9	8807.75	8809.46	8810.11	8807.97	8809.77	8810.7	8810.44	8808.19	8810.08	8810.9	8809.7	8806.36	8810.38	8811.0	8808.56	8810.69	8811.43	8808.74	8811.00	8811.6	8808.92	8811.30	8812.05	8809.10	8811.57	8812.0	8812.76	8809.28
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TELLURIDE STREETSCAPES - PHASE II
WEST COLUMBIA AVENUE PLAN AND PROFILE
STA. 27+50 TO STA. 30+00 (400 BLOCK)

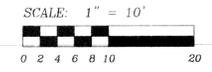
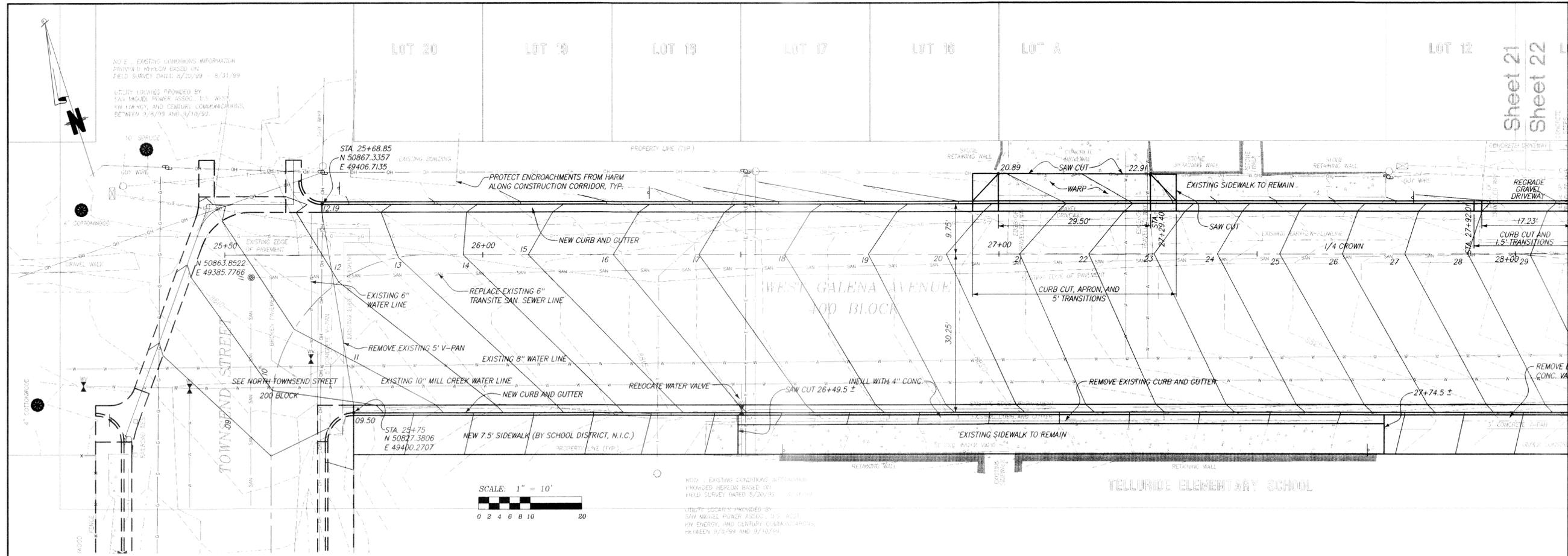
Client: William Frownefer
 300 Black Bear Road
 Telluride, Colorado 81435

Project Mgr: JG
 Drawn by: DJP
 Checked by: ELK
 Start date: 03-13-00

Dwg path: COLUMB1.DWG
 Sheet No. **18**
 Project #: 99402

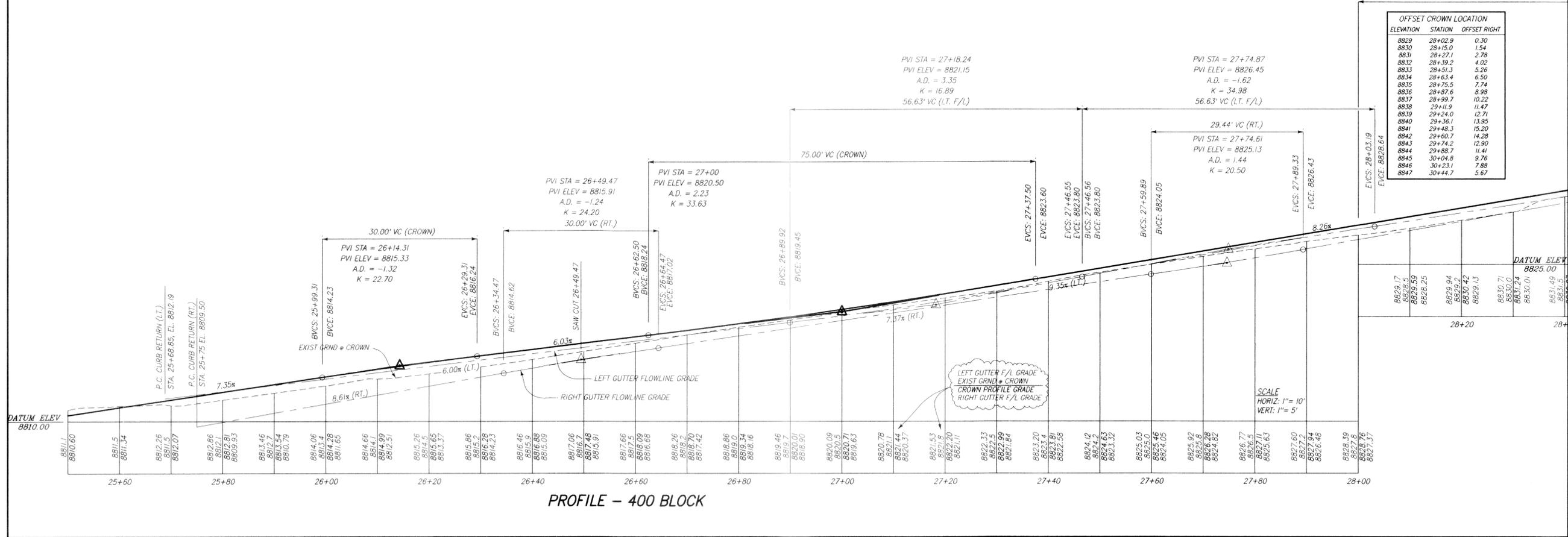
FOLEY ASSOCIATES, INC.
 ENGINEERING - PLANNING - SURVEYING
 PO Box 1206, 128 W. Pacific Ave., Suite B1
 Montrose, Colorado 81401
 phone 970-726-6133 fax 970-726-6050
 e-mail to: mail@foleyassoc.com

BUCKHORN GEOTECH
 Civil, Structural, and Geotechnical Engineers, Inc.
 222 South Park Avenue
 Montrose, Colorado 81401
 Phone (970) 246-8828 Fax (970) 249-0945



NOTE: EXISTING CONDITIONS INFORMATION PROVIDED HEREON BASED ON FIELD SURVEY DATED 8/20/95.
 UTILITY LOCATIONS PROVIDED BY SAN MICHAEL POWER ASSOC., U.S. WEST, KIN ENERGY, AND CENTURY COMMUNICATIONS, BETWEEN 8/8/99 AND 8/10/99.

ELEVATION	STATION	OFFSET RIGHT
8829	28+02.9	0.30
8830	28+15.0	1.54
8831	28+27.1	2.78
8832	28+39.2	4.02
8833	28+51.3	5.26
8834	28+63.4	6.50
8835	28+75.5	7.74
8836	28+87.6	8.98
8837	28+99.7	10.22
8838	29+11.9	11.47
8839	29+24.0	12.71
8840	29+36.1	13.95
8841	29+48.3	15.20
8842	29+60.7	16.44
8843	29+74.2	17.68
8844	29+88.7	18.92
8845	30+04.8	20.16
8846	30+23.1	21.40
8847	30+44.7	22.64



PROFILE - 400 BLOCK

TELLURIDE STREETSCAPES - PHASE II
WEST GALENA AVENUE PLAN AND PROFILE
 STA. 25+50 TO STA. 28+00 (400 BLOCK)

Client: Colorado
 Town of Telluride
 P.O. Box 397
 Telluride
 Colorado 81435

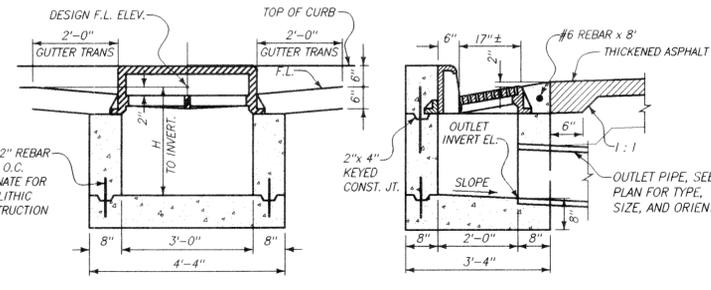
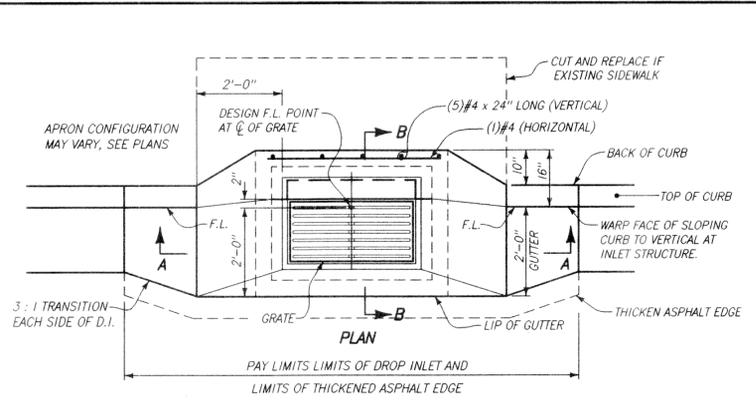
FOLEY ASSOCIATES, INC.
 ENGINEERING - PLANNING - SURVEYING

Client: Structural and Geotechnical Engineers, Inc.
 222 Sun Park Avenue
 Montrose, Colorado 81401
 Phone (970) 248-6828 Fax (970) 249-0945

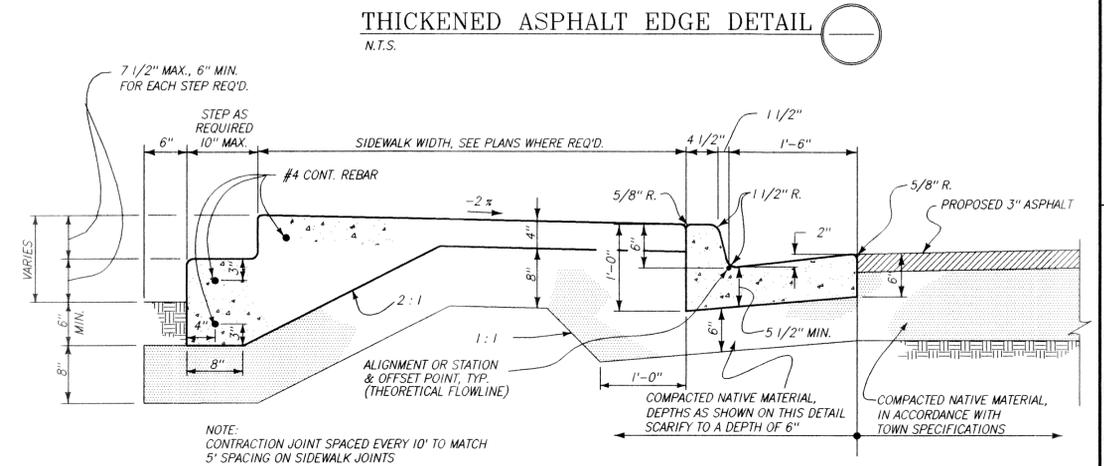
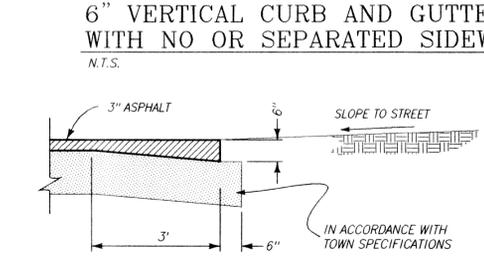
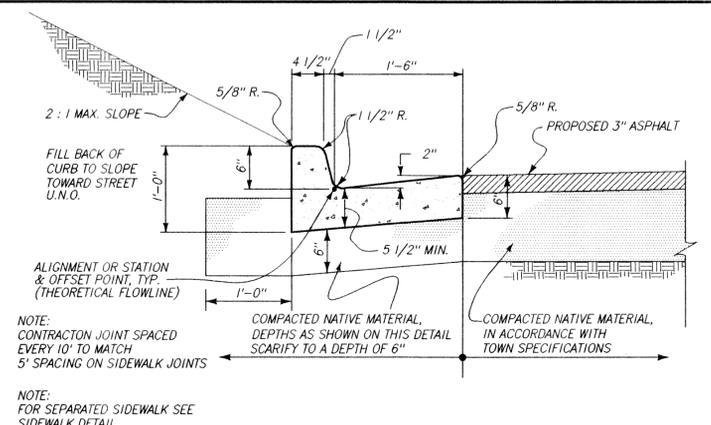
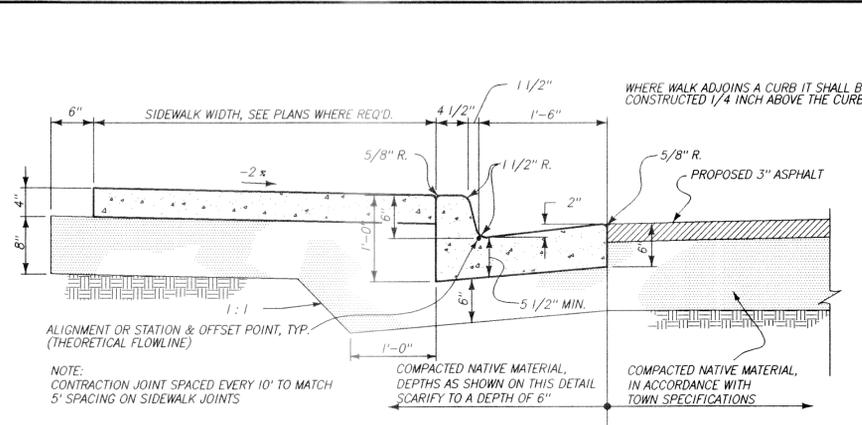
BUCKHORN/GEOTECH

Project Mgr: JG
 Drawn by: DJP
 Checked by: ELK
 Start date: 03-13-00
 Dwg path: GALENA2.DWG
 Sheet No. **21**
 Project #: 99102

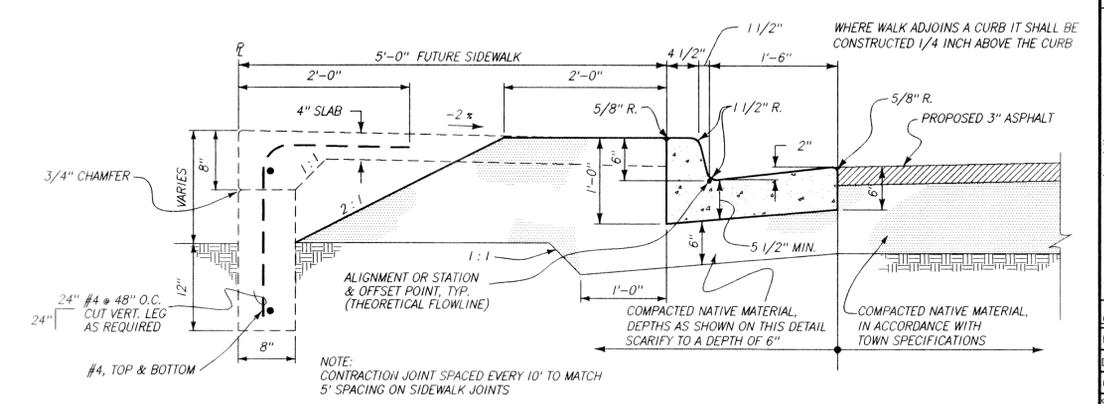
date	
description	
rev	
Project Mgr:	JG
Drawn by:	DJP
Checked by:	ELK
Start date:	03-13-00
Dwg path:	SSDETAILS1.DWG
Sheet No.	26
Project #:	99102



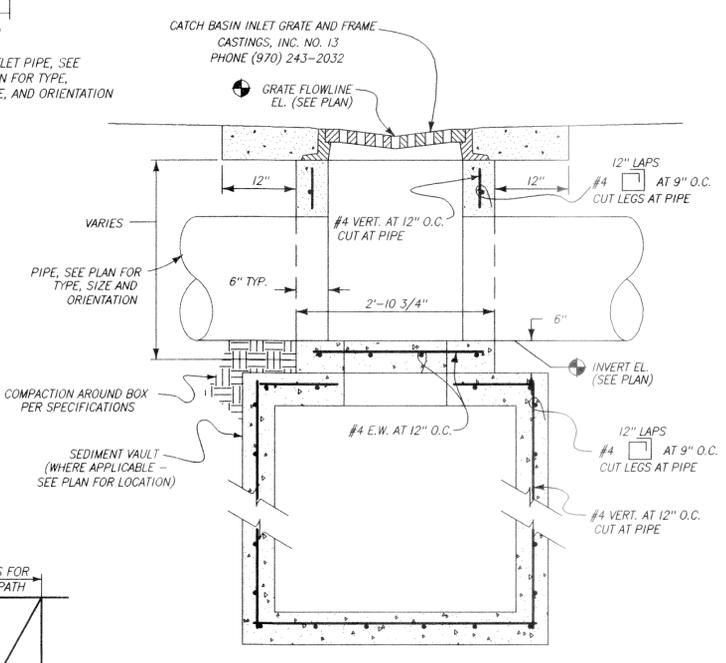
DROP INLET TYPE 2 DETAIL
N.T.S.
REFERENCE TOWN SPEC: SECTION 02520



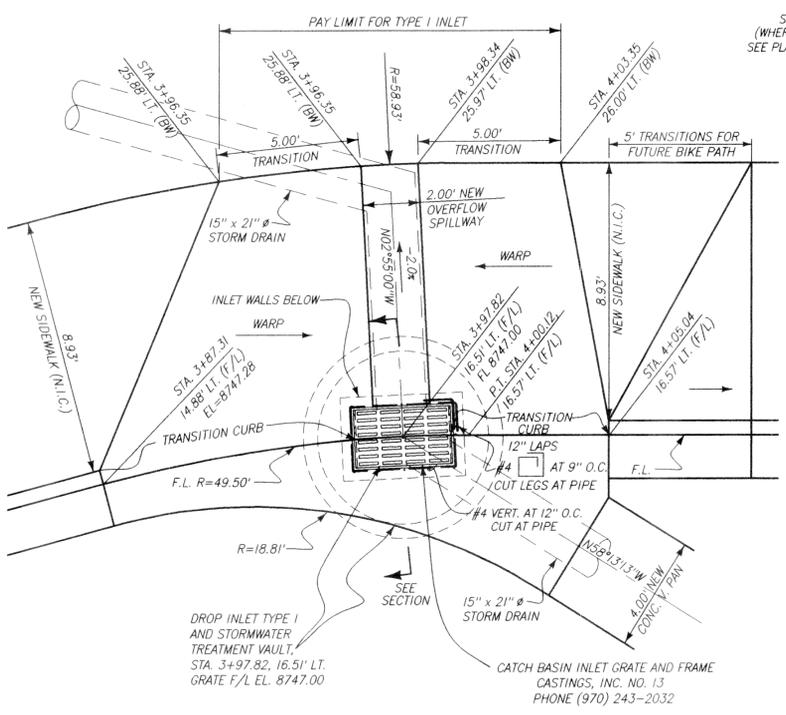
SIDEWALK STAIR STEP/THICKEN EDGE DETAIL
N.T.S.



RETAINING WALL DETAIL N. TOWNSEND, 200 BLOCK, WEST SIDE
N.T.S.



TYPE 1 INLET AND DESILTATION VAULT ASSEMBLY SECTION
SOUTH ASPEN STREET, 200 BLOCK (N.T.S.)

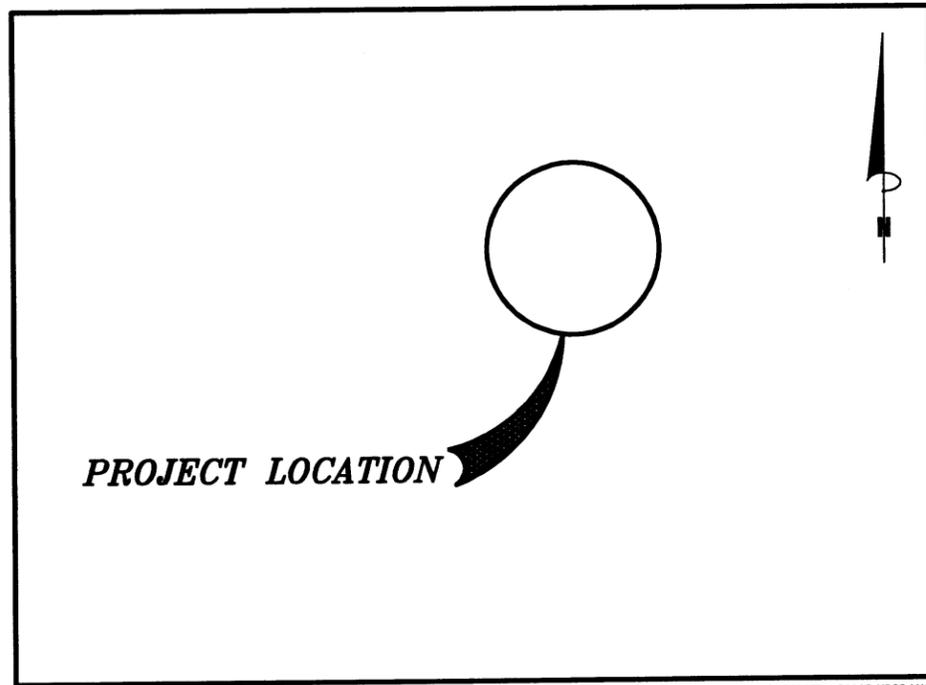


DROP INLET TYPE 1 DETAIL
N.T.S.
REFERENCE TOWN SPEC: SECTION 02520

**TELLURIDE STREETSAPES – PHASE IIA
STREET IMPROVEMENT CONSTRUCTION PLANS
TOWN OF TELLURIDE
SAN MIGUEL COUNTY, COLORADO
PROJECT # 99102
JUNE 20, 2001**

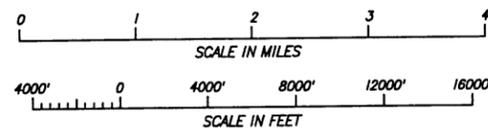
INDEX TO DRAWINGS

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- 2 NOTES & LEGEND
- 3 SCHEMATIC TYPICAL STREET CROSS SECTIONS
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- 5 WEST COLUMBIA AVENUE P & P, STA 20+00 TO STA 22+75 (500 BLK)
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- 7 WEST COLUMBIA AVENUE P & P, STA 26+00 TO STA 27+50 (400 BLK)
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- 11 PROJECT DETAILS
- 12 PROJECT DETAILS



SAN MIGUEL COUNTY 1:50000 USGS MAP

VICINITY MAP



APPROVED BY STREETSAPES TASK FORCE :

CHAIR DATE

APPROVED BY THE TOWN OF TELLURIDE :

TOWN ENGINEER DATE

APPROVED BY THE COLORADO DEPARTMENT OF TRANSPORTATION :

NAME AND TITLE DATE

<p>BUCKHORN GEOTECH</p> <p>Civil, Structural, and Geotechnical Engineers, Inc. 222 South Park Avenue Montrose, Colorado 81401 Phone (970) 249-6828 Fax (970) 249-0945</p>	<p>FOLEY ASSOCIATES, INC.</p> <p>ENGINEERING · PLANNING · SURVEYING</p> <p>PO Box 1365 125 W. Pacific Ave. Suite B1 Telluride, Colorado, 81435 phone 970-728-6153 fax 970-728-6050 e-mail to xmail@foleyassoc.com</p>
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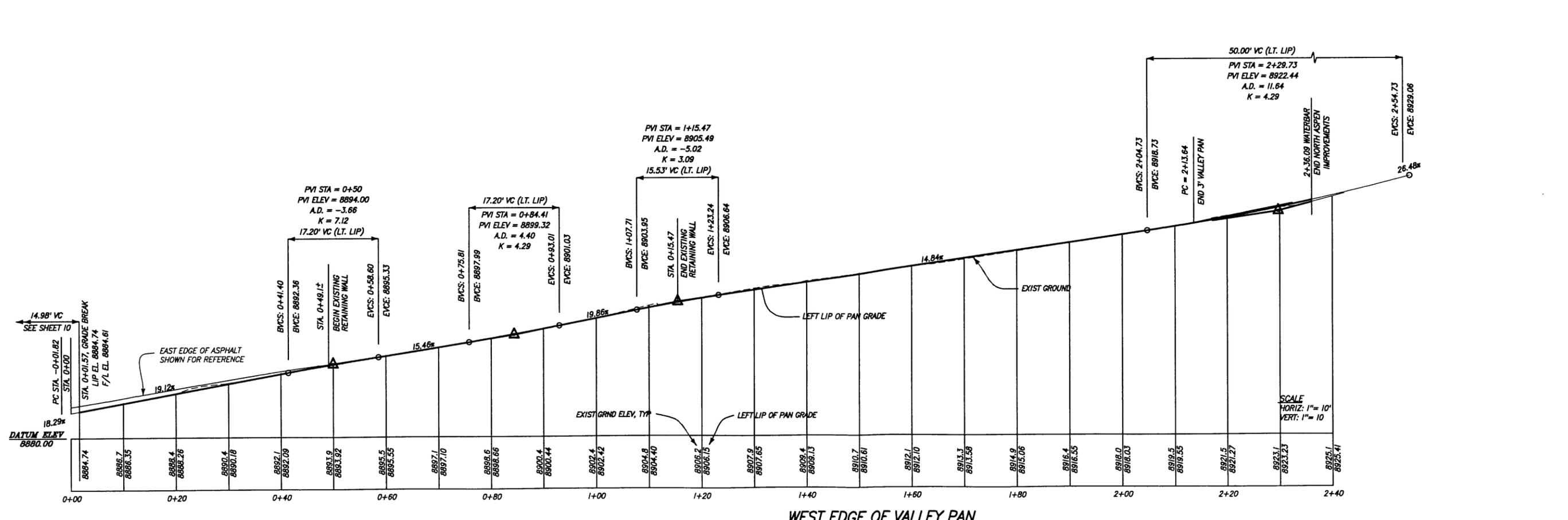
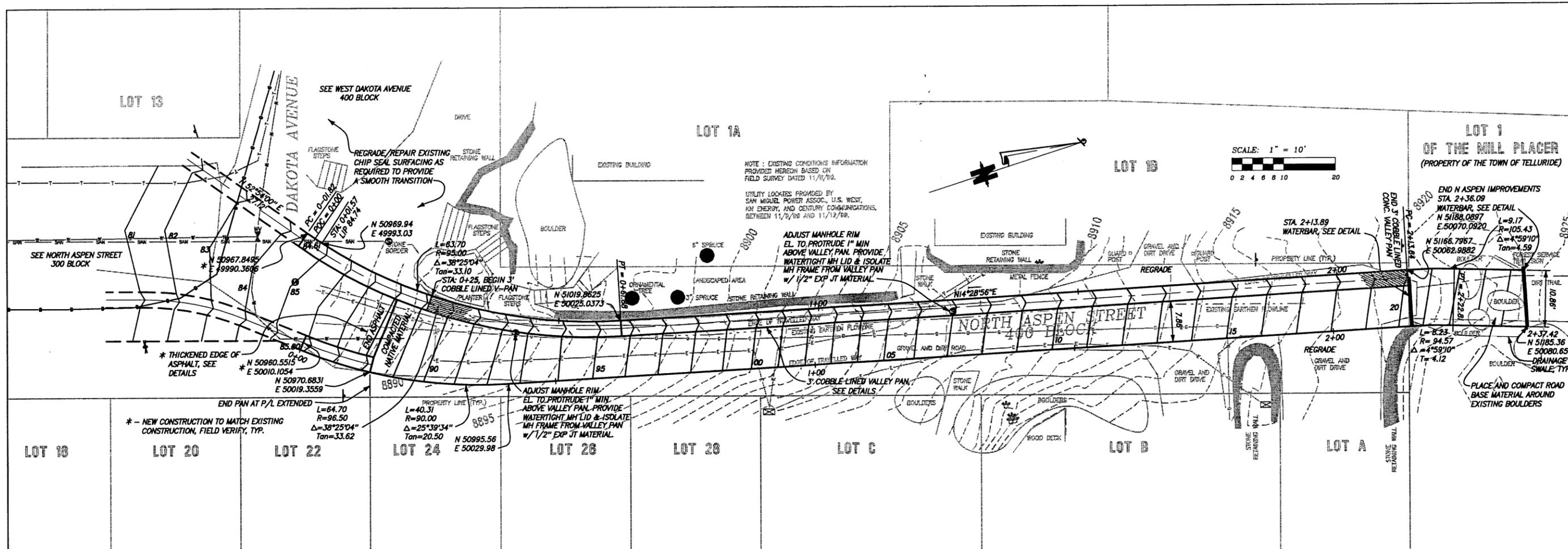
PROJECT MANAGER AND ENGINEER

PROJECT SURVEYOR

CALL UTILITY NOTIFICATION
CENTER OF COLORADO
1-800-922-1987

CALL 2-BUSINESS DAYS IN ADVANCE
BEFORE YOU DIG, GRADE, OR EXCAVATE
FOR THE MARKING OF UNDERGROUND
MEMBER UTILITIES

D:\Land Projects 3\SSc-Phase2A\dwg\Phase2a-4.dwg, 05/15/02 09:12:56 AM, HP LaserJet 5000 PCL 6 (temporary)4464.pc3



TELLURIDE STREETSCAPES - PHASE IIA
 NORTH ASPEN STREET PLAN AND PROFILE
 STA. 0+00 TO STA. 2+00 (400 BLOCK)

Contact:
 William Freireiter
 300 Black Bear Road
 Colorado 81435

Client:
 Town of Telluride, Colorado
 P.O. Box 397
 Telluride, Colorado 81435
 Phone 970-728-6153 Fax 970-728-6050
 e-mail to: small@foleyassoc.com

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BUCKHORNTECH
 Civil, Structural, and Geotechnical Engineers, Inc.
 222 South Park Avenue
 Montrose, Colorado 81401
 Phone (970) 249-6828 Fax (970) 249-0945

Rev.	date	description
1	1-30-02	DAP

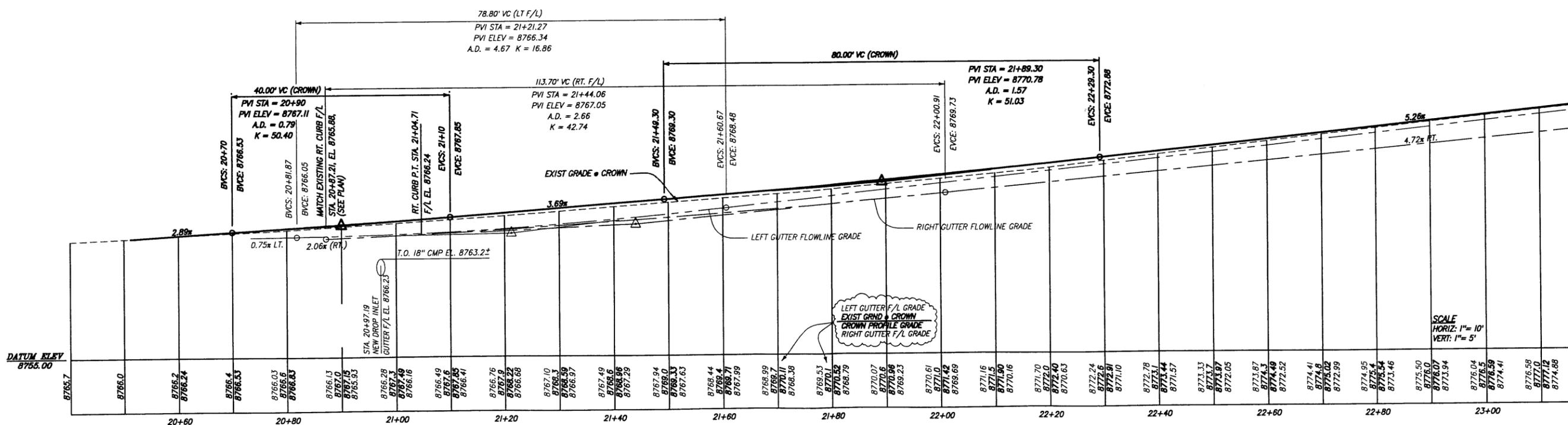
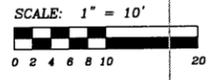
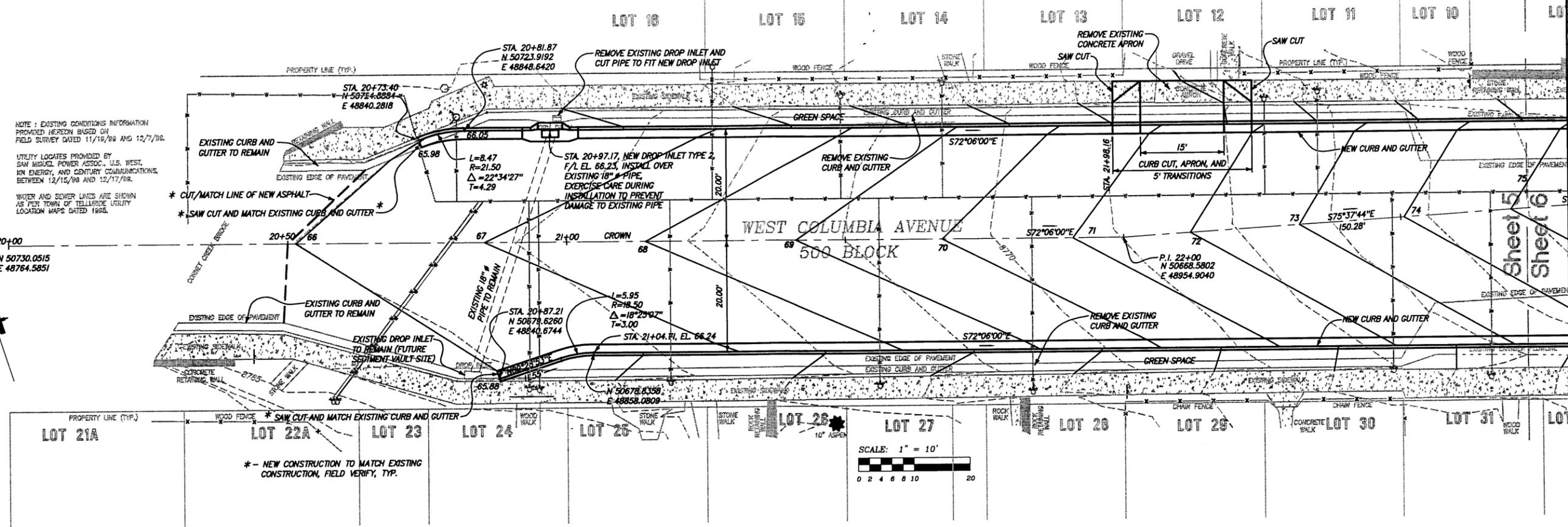
Project Mgr: ELK
 Drawn by: DJP
 Checked by: ELK
 Start date: 06-20-01
 Dwg path: PHASE2A-4
 Sheet No. **4**
 Project #: 99102

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DAVIS STREET

NOTE: EXISTING CONDITIONS INFORMATION PROVIDED HEREIN BASED ON FIELD SURVEY DATED 11/18/99 AND 12/7/99. UTILITY LOCATES PROVIDED BY SAN MIGUEL POWER ASSOC., U.S. WEST, XN ENERGY, AND CENTURY COMMUNICATIONS, BETWEEN 12/15/99 AND 12/17/99. WATER AND SEWER LINES ARE SHOWN AS PER TOWN OF TELLURIDE UTILITY LOCATION MAPS DATED 1995.

20+00
N 50730.0515
E 48764.5851



DATUM ELEV
8755.00

LEFT GUTTER F/L GRADE
EXIST GRND & CROWN
CROWN PROFILE F/L GRADE
RIGHT GUTTER F/L GRADE

SCALE
HORIZ: 1" = 10'
VERT: 1" = 5'

Rev.	description	date	by
4-2-01	DJP		

Project Mgr:	ELK
Drawn by:	DJP
Checked by:	ELK
Start date:	06-20-01
Dwg path:	PHASE2A-5-6
Sheet No.	5
Project #:	99102

BUCKHORNGEOTECH
Civil, Structural, and Geotechnical Engineers, Inc.
222 South Park Avenue
Montrose, Colorado 81401
Phone (970) 249-6628 Fax (970) 249-0945

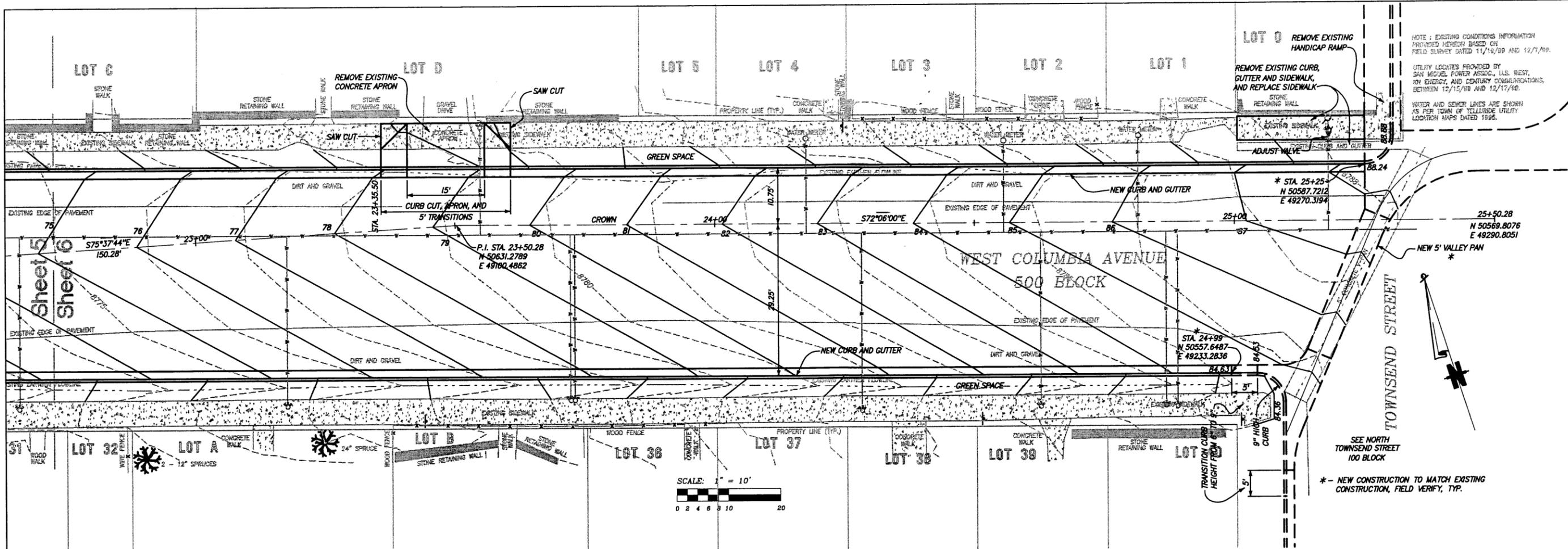
FOLEY ASSOCIATES, INC.
ENGINEERING - PLANNING - SURVEYING
PO Box 1385, 105 W. Pacific Ave. Suite B1
Telluride, Colorado 81453
Phone (970) 728-6153 Fax (970) 728-6000
e-mail to: small@foley-assoc.com

TELLURIDE STREETSCAPES - PHASE IIA
WEST COLUMBIA AVENUE PLAN AND PROFILE
STA. 20+00 TO STA. 22+75 (500 BLOCK)

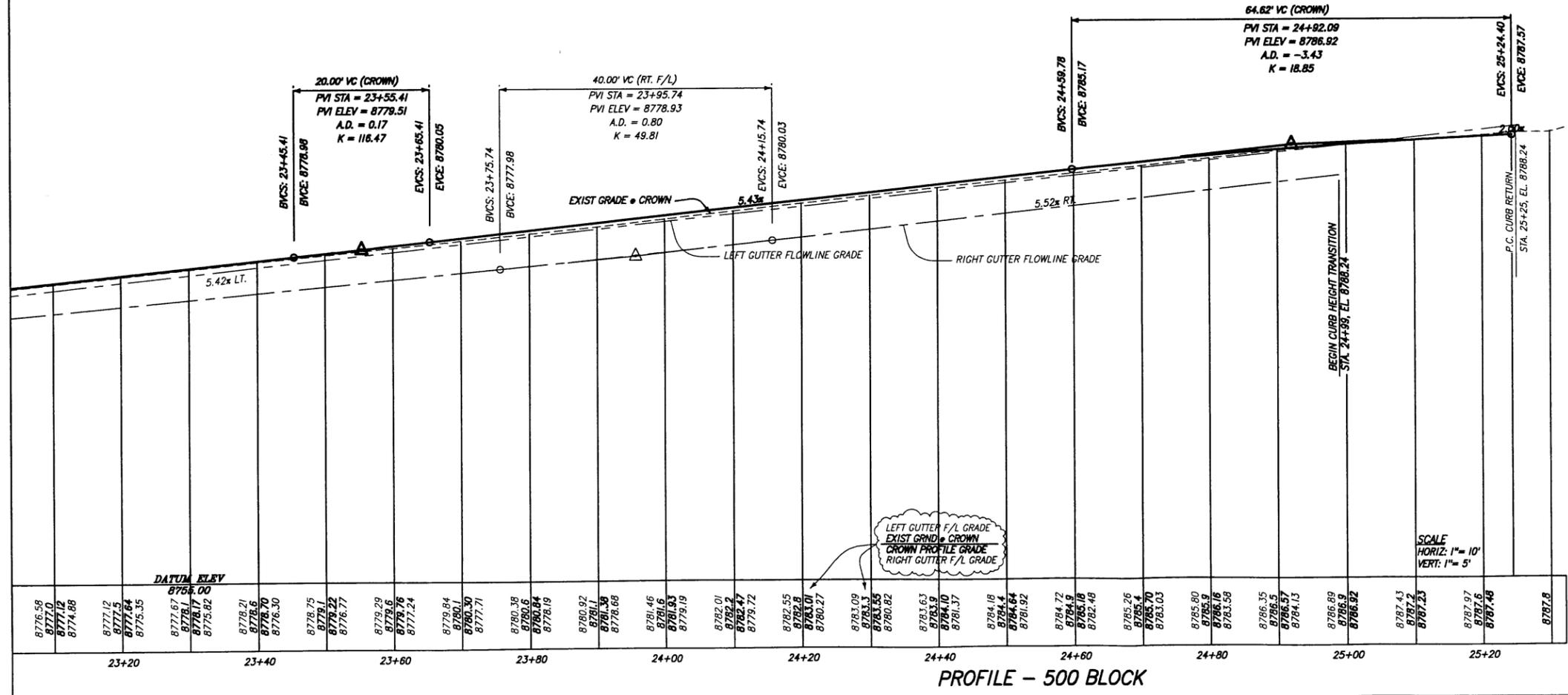
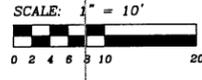
Contact:
William J. Pomeroy
300 Bluffs Road
Telluride
Colorado 81435

Client:
Town of Telluride, Colorado
P.O. Box 387
Telluride, Colorado 81435

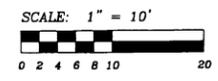
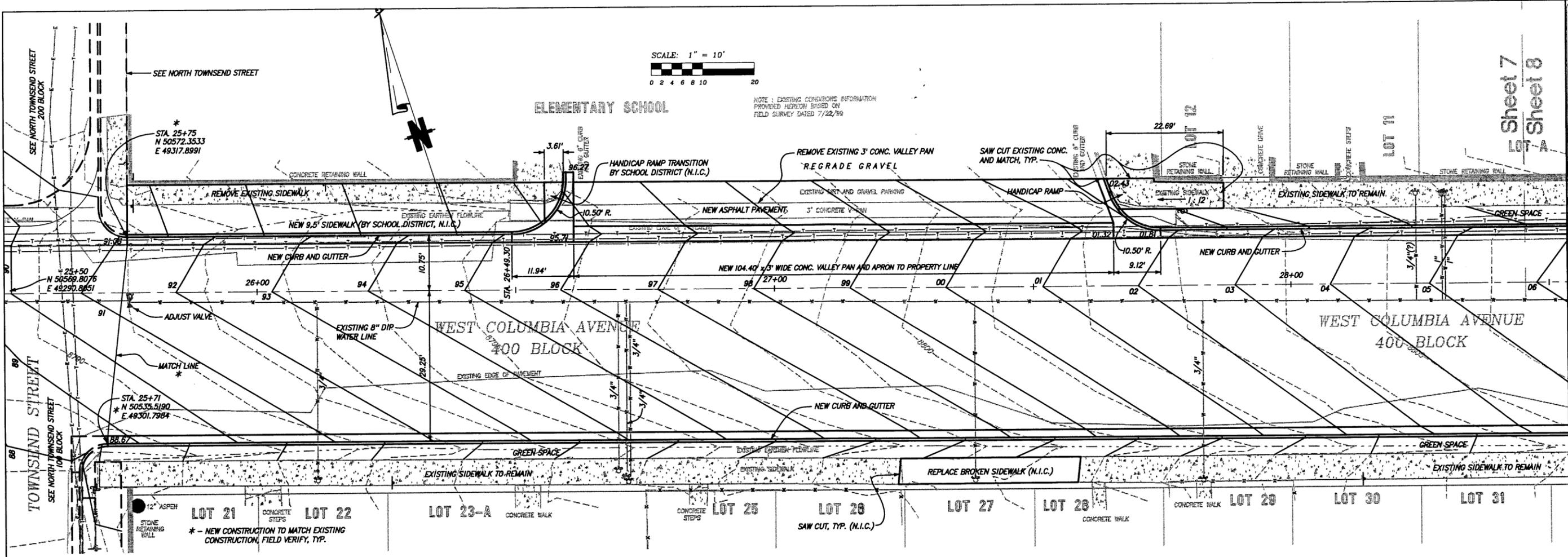
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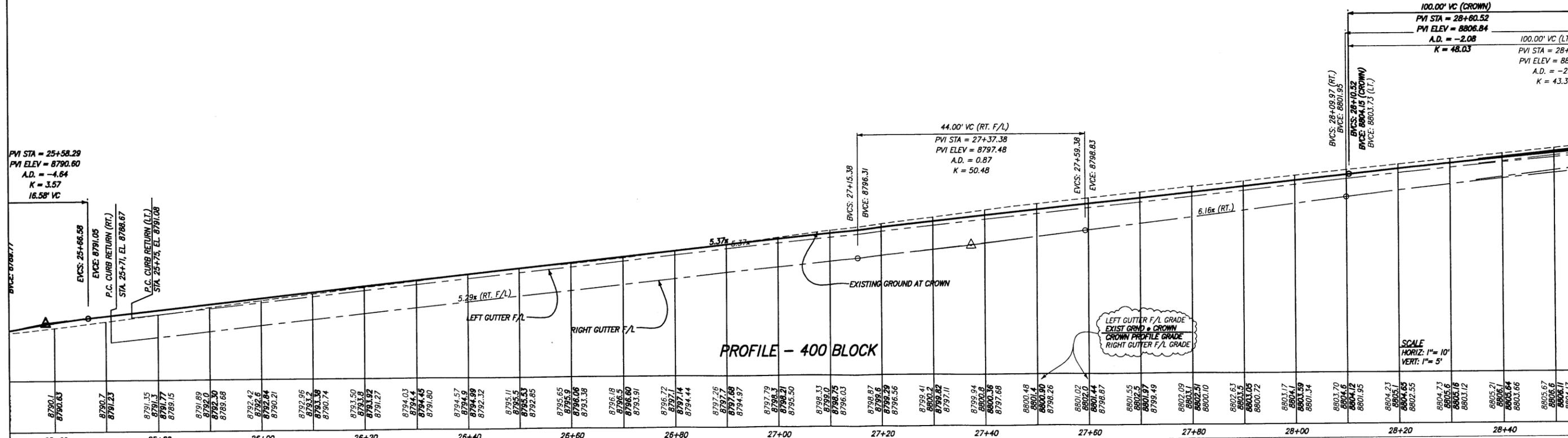
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 UTILITY LOCATES PROVIDED BY SAN MICHAEL POWER ASSOC., U.S. WEST, ION ENERGY AND COUNTY ORIGINATIONS, BETWEEN 12/15/99 AND 12/17/99.
 WATER AND SEWER LINES ARE SHOWN AS PER TOWN OF TELLURIDE UTILITY LOCATION MAPS DATED 1995.



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NOTE: EXISTING CONDITIONS INFORMATION PROVIDED HEREON BASED ON FIELD SURVEY DATED 7/22/99



PROFILE - 400 BLOCK

SCALE
HORIZ: 1" = 10'
VERT: 1" = 5'

Project #9192

7

Sheet 7
Sheet 8

Project Mgr: ELK
Drawn by: DJP
Checked by: ELK
Start date: 06-20-01
Dwg path: PHASE2A-7-10

Project #9192

Client: Town of Telluride, Colorado
P.O. Box 1007
Telluride, Colorado 81435
Phone 970-728-6153 Fax 970-728-6050
e-mail to xsmall@foleyassoc.com

Contact: William F. Frazier
300 S. Highway 77
Telluride, Colorado 81435

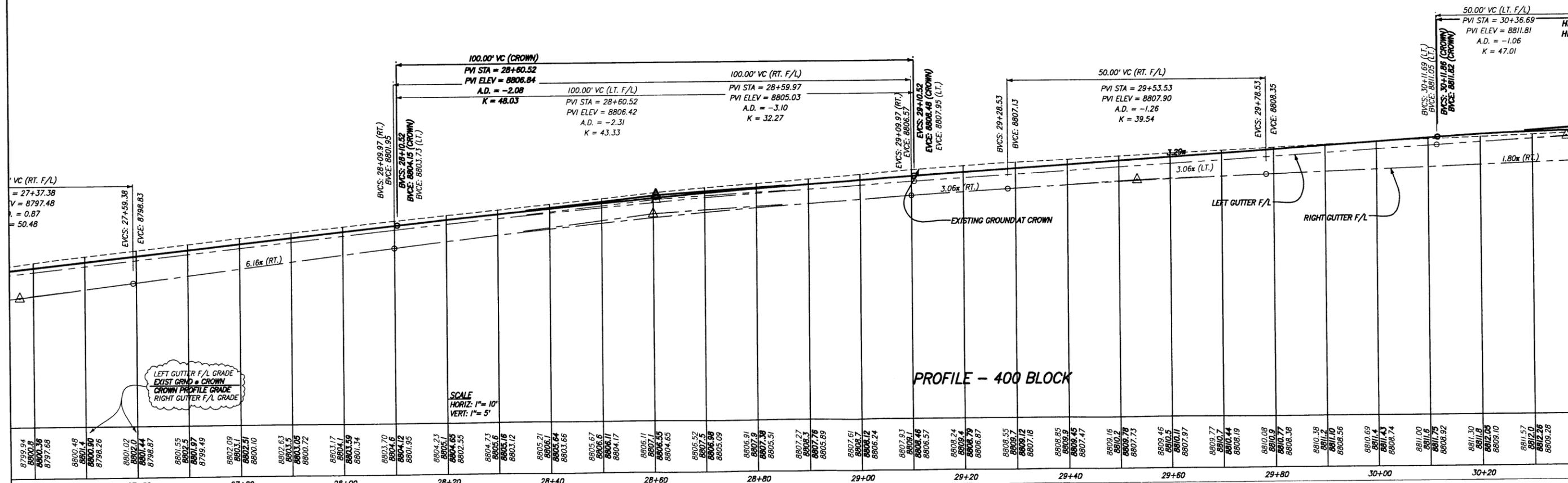
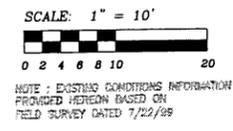
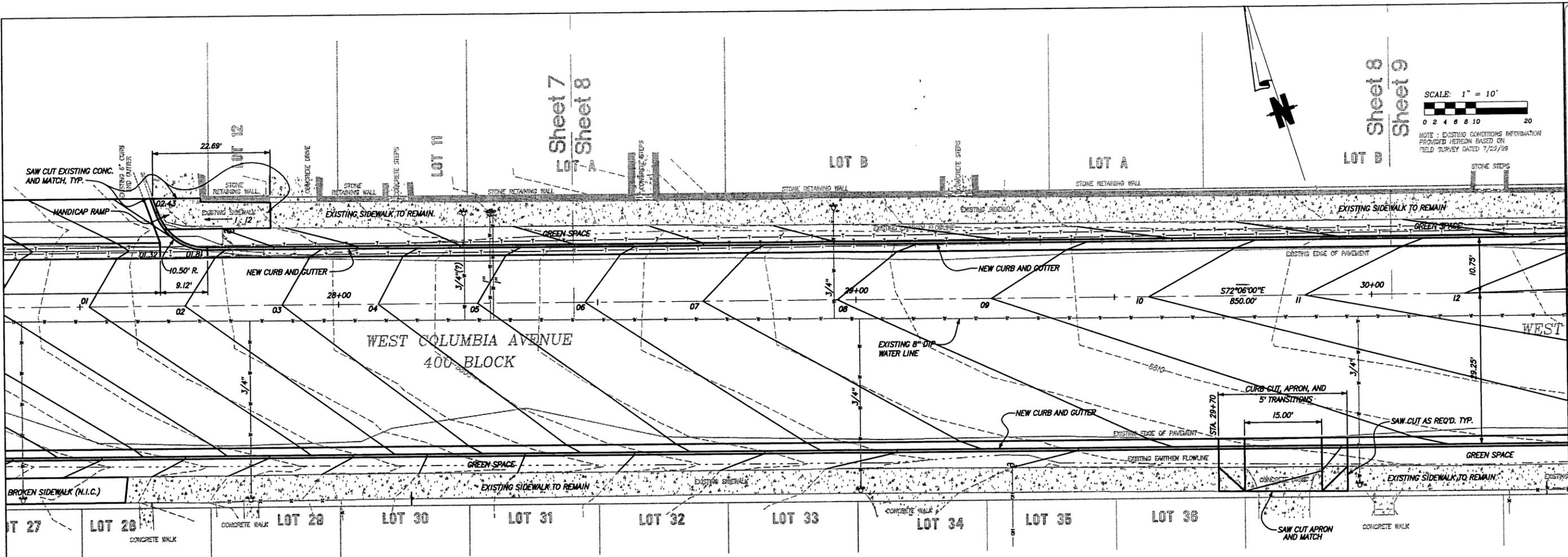
BUCKHORN GEOTECH
Civil, Structural, and Geotechnical Engineers, Inc.
222 South Park Avenue
Montrose, Colorado 81401
Phone (970) 249-6628 Fax (970) 249-0945

FOLEY ASSOCIATES, INC.
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Telluride, Colorado 81435
Phone 970-728-6153 Fax 970-728-6050
e-mail to xsmall@foleyassoc.com

TELLURIDE STREETSAPES - PHASE IIA
WEST COLUMBIA AVENUE PLAN AND PROFILE
STA. 26+00 TO STA. 27+50 (400 BLOCK)

SEE NORTH TOWNSEND STREET
200 BLOCK

D:\Land Projects\3\SSc-Phase2A\dwg\Phase2a-7-10.dwg, 05/15/02 09:18:02 AM, HP LaserJet 5000 PCL 6 (temporary)0491.pcl



TELLURIDE STREETSCAPES - PHASE IIA
 WEST COLUMBIA AVENUE PLAN AND PROFILE
 STA. 27+50 TO STA. 30+00 (400 BLOCK)

Contact:
 William Franzler
 300 Black Bear Road
 Telluride, Colorado 81435

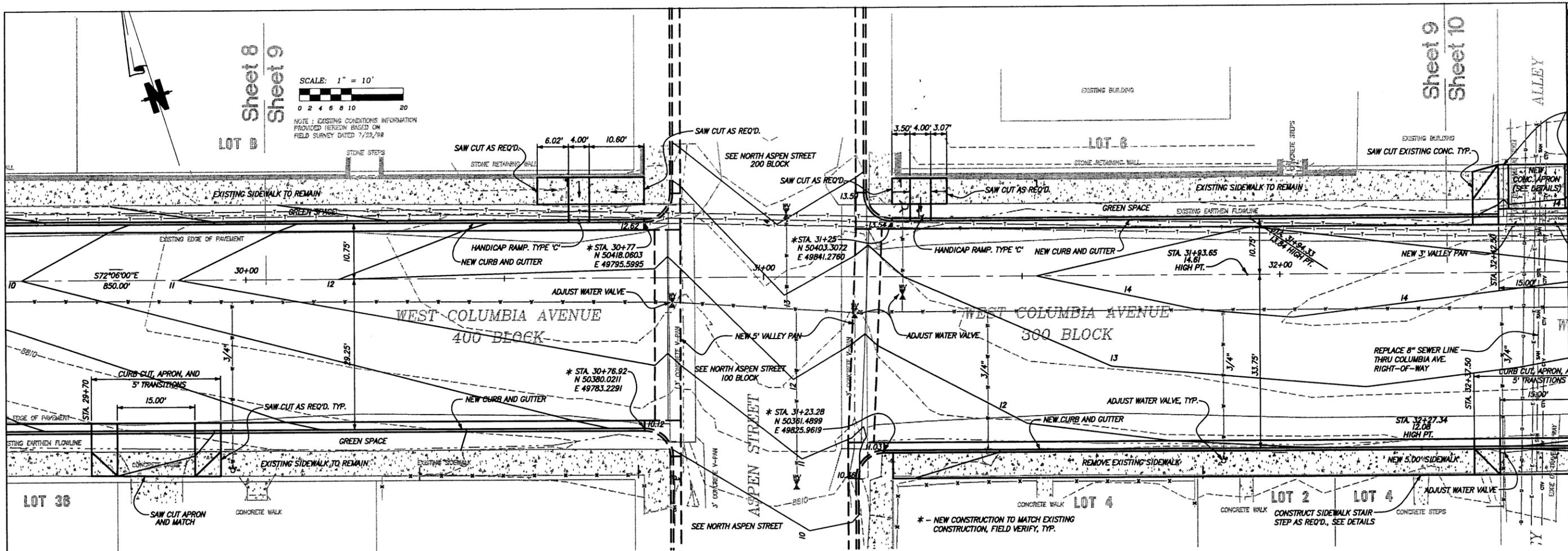
Client:
 Town of Telluride, Colorado
 P.O. Box 137
 Telluride, Colorado 81435

FOLEY ASSOCIATES, INC.
 ENGINEERING - PLANNING - SURVEYING
 PO Box 1385, 125 W. Pacific Ave., Suite B1
 Telluride, Colorado 81435
 Phone 970-728-6153 Fax 970-728-6050
 e-mail to: a.mall@foley-assoc.com

BUCKHORN GEOTECH
 Civil, Structural, and Geotechnical Engineers, Inc.
 222 South Park Avenue
 Montrose, Colorado 81401
 Phone (970) 249-8828 Fax (970) 249-0945

Project Mgr: ELK
 Drawn by: DJP
 Checked by: ELK
 Start date: 06-20-01
 Dwg path: PHASE2A-7-10
 Sheet No. **8**
 Project #: 99102

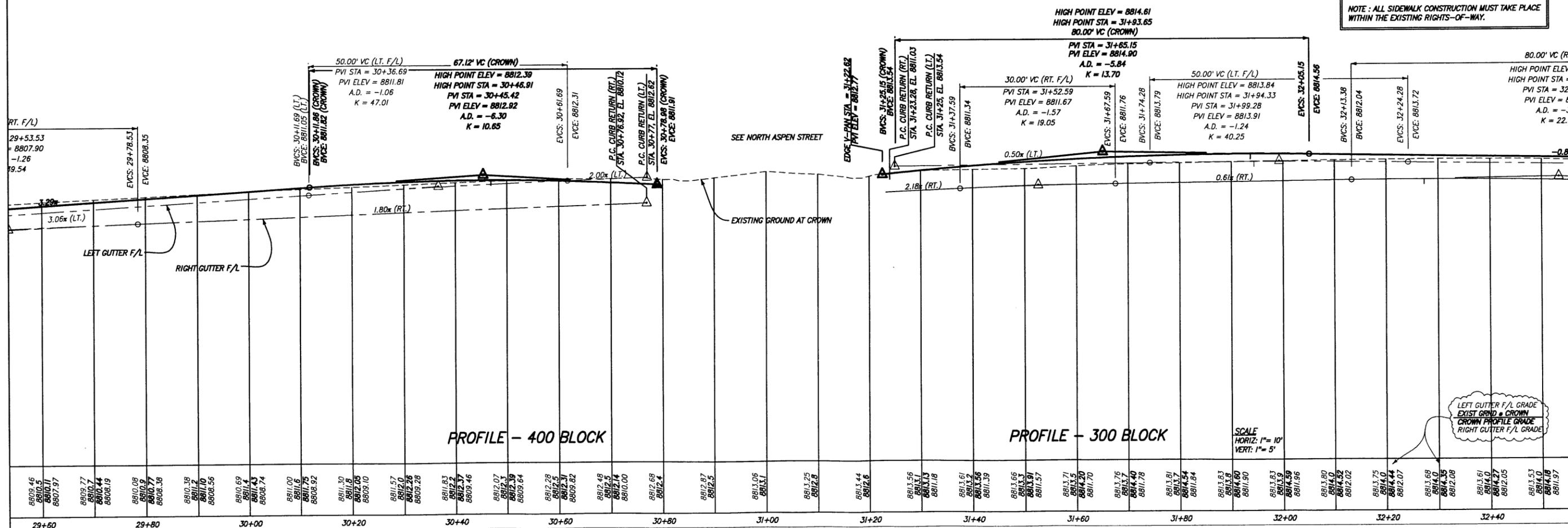
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SCALE: 1" = 10'
 0 2 4 6 8 10 20
 NOTE: EXISTING CONDITIONS INFORMATION PROVIDED HEREIN BASED ON FIELD SURVEY DATED 7/22/99

Sheet 8
 Sheet 9

Sheet 9
 Sheet 10



PROFILE - 400 BLOCK

PROFILE - 300 BLOCK

SCALE
 HORIZ: 1" = 10'
 VERT: 1" = 5'

NOTE: ALL SIDEWALK CONSTRUCTION MUST TAKE PLACE WITHIN THE EXISTING RIGHTS-OF-WAY.

LEFT GUTTER F/L GRADE
 EXIST GROUND & CROWN
 CROWN PROFILE GRADE
 RIGHT GUTTER F/L GRADE

TELLURIDE STREETS CAPES - PHASE IA
 WEST COLUMBIA AVENUE PLAN AND PROFILE
 STA. 30+00 TO 32+50 (400 AND 300 BLOCKS)

Contact:
 William Froumiller
 300 Blaine Road
 Telluride, Colorado 81435

Client:
 Town of Telluride, Colorado
 P.O. Box 387
 Telluride, Colorado 81435

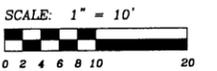
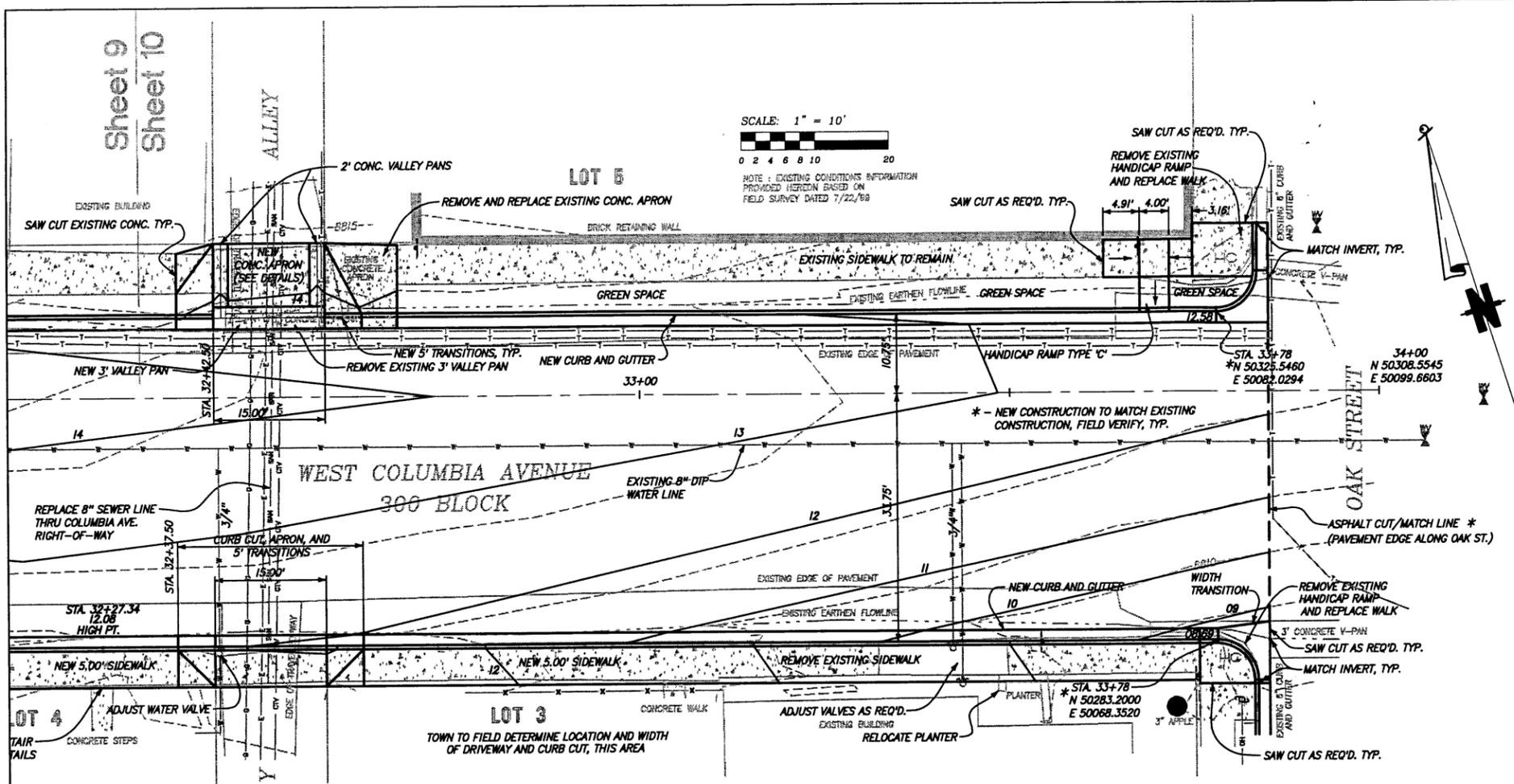
FOLEY ASSOCIATES, INC.
 ENGINEERING - PLANNING - SURVEYING
 PO Box 1385, 105 W. Pacific Ave., Suite B1
 Telluride, Colorado 81435
 phone 970-728-6153 fax 970-728-6000
 e-mail to: small@foleyassoc.com

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 222 South Park Avenue
 Montrose, Colorado 81401
 Phone (970) 249-6628 Fax (970) 249-0945

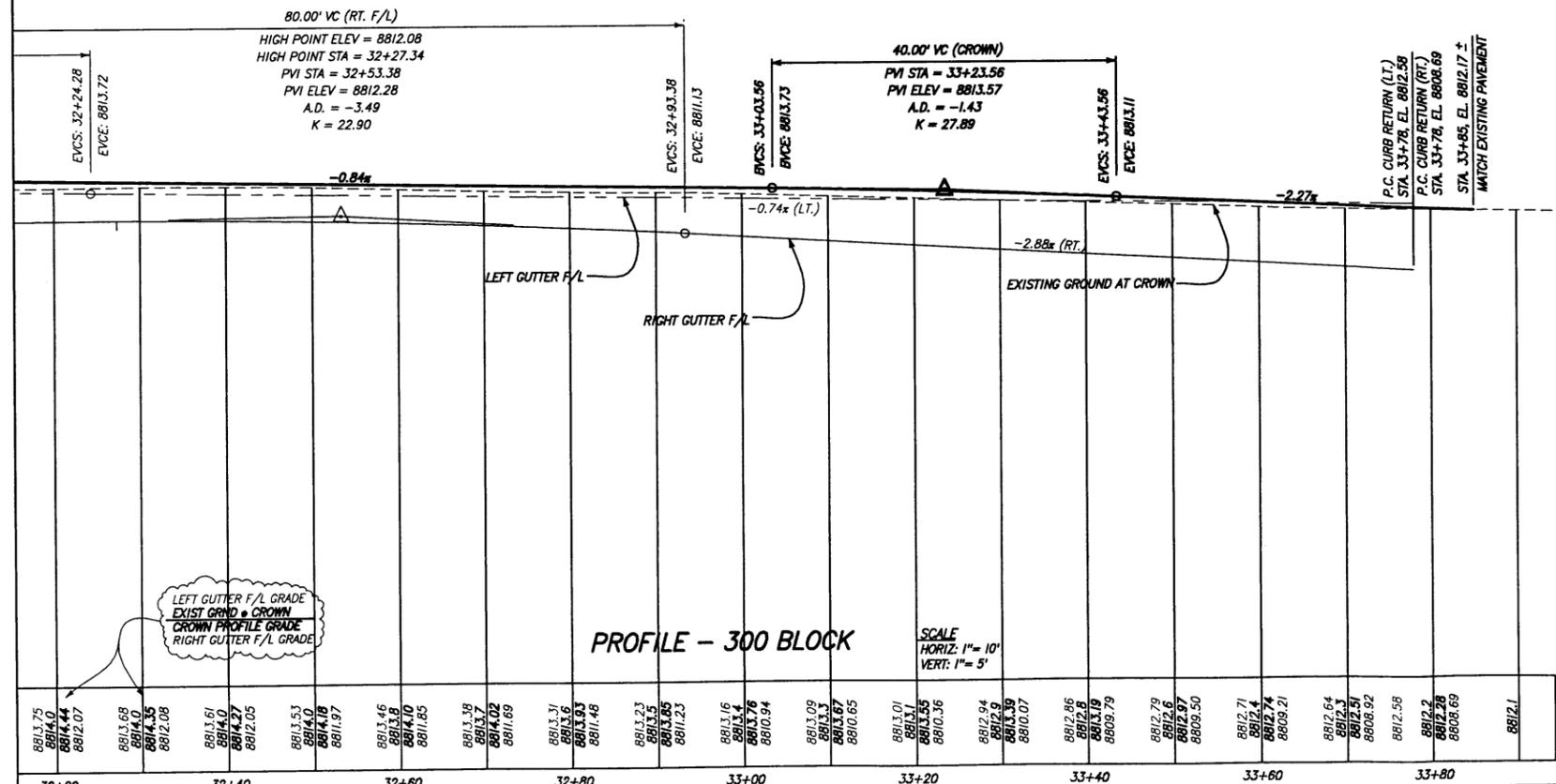
Rev.	Description	date	by	check
1	RELOCATED SOUTH GUTTER AND SIDEWALK (300 BLOCK)	4-24-01	DJP	
2	ADDED RIGHT-OF-WAY NOTE	1-18-02	DJP	

Project Mgr: ELK
 Drawn by: DJP
 Checked by: ELK
 Start date: 06-20-01
 Dwg path: PHASE2A-7-10
 Sheet No. **9**
 Project #: 99102

Sheet 9
Sheet 10

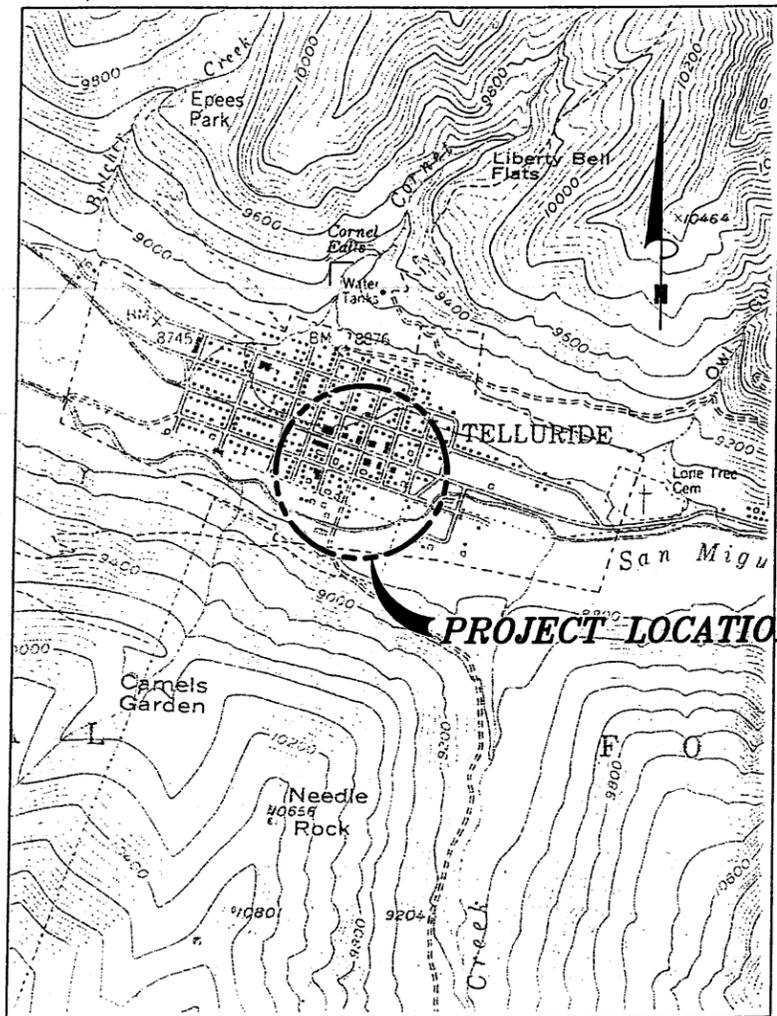


SCALE: 1" = 10'
0 2 4 6 8 10 20
NOTE: EXISTING CONDITIONS INFORMATION PROVIDED HEREON BASED ON FIELD SURVEY DATED 7/22/99



**TELLURIDE STREETSAPES – WAREHOUSE DISTRICT
STREET IMPROVEMENT CONSTRUCTION PLANS
TOWN OF TELLURIDE
SAN MIGUEL COUNTY, COLORADO
BUCKHORN GEOTECH, INC. PROJECT # 00-098**

MARCH 11, 2004



VICINITY MAP

INDEX TO DRAWINGS

- 1 COVER SHEET
- 2 NOTES & LEGEND
- 3 SCHEMATIC TYPICAL STREET CROSS SECTIONS
- 4 SOUTH FIR STREET P & P, 300 BLOCK
- 5 SOUTH FIR STREET P & P, 200 BLOCK
- 6 SOUTH PINE STREET P & P, 300 BLOCK
- 7 SOUTH PINE STREET P & P, 200 BLOCK
- 8 SOUTH SPRUCE STREET P & P, 200 BLOCK
- 9 SOUTH SPRUCE STREET P & P, 100 BLOCK
- 10 SOUTH WILLOW STREET P & P, 200 BLOCK
- 11 SOUTH WILLOW STREET P & P, 100 BLOCK
- 12 WEST SAN JUAN AVENUE P & P, 200 BLOCK
- 13 WEST SAN JUAN AVENUE P & P, 100/200 BLOCKS
- 14 WEST SAN JUAN AVENUE P & P, 100 BLOCK
- 15 WEST PACIFIC AVENUE P & P, 200 BLOCK
- 16 WEST PACIFIC AVENUE P & P, 100 BLOCK
- 17 EAST PACIFIC AVENUE P & P, 100 BLOCK
- 18 EAST PACIFIC AVENUE P & P, 200 BLOCK
- 19 EAST PACIFIC AVENUE P & P, 300 BLOCK
- 20 PROJECT DETAILS
- 21 PROJECT DETAILS
- 22 DAILY PLANET BUILDING
RETAINING WALL, STEPS, AND RAMP DETAILS
- 23 PRECAST CONCRETE VALLEY INLETS
- 24 PRECAST CONCRETE CURB INLETS
- 25 AH-HAA SCHOOL BUILDING STEPS DETAILS
- 26 ALTERNATE SUBGRADE TREATMENT SECTION

APPROVED BY STREETSAPES TASK FORCE :

CHAIR _____ DATE _____

APPROVED BY THE TOWN OF TELLURIDE :

TOWN ENGINEER _____ DATE _____

BUCKHORN GEOTECH

Civil, Structural, and Geotechnical Engineers, Inc.
222 South Park Avenue
Montrose, Colorado 81401
Phone (970) 249-6828 Fax (970) 249-0945

**PROJECT MANAGER
AND ENGINEER**

**FOLEY
ASSOCIATES, INC.**

ENGINEERING · PLANNING · SURVEYING
PO Box 1365 125 W. Pacific Ave. Suite B1
Telluride, Colorado, 81435
phone 970-728-6133 fax 970-728-6050
e-mail to smail@foleyassoc.com

PROJECT SURVEYOR

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1-800-922-1987

CALL 2-BUSINESS DAYS IN ADVANCE
BEFORE YOU DIG, GRADE, OR EXCAVATE
FOR THE MARKING OF UNDERGROUND
MEMBER UTILITIES

Abbreviations

A	-ANCHOR	M.H.	-MANHOLE
A.B.	-ANCHOR BOLT	MATL.	-MATERIAL
A.D.	-ALGEBRAIC DIFFERENCE	MAX.	-MAXIMUM
A.F.F.	-ABOVE FINISHED FLOOR	MFR.	-MANUFACTURER
A.F.G.	-ABOVE FINISHED GRADE	M.S.	-MILD STEEL
ANCH.	-ANCHOR	M.G.D.	-MILLION GALLONS PER DAY
L	-ANGLE	MIN.	-MINIMUM
APA	-AMERICAN PLYWOOD ASSOC.	MISC.	-MISCELLANEOUS
APPD.	-APPROVED	M.P.H.	-MILES PER HOUR
APPROX.	-APPROXIMATE	MTL.	-METAL
#	-AND	NEMA	-NATIONAL ELECTRIC MFR ASSOC.
AWWA	-AMERICAN WATER WORKS ASSOC.	NFPA	-NATIONAL FIRE PROTECTION ASSOC.
B.F.	-BLIND FLANGE	N.L.C.	-NOT IN CONTRACT
BFV	-BUTTERFLY VALVE	N.P.T.	-NATIONAL PIPE THREAD
BLDG.	-BUILDING	N.R.S.	-NON RISING STEM
BLK.	-BLOCK	N.	-NORTH
BLM.	-BENCH MARK	NOM.	-NOMINAL
B.O.F.	-BOTTOM OF FOOTING	N.S.	-NON-SHRINK
B.O.P.	-BEGINNING OF PROJECT	N.T.S.	-NOT TO SCALE
B.O.W.	-BOTTOM OF WALL	No.	-NUMBER
BP	-BASE PLATE		
BRG.	-BEARING	O.C.	-ON CENTER
BOTT.	-BOTTOM	O.D.	-OVERFLOW DRAIN or OUTSIDE DIAMETER
BTMNL.	-BETWEEN	O.F.	-OUTSIDE FACE
B.T.L.	-BRITISH THERMAL UNIT	O.H.	-OPPOSITE HAND or OVERHANG
B.U.	-BELL UP	OPNG.	-OPENING
BVCE	-BEGIN VERTICAL CURVE ELEVATION	OPP.	-OPPOSITE
BVCS	-BEGIN VERTICAL CURVE STATION	O.S. & Y.	-OUTSIDE SCREW AND YOKE
CBC	-CONCRETE BOX CULVERT		
C.D.O.T.	-COLORADO DEPARTMENT OF TRANSPORTATION	P.C.	-POINT OF CURVE
CFM	-CUBIC FEET PER MINUTE	P.C.C.	-POINT OF COMPOUND CURVE
CFS	-CUBIC FEET PER SECOND	P.E.	-POLYETHYLENE
E	-CENTERLINE	PL	-PLATE
C.L.P.	-CAST IN PLACE or CAST IRON PIPE	%	-PERCENT
CJ	-CONSTRUCTION JOINT	PENETR.	-PENETRATION
C.O.	-CLEAN OUT	PERIM.	-PERIMETER
C.R.	-CLEAR	P.I.	-POINT OF INTERSECTION
CUP	-CORRUGATED METAL PIPE	PNT	-POINT
COL.	-COLUMN	P.O.J.	-POINT ON JOINT
CONC.	-CONCRETE	P.R.C.	-POINT OF REVERSE CURVE
CONN.	-CONNECTION	PRELIM.	-PRELIMINARY
CONSTR.	-CONSTRUCTION	PROJ.	-PROJECTION
CONT.	-CONTINUOUS	P.S.	-PIPE SUPPORT
CONTR.	-CONTRACTOR	PSI	-POUNDS PER SQUARE INCH
COORD.	-COORDINATE	P.T.	-POINT OF TANGENCY
CONTR.	-CENTERED	P.V.C.	-POLYVINYL CHLORIDE
CRNL.	-CROWN	P.V.L.	-POINT OF VERTICAL INTERSECTION
CSP	-CORRUGATED STEEL PIPE	QTY.	-QUANTITY
C & G	-CURB & GUTTER		
Cu	-COPPER	R	-RADIUS
C.Y.	-CUBIC YARD	RCP	-REINFORCED CONCRETE PIPE
DET.	-DETAIL	RD	-ROAD
DC	-DEGREE OF CURVE	RED.	-REDUCER
D.I.	-DROP INLET	REF.	-REFERENCE (REFER TO)
DIA.(OR Ø)	-DIAMETER	REIN.	-REINFORCING
DIAG.	-DIAGONAL	REM.	-REMAINDER
DIM.	-DIMENSION	REQD.	-REQUIRED
D.I.P.	-DIPPLE IRON PIPE	REQUIT.	-REQUIREMENT
DIST.	-DISTANCE	RPM	-REVOLUTIONS PER MINUTE
DWG.	-DRAWING	RT.	-RIGHT
DWL	-DOWEL	S.	-SOUTH
E	-EAST	SCHED.	-SCHEDULE
EA.	-EACH	SECT.	-SECTION
E.F.	-EACH FACE	SHT.	-SHEET
EL. or ELEV.	-ELEVATION	SIML.	-SIMILAR
ELEC.	-ELECTRICAL	SJ	-SHOWN JOINT (CONTROL JOINT)
ELL	-ELBOW	SP.	-SPACE
ENGR.	-ENGINEER	S.P.D.	-STANDARD PROCTOR DENSITY
E.O.A.	-EDGE OF ASPHALT	SPEC.	-SPECIFICATIONS
E.O.P.	-EDGE OF PROJECT	SQL.	-SQUARE
E.O.R.	-EDGE OF ROAD	S.S.	-STAINLESS STEEL
EPS	-EXPANDED POLY-STYRENE	STA.	-STATION
EQ.	-EQUIL.	STD.	-STANDARD
EYCE	-END VERTICAL CURVE ELEVATION	STIFF.	-STIFFENER
EYCS	-END VERTICAL CURVE STATION	STGR.	-STAGGERED
EXST.	-EXISTING	STL.	-STEEL
EXP.	-EXPANSION	STR.	-STRUT
EXT.	-EXTERIOR	STRUCT.	-STRUCTURAL
E.W.	-EACH WAY	SUPT.	-SUPPORT
FAB.	-FABRICATE	S.Y.	-SQUARE YARD
FPS	-FEET PER SECOND	SYMM.	-SYMMETRICAL
FIN GRD	-FINISH GRADE	S/W	-SIDEWALK
F.H.	-FIRE HYDRANT	T	-TANGENT
FD	-FLOOR DRAIN	TB	-THRUST BLOCK
FLG	-FLANGE	TBM	-TEMPORARY BENCH MARK
FES	-FLARED END SECTION	T & B	-TOP AND BOTTOM
FDN.	-FOUNDATION	T & G	-TONGUE & GROOVE
FL or F/L	-FLOW LINE	TEMP.	-TEMPERATURE
FT.	-FOOT	T.H.	-TEST HOLE
FTG.	-FOOTING	T.O.S.C.	-TOP OF STRUCTURAL CONCRETE
		T.O.C.	-TOP OF CURB
GA.	-GAGE (OR GAUGE)	T.O.F.	-TOP OF FOOTING
GN	-GALLONS PER MINUTE	T.O.P.	-TOP OF PIPE
GALV.	-GALVANIZED	T.O.S.	-TOP OF STEEL
G	-GAS	T.O.W.	-TOP OF WALL
G.B.	-GRADE BREAK	T.P.	-TEST PIT
G.L.	-GLULAM	TRANSV.	-TRANSVERSE
GR.	-GRADE	TS	-TUBULAR STEEL
GRBM.	-GRADE BEAM	TYP.	-TYPICAL
GV	-GATE VALVE		
H.C.	-HANDICAP	U.B.C.	-UNIFORM BUILDING CODE
HR.	-HANGER	U.F.C.	-UNIFORM FIRE CODE
HT.	-HEIGHT	U.M.C.	-UNIFORM MECHANICAL CODE
H.P.	-HIGH POINT or HORSE POWER	U.N.O.	-UNLESS NOTED OTHERWISE
H.S.	-HIGH STRENGTH	U.P.C.	-UNIFORM PLUMBING CODE
H.W.L.	-HIGH WATER LEVEL		
HORIZ.	-HORIZONTAL	V	-VELOCITY
H.B.	-HOSE BIB	v	-VOLT
		VAR.	-VARIES
IN.	-INCH	VERT.	-VERTICAL
INFO.	-INFORMATION	V.C.	-VERTICAL CURVE
I.D.	-INSIDE DIAMETER	V.P.I.	-VERTICAL POINT OF INTERSECTION
INV.	-INVERT	V.P.T.	-VERTICAL POINT OF TANGENCY
IPS	-IRON PIPE SIZE	V.P.	-VALLEY PAN
		W or WTR	-WATER
JST.	-JOIST	W.	-WEST
JT.	-JOINT	W/	-WITH
L	-LENGTH	WD.	-WOOD
LT.	-LEFT	W.O.G.	-WATER, OIL, GAS
LAT.	-LATERAL	W/O	-WITHOUT
LBS.	-POUNDS	W.P.	-WORK POINT
LDGR.	-LEADER	WT.	-WEIGHT
LI.	-LINE LOAD	W.W.F.	-WELDED WIRE FABRIC
LH	-LONG LEG HORIZONTAL		
LLY	-LONG LEG VERTICAL		
LDNG.	-LONGITUDINAL		
L.P.	-LOW POINT		
L.W.L.	-LOW WATER LEVEL		
L.P.G.	-LIQUID PETROLEUM GAS		

Legend

Handicap Ramp	Existing Retaining Wall
Water Valve	Existing Fence
Tree	Existing Index Contour
Utility Pole	Existing Intermediate Contour
Sanitary Manhole	Overhead Utility Line (existing)
Parking Meter	Sanitary Sewer (existing)
Water Service Valve	Water (existing)
Cable Television Pedestal	Cable TV (subsurface, existing)
Fire Hydrant	Telephone (subsurface, existing)
Sign	Power (subsurface, existing)
Telephone Pedestal	Gas (subsurface, existing)
Transformer	Edge of Travelled Way (existing)
Existing Streetlamp	Edge of Pavement (existing)
Proposed Standard Streetlamp	Proposed crown of street
	Proposed finished contour, interval noted

87.53 • Proposed Spot Elevation of Flowline of Gutter (Unless noted otherwise)

List of Agencies

TOWN HALL	113 W. COLUMBIA AVE.	(970) 728-3071
TELLURIDE MAINTENANCE FACILITY		(970) 728-3516
GAS	KN ENERGY	(800) 563-0012
ELECTRIC	SAN MIGUEL POWER	(970) 728-3825
TELEPHONE	U S WEST (REPAIR)	(800) 954-1211
SHERIFF'S DEPARTMENT		(970) 728-3081
FIRE DEPARTMENT		(970) 728-3801
MARSHAL'S OFFICE		(970) 728-3818
ADELPHIA	CABLE TELEVISION	(800) 626-6299
TELLURIDE CABLEVISION	CABLE TELEVISION	(970) 728-4436

General Notes

- ALL EXISTING SIGNAGE TO BE RELOCATED BY THE TOWN OF TELLURIDE. SIGNAGE SHALL BE IN ACCORDANCE WITH THE TOWN OF TELLURIDE STANDARDS. ALL CONSTRUCTION TRAFFIC CONTROL SIGNAGE IS THE RESPONSIBILITY OF THE CONTRACTOR, IN ACCORDANCE WITH MUTCD.
- ALL WATER AND SEWER SYSTEMS RELOCATIONS SHALL BE MADE BY TOWN FORCES AS REQUIRED.
- ALL WORK TO BE DONE IN ACCORDANCE WITH THE TOWN OF TELLURIDE, COLORADO STANDARD SPECIFICATIONS, AS CURRENTLY ADOPTED.
- DIMENSIONS SHOWN ON PLANS WITH AN ASTERISK (*) ARE APPROXIMATE AND SHALL BE FIELD VERIFIED.
- CONTRACTOR SHALL BE RESPONSIBLE FOR ADJUSTING THE ELEVATIONS OF ALL VALVE RISERS, MANHOLES, AND OTHER UTILITY APPURTENANCES AS REQUIRED TO ACHIEVE FINISHED GRADES SHOWN ON THE DRAWINGS. THE DRAWINGS MAY NOT SHOW LOCATIONS OF ALL EXISTING APPURTENANCES.
- RELOCATION OF UTILITY POLES, PEDESTALS, TRANSFORMERS, ETC. SHALL BE COORDINATED WITH THE TOWN OF TELLURIDE PUBLIC WORKS AND THE APPROPRIATE UTILITY COMPANY. RELOCATIONS SHALL BE VERIFIED IN THE FIELD.
- UNLESS OTHERWISE NOTED, CONTRACTOR SHALL REMOVE AND DISPOSE OF ALL EXISTING ASPHALT PAVEMENT MATERIALS, CONCRETE VALLEY PANS, CURB AND GUTTER, ALLEY AND DRIVEWAY RAMPS, SIDEWALKS, AND OTHER FEATURES LOCATED WITHIN THE PROPOSED STREET SECTIONS. CONTRACTORS' ACTIVITIES SHALL BE LIMITED TO THE PUBLIC RIGHT-OF-WAY, EXCEPT WHERE NOTED ON THESE DRAWINGS.
- NEW CURB CUTS SHALL MATCH DEVELOPMENT PLANS.
- CONTRACTOR SHALL PROTECT EXISTING STRUCTURES AND OTHER ENCROACHMENTS INTO THE RIGHT-OF-WAY FROM HARM DURING CONSTRUCTION.
- CONTRACTOR SHALL HAVE ONE SET OF SIGNED APPROVED PLANS ON THE JOB SITE AT ALL TIMES DURING CONSTRUCTION.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE LOCATION OF ALL EXISTING UTILITIES, INCLUDING ANY UTILITIES NOT SHOWN ON THE DRAWINGS. WHEN WORKING NEAR EXISTING UTILITIES, THE CONTRACTOR SHALL EXERCISE SUFFICIENT CARE TO PREVENT DAMAGE TO THE LINES IN THE EVENT THAT THE INFORMATION SHOWN ON THE DRAWINGS DOES NOT REFLECT ACTUAL FIELD CONDITIONS.
- BENCHMARK: HORIZONTAL AND VERTICAL CONTROL BY FOLEY ASSOCIATES.
- ALL CONCRETE AND ASPHALT CUTTING SHALL BE DONE TO CLEAN, NEAT EDGES.
- INSTALLATION OF STREETLAMPS AND BASES SHALL BE IN ACCORDANCE WITH TOWN OF TELLURIDE STANDARD SPECIFICATIONS OR AT THE TOWN'S DIRECTION.
- ALL SIDEWALK CONSTRUCTION MUST TAKE PLACE WITHIN THE EXISTING RIGHTS-OF-WAY.

CALL UTILITY NOTIFICATION CENTER OF COLORADO
1-800-922-1987
 CALL 2-BUSINESS DAYS IN ADVANCE BEFORE YOU DIG, GRADE, OR EXCAVATE FOR THE MARKING OF UNDERGROUND MEMBER UTILITIES

TELLURIDE STREETSAPES - WAREHOUSE DISTRICT
 GENERAL NOTES

Contact:
 Karen Williams
 300 Black Bear Road
 Telluride
 Colorado 81435

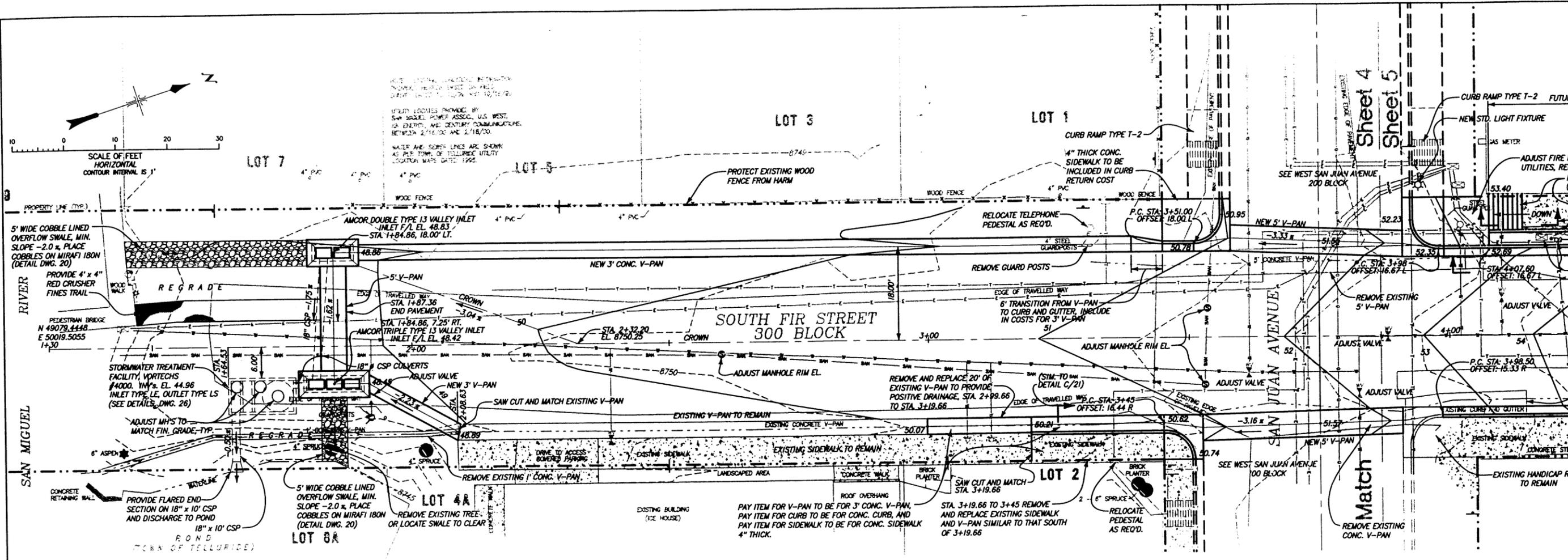
Client:
 Town of Telluride, Colorado
 P.O. Box 387
 Telluride
 Colorado 81435

FOLEY ASSOCIATES, INC.
 ENGINEERING - PLANNING - SURVEYING
 PO Box 188, 108 W. Pacific Ave., Suite B1
 Telluride, CO 81435
 Phone 970-728-6133 Fax 970-728-6050
 e-mail to info@foleyassoc.com

BUCKHORNTECH
 Civil, Structural, and Geotechnical Engineers, Inc.
 222 South Park Avenue
 Montrose, Colorado 81401
 Phone (970) 249-8828 Fax (970) 249-0645

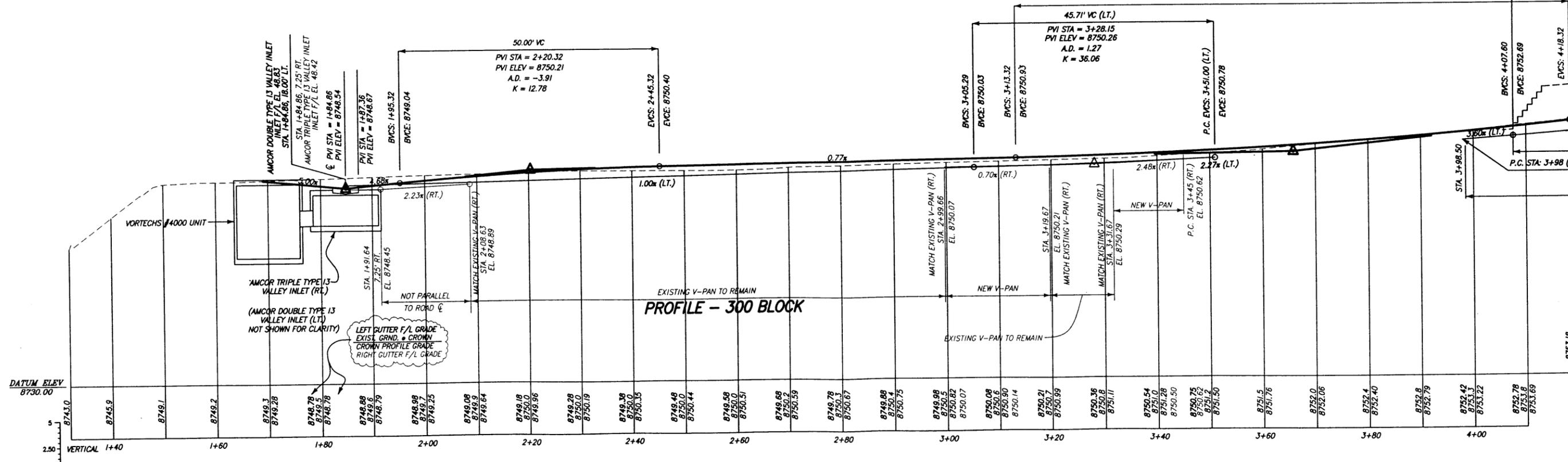
By	
Date	
Description	
Rev.	
Project Mgr:	JG
Drawn by:	DJP
Checked by:	ELK
Start date:	3-11-04
Dwg path:	3NOTEPAG.DWG
Sheet No.	2
Project #:	00-098

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Project ACQ M260--015//14768

PVI STA = 3+65.82
 PVI ELEV = 8751.33
 A.D. = 4.54
 K = 23.14
 105.00' VC



TELLURIDE STREETSCAPES - WAREHOUSE DISTRICT
 SOUTH FIR STREET PLAN AND PROFILE
 STA. 1+50 TO STA. 4+00 (300 BLOCK)

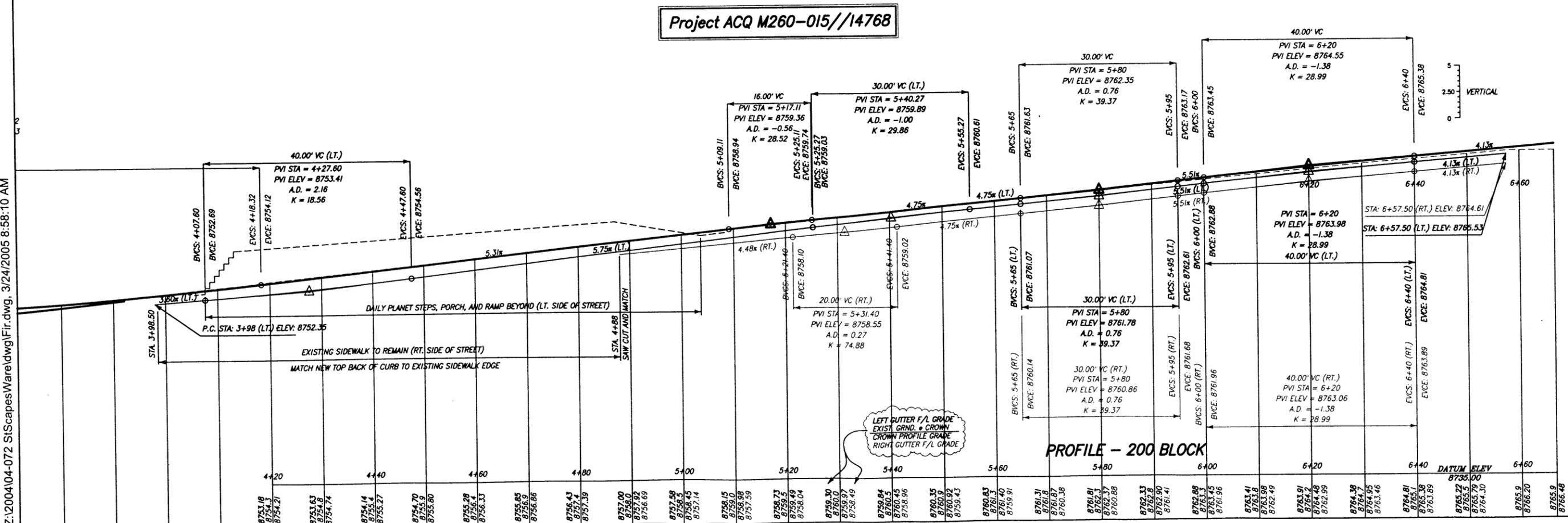
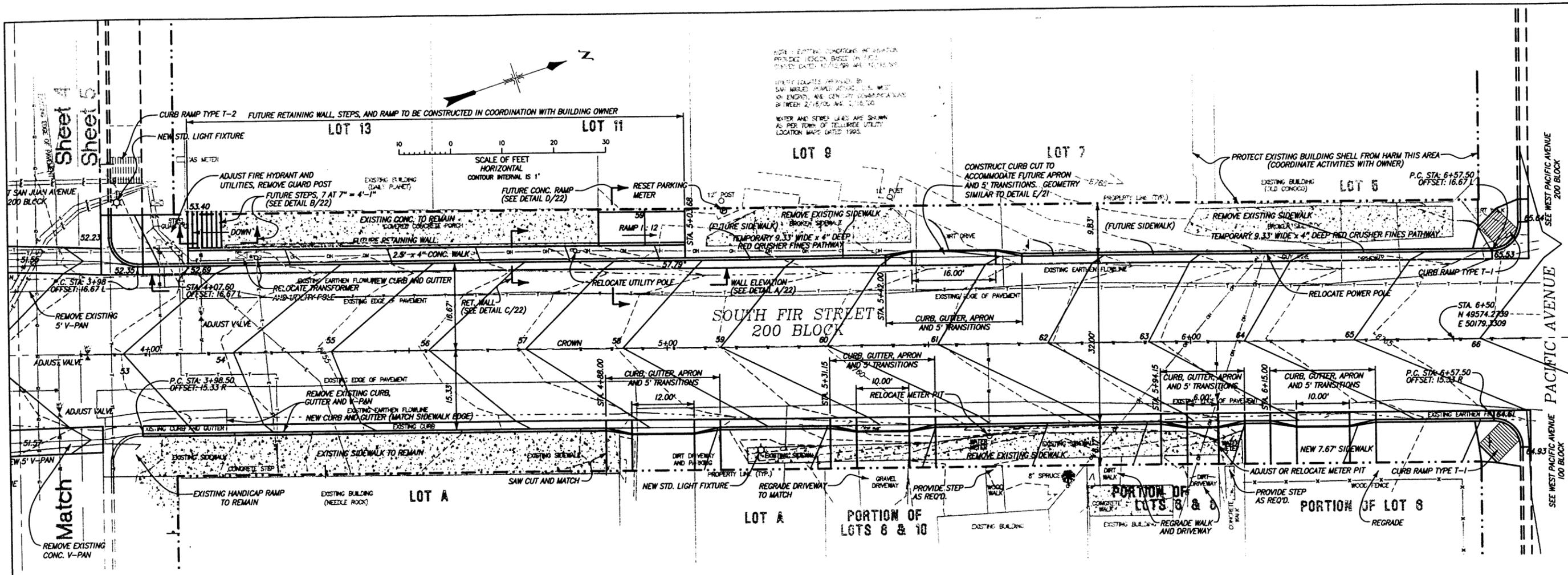
Client: Town of Telluride, Colorado
 Contact: Karen Guglielmo, 1370 Black Bear Road, Telluride, Colorado 81435

FOLEY ASSOCIATES, INC.
 ENGINEERING - PLANNING - SURVEYING
 PO Box 1881, 125 W. Pacific Ave. Suite B1, Telluride, Colorado 81405
 Phone (970) 249-6033, Fax (970) 249-6050, Email to: small@foleyassoc.com

BUCKHORN GEOTECH
 Civil, Structural, and Geotechnical Engineers, Inc.
 222 South Colorado, Suite 100, Montrose, Colorado 81401
 Phone (970) 249-8628, Fax (970) 249-0945

Project Mgr: ELK
 Drawn by: DWP
 Checked by: ELK
 Start date: 7-19-04
 Dwg path: FIR.DWG
 Sheet No. **4**
 Project #: 04-072

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TELLURIDE STREETSCAPES - WAREHOUSE DISTRICT
SOUTH FIR STREET PLAN AND PROFILE
 STA. 4+00 TO STA. 6+65 (200 BLOCK)

Contact:
 Korey Cunningham
 1370 West Pacific Road
 Telluride Colorado 81435

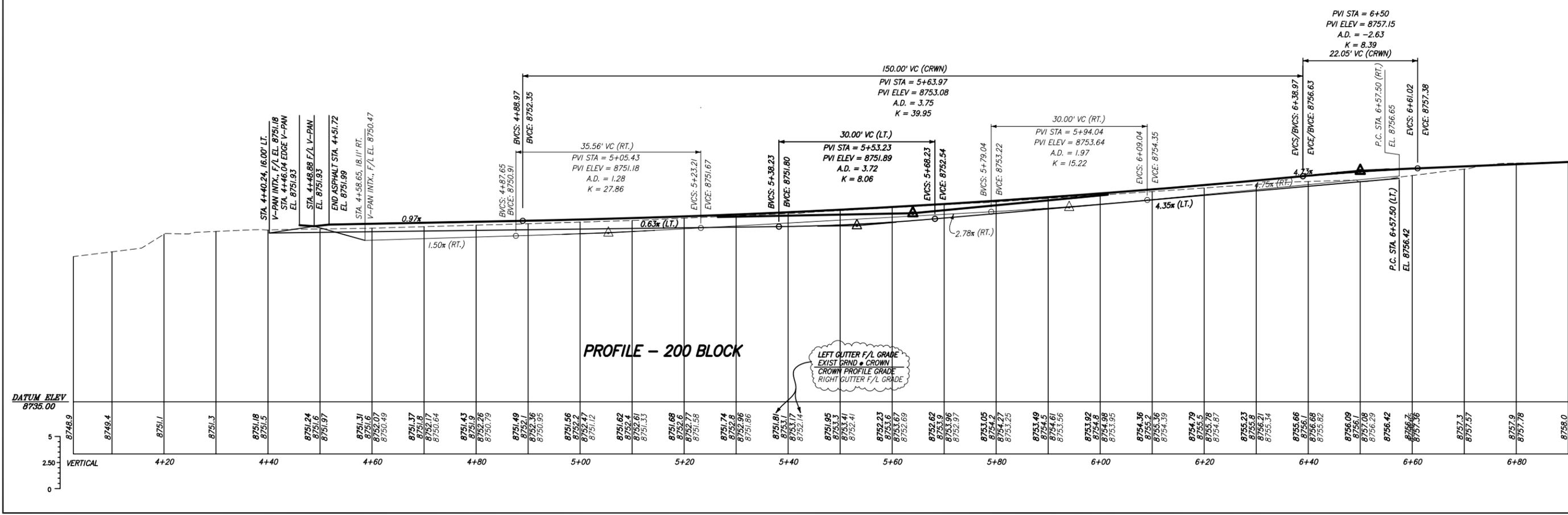
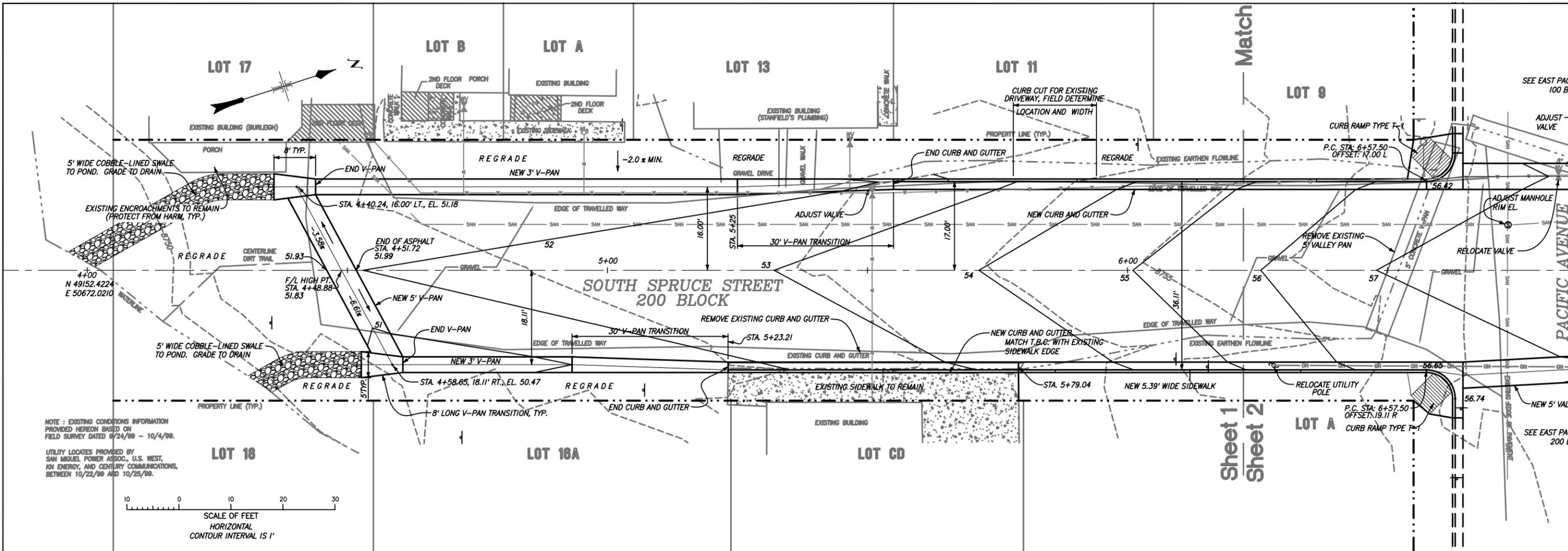
Client:
 Town of Telluride, Colorado
 Planning Dept., 387
 Pacific Avenue
 Telluride Colorado 81435

FOLEY ASSOCIATES, INC.
 ENGINEERING - PLANNING - SURVEYING
 1010 Br. 1385, 125 W. Pacific Ave., Suite B1
 Telluride, Colorado 81435
 Phone: 970-728-6153 Fax: 970-728-6050
 e-mail: info@foleyassoc.com

BUCKHORNTECH
 Civil, Structural, and Geotechnical Engineers, Inc.
 222 South Park Avenue
 Montrose, Colorado 81401
 Phone (970) 249-8828 Fax (970) 249-0945

REV.	DATE	DESCRIPTION
1	3-25-05	D.P.

Project Mgr: ELK
 Drawn by: DJP
 Checked by: ELK
 Start date: 7-19-04
 Dwg path: FIR.DWG
 Sheet No. **5**
 Project #: 04-072



TELLURIDE STREETSAPES - WAREHOUSE DISTRICT
SOUTH SPRUCE STREET PLAN AND PROFILE
STA. 4+00 TO STA. 6+50 (200 BLOCK)

Client: Karen Gagliemone, 300 Pacific Avenue, Telluride, Colorado 81435
Town of Telluride, P.O. Box 397, Telluride, Colorado 81435

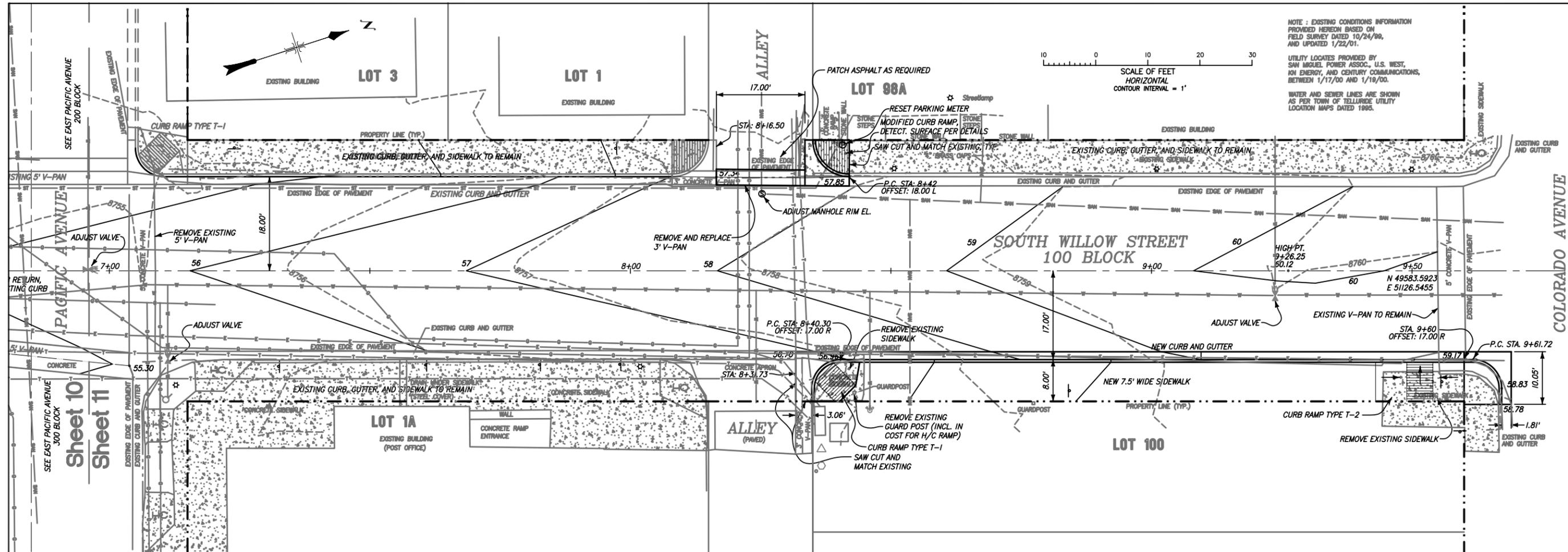
Client: PO Box 1385, 125 W. Pacific Ave. Suite B1, Telluride, Colorado, 81435
phone 970-258-6133, fax 970-258-6130, e-mail to vsmain@foleyassoc.com

FOLEY ASSOCIATE, INC.
ENGINEERING - PLANNING - SURVEYING

BUCKHORN GEOTECH
Civil, Structural, and Geotechnical Engineers, Inc.
222 South Park Avenue, Montrose, Colorado 81401
Phone (970) 249-8828 Fax (970) 249-0946

Rev.	date	description
8756.0		
8757.0		

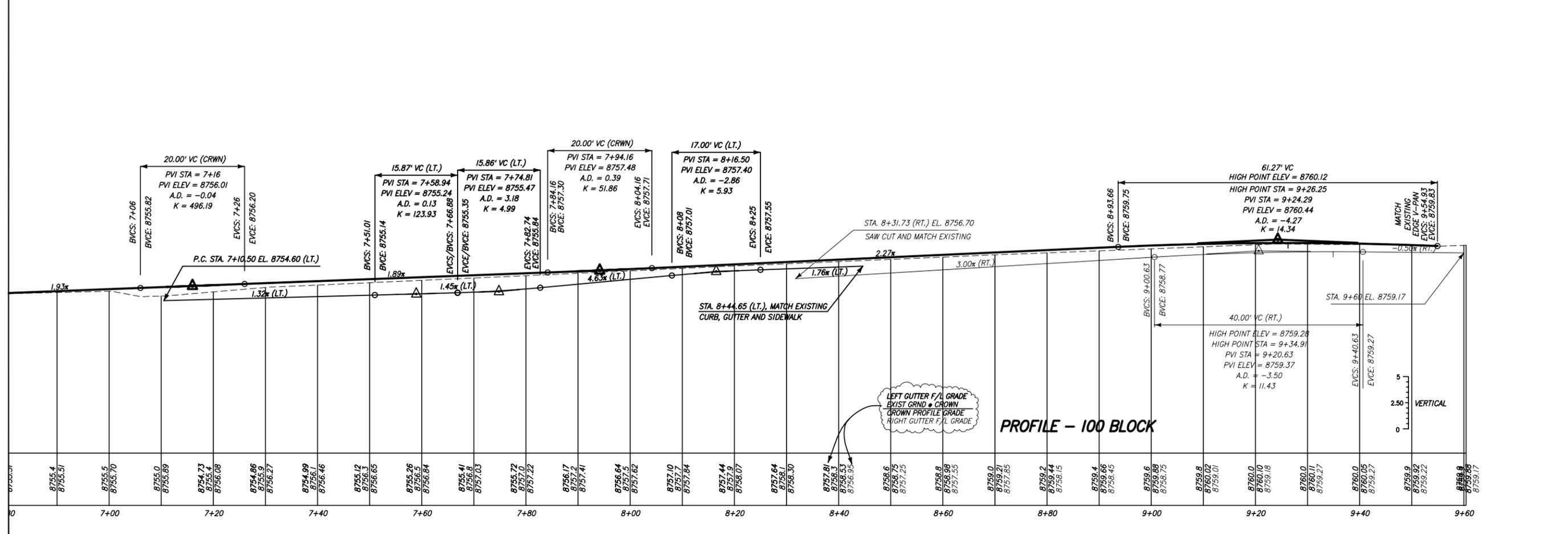
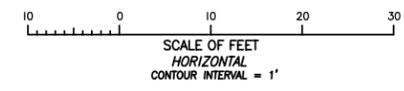
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Drawn by: DJP
Checked by: ELK
Start date: 4-14-04
Dwg path: SPRUCE1.DWG
Sheet No. **8**
Project #: 04-072



NOTE: EXISTING CONDITIONS INFORMATION PROVIDED HEREON BASED ON FIELD SURVEY DATED 10/24/00, AND UPDATED 1/22/01.

UTILITY LOCATES PROVIDED BY SAN MIGUEL POWER ASSOC., U.S. WEST, KN ENERGY, AND CENTURY COMMUNICATIONS, BETWEEN 1/17/00 AND 1/19/00.

WATER AND SEWER LINES ARE SHOWN AS PER TOWN OF TELLURIDE UTILITY LOCATION MAPS DATED 1995.



TELLURIDE STREETSCAPES - WAREHOUSE DISTRICT
 SOUTH WILLOW STREET PLAN AND PROFILE
 STA. 7+00 TO STA. 9+75 (100 BLOCK)

Client: Town of Telluride
 100 S. Broad Street
 Telluride, Colorado 81435

Contact: Karen Coughlene
 300 S. Broad Street
 Telluride, Colorado 81435

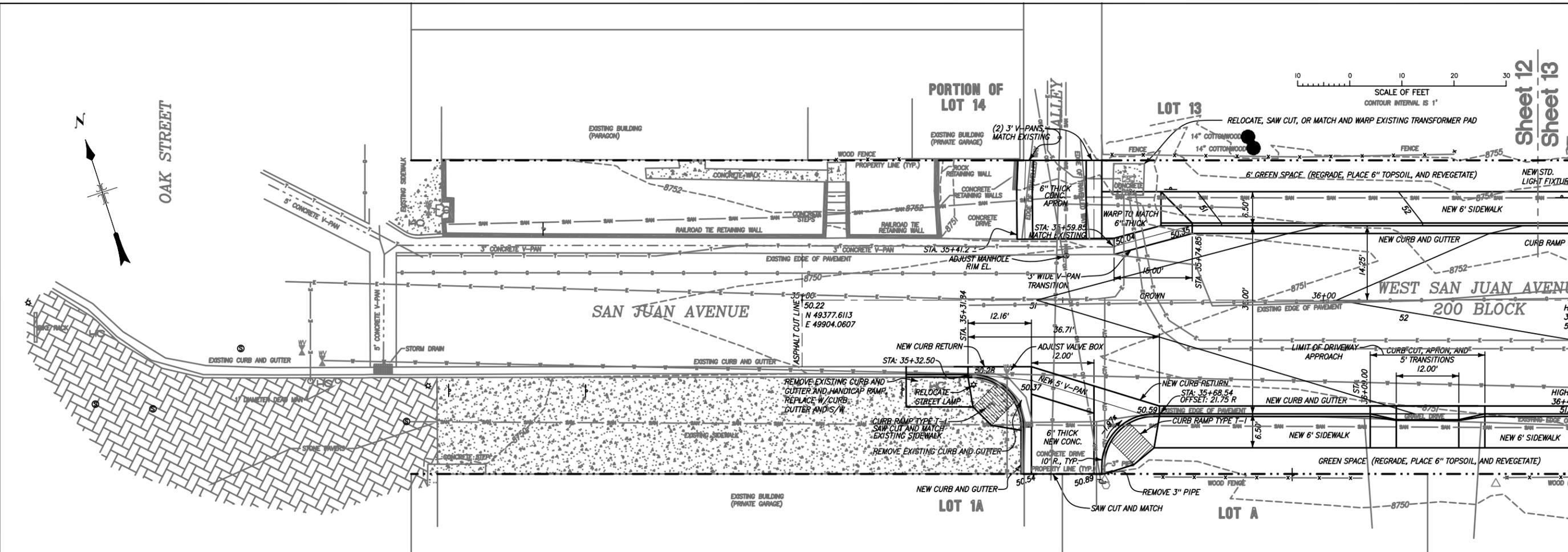
FOLEY ASSOCIATES, INC.
 ENGINEERING - PLANNING - SURVEYING

PO Box 1385, 125 W. Pacific Ave. Suite B1
 Telluride, Colorado, 81435
 phone 970-258-6133 fax 970-258-6050
 e-mail to vma@foleyassoc.com

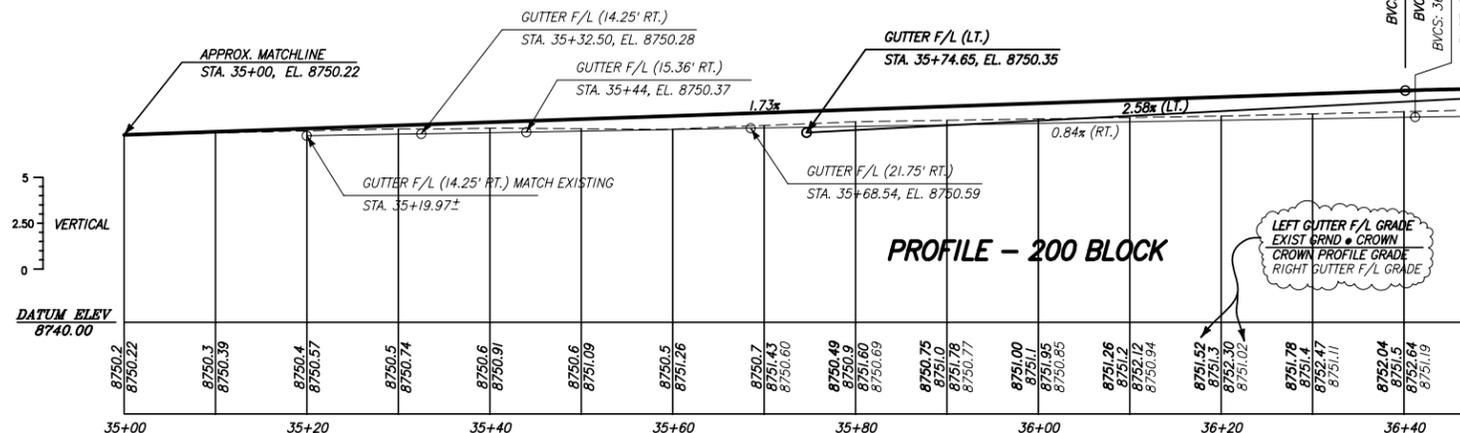
BUCKHORN GEOTECH
 Civil, Structural, and Geotechnical Engineers, Inc.
 222 South Park Avenue
 Montrose, Colorado 81401
 Phone (970) 249-8828 Fax (970) 249-0946

Rev.	description	date	by

Project Mgr: ELK
 Drawn by: DJP
 Checked by: ELK
 Start date: 4-14-04
 Dwg path: WILLOW.DWG
 Sheet No. **11**
 Project #: 04-072



Project ACQ M260-015//14768



Client: **Town of Telluride**
 100 Broad Street
 Telluride, Colorado 81435
 Phone: 970-258-5030
 Fax: 970-258-5030
 Email: town@telluride.com

Contact: **Karen Gagliardi**
 300 S. Main Street
 Telluride, Colorado 81435

Project Mgr: **ELK**

Drawn by: **DJP**

Checked by: **ELK**

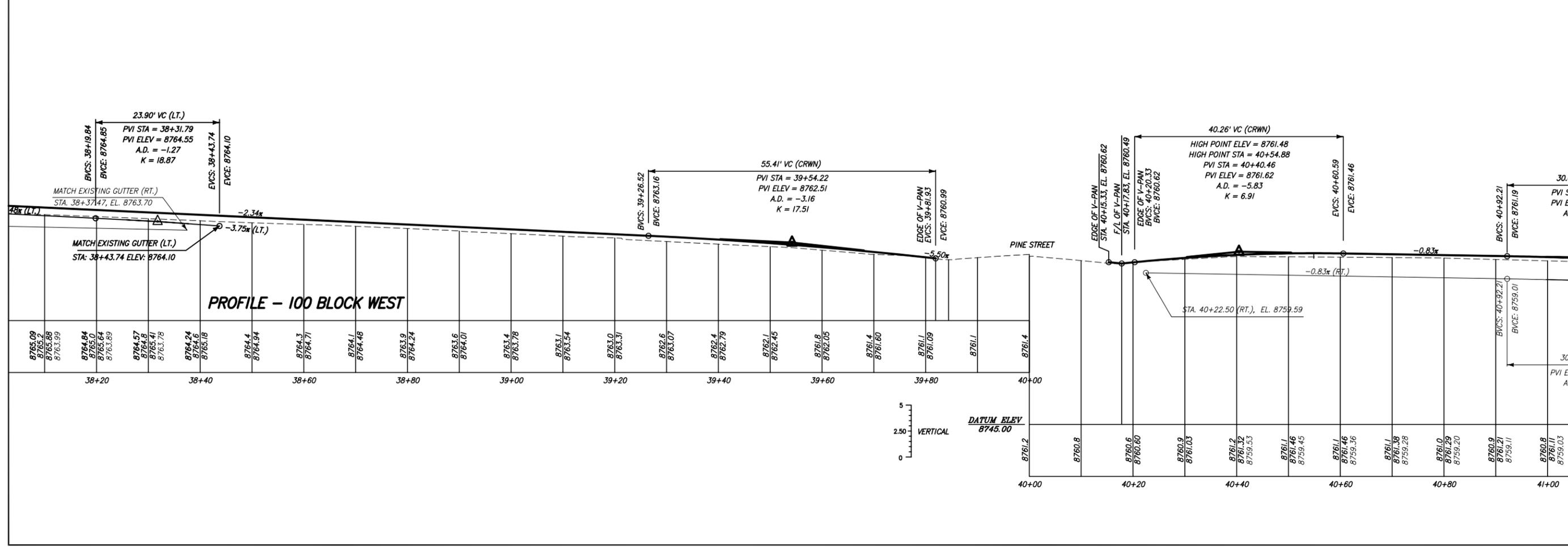
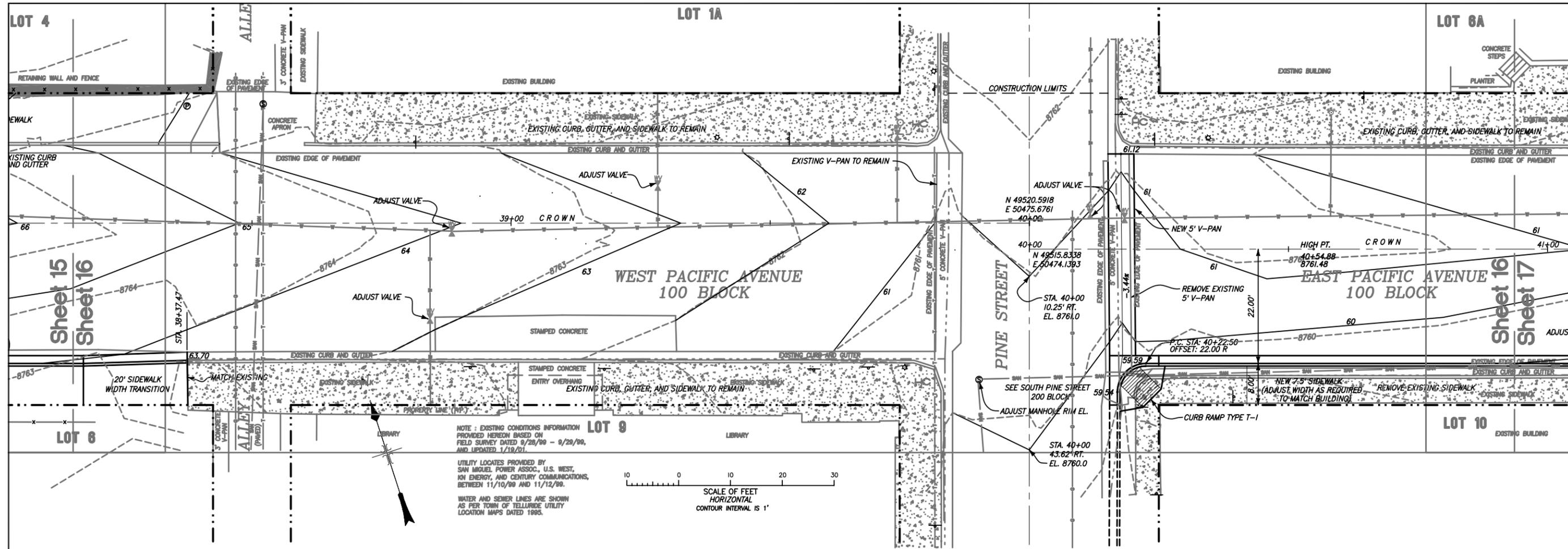
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Dwg path: **SANJUAN.DWG**

Sheet No. **12**

Project #4-072

Project ACQ M260-015//14768



TELLURIDE STREETSCAPES - WAREHOUSE DISTRICT
WEST & EAST PACIFIC AVENUE PLAN AND PROFILE
 STA. 38+20 TO STA. 41+00 (100 BLOCKS)

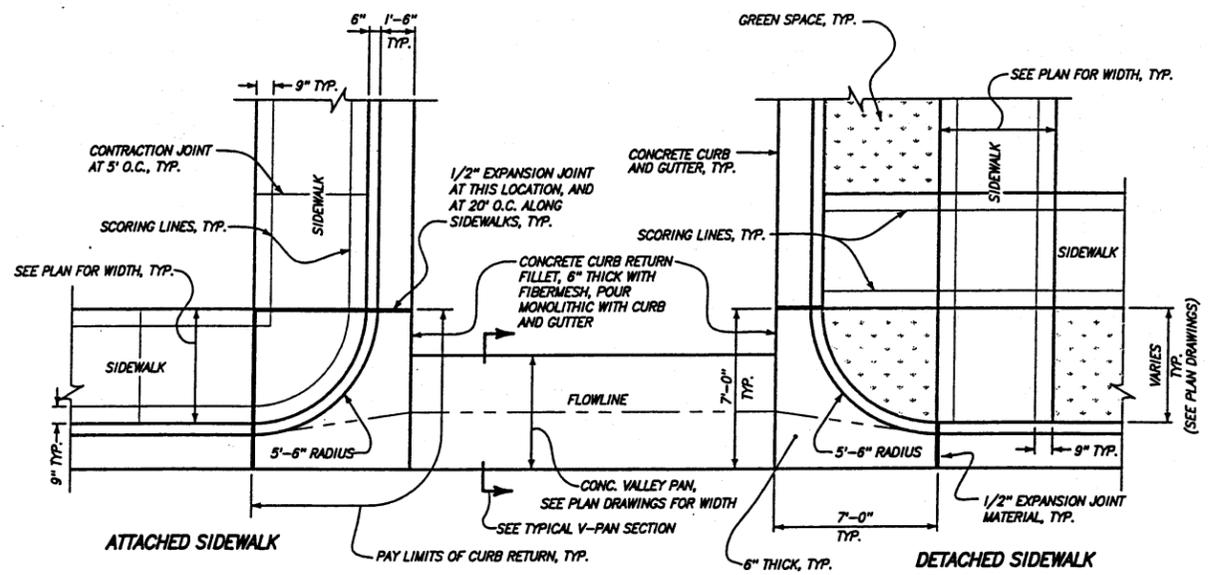
BUCKHORN GEOTECH
 Civil, Structural, and Geotechnical Engineers, Inc.
 222 South Park Avenue
 Montrose, Colorado 81401
 Phone (970) 249-8828 Fax (970) 249-0946

FOLEY ASSOCIATES, INC.
 ENGINEERING - PLANNING - SURVEYING
 PO Box 1385 125 W. Pacific Ave. Suite B1
 Telluride, Colorado, 81435
 phone 970-256-6133 fax 970-256-6100
 e-mail to vsm@foleyassoc.com

Client: Town of Telluride
 P.O. Box 397
 Telluride, Colorado 81435

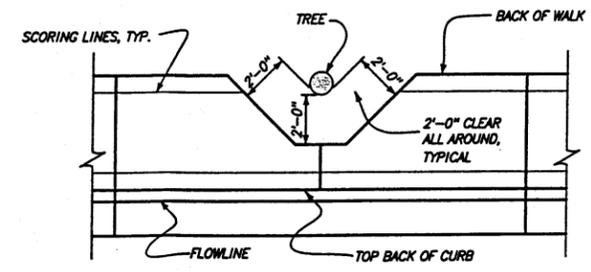
Contact: Karen Gagliemone
 300 Pacific Avenue
 Telluride, Colorado 81435

Project Mgr: ELK
 Drawn by: DJP
 Checked by: ELK
 Start date: 10-14-04
 Dwg path: Pacific1.dwg
 Sheet No. **16**
 Project #: 04-072

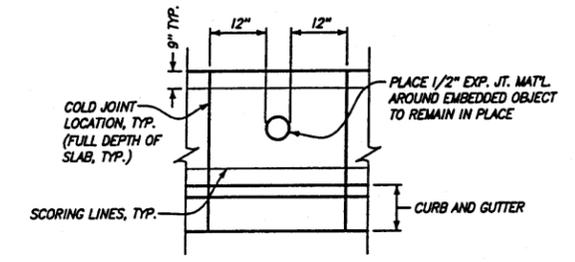


TYPICAL INTERSECTION DETAIL
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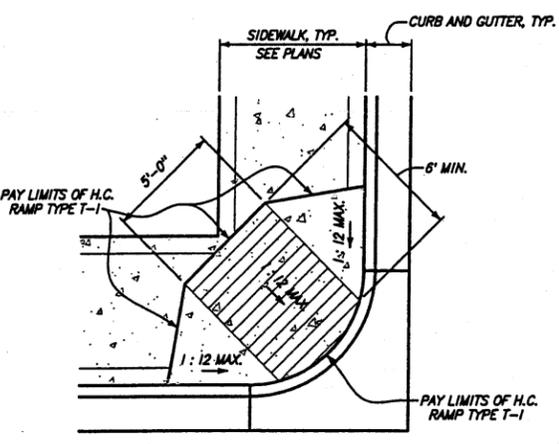
TOWN OF TELLURIDE STANDARD DETAIL 2625 D (MODIFIED)



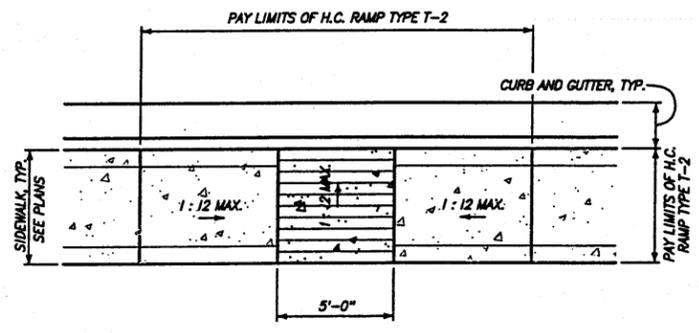
TREE BLOCKOUT DETAIL
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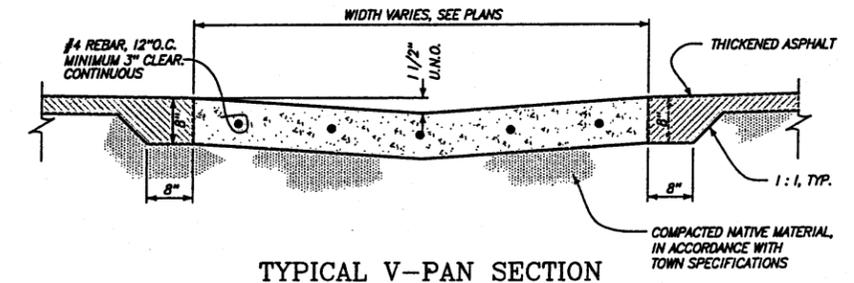
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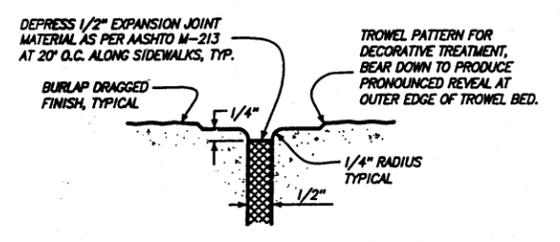
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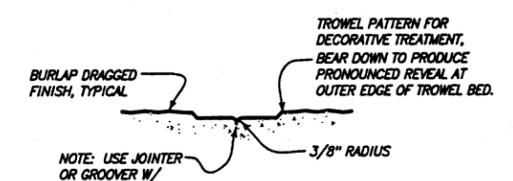
TYPICAL HANDICAP RAMP TYPE T-2
 N.T.S.



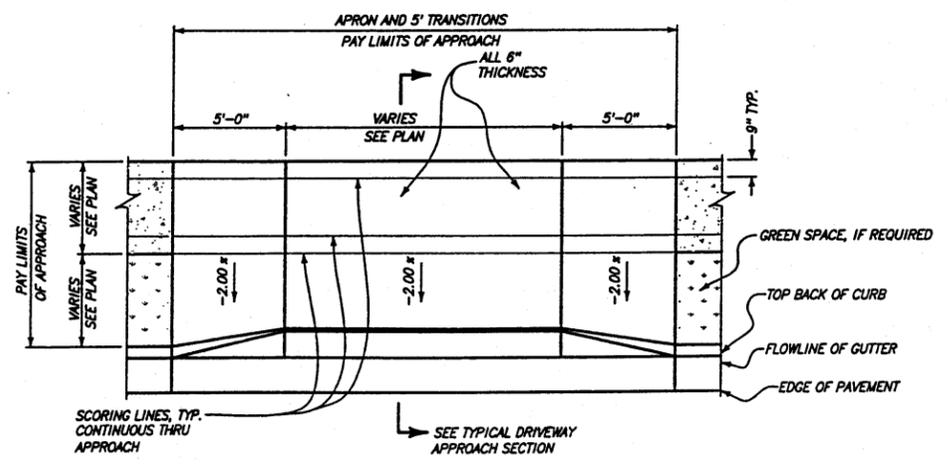
TYPICAL V-PAN SECTION
 N.T.S.



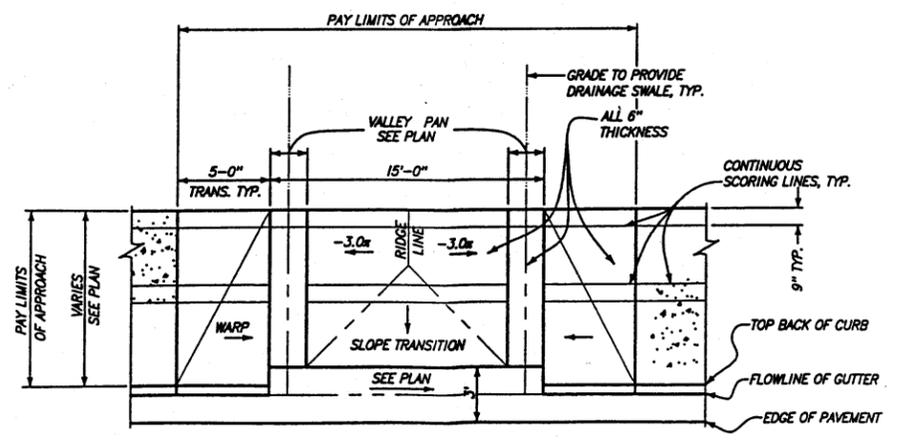
EXPANSION JOINT DETAIL
 N.T.S.



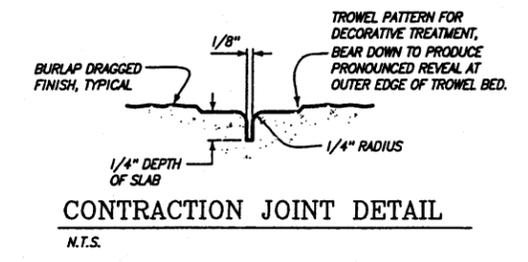
TOOLED JOINT/SCORING DETAIL
 N.T.S.



NEW DRIVEWAY OR ALLEY APPROACH DETAIL
 N.T.S.



ALLEY APPROACH WITH V-PANS DETAIL
 N.T.S.



CONTRACTION JOINT DETAIL
 N.T.S.

TELLURIDE STREETSCAPES - WAREHOUSE DISTRICT
 PROJECT DETAILS

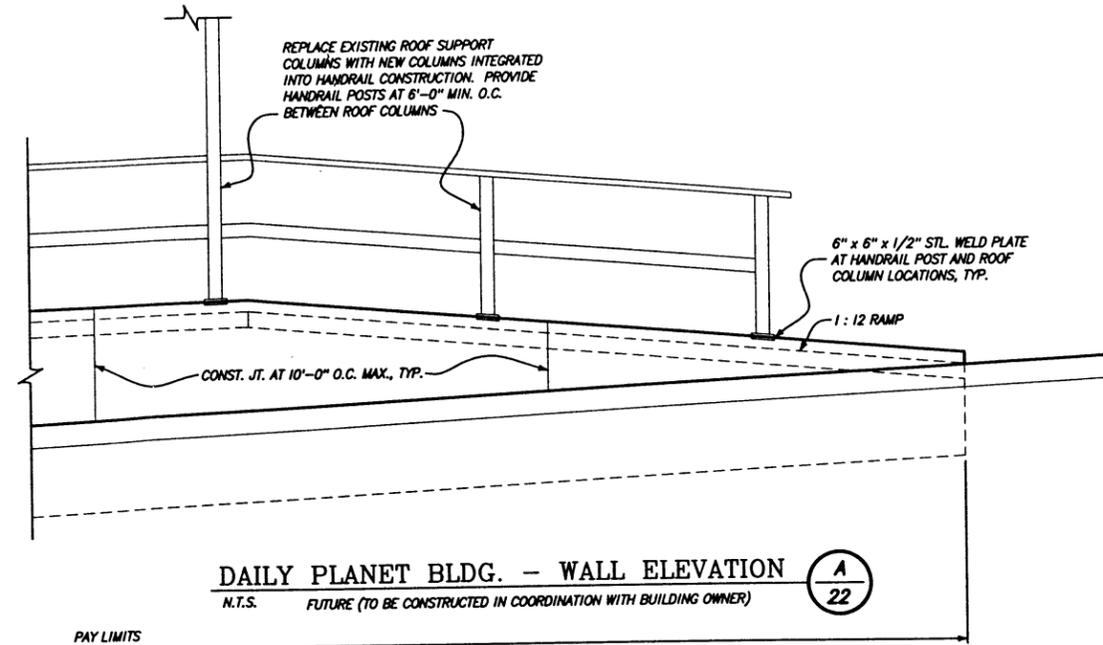
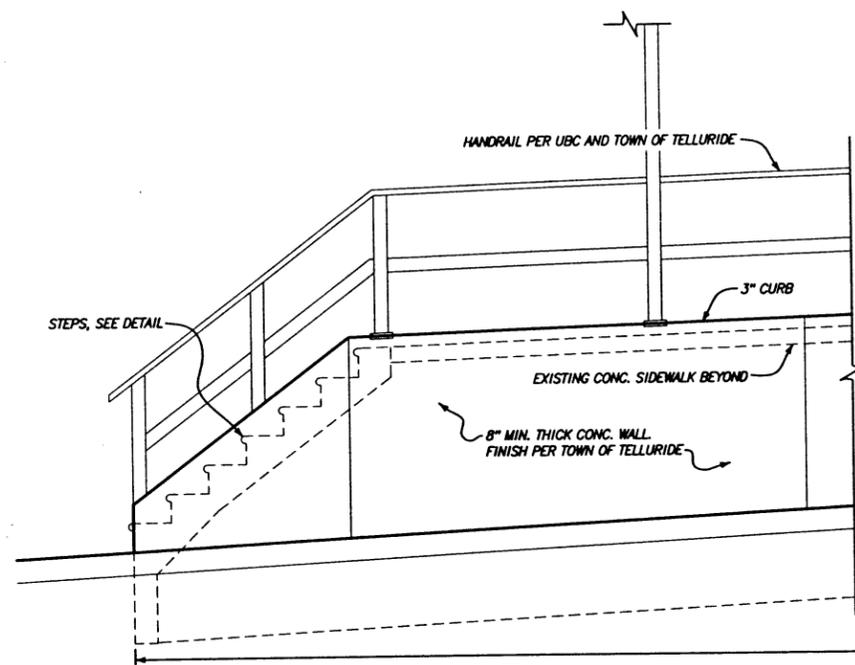
Contact:
 Keren Campbell
 300 Bascom Road
 Telluride
 Colorado 81435

Client:
 Town of Telluride, Colorado
 P.O. Box 397
 Telluride
 Colorado 81435

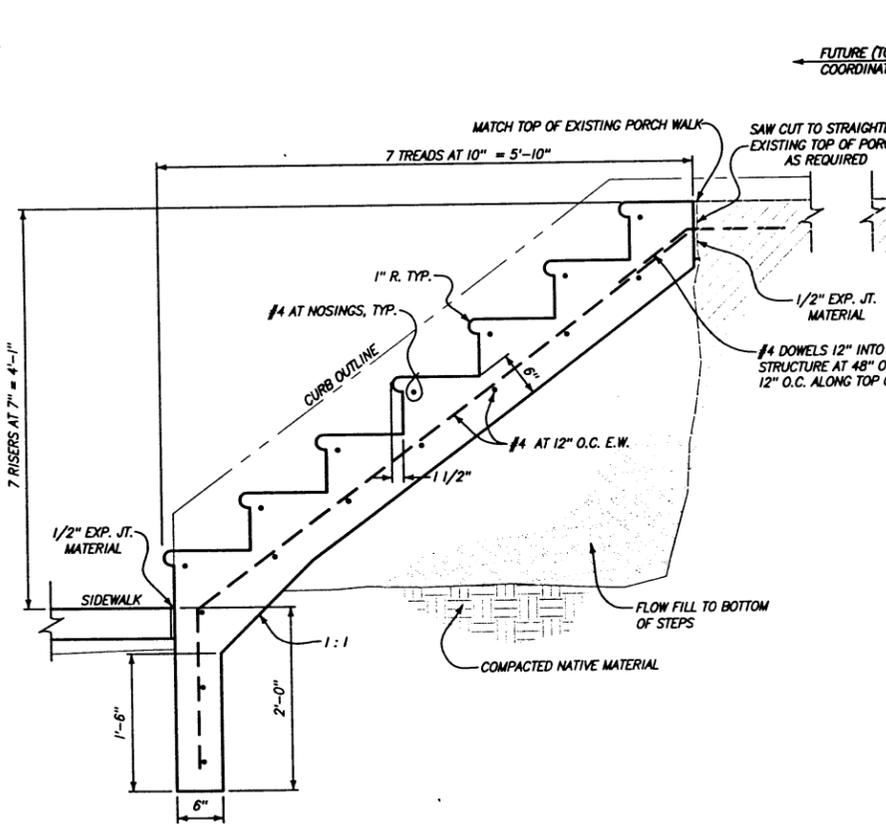
FOLEY ASSOCIATE, INC.
 ENGINEERING - PLANNING - SURVEYING
 PO Box 1385, 128 W. Pacific Ave., Suite B1
 Telluride, Colorado, 81435
 Phone (970) 728-6153 Fax (970) 728-8590
 e-mail to smallfoleyassoc.com

BUCKHORNTECH
 Civil, Structural, and Geotechnical Engineers, Inc.
 222 South Park Avenue
 Montrose, Colorado 81401
 Phone (970) 249-8020 Fax (970) 249-0845

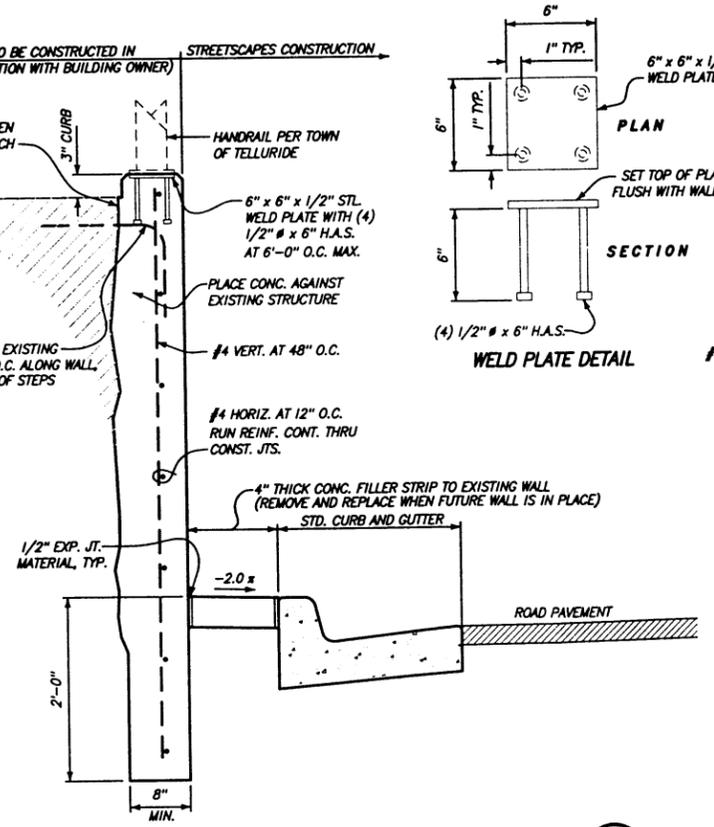
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Drawn by:	DJP
Checked by:	ELK
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Dwg path:	SSDETAILS.DWG
Sheet No.	20
Project #:	00-098



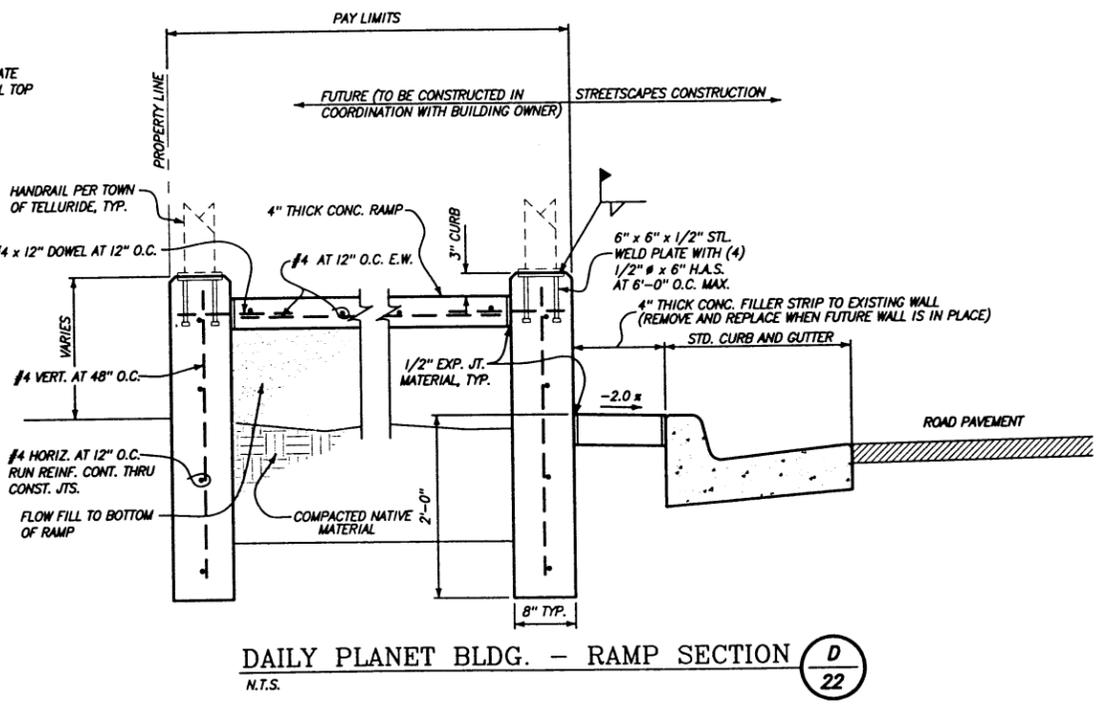
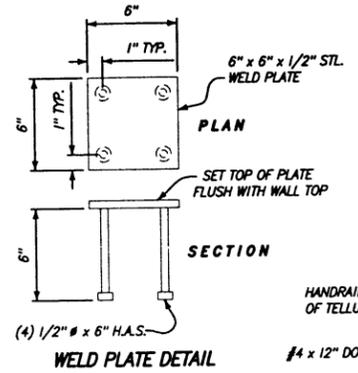
DAILY PLANET BLDG. - WALL ELEVATION A
 N.T.S. FUTURE (TO BE CONSTRUCTED IN COORDINATION WITH BUILDING OWNER)



DAILY PLANET BLDG. - STEPS SECTION B
 N.T.S.



DAILY PLANET BLDG. - WALL SECTION C
 N.T.S.



DAILY PLANET BLDG. - RAMP SECTION D
 N.T.S.

TELLURIDE STREETSCAPES - WAREHOUSE DISTRICT
 DAILY PLANET BUILDING
 RETAINING WALL, STEPS, AND RAMP DETAILS

Contact:
 Karen Guglielmino
 1370 Black Bear Road
 Telluride
 Colorado 81435

Client:
 Town of Telluride, Colorado
 P.O. Box 397
 Telluride
 Colorado 81435

FOLEY ASSOCIATES, INC.
 ENGINEERING - PLANNING - SURVEYING

PO Box 1385 125 W. Pacific Ave. Suite B1
 Telluride, Colorado 81435
 phone 970-728-6153 fax 970-728-6050
 e-mail to sm@foleyassoc.com

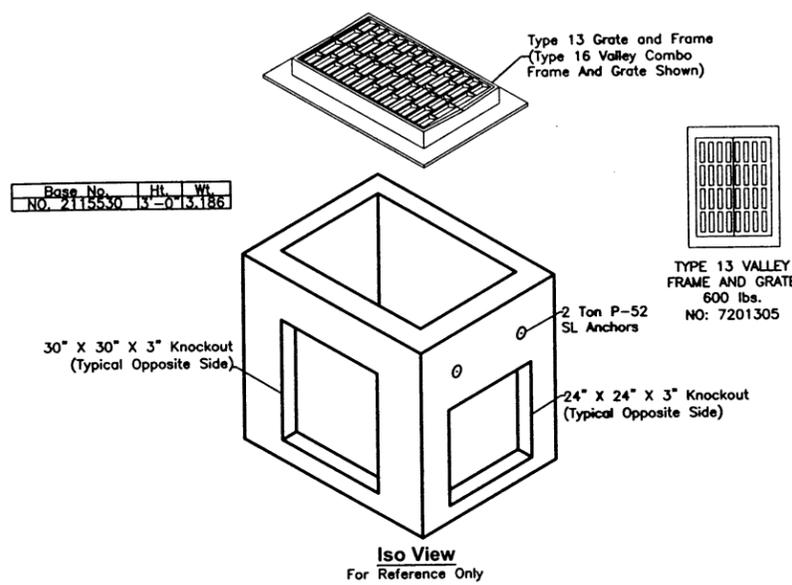
BUCKHORN GEOTECH
 Civil, Structural, and Geotechnical Engineers, Inc.
 222 South Park Avenue
 Montrose, Colorado 81401
 Phone (970) 249-6628 Fax (970) 249-0845

Rev.	description	date	by
1	FINAL REVISION	3-25-05	DJP

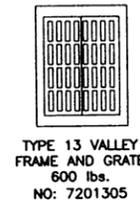
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 Drawn by: DJP
 Checked by: ELK
 Start date: 7-19-04
 Dwg path: SSDETAILS2.DWG
 Sheet No. **22**
 Project #: 04-072

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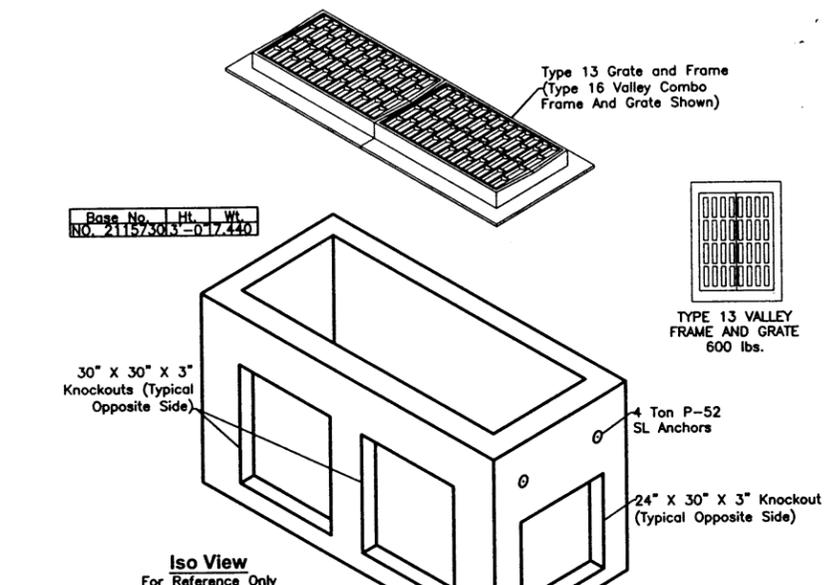
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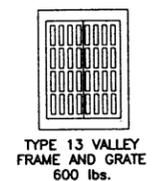
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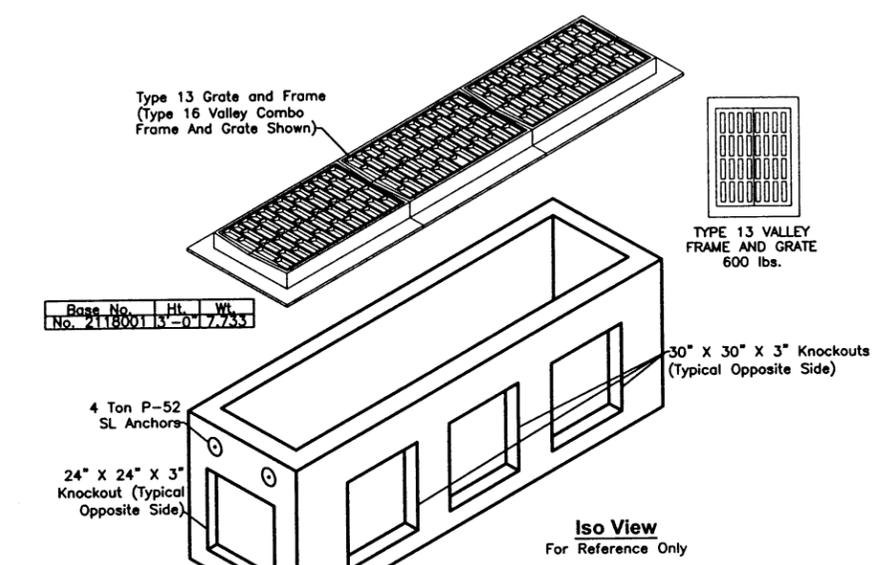
SINGLE TYPE 13 VALLEY INLET DETAILS
 INCLUDE 6" THICK CONCRETE APRON WORK SHOWN ON THE PLANS IN COST FOR INLET, TYPICAL (VERIFY ALL BASE AND GRATE MODEL NUMBERS WITH MANUFACTURER)



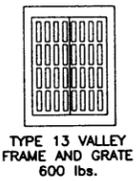
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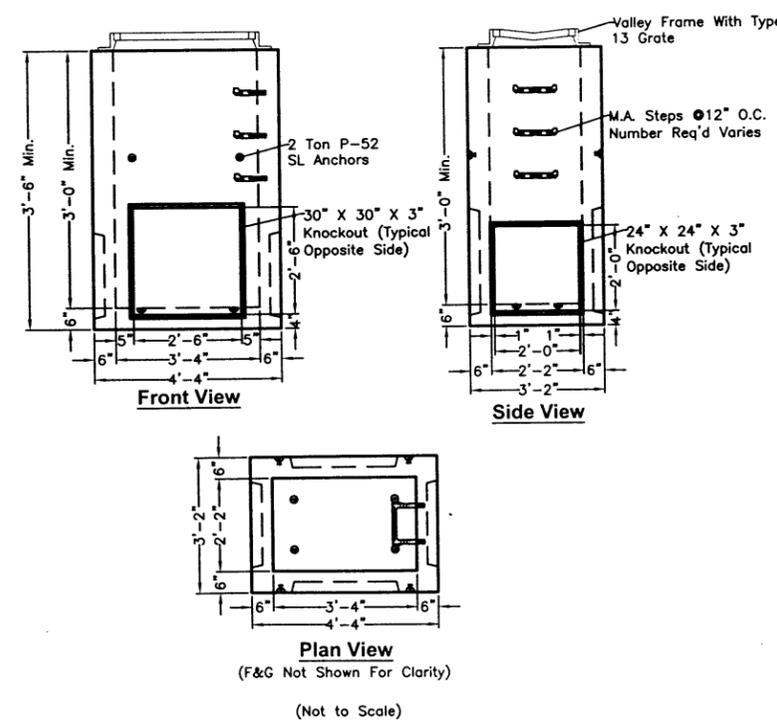
DOUBLE TYPE 13 VALLEY INLET DETAILS
 INCLUDE 6" THICK CONCRETE APRON WORK SHOWN ON THE PLANS IN COST FOR INLET, TYPICAL (VERIFY ALL BASE AND GRATE MODEL NUMBERS WITH MANUFACTURER)



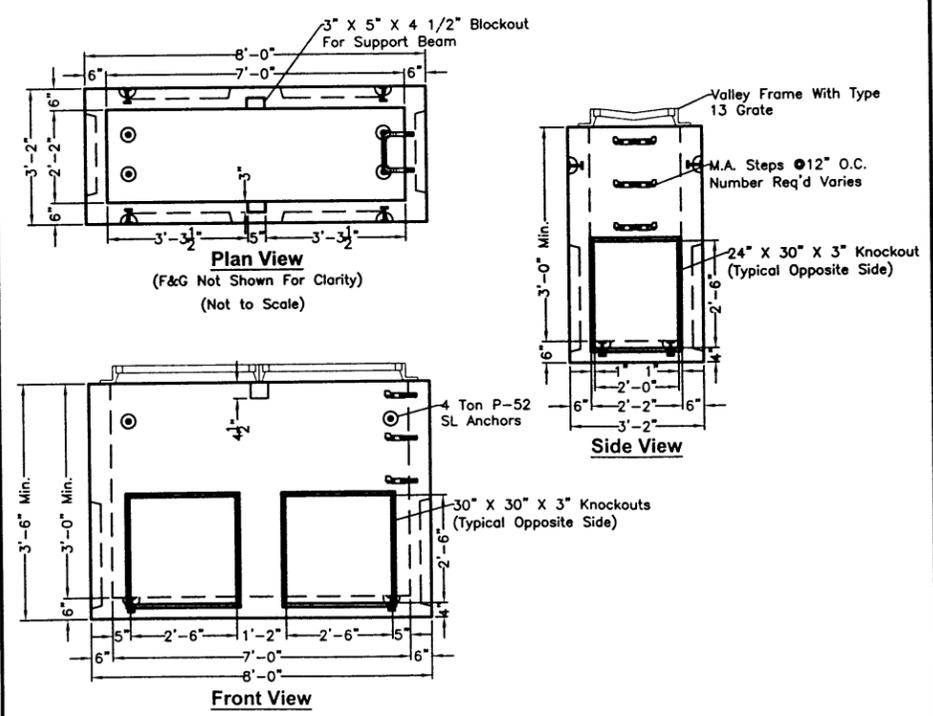
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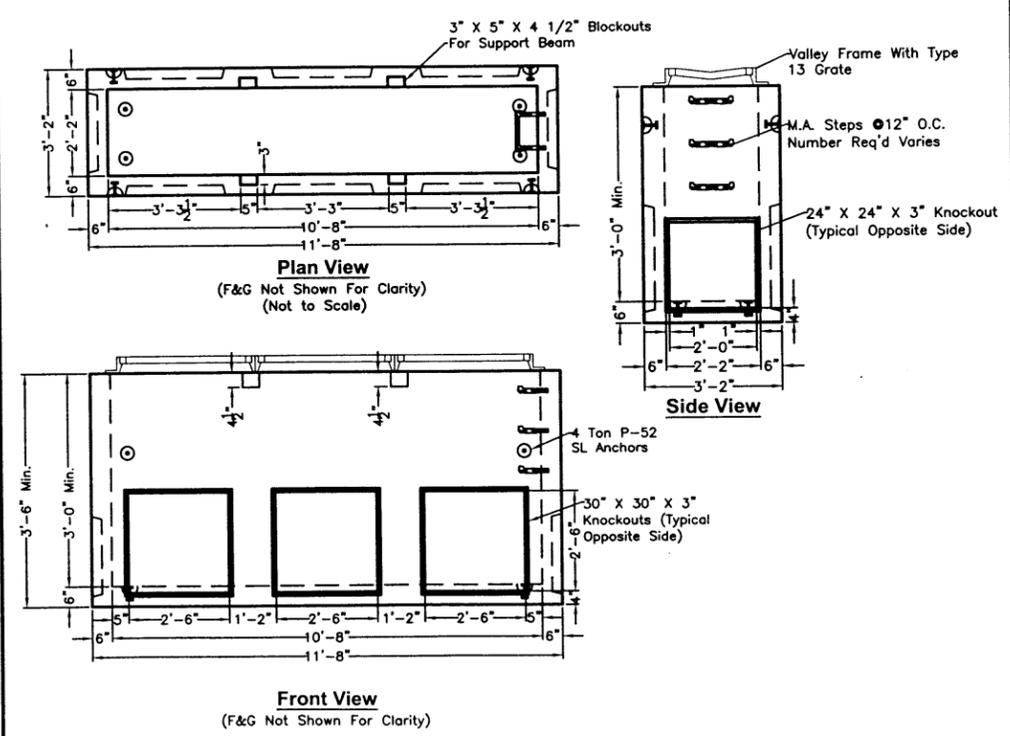
TRIPLE TYPE 13 VALLEY INLET DETAILS
 INCLUDE 6" THICK CONCRETE APRON WORK SHOWN ON THE PLANS IN COST FOR INLET, TYPICAL (VERIFY ALL BASE AND GRATE MODEL NUMBERS WITH MANUFACTURER)



NOTE:
 ALL INLET STRUCTURES AND PIPING SHALL HAVE HEATING CABLE INSTALLED ACCORDING TO MANUFACTURER'S RECOMMENDATIONS, OR TOWN OF TELLURIDE STANDARDS.



NOTE:
 ALL INLET STRUCTURES AND PIPING SHALL HAVE HEATING CABLE INSTALLED ACCORDING TO MANUFACTURER'S RECOMMENDATIONS, OR TOWN OF TELLURIDE STANDARDS.



Oldcastle Precast
AMCOR Division
 6200 Riverdale Pkwy, Littleton, CO 80120
 Phone: (303) 778-1200 / 1-800-742-4528
 Fax: (303) 778-1200

NOTE:
 ALL INLET STRUCTURES AND PIPING SHALL HAVE HEATING CABLE INSTALLED ACCORDING TO MANUFACTURER'S RECOMMENDATIONS, OR TOWN OF TELLURIDE STANDARDS.

TELLURIDE STREETSCAPES - WAREHOUSE DISTRICT
 PROJECT DETAILS
 PRECAST CONCRETE VALLEY INLETS

FOLEY ASSOCIATES, INC.
 ENGINEERING - PLANNING - SURVEYING
 PO Box 1385, 125 W. Pacific Ave. Suite B1
 Telluride, Colorado 81435
 phone 970.728.6153 fax 970.728.6950
 e-mail to: info@foleyassoc.com

BUCKHORN GEOTECH
 Civil, Structural, and Geotechnical Engineers, Inc.
 222 South Park Avenue
 Montrose, Colorado 81401
 Phone (970) 249-8828 Fax (970) 249-8945

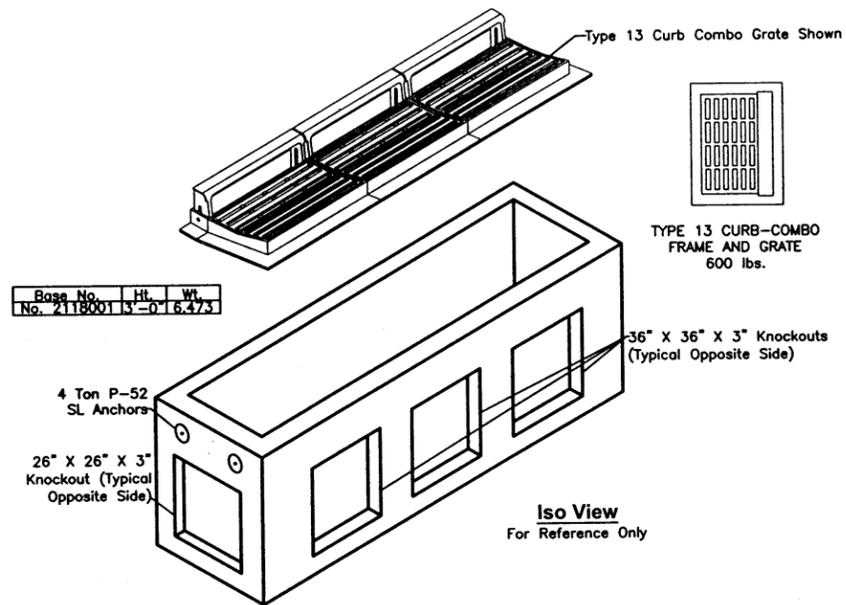
Rev.	DATE	DESCRIPTION
1	3-25-05	DJP

Project Mgr: ELK
 Drawn by: DJP
 Checked by: ELK
 Start date: 7-19-04
 Dwg path: AmcorDetails.dwg
 Sheet No. **23**
 Project #: 00-098

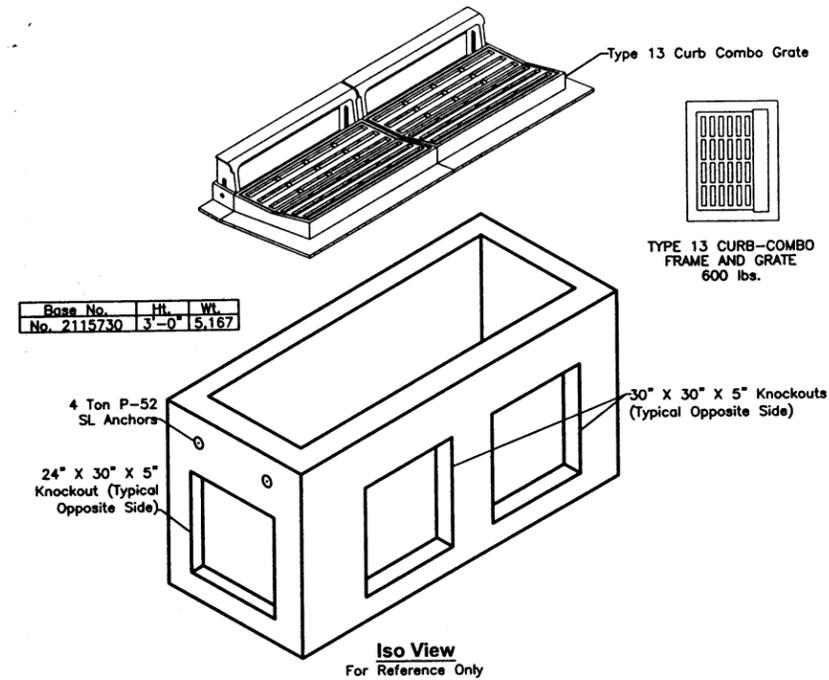
Contact:
 Keny Coulter
 1370 Black Bear Road
 Telluride
 Colorado 81435

Client:
 Colorado
 Town of Telluride
 Colorado 81435

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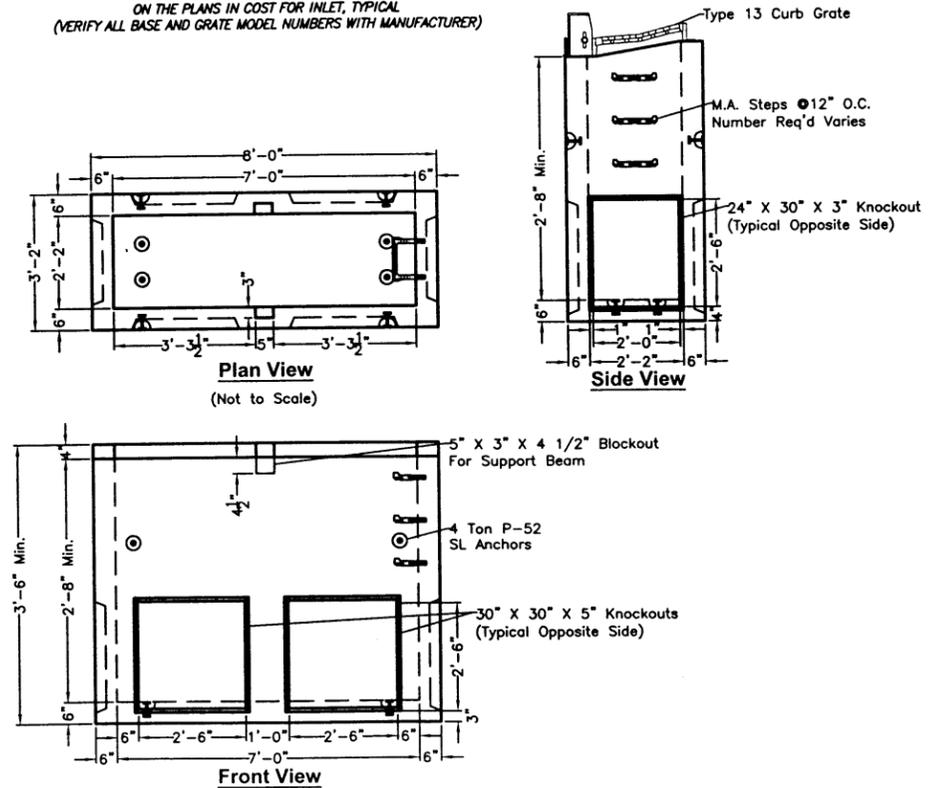
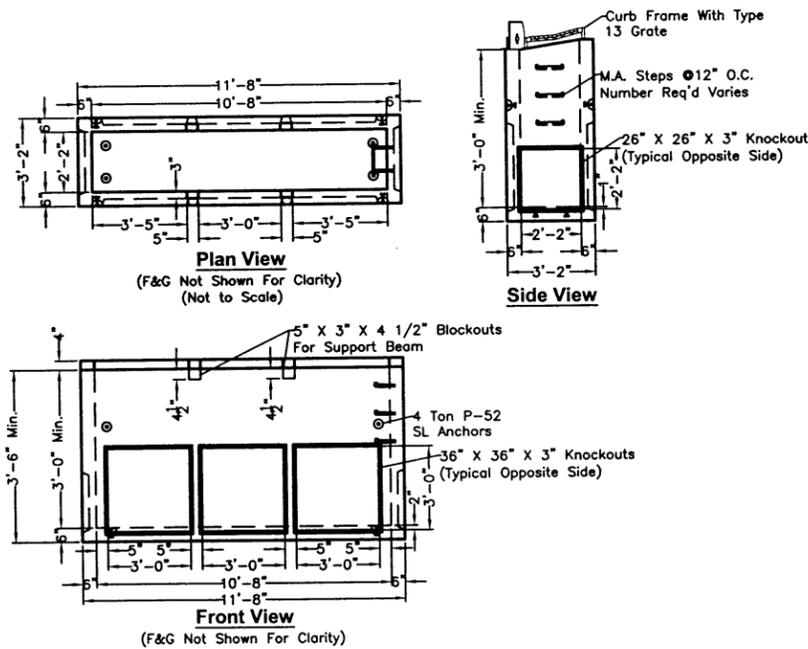
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Base No.	Ht.	Wt.
No. 2115730	3'-0"	5,167

TRIPLE TYPE 13 CURB INLET DETAILS
 INCLUDE 6" THICK CONCRETE APRON WORK SHOWN
 ON THE PLANS IN COST FOR INLET, TYPICAL
 (VERIFY ALL BASE AND GRATE MODEL NUMBERS WITH MANUFACTURER)

DOUBLE TYPE 13 CURB INLET DETAILS
 INCLUDE 6" THICK CONCRETE APRON WORK SHOWN
 ON THE PLANS IN COST FOR INLET, TYPICAL
 (VERIFY ALL BASE AND GRATE MODEL NUMBERS WITH MANUFACTURER)



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 TOWN OF TELLURIDE STANDARDS.

Oldcastle Precast
AMCOR Division
 6700 Riverdale Hwy, Littleton, CO 80120
 Phone: (303) 796-1200 / 1-800-742-6338
 Fax: (303) 796-1200

NOTE:
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 HEATING CABLE INSTALLED ACCORDING TO
 MANUFACTURER'S RECOMMENDATIONS, OR
 TOWN OF TELLURIDE STANDARDS.

TELLURIDE STREETSCAPES - WAREHOUSE DISTRICT
PROJECT DETAILS
PRECAST CONCRETE CURB INLETS

FOLEY ASSOCIATES, INC.
 ENGINEERING - PLANNING - SURVEYING
 PO Box 1885, 125 W. Pacific Ave, Suite 81
 Telluride, Colorado 81435
 phone 970 726-6153 fax 970-726-6050
 e-mail to small@foleyassoc.com

BUCKHORN GEOTECH
 Civil, Structural and Geotechnical Engineers, Inc.
 222 South Park Avenue
 Montrose, Colorado 81401
 Phone (970) 249-6628 Fax (970) 249-0945

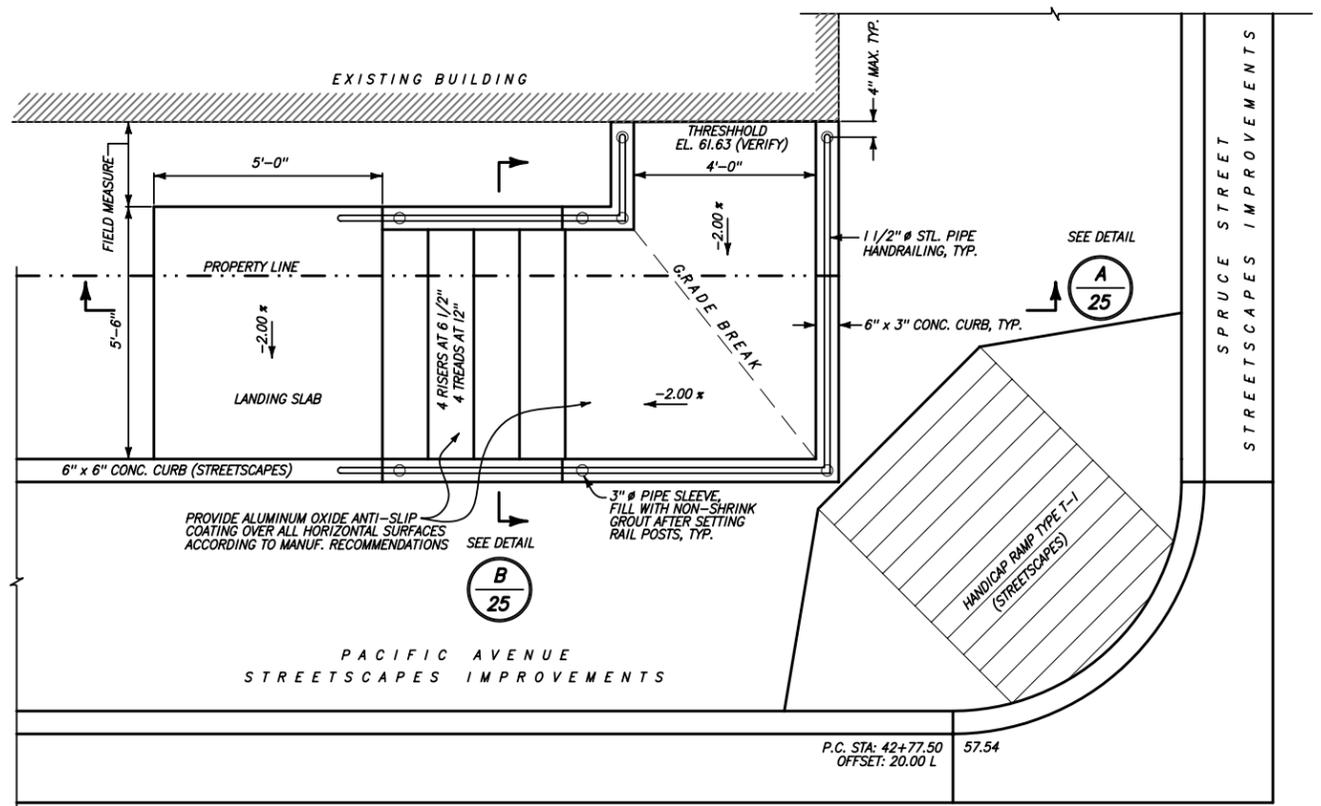
Client: Town of Telluride, Colorado
 Project: Warehouse Road
 Telluride, Colorado 81435

Contact: Karen C. Gorman, Road
 1370 Highway 147
 Telluride, Colorado 81435

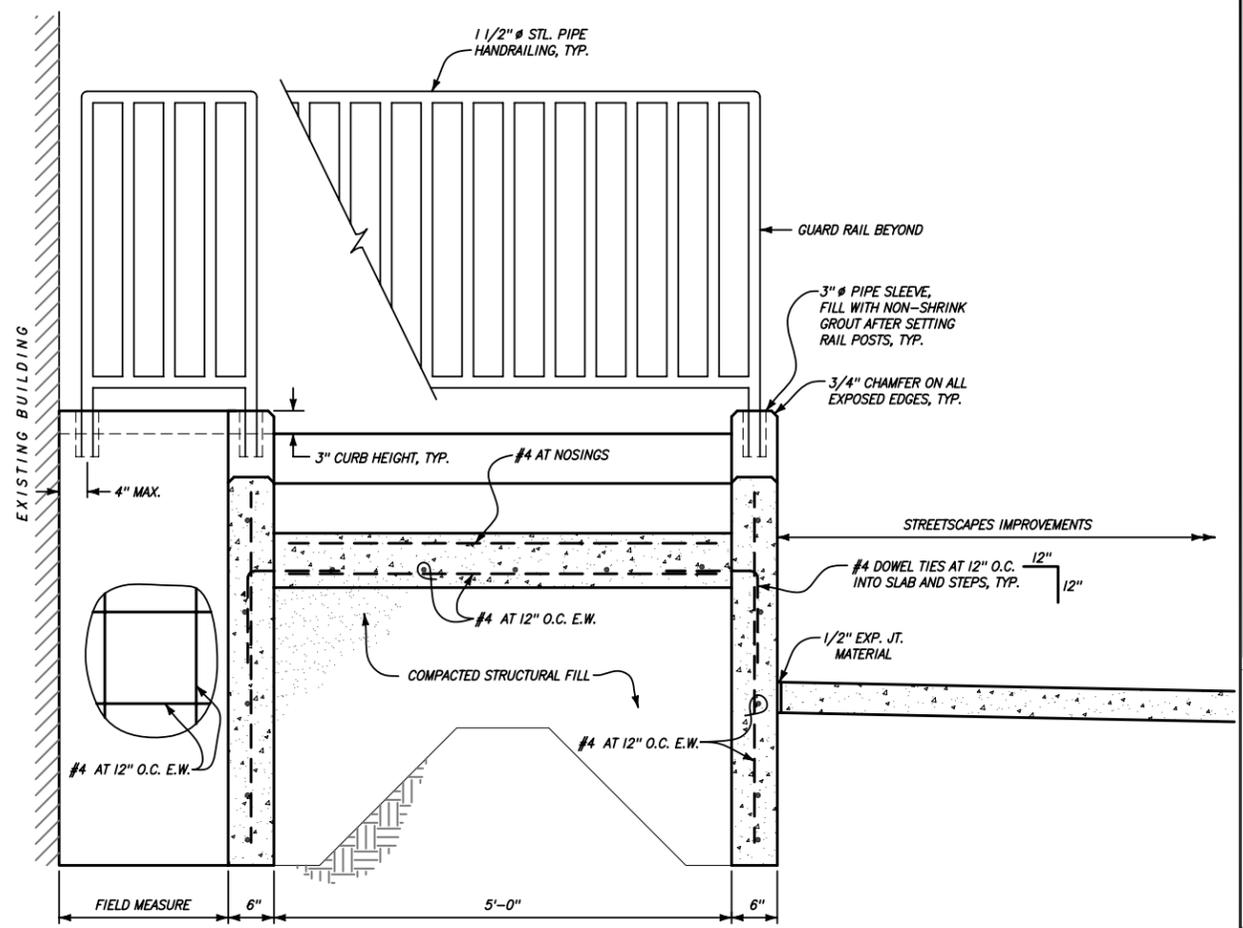
Rev.	description	date	by
1	FINAL REVISION	3-25-05	DJP

Project Mgr: ELK
 Drawn by: DJP
 Checked by: ELK
 Start date: 7-01-04
 Dwg path: AmcorDetails.dwg
 Sheet No. **24**
 Project #: 00-098

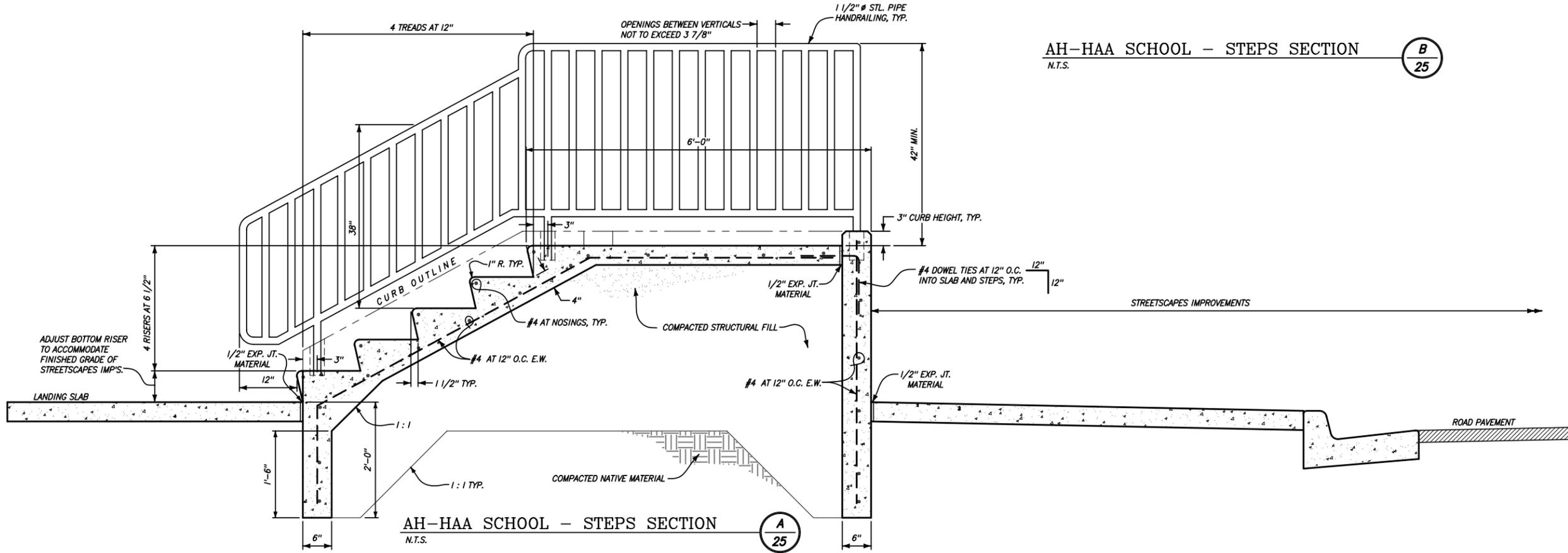
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AH-HAA SCHOOL - STEPS PLAN
N.T.S.



AH-HAA SCHOOL - STEPS SECTION
N.T.S.



AH-HAA SCHOOL - STEPS SECTION
N.T.S.

TELLURIDE STREETSCAPES - WAREHOUSE DISTRICT
AH-HAA SCHOOL BUILDING
STEPS DETAILS

FOLEY ASSOCIATES, INC.
ENGINEERING - PLANNING - SURVEYING
PO Box 1385, 125 W. Pacific Ave. Suite B1
Telluride, Colorado, 81435
phone 970-728-6133 fax 970-728-6050
e-mail to vsmain@foleyassoc.com

BUCKHORN GEOTECH
Civil, Structural, and Geotechnical Engineers, Inc.
222 South Park Avenue
Montrose, Colorado 81401
Phone (970) 249-8828 Fax (970) 249-0946

Rev.	description	date	by
1	FINAL REVISION	3-29-05	DJP

Project Mgr: ELK
 Drawn by: DJP
 Checked by: ELK
 Start date: 7-01-04
 Dwg path: SSDETAILS2.DWG
 Sheet No. **25**
 Project #: 04-072

**TELLURIDE STREETSAPES – EAST TELLURIDE
CONSTRUCTION PLANS
TOWN OF TELLURIDE
SAN MIGUEL COUNTY, COLORADO
BUCKHORN GEOTECH, INC. PROJECT # 04-072-02**

JULY 2006

INDEX TO EAST DISTRICT DRAWINGS

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G2 NOTES & LEGEND

EAST COLORADO AVENUE DRAWINGS:

A3 SCHEMATIC STREET CROSS SECTIONS

A4 EAST COLORADO PROJECT DETAILS

A5 EAST COLORADO PROJECT DETAILS

A6 EAST COLORADO PROJECT DETAILS

A7 EAST COLORADO PROJECT DETAILS

A8 EAST COLORADO PLAN & PROFILE, STA 0+00 TO 2+00, 400 BLOCK

A9 EAST COLORADO PLAN & PROFILE, STA 2+00 TO 4+90, 400 & 500 BLOCKS

A10 EAST COLORADO PLAN & PROFILE, STA 4+90 TO 7+80, 500 & 600 BLOCKS

A11 EAST COLORADO PLAN & PROFILE, STA 7+90 TO 10+70, 600 & 700 BLOCKS

A12 EAST COLORADO PLAN & PROFILE, STA 10+70 TO 13+50, 700 & 800 BLOCKS

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EAST DISTRICT NORTH/SOUTH STREETS DRAWINGS

B3 SCHEMATIC STREET CROSS SECTIONS

B4 NORTH/SOUTH STREETS PROJECT DETAILS

B5 MAPLE STREET PLAN & PROFILE STA. 0+00 TO 2+10, 100 BLOCK NORTH

B6 MAPLE STREET PLAN & PROFILE STA. 2+10 TO 3+03, 100 BLOCK NORTH

B7 HEMLOCK STREET PLAN & PROFILE STA. 00+00 TO 2+00, 100 BLOCK NORTH

B8 HEMLOCK STREET PLAN & PROFILE STA. 2+00 TO 3+10, 100 BLOCK NORTH

B9 COLUMBINE STREET PLAN & PROFILE STA. 0+00 TO 1+30, 100 BLOCK NORTH

B10 COLUMBINE STREET PLAN & PROFILE STA. 1+30 TO 2+95, 100 BLOCK NORTH

B11 PINON STREET PLAN & PROFILE, 100 BLOCK NORTH

EAST COLUMBIA AVENUE DRAWINGS

C3 SCHEMATIC STREET CROSS SECTION

C4 EAST COLUMBIA AVENUE PROJECT DETAILS

C5 EAST COLUMBIA AVENUE PLAN & NORTH PROFILES 400 BLOCK, STA. 49+00 TO 51+00

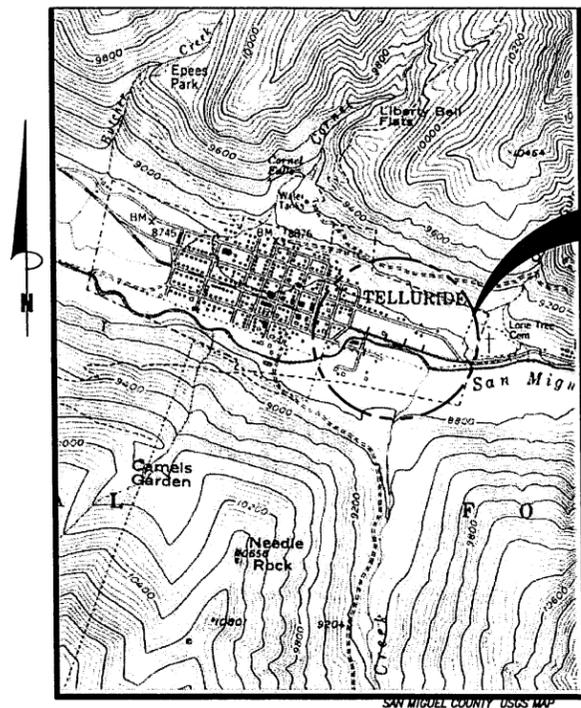
C6 EAST COLUMBIA AVENUE PLAN & NORTH PROFILES 400 & 500 BLOCKS, STA. 51+00 TO 53+00

C7 EAST COLUMBIA AVENUE PLAN & NORTH PROFILES 500 BLOCK, STA. 53+00 TO 55+00

C8 EAST COLUMBIA AVENUE PLAN & SOUTH PROFILES 400 BLOCK, STA. 49+00 TO 51+00

C9 EAST COLUMBIA AVENUE PLAN & SOUTH PROFILES 400 & 500 BLOCKS, STA. 51+00 TO 53+00

C10 EAST COLUMBIA AVENUE PLAN & SOUTH PROFILES 500 BLOCK, STA. 53+00 TO 55+00



PROJECT LOCATION

APPROVED BY STREETSAPES TASK FORCE :

CHAIR _____ DATE _____

APPROVED BY THE TOWN OF TELLURIDE :

TOWN ENGINEER _____ DATE _____

VICINITY MAP

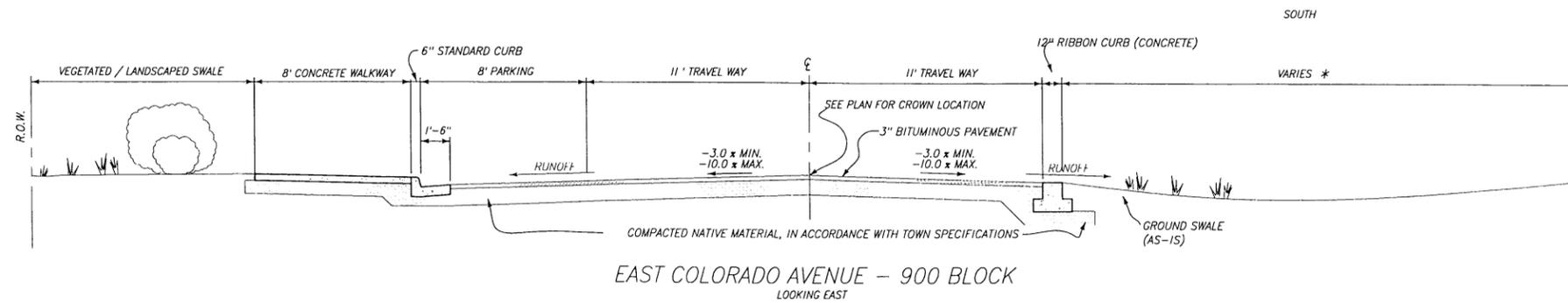
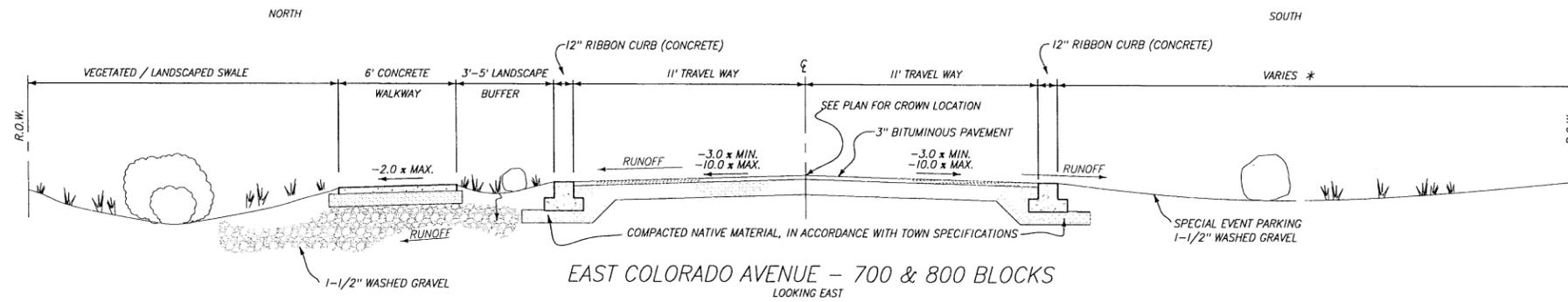
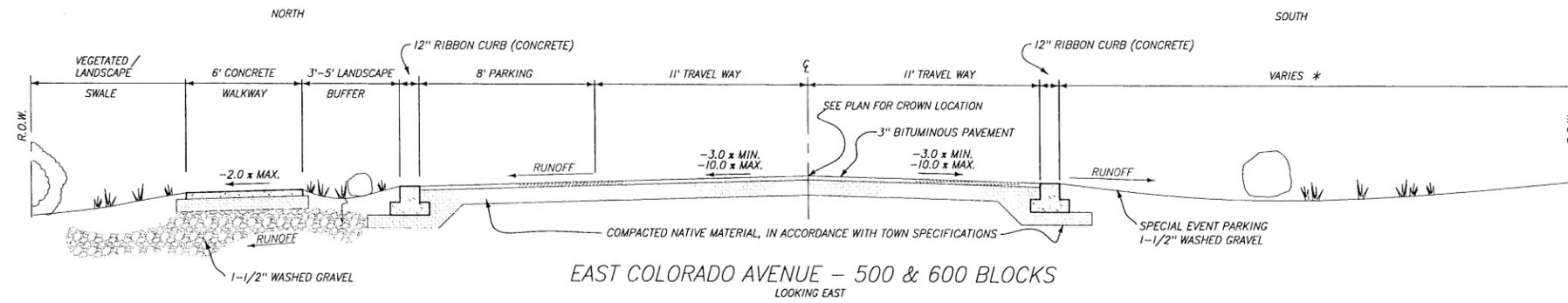
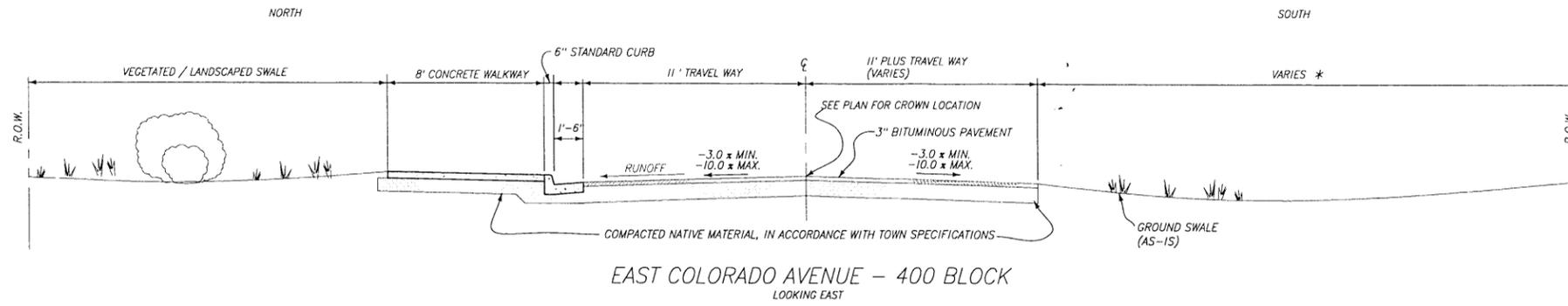
CALL UTILITY NOTIFICATION
CENTER OF COLORADO
1-800-922-1987

CALL 2-BUSINESS DAYS IN ADVANCE
BEFORE YOU DIG, GRADE, OR EXCAVATE
FOR THE MARKING OF UNDERGROUND
MEMBER UTILITIES

<p>BUCKHORN GEOTECH</p> <p>Civil, Structural, and Geotechnical Engineers, Inc. 222 South Park Avenue Montrose, Colorado 81401 Phone (970) 249-6828 Fax (970) 249-0945</p>	<p>FOLEY ASSOCIATES, INC.</p> <p>ENGINEERING • PLANNING • SURVEYING</p> <p>PO Box 1385 125 W. Pacific Ave. Suite B1 Telluride, Colorado, 81435 phone 970-728-6153 Fax 970-728-6050 e-mail to xmail@foleyassoc.com</p>
--	--

**PROJECT MANAGER
AND ENGINEER**

PROJECT SURVEYOR



TELLURIDE STREETSCAPES - EAST TELLURIDE
EAST COLORADO AVENUE PLAN AND PROFILE
SCHEMATIC STREET CROSS-SECTIONS

Contact:
Karen Guglielmino
1370 Buck Beer Road
Colorado 81435

Client:
Town of Telluride, Colorado
P.O. Box 397
Telluride, Colorado 81435

FOLEY ASSOCIATES, INC.
ENGINEERING • PLANNING • SURVEYING

PO Box 1585 125 W. Pacific Ave., Suite B1
Telluride, CO 81435
Phone 970-728-6153 Fax 970-728-6050
e-mail to xmail@foleyassoc.com

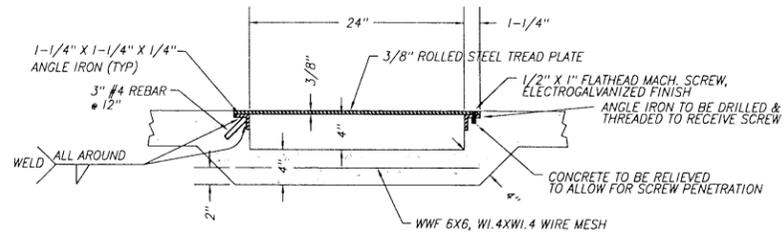
BUCKHORN GEOTECH
Civil, Structural, and Geotechnical Engineers, Inc.

222 South Park Avenue
Montrose, Colorado 81401
Phone (970) 249-8828 Fax (970) 249-0845

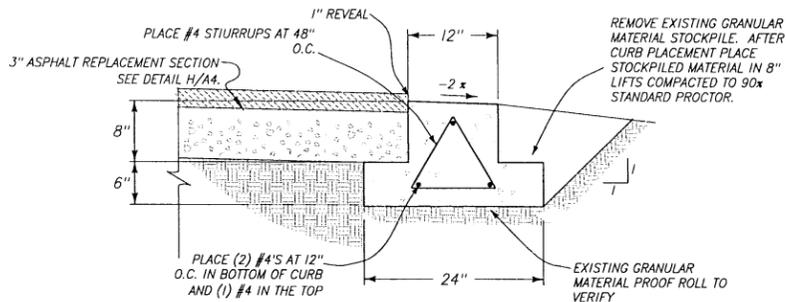
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1	8/1/05	ISSUED	
2	11/7/05	EAST COLORADO BID PACKAGE	
3	11/29/05	REVISED EAST COLORADO BID PACKAGE	
4		RENUMBERED SHEETS FOR EAST DISTRICT DRAWING SET	5/03/06 DC

Project Mgr: ELK
 Drawn by: DJP
 Checked by: ELK
 Start date: 4-16-02
 Dwg path: Colorado.dwg
 Sheet No. **A3**
 Project #: 04-072

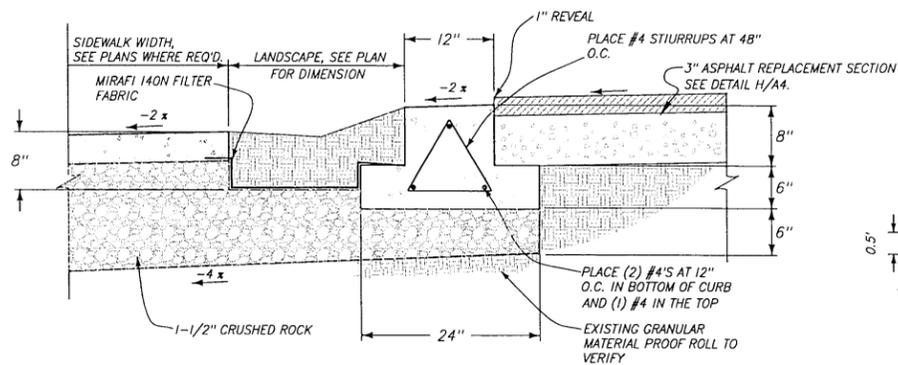
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SIDEWALK CHASE DETAIL
N.T.S. A
A4

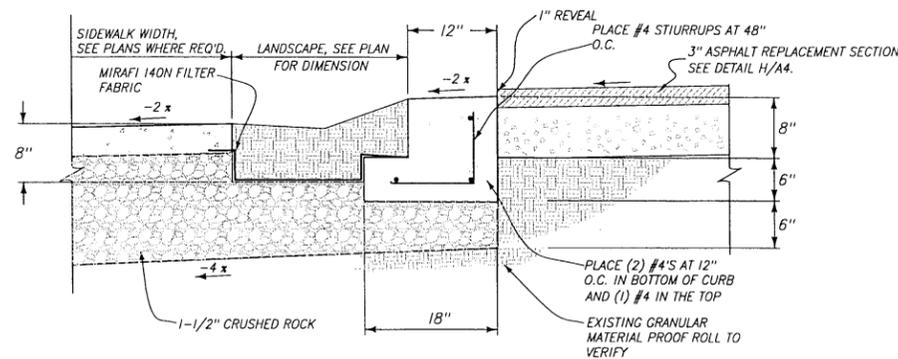


TYPICAL RIBBON CURB DETAIL - SOUTH SIDE OF EAST COLORADO
N.T.S.



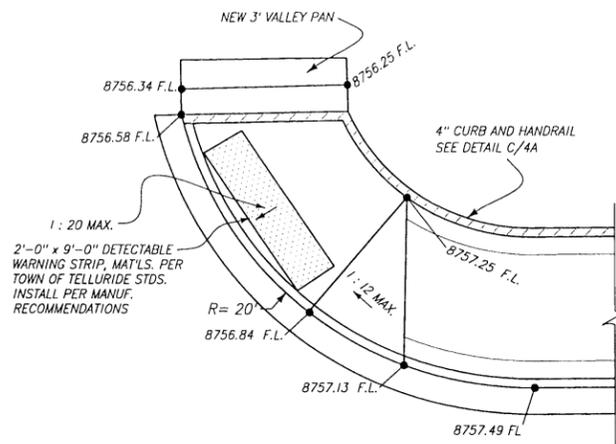
TYPICAL RIBBON CURB DETAIL - NORTH SIDE OF EAST COLORADO
N.T.S. D
A4

NOTE: SCORE CONCRETE AT 5'-0" O.C. SEE DETAIL E/A4. PLACE EXPANSION JOINT PER F/4 AT 100' O.C. MAINTAIN 3" CLEAR BETWEEN REBAR AND JOINT.

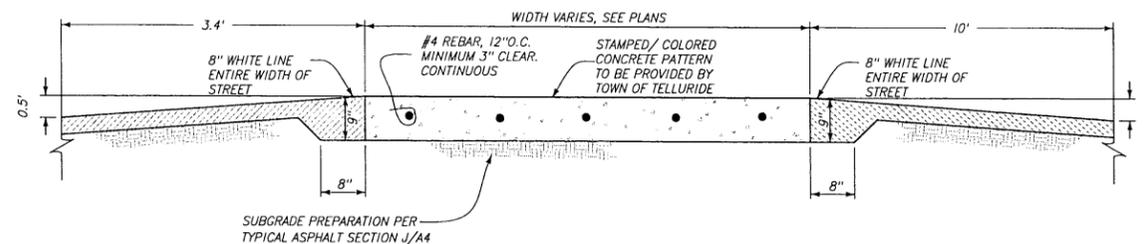


ALTERNATE RIBBON CURB DETAIL - NORTH SIDE OF EAST COLORADO SOUTH SIDE SIMILAR
N.T.S. H
A4

NOTE: SCORE CONCRETE AT 5'-0" O.C. SEE DETAIL E/4. PLACE EXPANSION JOINT PER F/4 AT 100' O.C. MAINTAIN 3" CLEAR BETWEEN REBAR AND JOINT.

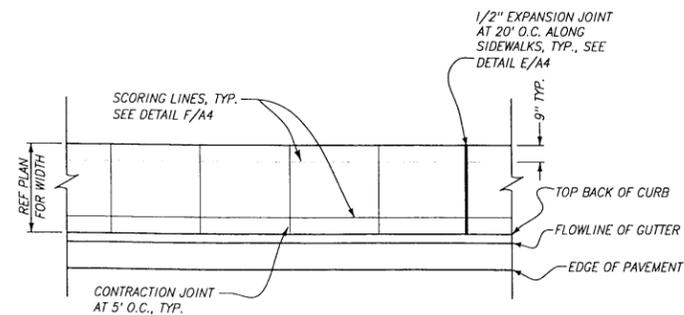


SIDEWALK CHASE DETAIL
N.T.S. B
A4

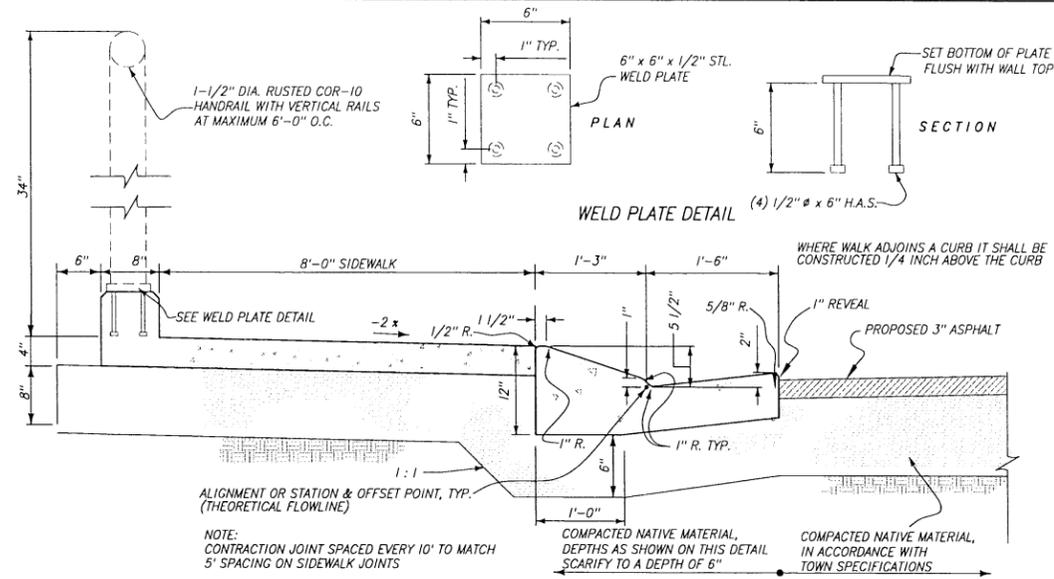


TYPICAL RAISED CROSSWALK SECTION
N.T.S. E
A4

LOCATIONS:
A STATION 2+60.47
B STATION 5+56.29
C STATION 8+55.33

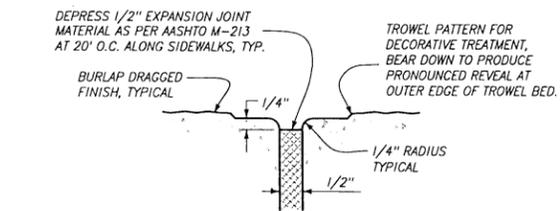


SIDEWALK SCORING PATTERN - ALL WALKWAYS
N.T.S. I
A4



EAST COLORADO - 400 BLOCK - NORTH SIDE
N.T.S. C
A4

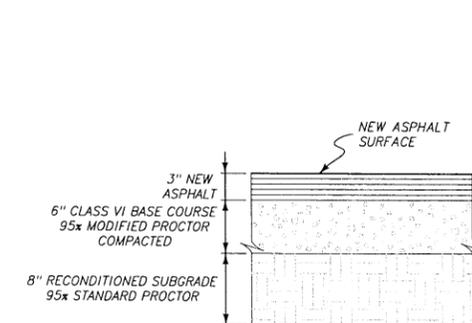
NOTE: CURB AND RAIL ARE REQUIRED AT INTERSECTION OF ALDER TO STATION 0+50.0. BEYOND THIS POINT DO NOT PLACE CURB OR RAIL.



EXPANSION JOINT DETAIL
N.T.S. F
A4



TOOLED JOINT/SCORING DETAIL
N.T.S. G
A4



TYPICAL ASPHALT SECTION
N.T.S. J
A4

PROJECT STE M260-020 // 15336

TELLURIDE STREETSCAPES - EAST TELLURIDE
EAST COLORADO AVENUE PLAN AND PROFILE
PROJECT DETAILS

Contact:
Karen Guglielmo
1370 Black Bear Road
Telluride, Colorado 81435

Client:
Town of Telluride, Colorado
P.O. Box 387
Telluride, Colorado 81435
phone 970-726-6153 fax 970-726-6050
email to: sma@foleyassoc.com

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ENGINEERING - PLANNING - SURVEYING
PO Box 1385 125 W. Pacific Ave., Suite B1
Telluride, Colorado 81435
phone 970-726-6153 fax 970-726-6050
email to: sma@foleyassoc.com

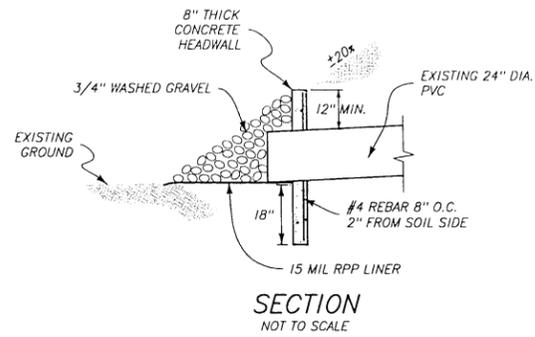
BUCKHORN GEOTECH
Civil, Structural, and Geotechnical Engineers, Inc.
222 South Park Avenue
Montrose, Colorado 81401
Phone (970) 248-8828 Fax (970) 248-0945

Rev.	Issued	Description	By	Date
1	8/1/05	ISSUED		
2	11/7/05	EAST COLORADO BID PACKAGE		
3	12/20/05	REVISED EAST COLORADO BID PACKAGE		
4		RENUMBERED SHEETS FOR EAST DISTRICT DRAWING SET		5/03/06 DC

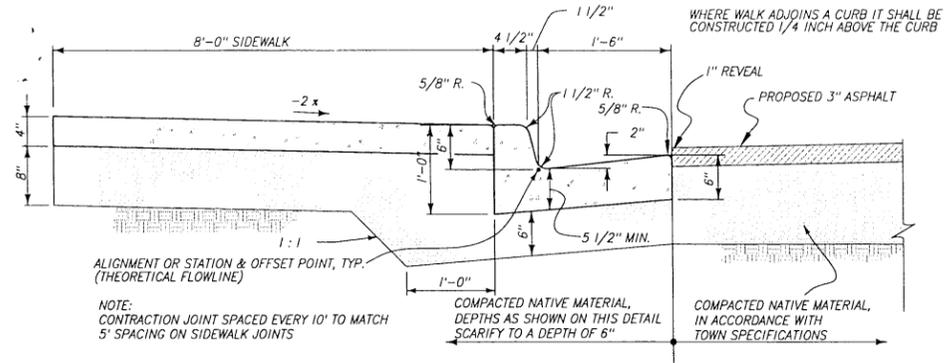
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Drawn by: DJP
Checked by: ELK
Start date: 4-16-02
Dwg path: Colorado.dwg
Sheet No. A4

Project #: 04-072

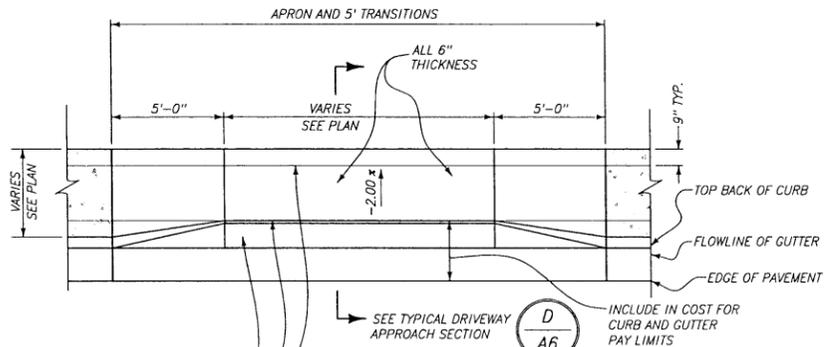
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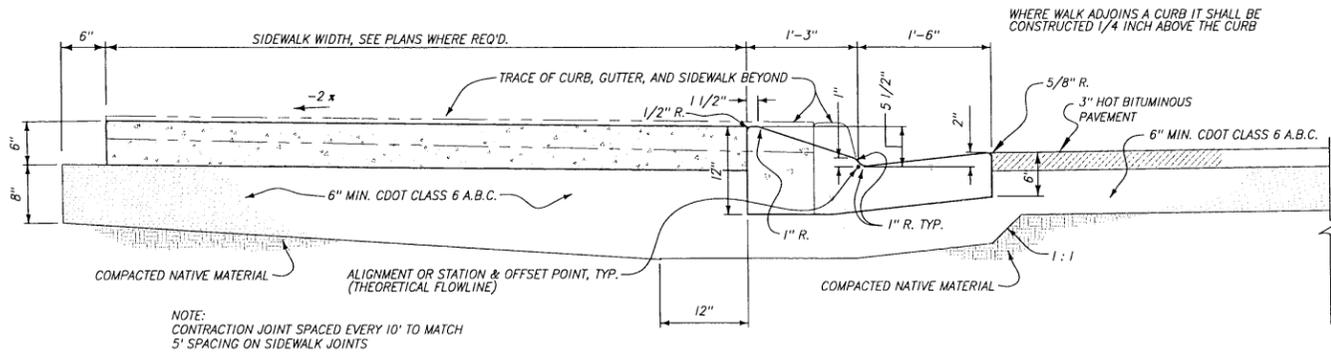
CONCRETE HEADWALL
N.T.S. A
A6



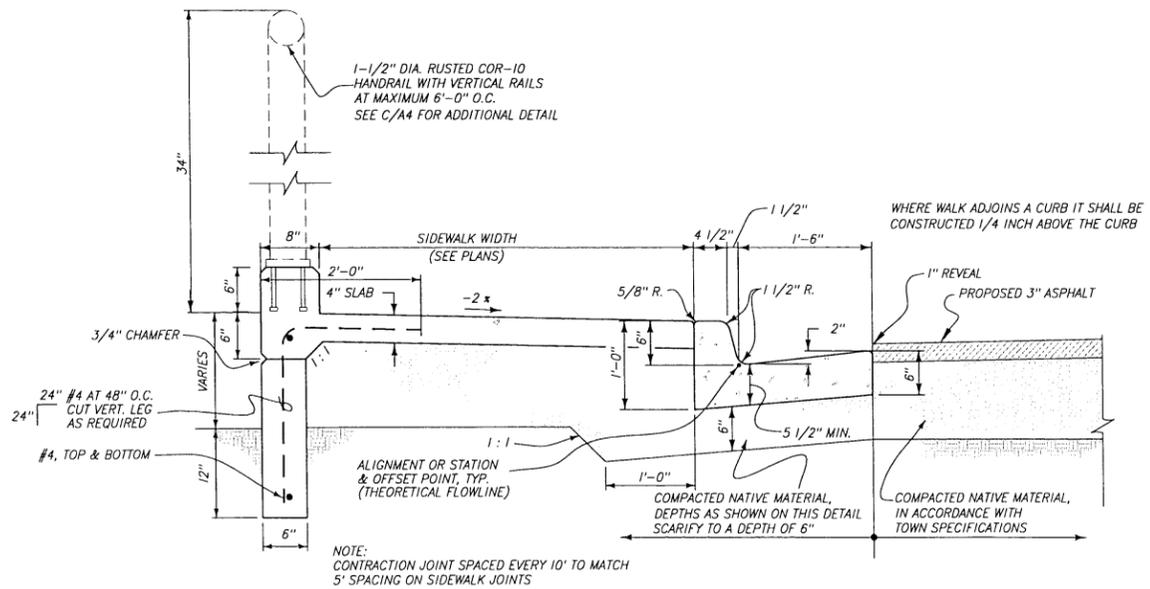
EAST COLORADO - 1000 BLOCK - NORTH SIDE
N.T.S. B
A6



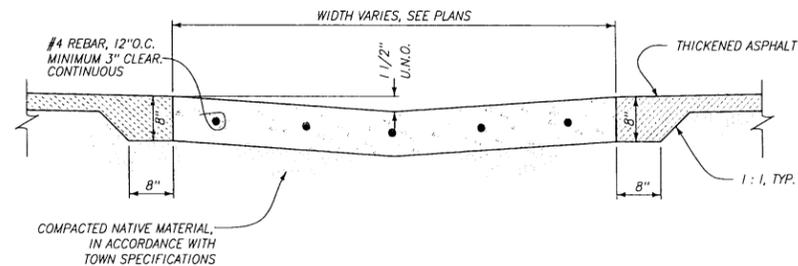
ROLL CURB PLAN
N.T.S. C
A6



ROLL CURB SECTION
N.T.S. D
A6



RETAINING WALL DETAIL
E. COLORADO, 900 BLOCK, NORTH SIDE
N.T.S. E
A6



TYPICAL V-PAN DETAIL
N.T.S. F
A6

TELLURIDE STREETSCAPES - EAST TELLURIDE
EAST COLORADO AVENUE PLAN AND PROFILE
PROJECT DETAILS

Contact:
Karen Guglielmo
1370 Black Bear Road
Telluride
Colorado 81435

Client:
Town of Telluride, Colorado
P.O. Box 397
Telluride
Colorado 81435

FOLEY ASSOCIATES, INC.
ENGINEERING - PLANNING - SURVEYING
PO Box 1385 125 W. Pacific Ave. Suite B1
Telluride, Colorado, 81435
Phone 970/726-0132 Fax 970/726-0050
e-mail: info@foleyassoc.com

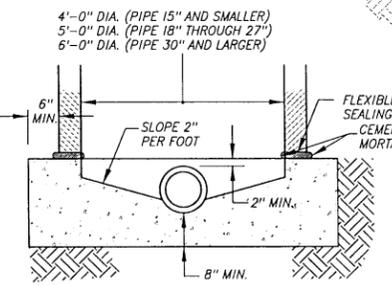
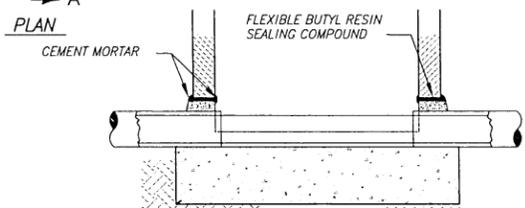
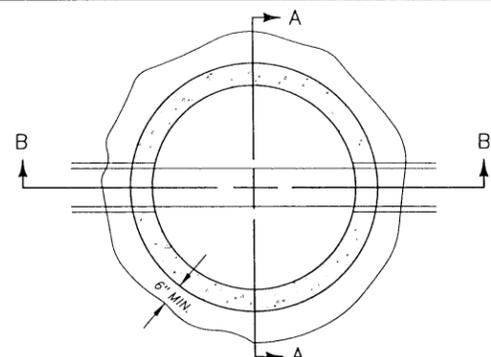
BUCKHORN GEOTECH
Civil, Structural, and Geotechnical Engineers, Inc.
1000 Pine Avenue
Montrose, Colorado 81401
Phone (970) 248-8828 Fax (970) 248-0945
e-mail: info@buckhorngeotech.com

Rev.	description	date	by
8/1/05	ISSUED		
11/7/05	EAST COLORADO BID PACKAGE		
12/29/05	REVISED EAST COLORADO BID PACKAGE		
	RENUMBERED SHEETS FOR EAST DISTRICT DRAWING SET	5/03/06	DC

Project Mgr: ELK
Drawn by: DJP
Checked by: ELK
Start date: 4-16-02
Dwg path: Colorado.dwg
Sheet No.

A6

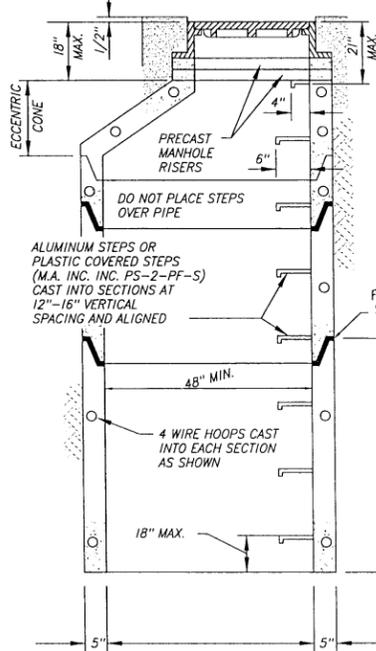
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NOTES:

1. JOINTS TO BE SET IN FLEXIBLE BUTYL RESIN SEALING COMPOUND AND GROUTED WITH MORTAR 5/8" THICK AND EXTENDING 4" EACH SIDE OF JOINT INSIDE AND OUTSIDE.
2. BASES SHALL BE REINFORCED WHEN THE DISTANCE FROM INVERT TO TOP OF COVER WILL EXCEED 15 FT. REINFORCING TO BE APPROVED BY THE ENGINEER.
3. SQUARE BASES ARE ACCEPTABLE.

STANDARD MANHOLE BASE

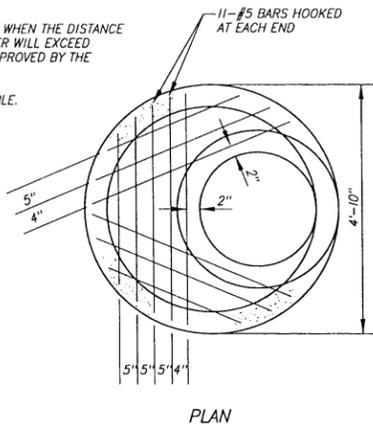


TYPICAL MANHOLE SECTION WITH ECCENTRIC CONE

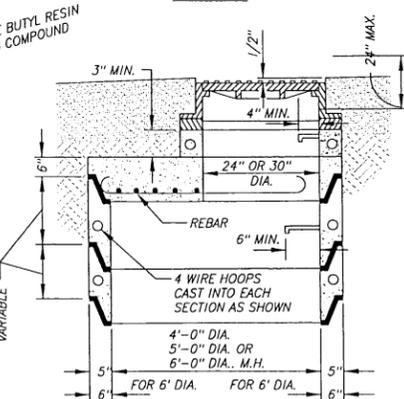
4'-0" DIA. (PIPE 15" & SMALLER)
5'-0" DIA. (PIPE 18" THROUGH 27")
6'-0" DIA. (PIPE 30" & LARGER)

STANDARD MANHOLE DETAILS

N.T.S.



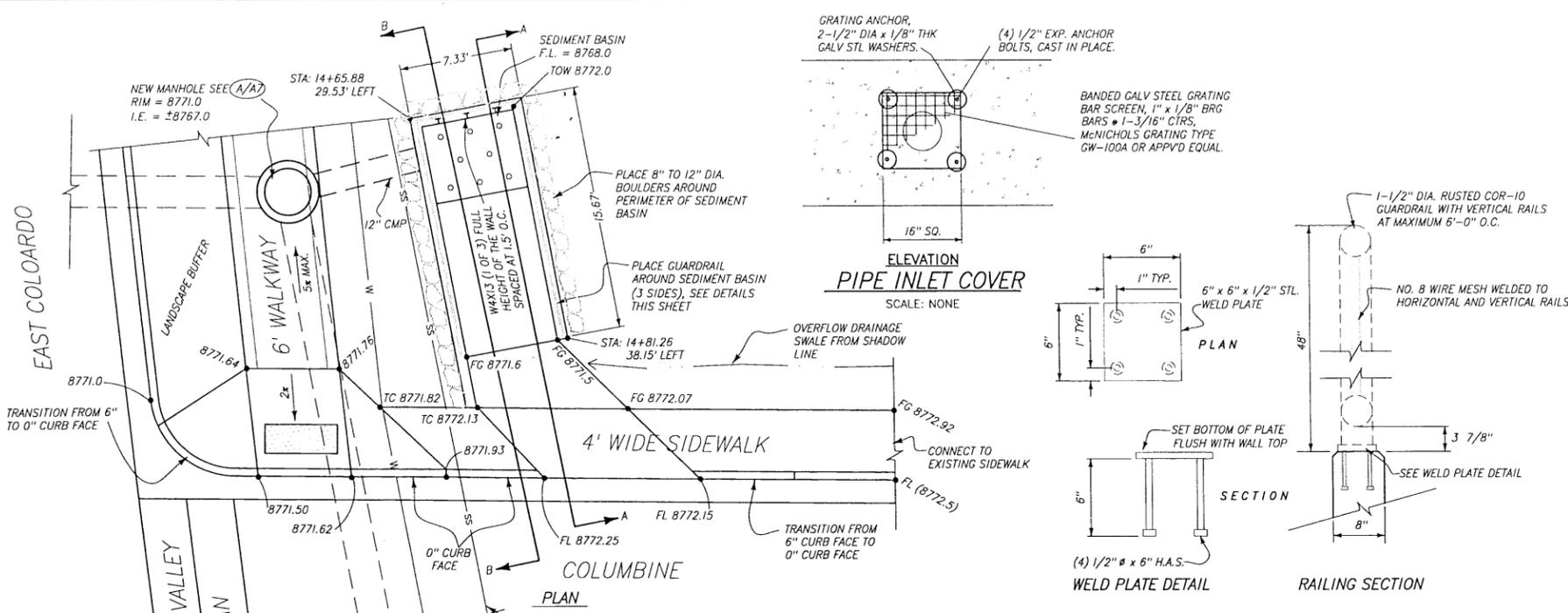
PLAN



ALTERNATE FLAT TOP

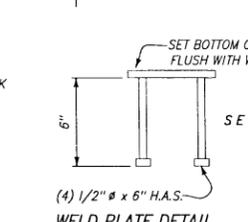
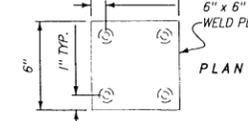
MANHOLE BARRELS AND ALTERNATE TOPS

A
A7

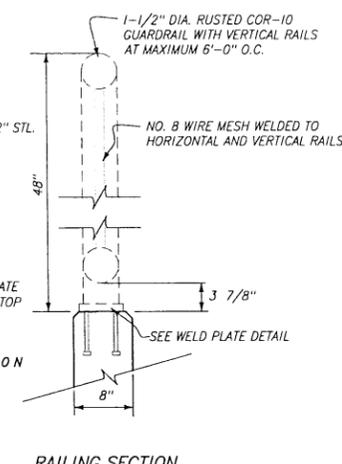


ELEVATION PIPE INLET COVER

SCALE: NONE

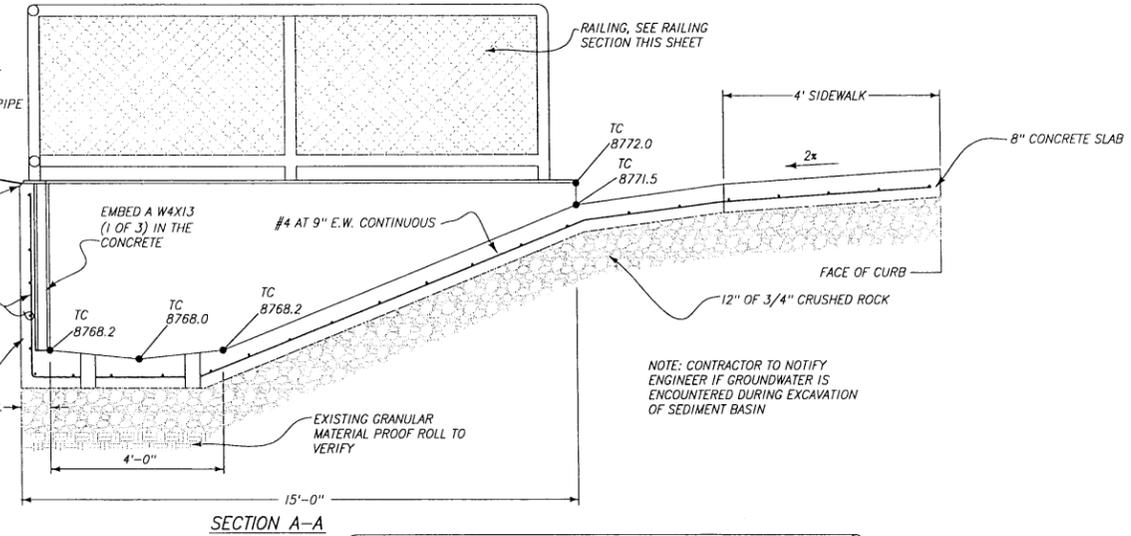


WELD PLATE DETAIL

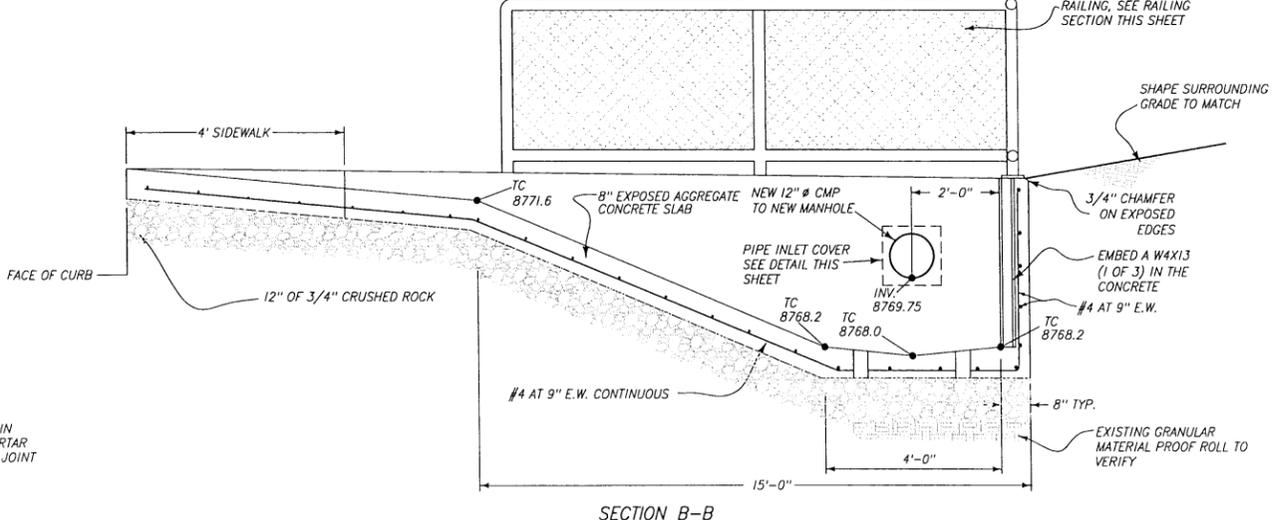


RAILING SECTION

- VERIFY LOCATION AND DEPTH OF EXISTING WATER AND SEWER LINE PRIOR TO SEDIMENT BASIN EXCAVATION. VERIFY REQUIRED PIPE PROTECTION WITH THE TOWN OF TELLURIDE.
- VERIFY LOCATION AND DEPTH OF EXISTING WATER AND SEWER LINE PRIOR TO SEDIMENT BASIN EXCAVATION. VERIFY REQUIRED PIPE PROTECTION WITH THE TOWN OF TELLURIDE.



SECTION A-A



SECTION B-B

SEDIMENT BASIN DETAILS

N.T.S.

B
A7

- NOTES:**
1. ALL JOINTS TO BE SET IN FLEXIBLE BUTYL RESIN SEALING COMPOUND AND PLASTERED WITH MORTAR 5/8" THICK AND EXTENDING 4" EACH SIDE OF JOINT INSIDE AND OUTSIDE.
 2. MORTAR ON RISER RINGS IS ACCEPTABLE.
 3. MANHOLES INSTALLED OUTSIDE OF STREET RIGHT-OF-WAY MAY REQUIRE LOCKING COVERS.
 4. "CORNERSTONE SEWER" TO BE IMPRINTED ON COVER.

PROJECT STE M260-020 // 15336

TELLURIDE STREETSCAPES - EAST TELLURIDE
EAST COLORADO AVENUE PLAN AND PROFILE
PROJECT DETAILS

Contact:
Karen Guglielmino
1370 West Colfax Road
Telluride
Colorado 81435

Client:
Town of Telluride, Colorado
P.O. Box 397
Telluride, Colorado 81435

FOLEY ASSOCIATES, INC.
ENGINEERING - PLANNING - SURVEYING

PO Box 188, 105 W. Pacific Ave., Suite B1
Telluride, Colorado, 81435
phone 970-728-6153 fax 970-728-6050
e-mail to xmail@foleyassoc.com

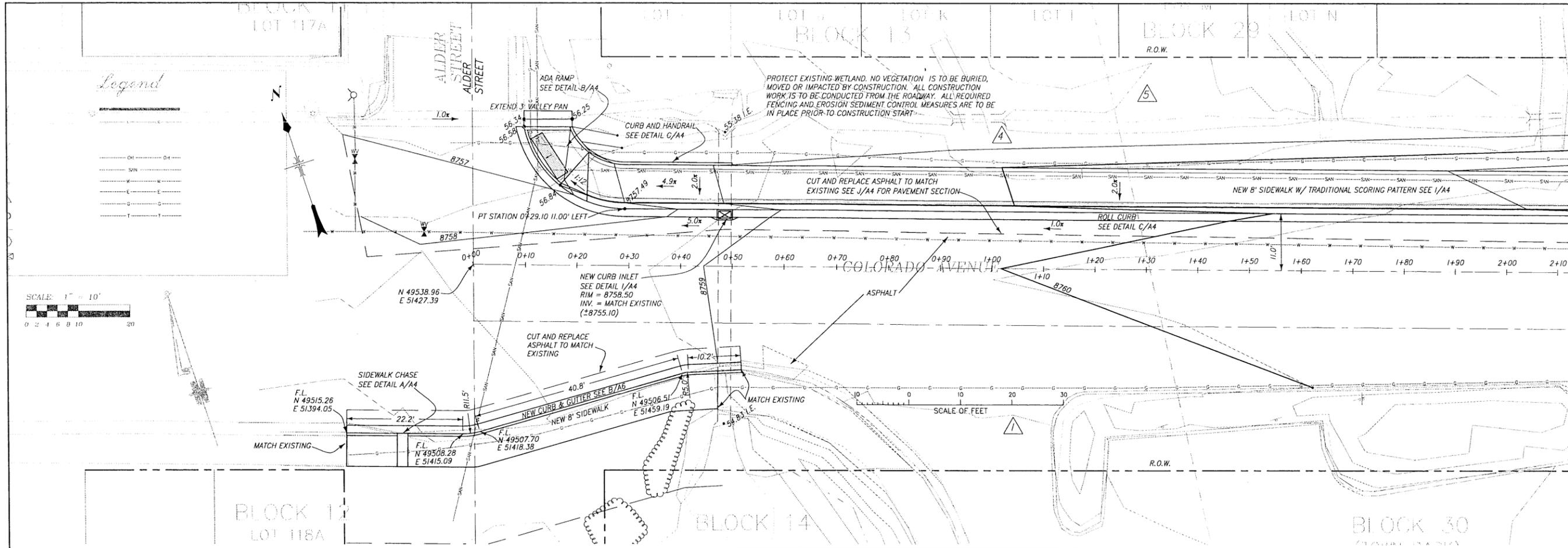
BUCKHORN GEOTECH
Civil, Structural, and Geotechnical Engineers, Inc.

222 South Park Avenue
Montrose, Colorado 81401
Phone (970) 248-8828 Fax (970) 248-0945

Rev.	date	description
1	8/7/05	SCHEMATIC PLAN
2	11/7/05	EAST COLORADO BID PACKAGE
3	12/20/05	REVISED EAST COLORADO BID PACKAGE
4	5/03/06	RENUMBERED SHEETS FOR EAST DISTRICT DRAWING SET

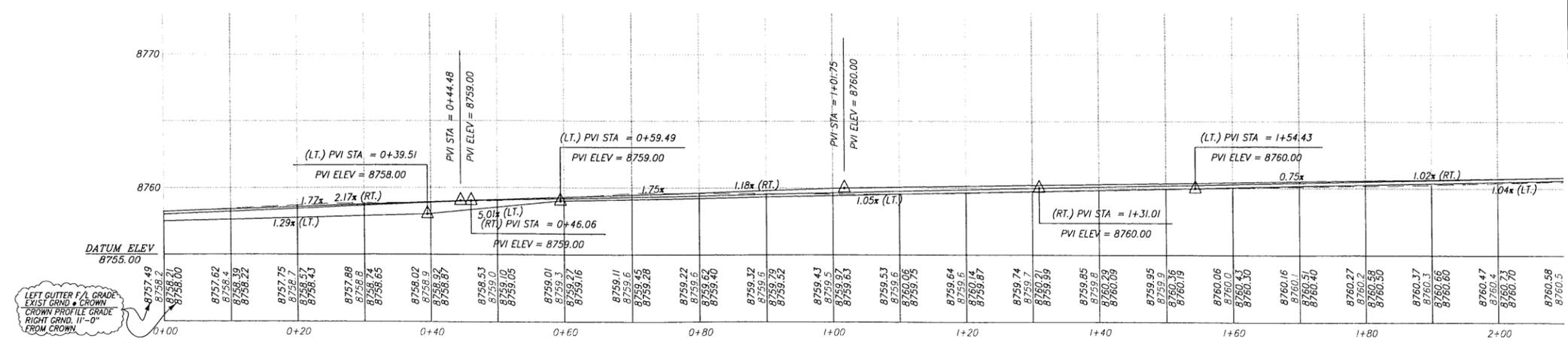
Project Mgr: ELK
Drawn by: DJP
Checked by: ELK
Start date: 4-16-02
Dwg path: Colorado.dwg
Sheet No. A7
Project #: 04-072

\\2003server\CIVIL\2004\04-072-Task 2 Streetscapes East\dwg\Colorado.dwg, 7/13/2006 9:20:53 AM, Adobe PDF



NOTES:

- △ MATCH MASTER PLAN OF PARK IMPROVEMENTS ALONG THE SOUTH SIDE OF COLORADO.
- ④ UNLESS OTHERWISE NOTED, ALL EXISTING LANDSCAPING TO REMAIN.
- ⑤ SEE STREETSCAPE R.O.W. GUIDELINES FOR LANDSCAPE SPECIFICATIONS.



PROFILE - 400 BLOCK

PROJECT STE M260-020 // 15336

TELLURIDE STREETSCAPES - EAST TELLURIDE
 EAST COLORADO AVENUE PLAN AND PROFILE
 STA. 0+00 TO STA. 2+00 (400 BLOCK)

FOLEY ASSOCIATES, INC.
 ENGINEERING • PLANNING • SURVEYING
 PO Box 1836 125 W. Pacific Ave. Suite B1
 Telluride, CO 81435
 phone 970-728-6153 fax 970-728-6050
 e-mail: info@foleyassoc.com

BUCKHORN GEOTECH
 Civil, Structural, and Geotechnical Engineers, Inc.
 222 South Park Avenue
 Montrose, Colorado 81401
 Phone (970) 249-8828 Fax (970) 249-0845

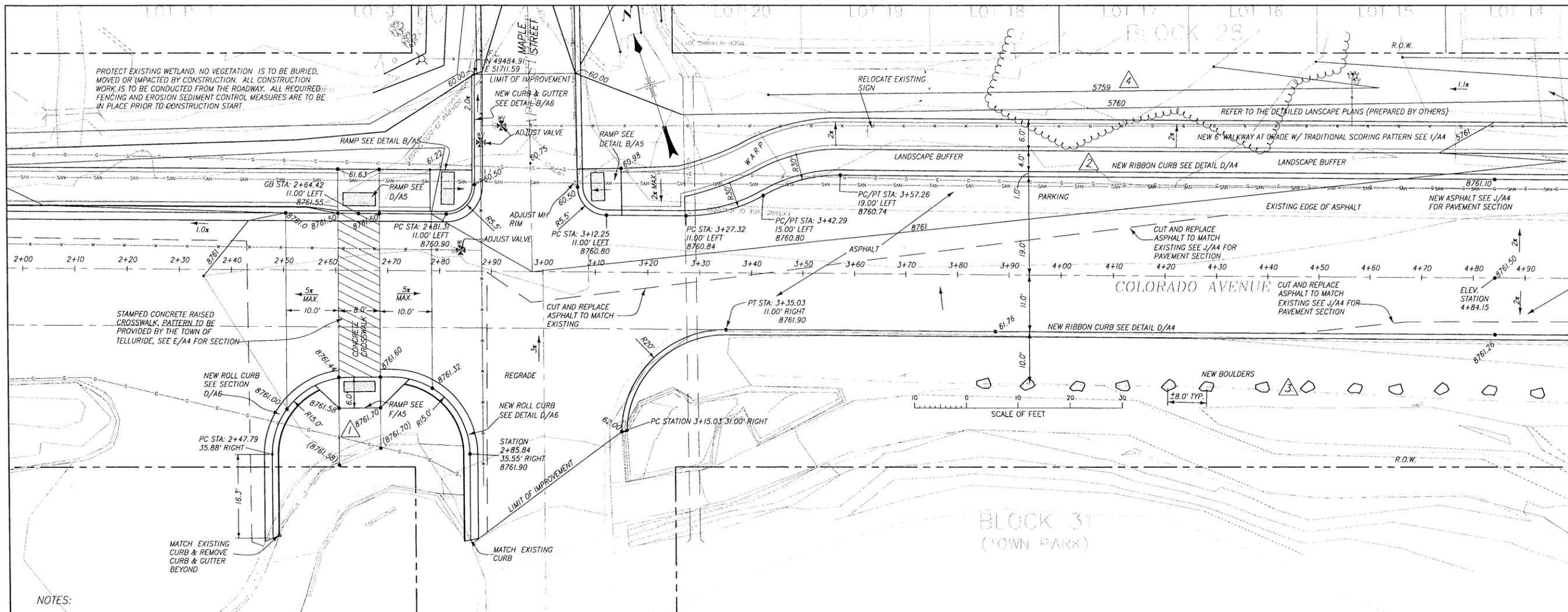
Rev.	date	description
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5/19/04		REVISED
6/18/04		REVISED
10/17/04		REVISED
10/22/04		REVISED
11/2/04		REVISED
1/10/05		REVISED
12/29/05		REVISED EAST COLORADO BID PACKAGE
		RENUMBERED SHEETS FOR EAST DISTRICT DRAWING SET 8/03/06 DC

Project Mgr: ELK
 Drawn by: BLR
 Checked by: ELK
 Start date: 4-16-02
 Dwg path: Colorado.dwg
 Sheet No. **A8**
 Project #: 04-072

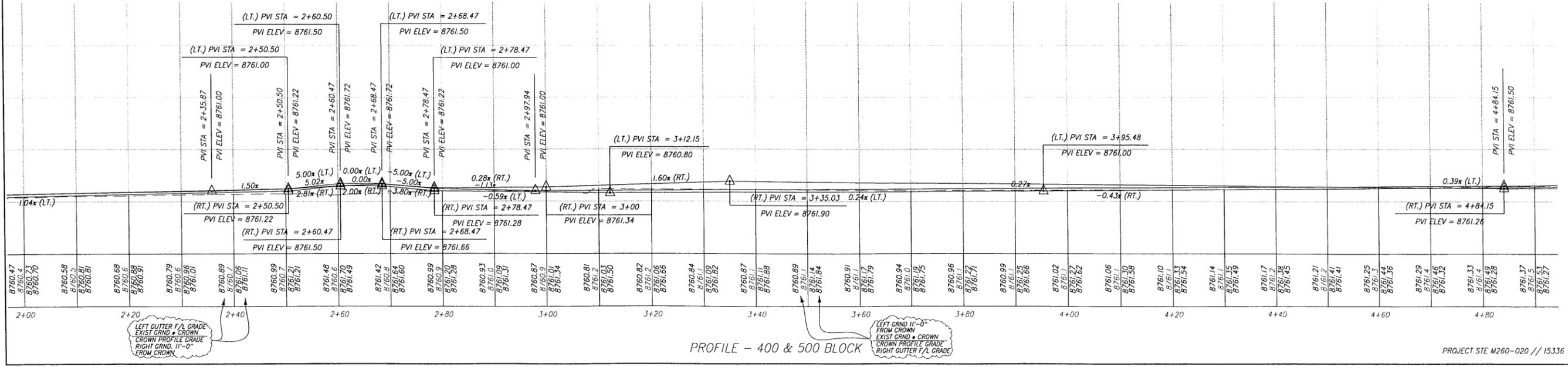
Contact:
 Karen Guglielmino
 1370 Black Bear Road
 Colorado 81435

Client:
 Town of Telluride, Colorado
 P.O. Box 397
 Colorado 81435

\\2003server\CIVIL\2004\04-072-Task 2 Streetscapes East\dwg\Colorado.dwg, 7/13/2006 9:22:06 AM, Adobe PDF



- NOTES:
- 1 MATCH MASTER PLAN OF PARK IMPROVEMENTS ALONG THE SOUTH SIDE OF COLORADO.
 - 2 LANDSCAPE BUFFER BETWEEN WALKWAY AND RIBBON CURB TO VARY, MINIMUM 3.5 FT. AND MAXIMUM 5.5 FT.
 - 3 PLACE 24"-36" DIAMETER BOULDERS SOUTH OF SPECIAL EVENTS PARKING. FINISH PARKING AREA WITH 1-1/2" ANGULAR WASHED GRAVEL.
 - 4 UNLESS OTHERWISE NOTED, ALL EXISTING LANDSCAPING TO REMAIN.
 - 5 SEE STREETSCAPE R.O.W. GUIDELINES FOR LANDSCAPE SPECIFICATIONS.



TELLURIDE STREETSCAPES - EAST TELLURIDE
EAST COLORADO AVENUE PLAN AND PROFILE
STA. 2+00 TO STA. 4+90 (400 & 500 BLOCKS)

Contact:
Karen Gugelmeire
1370 Black Bear Road
Telluride, Colorado 81435
Colorado 81435

Client:
Town of Telluride, Colorado
P.O. Box 397
Telluride, Colorado 81435
Colorado 81435

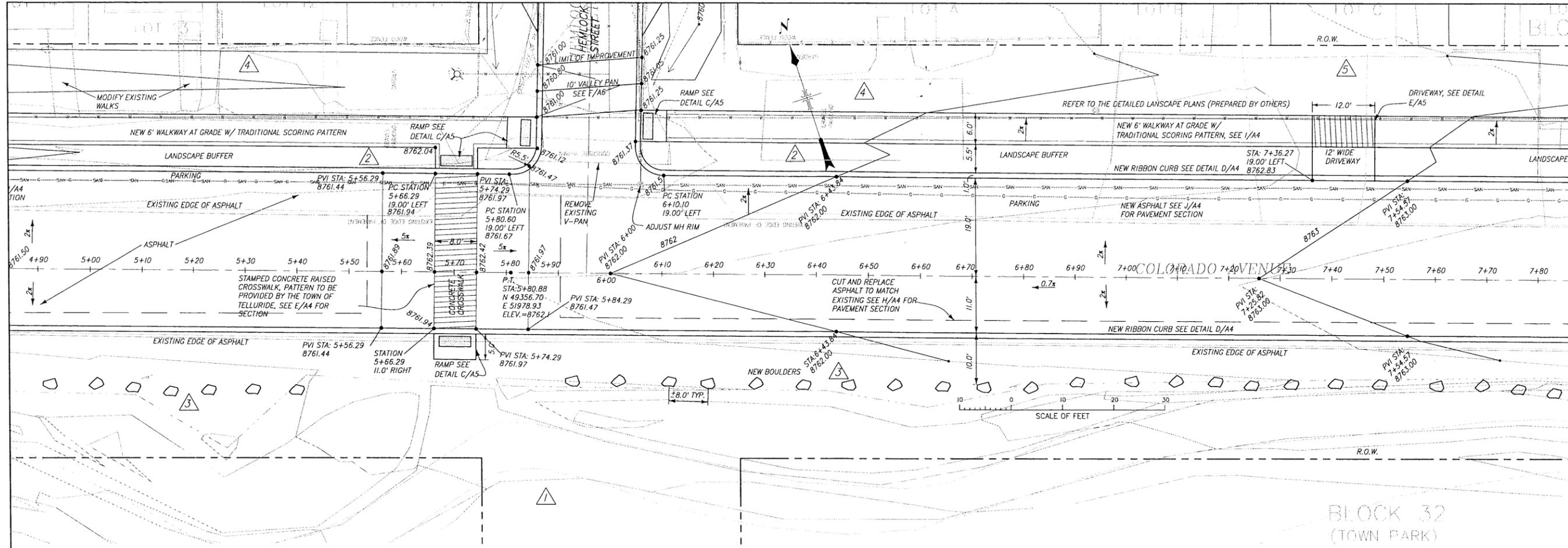
FOLEY ASSOCIATES, INC.
ENGINEERING • PLANNING • SURVEYING
PO Box 125 W Pacific Ave Suite B1
Telluride, Colorado 81401
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email fo@foleyassoc.com

BUCKHORN GEOTECH
Civil, Structural, and Geotechnical Engineers, Inc.
222 South Park Avenue
Montrose, Colorado 81401
Phone (970) 249-6828 Fax (970) 249-0945

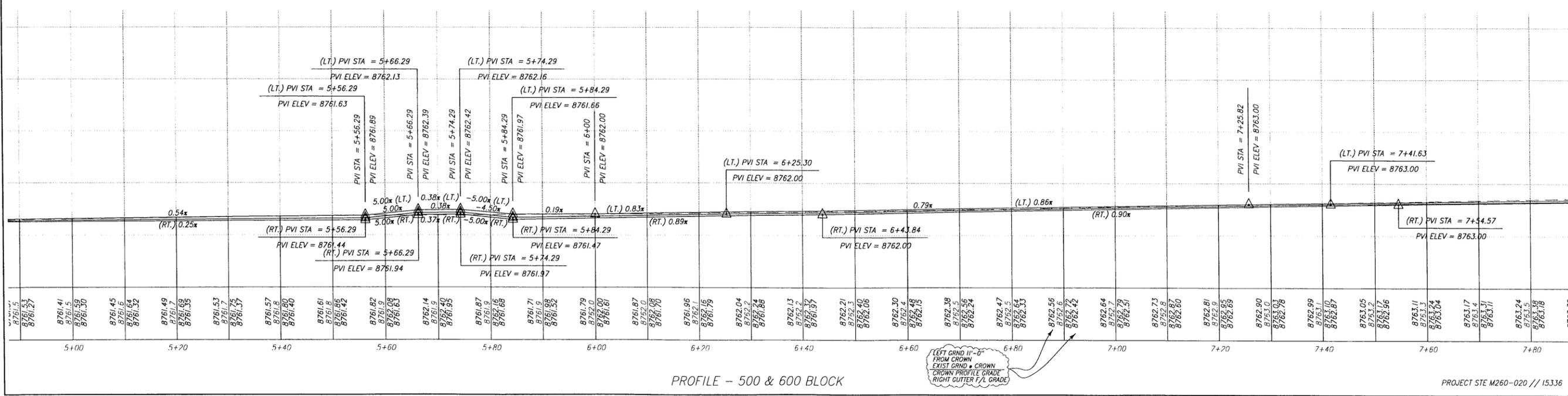
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2	6/16/04	REVISED	ELK	
3	6/18/04	REVISED	BLR	
4	10/7/04	REVISED	ELK	
5	10/22/04	REVISED	ELK	
6	11/2/04	REVISED	ELK	
7	1/10/05	REVISED	ELK	
8	11/7/05	EAST COLORADO BID PACKAGE	ELK	
9	12/20/05	REVISED EAST COLORADO BID PACKAGE	ELK	
10		RENUMBERED SHEETS & REMOVED CROSS WALK	ELK	

Dwg path: Colorado.dwg
Sheet No. **A9**
Project #: 04-072

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- NOTES:
- 1 MATCH MASTER PLAN OF PARK IMPROVEMENTS ALONG THE SOUTH SIDE OF COLORADO.
 - 2 LANDSCAPE BUFFER BETWEEN WALKWAY AND RIBBON CURB TO VARY, MINIMUM 3.5 FT. AND MAXIMUM 5.5 FT.
 - 3 PLACE 24"-36" DIAMETER BOULDERS SOUTH OF SPECIAL EVENTS PARKING. FINISH PARKING AREA WITH 1-1/2" ANGULAR WASHED GRAVEL.
 - 4 UNLESS OTHERWISE NOTED, ALL EXISTING LANDSCAPING TO REMAIN.
 - 5 SEE STREETSCAPE R.O.W. GUIDELINES FOR LANDSCAPE SPECIFICATIONS.



PROFILE - 500 & 600 BLOCK

TELLURIDE STREETSCAPES - EAST TELLURIDE
EAST COLORADO AVENUE PLAN AND PROFILE
 STA. 4+90 TO STA. 7+80 (500 & 600 BLOCKS)

Contact:
 Keren Guglielmino
 1370 Back Bear Road
 Telluride, Colorado 81435

Client:
 Town of Telluride, Colorado
 P.O. Box 397
 Telluride, Colorado 81435

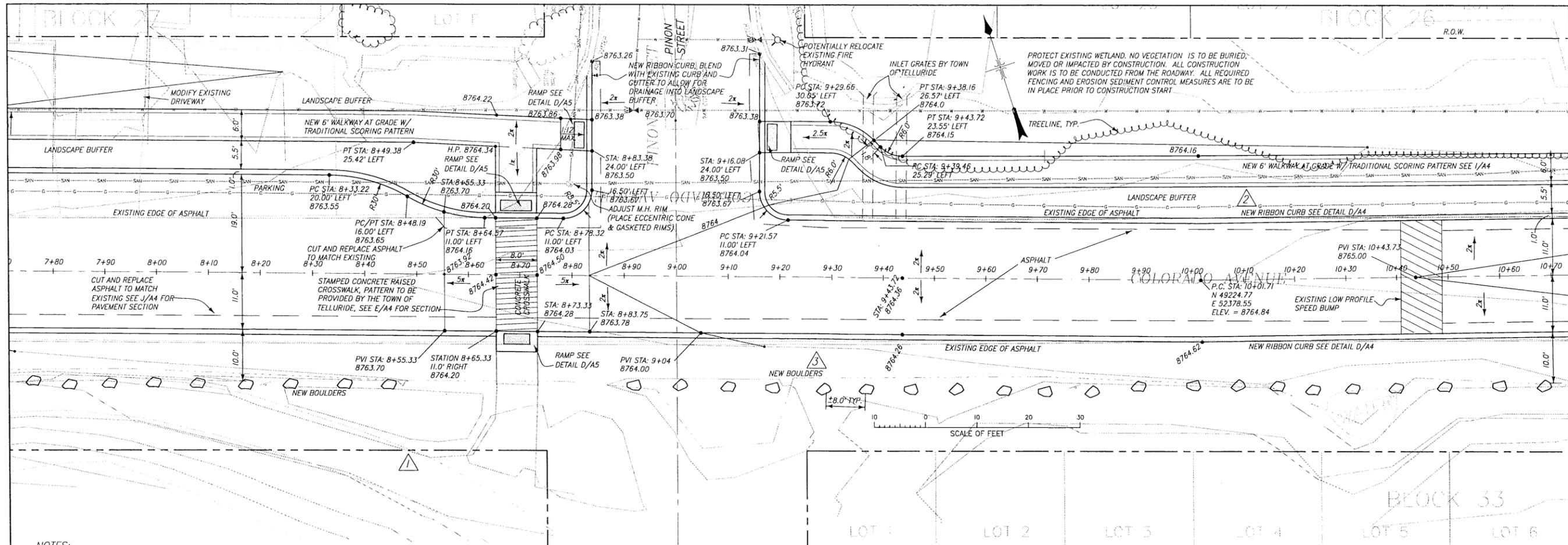
FOLEY ASSOCIATES, INC.
 ENGINEERING - PLANNING - SURVEYING
 P.O. Box 1385 125 W. Pacific Ave., Suite B1
 Montrose, Colorado 81401
 phone 970-726-6133 fax 970-726-0650
 email to xmail@foleyassoc.com

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 Civil, Structural and Geotechnical Engineers, Inc.
 222 South Park Avenue
 Montrose, Colorado 81401
 Phone (970) 249-6828 Fax (970) 249-0945

Rev.	date	description
5/25/04		SCHEMATIC PLAN
6/16/04		REVISED
6/19/04		REVISED
10/7/04		REVISED
10/22/04		REVISED
11/2/04		REVISED
11/10/05		REVISED
12/29/05		REVISED EAST COLORADO BID PACKAGE
4/25/06		REVISED OR ADDED CURB ELEVATIONS
		RENUMBERED SHEETS & REMOVED CROSS WALK

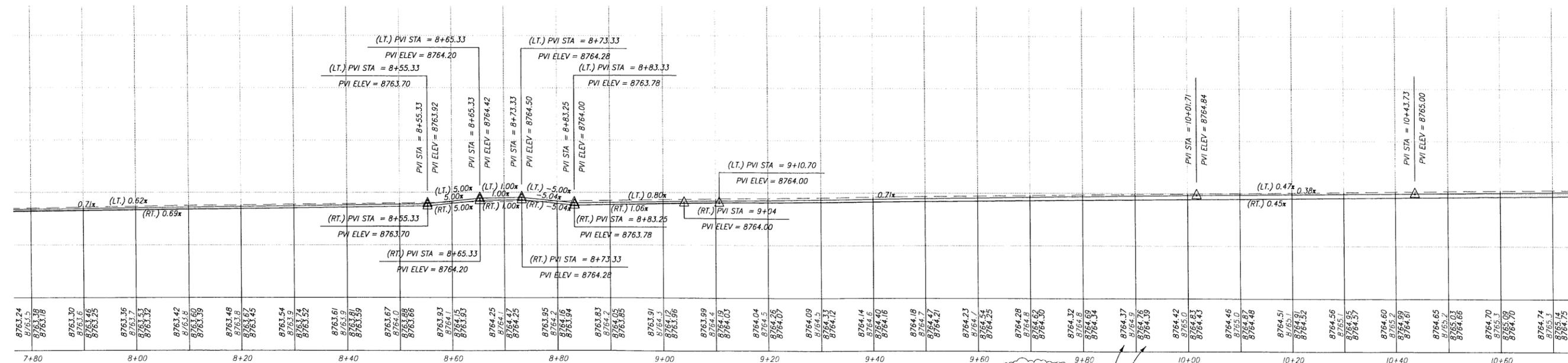
Project Mgr: ELK
 Drawn by: BLR
 Checked by: ELK
 Start date: 4-16-02
 Dwg path: Colorado.dwg
 Sheet No. **A10**
 Project #: 04-072

\\2003server\CIVIL\2004\04-072-Task 2 Streetscapes East\dwg\Colorado.dwg, 7/13/2006 9:09:33 AM, Adobe PDF



NOTES:

- 1 MATCH MASTER PLAN OF PARK IMPROVEMENTS ALONG THE SOUTH SIDE OF COLORADO.
- 2 LANDSCAPE BUFFER BETWEEN WALKWAY AND RIBBON CURB TO VARY, MINIMUM 3.5 FT. AND MAXIMUM 5.5 FT.
- 3 PLACE 24"-36" DIAMETER BOULDERS SOUTH OF SPECIAL EVENTS PARKING. FINISH PARKING AREA WITH 1-1/2" ANGULAR WASHED GRAVEL.
- 4 UNLESS OTHERWISE NOTED, ALL EXISTING LANDSCAPING TO REMAIN.
- 5 SEE STREETSCAPE R.O.W. GUIDELINES FOR LANDSCAPE SPECIFICATIONS.



PROFILE - 600 & 700 BLOCK

TELLURIDE STREETSCAPES - EAST TELLURIDE
EAST COLORADO AVENUE PLAN AND PROFILE
 STA. 7+80 TO STA. 10+70 (600 & 700 BLOCKS)

Contact:
 Karen Gugelmeier
 1370 S. Boulder Road
 Telluride, Colorado 81435

Client:
 Town of Telluride, Colorado
 P.O. Box 97
 Telluride, Colorado 81435
 Colorado 81435

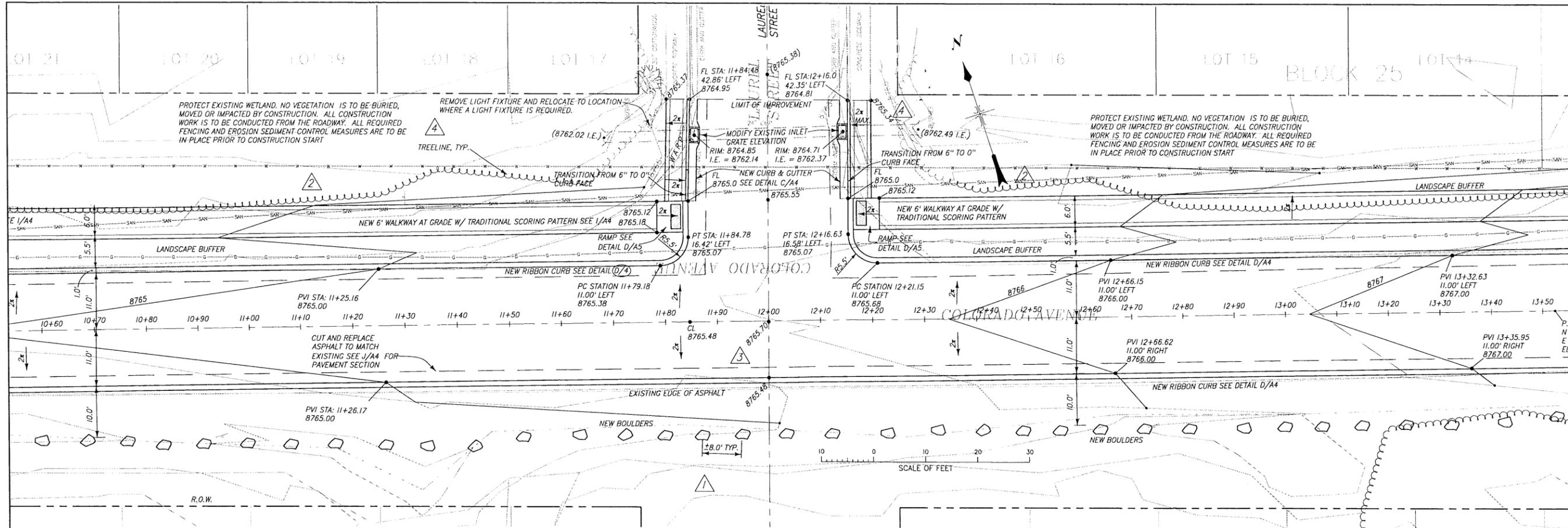
FOLEY ASSOCIATES, INC.
 ENGINEERING - PLANNING - SURVEYING

PO Box 1385 125 W. Pacific Ave. Suite B1
 Telluride, Colorado, 81435
 phone 970-726-6153 fax 970-726-6050
 e-mail to xmail@foleyassoc.com

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 Civil, Structural, and Geotechnical Engineers, Inc.
 222 South Park Avenue
 Montrose, Colorado 81401
 Phone (970) 249-8628 Fax (970) 249-0845
 e-mail to xmail@buckhorn.com

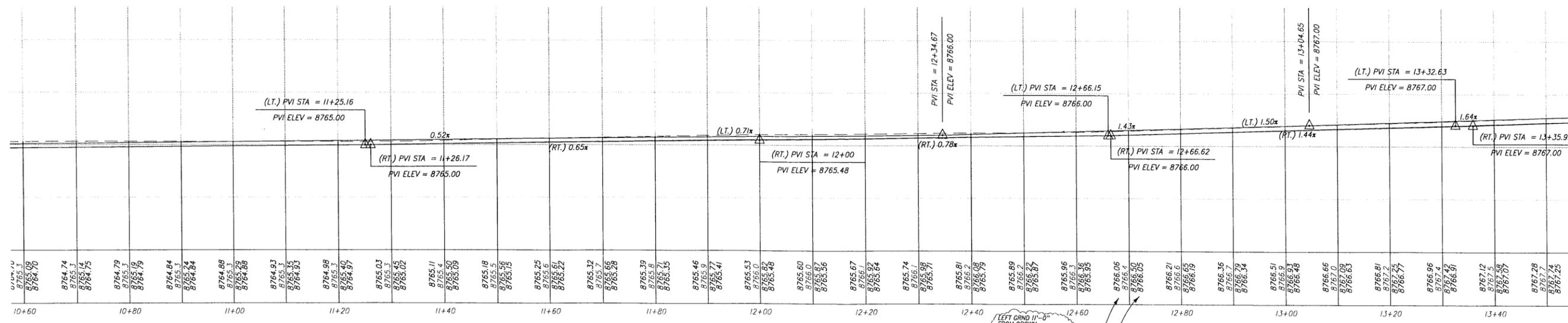
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3	6/18/04	REVISED
4	10/17/04	REVISED
5	10/23/04	REVISED
6	11/20/04	REVISED
7	11/22/04	REVISED
8	12/20/05	REVISED EAST COLORADO BID PACKAGE
9	4/25/06	REVISED FOR ADJUST CURB FINISHINGS

Project Mgr: ELK
 Drawn by: BLR
 Checked by: ELK
 Start date: 4-16-02
 Dwg path: Colorado.dwg
 Sheet No. **A11**
 Project #: 04-072



NOTES:

- 1 MATCH MASTER PLAN OF PARK IMPROVEMENTS ALONG THE SOUTH SIDE OF COLORADO.
- 2 LANDSCAPE BUFFER BETWEEN WALKWAY AND RIBBON CURB TO VARY, MINIMUM 3.5 FT. AND MAXIMUM 5.5 FT.
- 3 PLACE 24"-36" DIAMETER BOULDERS SOUTH OF SPECIAL EVENTS PARKING. FINISH PARKING AREA WITH 1-1/2" ANGULAR WASHED GRAVEL.
- 4 UNLESS OTHERWISE NOTED, ALL EXISTING LANDSCAPING TO REMAIN.
- 5 SEE STREETScape R.O.W. GUIDELINES FOR LANDSCAPE SPECIFICATIONS.



PROFILE - 700 & 800 BLOCK

PROJECT STE M260-020 // 15336

TELLURIDE STREETSCAPES - EAST TELLURIDE
 EAST COLORADO AVENUE PLAN AND PROFILE
 STA. 10+70 TO STA. 13+50 (700 & 800 BLOCKS)

Contact:
 Karen Guglielmino
 1370 Black Bear Road
 Colorado 81435

Client:
 Town of Telluride, Colorado
 P.O. Box 397
 Telluride, Colorado 81435

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 ENGINEERING • PLANNING • SURVEYING

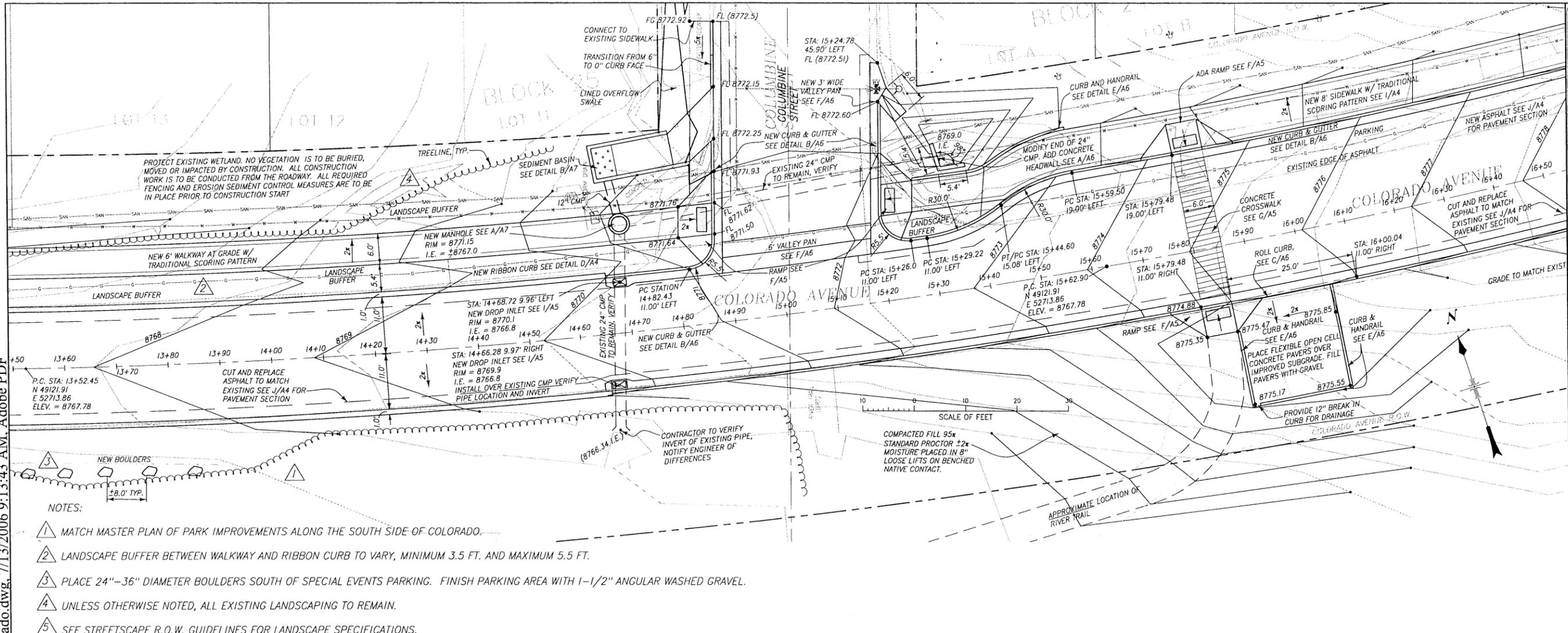
PO Box 1385 125 W. Pacific Ave. Suite B1
 Co. Springs, CO 81601
 phone 970-726-5153 fax 970-726-6050
 e-mail: smail@foleyassoc.com

BUCKHORN GEOTECH
 Civil, Structural, and Geotechnical Engineers, Inc.
 222 South Park Avenue
 Montrose, Colorado 81401
 Phone (970) 249-8828 Fax (970) 249-0945

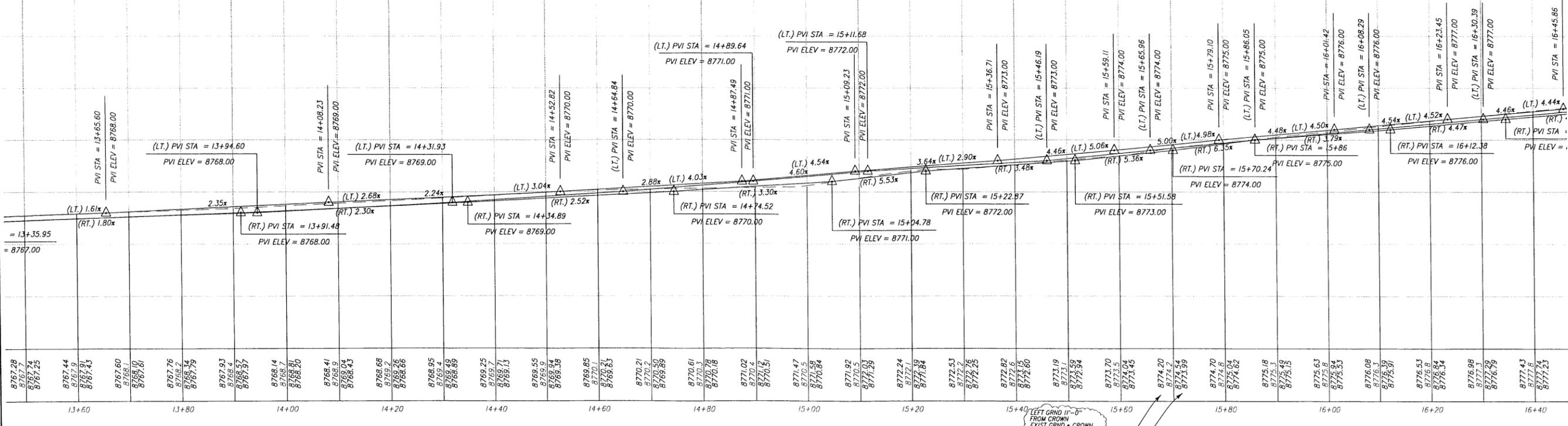
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6/19/04		REVISED
6/19/04		REVISED
10/7/04		REVISED
10/27/04		REVISED
11/2/05		REVISED
11/7/05		EAST COLORADO BID PACKAGE
12/29/05		REVISED EAST COLORADO BID PACKAGE
4/25/06		REVISED OR ADDED CURB ELEVATIONS
		RENUMBERED SHEETS FOR EAST DISTRICT DRAWING SET

Project Mgr: ELK
 Drawn by: BLR
 Checked by: ELK
 Start date: 4-16-02
 Dwg path: Colorado.dwg
 Sheet No. **A12**
 Project #: 04-072

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- NOTES:
- 1 MATCH MASTER PLAN OF PARK IMPROVEMENTS ALONG THE SOUTH SIDE OF COLORADO.
 - 2 LANDSCAPE BUFFER BETWEEN WALKWAY AND RIBBON CURB TO VARY, MINIMUM 3.5 FT. AND MAXIMUM 5.5 FT.
 - 3 PLACE 24"-36" DIAMETER BOULDERS SOUTH OF SPECIAL EVENTS PARKING. FINISH PARKING AREA WITH 1-1/2" ANGULAR WASHED GRAVEL.
 - 4 UNLESS OTHERWISE NOTED, ALL EXISTING LANDSCAPING TO REMAIN.
 - 5 SEE STREETSCAPE R.O.W. GUIDELINES FOR LANDSCAPE SPECIFICATIONS.



TELLURIDE STREETSCAPES - EAST TELLURIDE
EAST COLORADO AVENUE PLAN AND PROFILE
STA. 13+50 TO STA. 16+50 (800 & 900 BLOCKS)

Contact:
Keren Coughlene
1370 Black Bear Road
Telluride
Colorado 81435

Client:
Town of Telluride
P.O. Box 397
Telluride
Colorado 81435

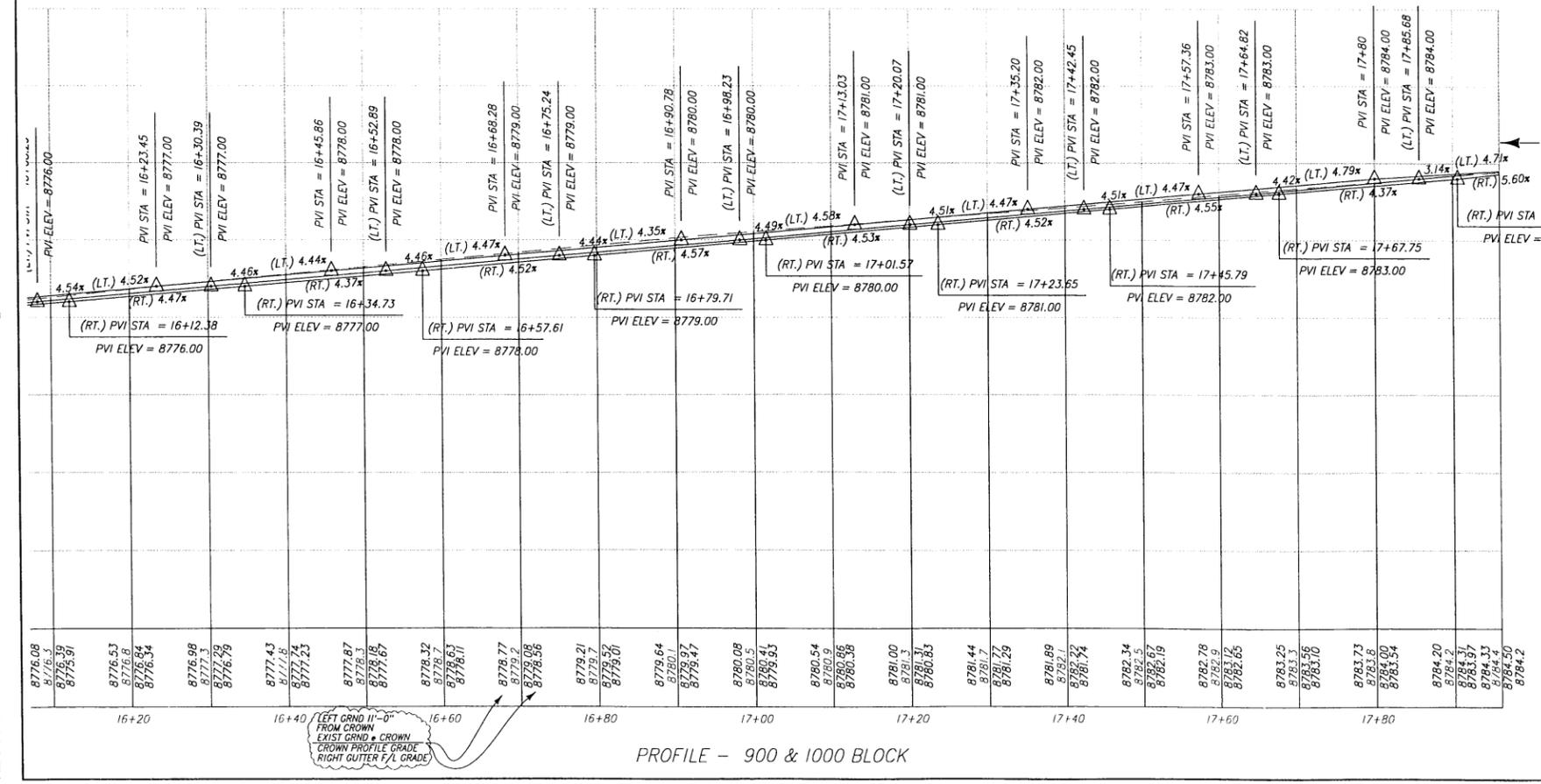
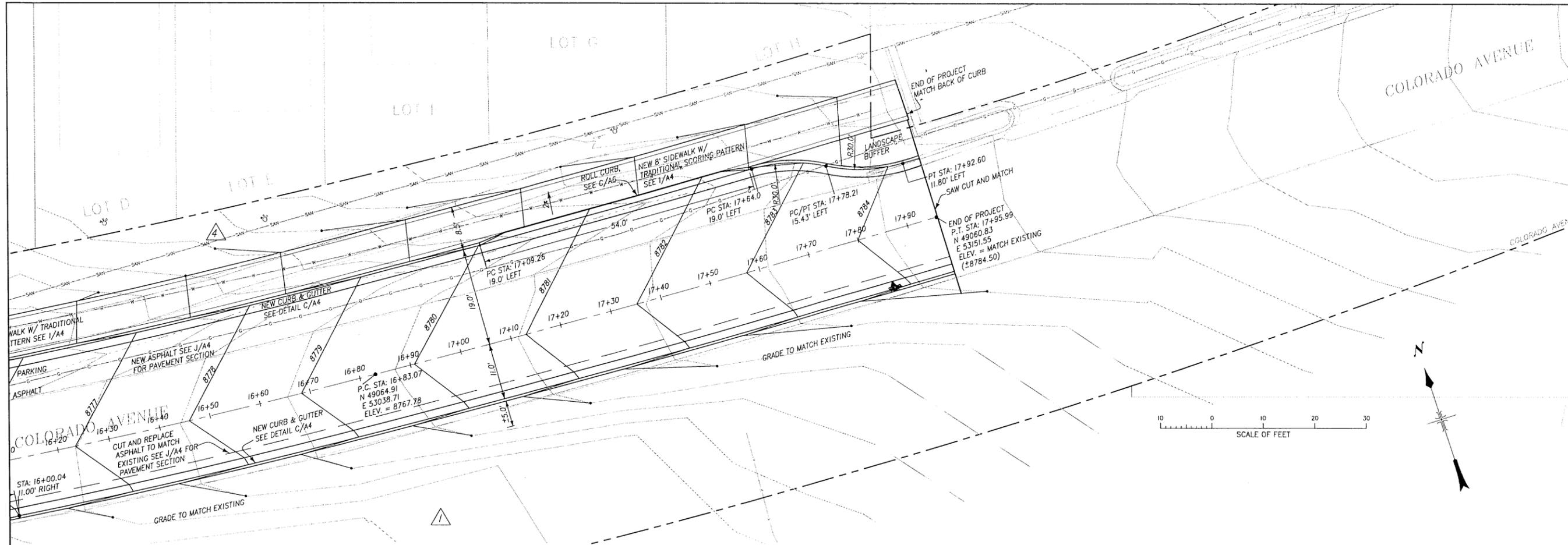
FOLEY ASSOCIATES, INC.
ENGINEERING - PLANNING - SURVEYING
PO Box 1385 125 W. Pacific Ave. Suite B1
Telluride, Colorado 81435
phone 970-726-6163 fax 970-726-6050
e-mail to: amul@foleyassoc.com

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Civil, Structural, and Geotechnical Engineers, Inc.
222 South Park Avenue
Montrose, Colorado 81401
Phone (970) 249-8828 Fax (970) 249-9845

Rev.	date	description
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6/18/04		REVISED
10/27/04		REVISED
10/22/04		REVISED
11/2/04		REVISED
11/10/05		REVISED
11/17/05		REVISED EAST COLORADO BID PACKAGE
12/27/05		REVISED EAST COLORADO BID PACKAGE
		RENUMBERED SHEETS & REMOVED CROSS WALK

Project Mgr: ELK
Drawn by: DJP
Checked by: ELK
Start date: 4-16-02
Dwg path: Colorado.dwg
Sheet No. **A13**

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- NOTES:
- 1 MATCH MASTER PLAN OF PARK IMPROVEMENTS ALONG THE SOUTH SIDE OF COLORADO.
 - 4 UNLESS OTHERWISE NOTED, ALL EXISTING LANDSCAPING TO REMAIN.
 - 5 SEE STREETSCAPE R.O.W. GUIDELINES FOR LANDSCAPE SPECIFICATIONS.

TELLURIDE STREETSCAPES - EAST TELLURIDE
EAST COLORADO AVENUE PLAN AND PROFILE
STA. 16+20 TO STA. 17+96 (900 & 1000 BLOCKS)

Contact:
 Karen Gugliemone
 1370 Buck Beer Road
 Colorado 81435

Client:
 Town of Telluride, Colorado
 P.O. Box 397
 Colorado 81435

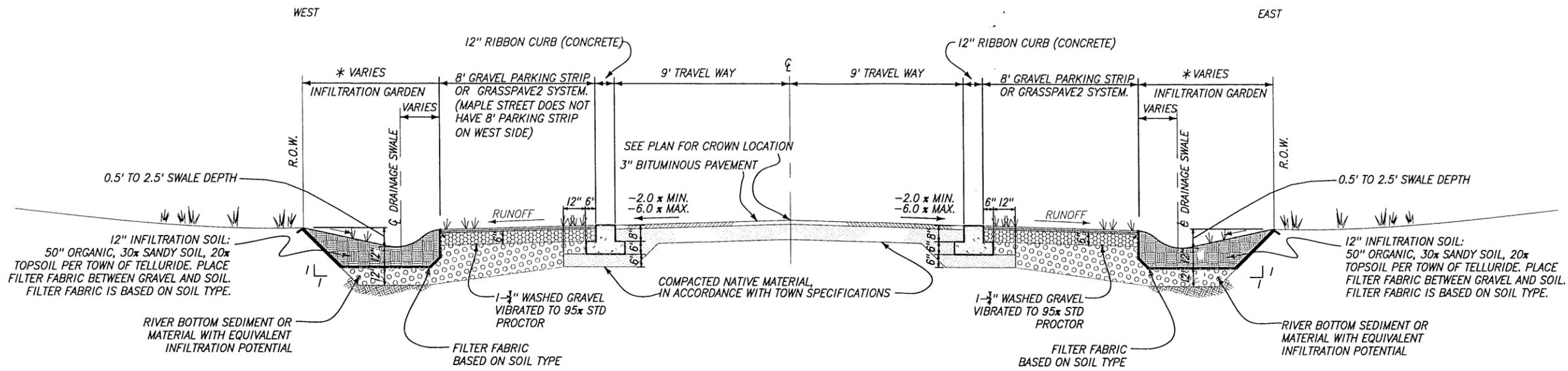
FOLEY ASSOCIATES, INC.
 ENGINEERING • PLANNING • SURVEYING
 PO Box 188, 125 W. Pacific Ave. Suite B1
 Telluride, CO 81435
 phone 970-726-1533 fax 970-726-6050
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BUCKHORN GEOTECH
 Civil, Structural, and Geotechnical Engineers, Inc.
 222 South Park Avenue
 Montrose, Colorado 81401
 Phone (970) 249-8828 Fax (970) 249-0945

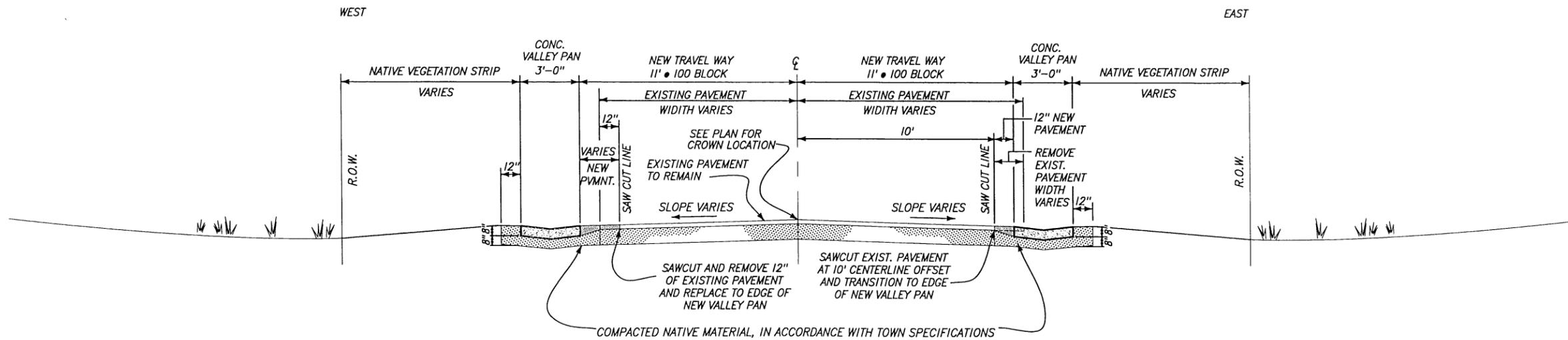
Rev.	Description	Date
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6/18/04	REVISED PLAN	
10/7/04	REVISED PLAN	
10/22/04	REVISED PLAN	
11/2/04	REVISED PLAN	
11/7/05	EAST COLORADO BID PACKAGE	
12/20/05	REVISED EAST COLORADO BID PACKAGE	
	RENUMBERED SHEETS FOR EAST DISTRICT DRAWING SET	5/03/06 DC

Project Mgr: ELK
 Drawn by: BLR
 Checked by: ELK
 Start date: 4-16-02
 Dwg path: Colorado.dwg
 Sheet No. **A14**
 Project #: 04-072

\\2003server\CIVIL\2004\04-072-Task 2 Streetscapes East.dwg\Colorado-X-STREETS.dwg, 7/18/2006 2:31:19 PM, Adobe PDF



NORTH MAPLE - 100 BLOCK
NORTH HEMLOCK - 100 BLOCK
LOOKING NORTH



NORTH COLUMBINE STREET - 100 BLOCK
LOOKING NORTH

TELLURIDE STREETSCAPES - EAST TELLURIDE
EAST COLORADO AVENUE CROSS STREETS
SCHEMATIC STREET CROSS-SECTIONS

Contact:
Korn Copelmore
1370 Block Bear Road
Telluride
Colorado 81435

Client:
Town of Telluride
P.O. Box 397
Telluride
Colorado 81435

FOLEY ASSOCIATES, INC.

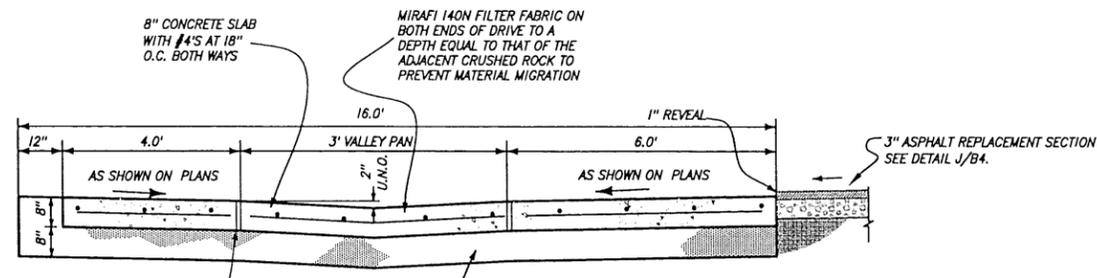
ENGINEERING - PLANNING - SURVEYING
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Telluride, Colorado, 81435
phone 970/726-6133 fax 970/728-6050
email to email@foleyassoc.com

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Phone (970) 246-8828 Fax (970) 249-8945
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Rev.	date	description
1	4/03/06 DDC	TASK FORCE REVIEW SUBMITTAL
2	5/16/06 DDC	REVISIONS PER TOWN COMMENTS DATED 4/21/06
3	6/12/06 DDC	REVISIONS PER TOWN COMMENTS DATED 5/16/06
4	7/18/06 DDC	FINAL CONSTRUCTION SET

Project Mgr: ELK
Drawn by: DDC
Checked by: ELK
Start date: 4-16-02
Dwg path: Colorado-X-Streets.dwg
Sheet No.
B3
Project #: 04-072-02

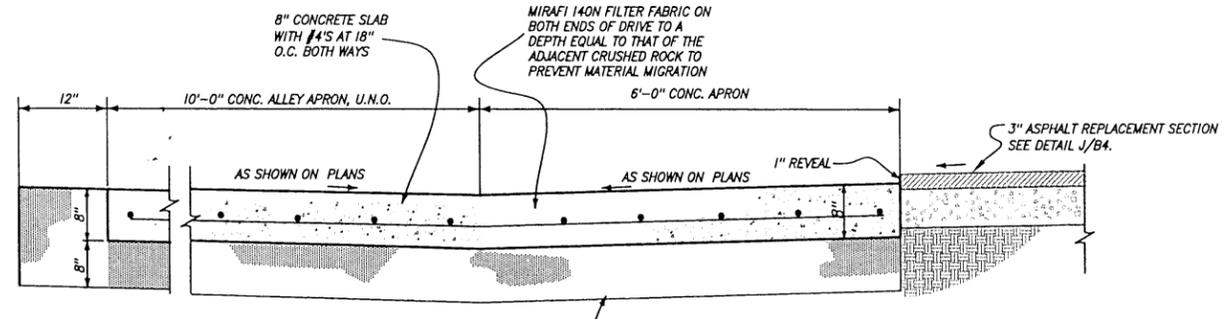


TYPICAL ALLEY DETAIL

N.T.S.

A
B4

DETAIL SCHEDULE		
STREET	STATION	SHEET
MAPLE STREET	1+62.41 TO 1+92.41 RIGHT	SHEET 5
HEMLOCK STREET	1+26 TO 1+56 RIGHT	SHEET 7 & 8
HEMLOCK STREET	1+63.50 TO 2+08 LEFT	SHEETS 7 & 8

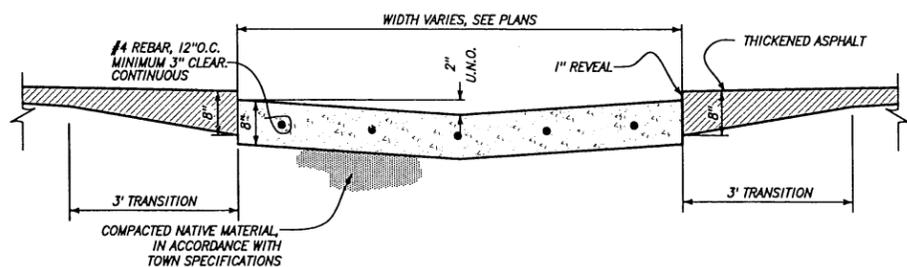


TYPICAL ALLEY DETAIL

N.T.S.

B
B4

DETAIL SCHEDULE		
STREET	STATION	SHEET
MAPLE STREET	1+62.41 TO 192.41 LEFT	SHEETS 7 & 8

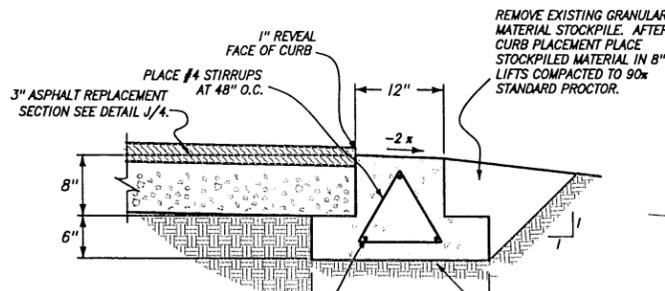


TYPICAL V-PAN DETAIL

N.T.S.

C
B4

DETAIL SCHEDULE		
STREET	STATION	SHEET
HEMLOCK STREET	1+63.50 TO 2+08 LEFT	SHEETS 7 & 8
COLUMBINE STREET	1+82.34 TO 2+84.68 LEFT	SHEET 10
	1+51.80 TO 2+94.68 RIGHT	SHEET 10

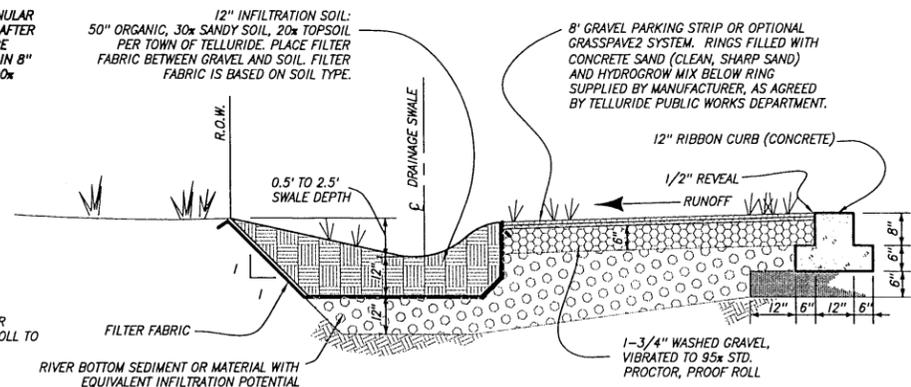


TYPICAL RIBBON CURB DETAIL

N.T.S.

D
B4

DETAIL SCHEDULE		
STREET	STATION	SHEET
MAPLE STREET	0+38 TO 3+04 LEFT	SHEETS 5 & 6
	0+38 TO 3+04 RIGHT	SHEETS 5 & 6
HEMLOCK STREET	0+39 TO 3+10 LEFT	SHEETS 7 & 8
	0+39 TO 3+10 RIGHT	SHEETS 7 & 8

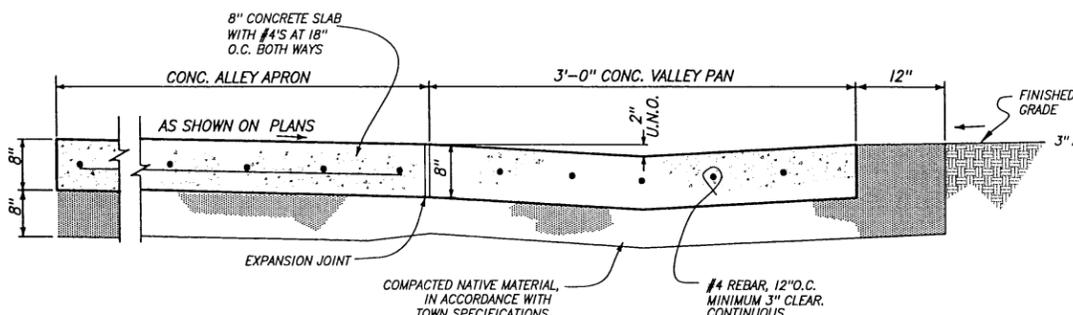


TYPICAL INFILTRATION GARDEN

N.T.S.

E
B4

DETAIL SCHEDULE		
STREET	STATION	SHEET
MAPLE STREET	0+50 TO 2+90 LEFT	SHEETS 5 & 6
	0+40 TO 2+90 RIGHT	SHEETS 5 & 6
HEMLOCK STREET	0+45 TO 2+90 LEFT	SHEETS 7 & 8
	0+45 TO 2+90 RIGHT	SHEETS 7 & 8

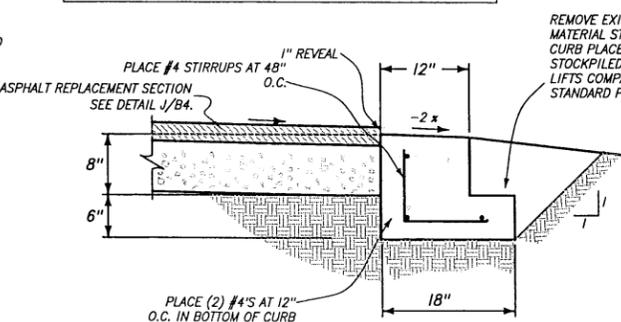


ALLEY APPROACH WITH V-PAN DETAIL

N.T.S.

G
B4

DETAIL SCHEDULE		
STREET	STATION	SHEET
MAPLE STREET	1+68.41 TO 1+88.28 LEFT	SHEETS 5

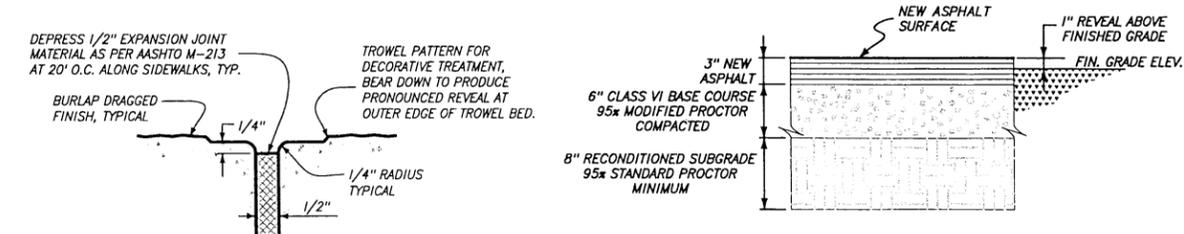


ALTERNATE RIBBON CURB DETAIL

N.T.S.

H
B4

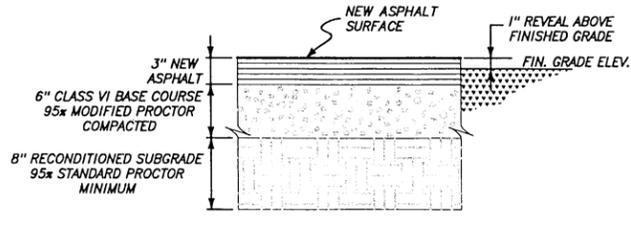
DETAIL SCHEDULE		
STREET	STATION	SHEET
MAPLE STREET	0+50 TO 2+90 LEFT	SHEETS 5 & 6
	0+40 TO 2+90 RIGHT	SHEETS 5 & 6
HEMLOCK STREET	0+45 TO 2+90 LEFT	SHEETS 7 & 8
	0+45 TO 2+90 RIGHT	SHEETS 7 & 8



EXPANSION JOINT DETAIL

N.T.S.

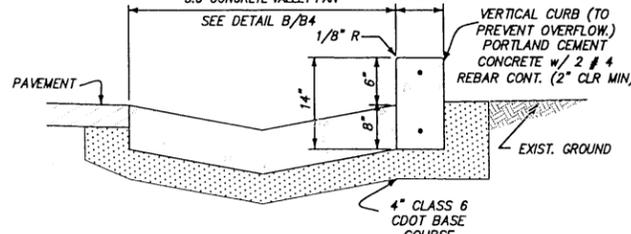
I
B4



TYPICAL ASPHALT SECTION

N.T.S.

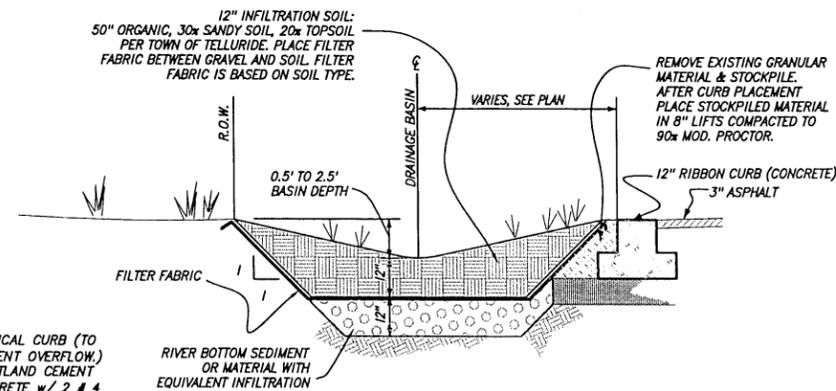
J
B4



TYPICAL VERT. CURB SECTION

N.T.S.

K
B4



TYPICAL DRAINAGE BASIN

N.T.S.

F
B4

DETAIL SCHEDULE		
STREET	STATION	SHEET
MAPLE STREET	1+52.50 & 2+03.50 LEFT	SHEETS 5 & 6
	1+59.00 & 1+95.50 RIGHT	SHEETS 5 & 6
HEMLOCK STREET	XX LEFT	SHEETS 7 & 8
	XX RIGHT	SHEETS 7 & 8

TELLURIDE STREETSCAPES - EAST TELLURIDE
EAST COLORADO AVENUE CROSS STREETS
PROJECT DETAILS

Contact:
Karen Guglielmino
1370 Becket Beer Road
Colorado 81435

Client:
Town of Telluride, Colorado
P.O. Box 387
Telluride, Colorado 81435

FOLEY
Associates, Inc.

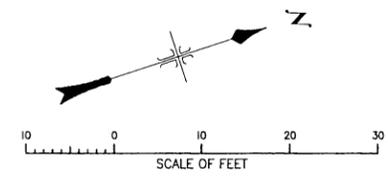
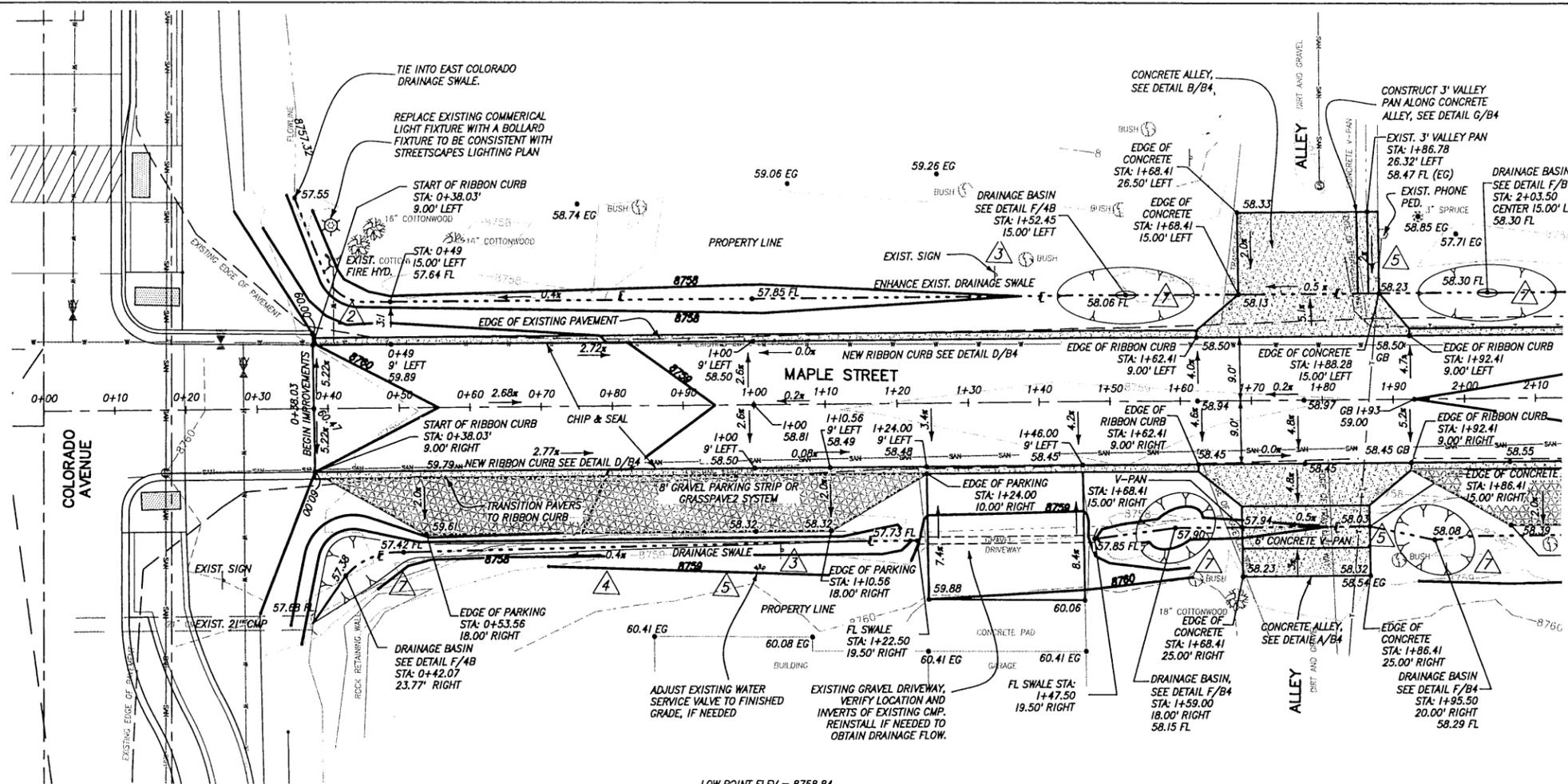
ENGINEERING - PLANNING - SURVEYING
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Telluride, Colorado 81435
phone 970-728-1531 fax 970-728-6050
e-mail to: mail@foleyassoc.com

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Phone (970) 248-8828 Fax (970) 248-0845
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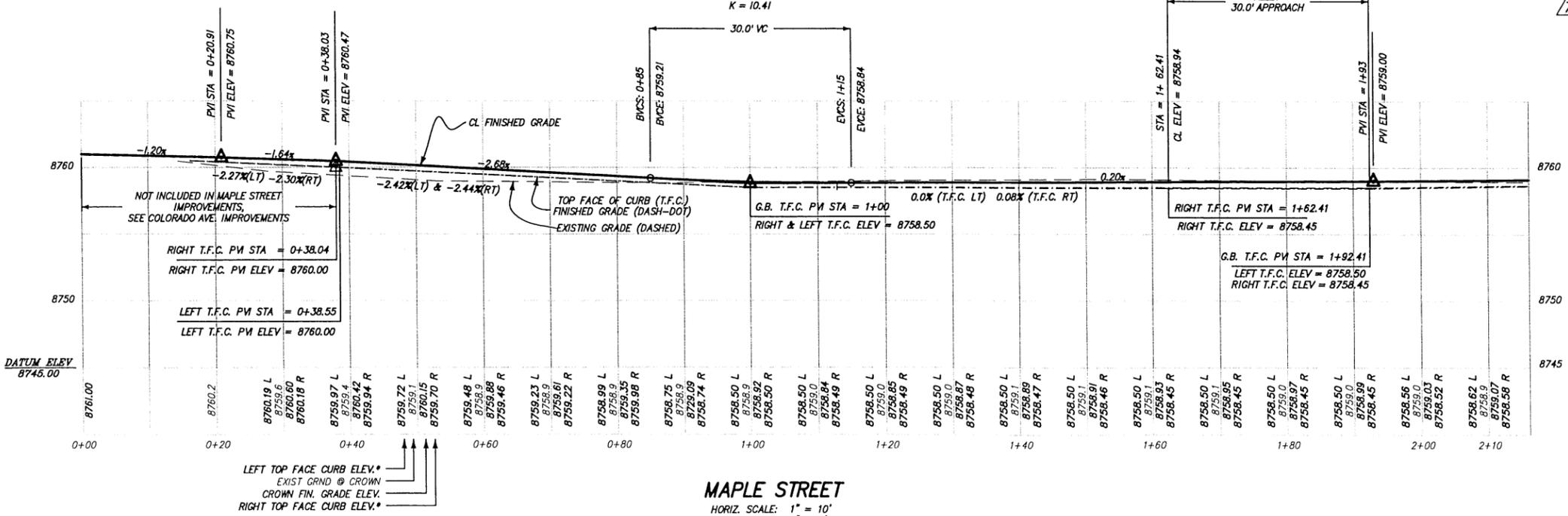
Rev.	Description	Date
1	TASK FORCE REVIEW SUBMITTAL	4/03/06 DDC
2	REVISIONS PER TOWN COMMENTS DATED 4/21/06	5/19/06 DDC
3	REVISIONS PER TOWN COMMENTS DATED 6/8/06	6/13/06 DDC
4	FINAL CONSTRUCTION SET	7/19/06 DDC

Project Mgr: ELK
Drawn by: DDC
Checked by: ELK
Start date: 4-18-02
Dwg path: Colorado-X-Streets.dwg
Sheet No.
B4
Project #: 04-072-02



- NOTES:
- 1 REMOVE ALL EXISTING ASPHALT SEE SECTION J/B4 FOR STRUCTURAL PAVEMENT SECTION
 - 2 LOCATE EXISTING WATER LINE PRIOR TO CONSTRUCTION
 - 3 REINSTALL EXISTING SIGN AFTER REGRADING. SEE NOTE 1 SHT 2.
 - 4 REGRADE SO THAT SURFACE WATER DRAINS TOWARDS DRAINAGE SWALE. DRAINAGE SWALE AND DRAINAGE BASINS MUST BE LOCATED IN R.O.W.
 - 5 TO RESOLVE ANY UTILITY CONFLICTS WITH THE CONSTRUCTION OF DRAINAGE SWALES OR DRAINAGE BASINS (WATER, SEWER, TELEPHONE, ETC), THE CONTRACTOR IS TO COORDINATE WITH THE TOWN OF TELLURIDE STAFF PRIOR TO CONSTRUCTION. USE EXTREME CAUTION WHEN CONSTRUCTING DRAINAGE SWALES AND DRAINAGE BASINS WHERE UTILITIES EXIST.
 - 6 FINISHED GRADE FOR EDGE OF PAVEMENT IS 1" ABOVE TOP FACE OF RIBBON CURB ELEVATION.
 - 7 THE DRAINAGE BASIN SIZES WILL HAVE TO BE DETERMINED IN THE FIELD WITH THE TOWN OF TELLURIDE STAFF COORDINATION. GRADE THE BASINS TO MATCH EXISTING CONDITIONS, MAINTAIN A 0.4% GRADE AND DO NOT CREATE DRAINAGE PROBLEMS FOR EXISTING HOMES.

LOW POINT ELEV = 8758.84
 LOW POINT STA = 1+12.87
 PVI STA = 1+00
 PVI ELEV = 8758.81
 A.D. = 2.88
 K = 10.41



LEFT TOP FACE CURB ELEV.*
 EXIST GRND @ CROWN
 CROWN FIN. GRADE ELEV.
 RIGHT TOP FACE CURB ELEV.*

*NOTE: E.O.P. FIN. GRADE IS 1" ABOVE TOP FACE OF CURB ELEV.

MAPLE STREET
 HORIZ. SCALE: 1" = 10'
 VERT. SCALE: 1" = 5'

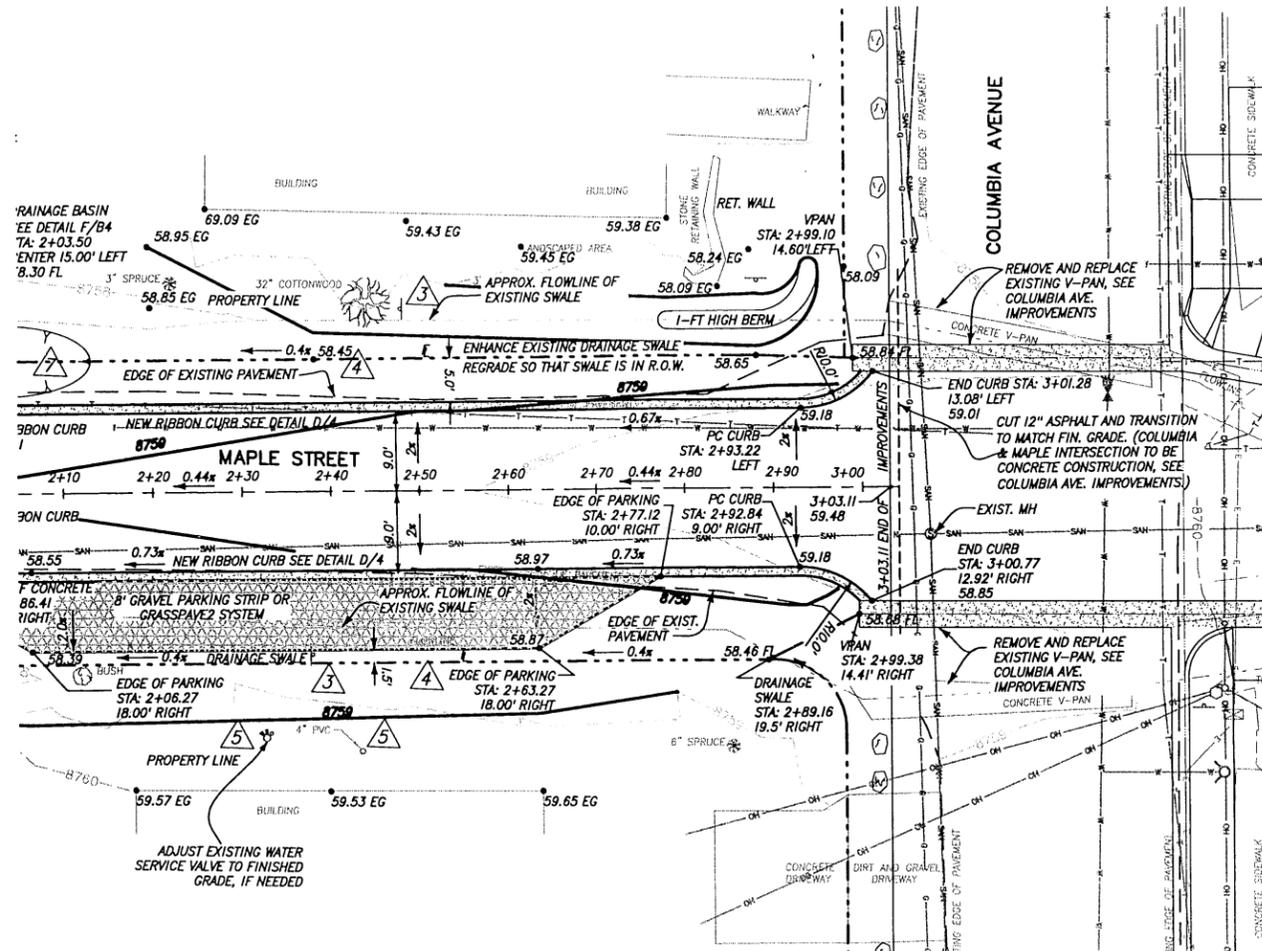
TELLURIDE STREETSCAPES - EAST TELLURIDE
 MAPLE STREET PLAN & PROFILE I
 STA. 0+00 TO STA. 2+10

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 PO Box 1385 125 W. Pacific Ave. Suite B1
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 email to: amath@foleyassoc.com

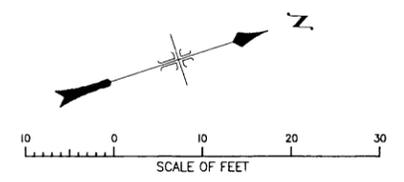
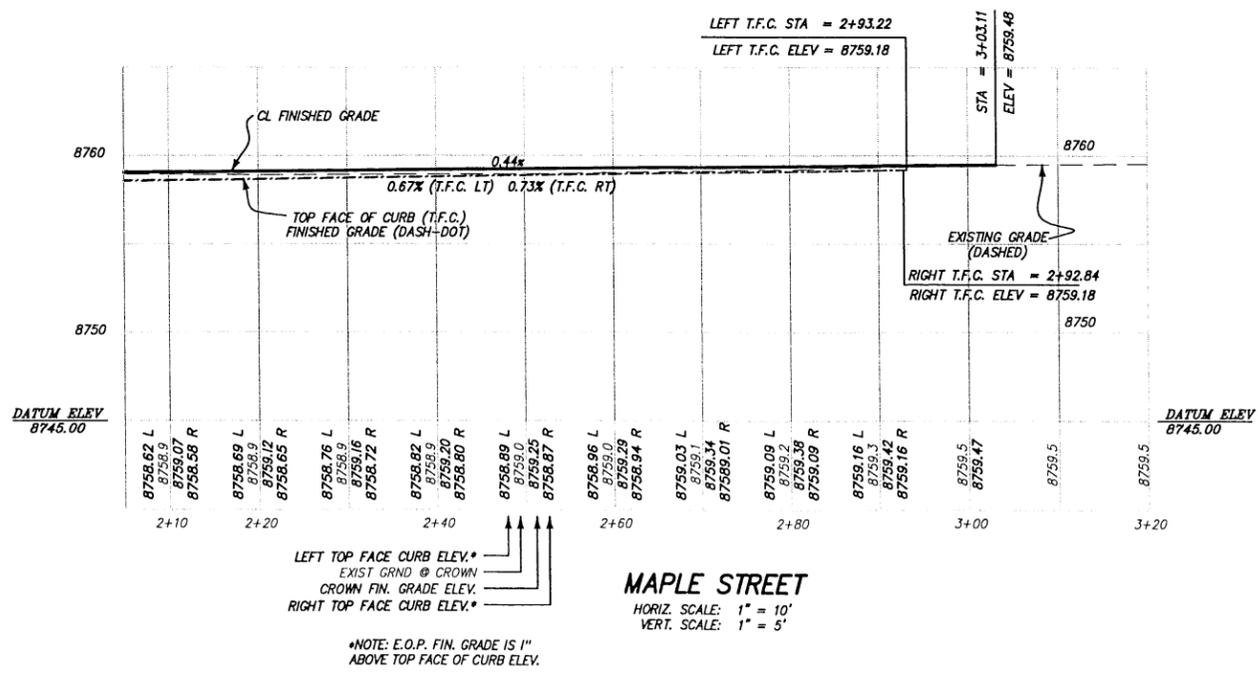
BUCKHORN GEOTECH
 Civil, Structural, and Geotechnical Engineers, Inc.
 222 South Park Avenue
 Montrose, Colorado 81401
 Phone (970) 249-8828 Fax (970) 249-0945
 www.buckhorngeo.com

Rev.	Description	Date	By
1	TASK FORCE REVIEW SUBMITTAL	4/03/06	DDC
2	REVISIONS PER TOWN COMMENTS DATED 4/21/06	5/16/06	DDC
3	REVISIONS PER TOWN COMMENTS DATED 5/23/06	5/31/06	DDC
4	REVISED TO DRAIN FROM NORTH TO SOUTH	6/29/06	DDC
5	FINAL CONSTRUCTION SET	7/19/06	DDC

Project Mgr: ELK
 Drawn by: DDC
 Checked by: ELK
 Start date: 4-18-02
 Dwg path: Colorado-X-Streets.dwg
 Sheet No. **B5**
 Project #: 04-072-02



- NOTES:
- 1 REMOVE ALL EXISTING ASPHALT SEE SECTION J/B4 FOR STRUCTURAL PAVEMENT SECTION
 - 2 LOCATE EXISTING WATER LINE PRIOR TO CONSTRUCTION
 - 3 REINSTALL EXISTING SIGN AFTER REGRADING. SEE NOTE 1 SHT 2.
 - 4 REGRADE SO THAT SURFACE WATER DRAINS TOWARDS DRAINAGE SWALE. DRAINAGE SWALE AND DRAINAGE BASINS MUST BE LOCATED IN R.O.W.
 - 5 TO RESOLVE ANY UTILITY CONFLICTS WITH THE CONSTRUCTION OF DRAINAGE SWALES OR DRAINAGE BASINS (WATER, SEWER, TELEPHONE, ETC), THE CONTRACTOR IS TO COORDINATE WITH THE TOWN OF TELLURIDE STAFF PRIOR TO CONSTRUCTION. USE EXTREME CAUTION WHEN CONSTRUCTING DRAINAGE SWALES AND DRAINAGE BASINS WHERE UTILITIES EXIST.
 - 6 FINISHED GRADE FOR EDGE OF PAVEMENT IS 1" ABOVE TOP FACE OF RIBBON CURB ELEVATION.
 - 7 THE DRAINAGE BASIN SIZES WILL HAVE TO BE DETERMINED IN THE FIELD WITH THE TOWN OF TELLURIDE STAFF COORDINATION. GRADE THE BASINS TO MATCH EXISTING CONDITIONS, MAINTAIN A 0.4% GRADE AND DO NOT CREATE DRAINAGE PROBLEMS FOR EXISTING HOMES.



TELLURIDE STREETSCAPES - EAST TELLURIDE
MAPLE STREET PLAN & PROFILE
STA. 2+10 TO STA. 3+20

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ENGINEERING - PLANNING - SURVEYING

BUCKHORN GEOTECH
Civil, Structural and Geotechnical Engineers, Inc.

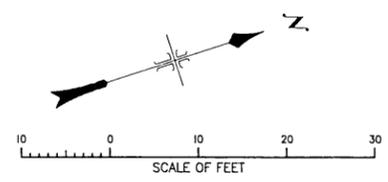
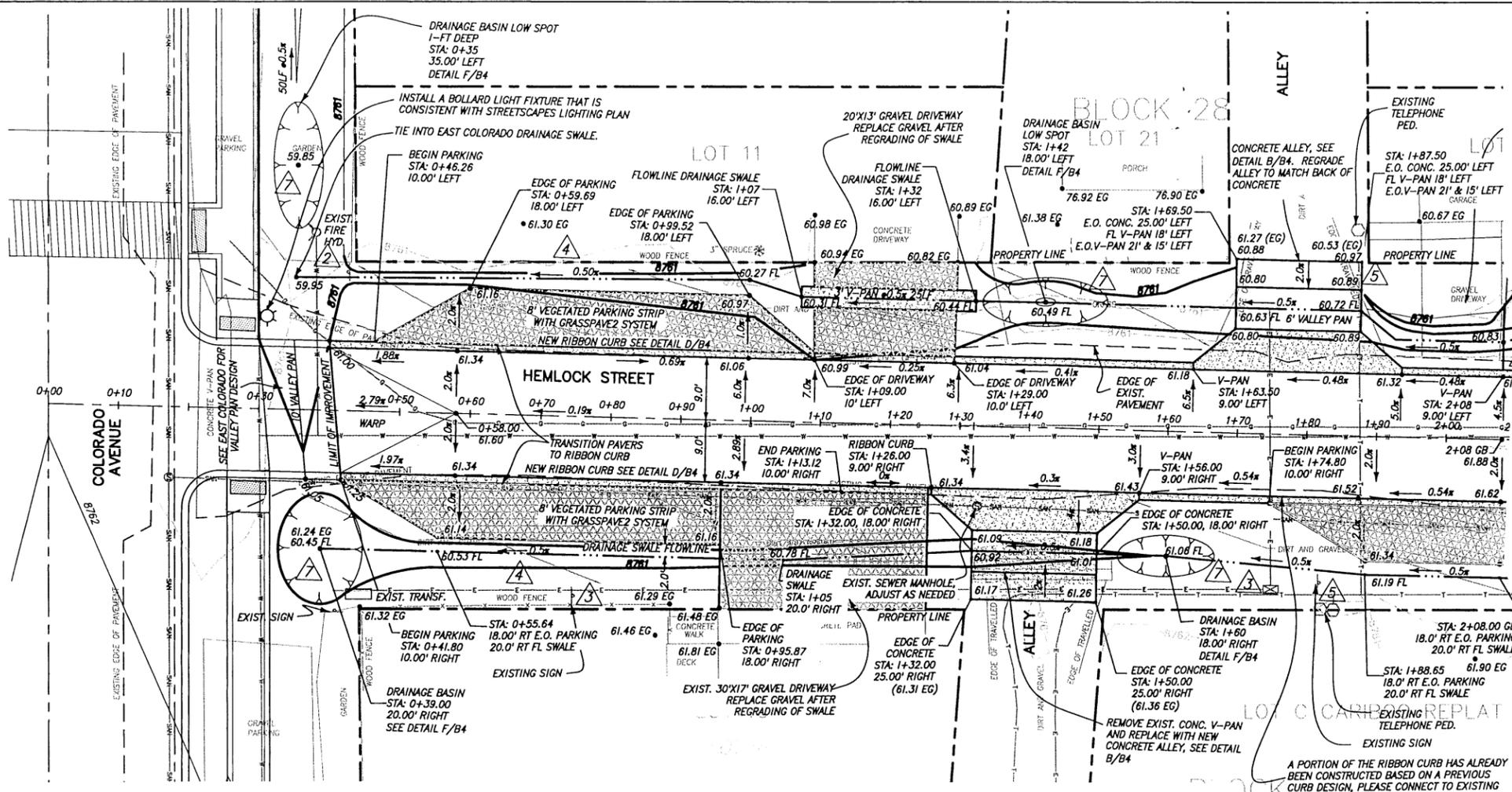
Rev.	description	date	by
1	TASK FORCE REVIEW SUBMITTAL	4/23/06 DDC	
2	REVISIONS PER TOWN COMMENTS DATED 4/21/06	5/15/06 DDC	
3	REVISIONS PER TOWN COMMENTS DATED 4/23/06	5/31/06 DDC	
4	REVISED TO DRAIN FROM NORTH TO SOUTH	6/29/06 DDC	
5	FINAL CONSTRUCTION SET	7/19/06 DDC	

Project Mgr: ELK
Drawn by: DDC
Checked by: ELK
Start date: 4-18-02
Dwg path: Colorado-X-Streets.dwg
Sheet No.
Project #: 04-072-02

B6

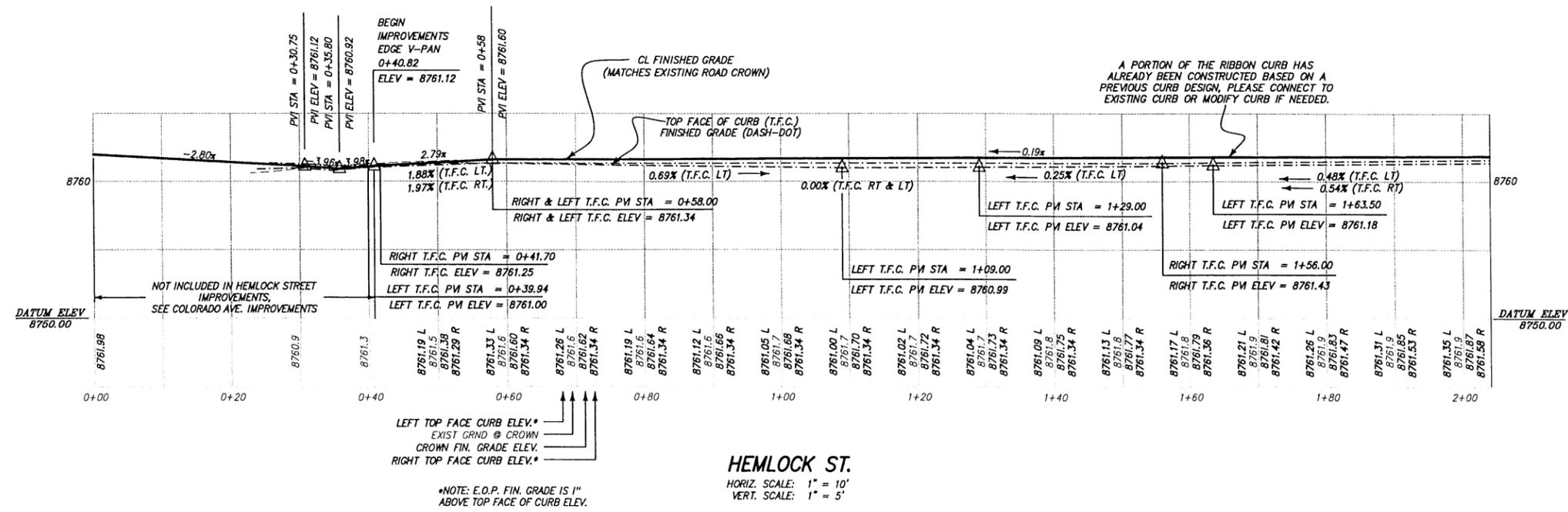
Contact:
Kevin Guhlhammer
1370 Black Bear Road
Colorado 81435

Client:
Town of Telluride, Colorado
P.O. Box 387
Telluride, Colorado 81435



NOTES:

- 1 REMOVE ALL EXISTING ASPHALT SEE SECTION J/B4 FOR STRUCTURAL PAVEMENT SECTION
- 2 LOCATE EXISTING WATER LINE PRIOR TO CONSTRUCTION
- 3 REINSTALL EXISTING SIGN AFTER REGRADING. SEE NOTE 1 SHT 2.
- 4 REGRADE SO THAT SURFACE WATER DRAINS TOWARDS DRAINAGE SWALE. DRAINAGE SWALE AND DRAINAGE BASINS MUST BE LOCATED IN R.O.W.
- 5 TO RESOLVE ANY UTILITY CONFLICTS WITH THE CONSTRUCTION OF DRAINAGE SWALES OR DRAINAGE BASINS (WATER, SEWER, TELEPHONE, ETC), THE CONTRACTOR IS TO COORDINATE WITH THE TOWN OF TELLURIDE STAFF PRIOR TO CONSTRUCTION. USE EXTREME CAUTION WHEN CONSTRUCTING DRAINAGE SWALES AND DRAINAGE BASINS WHERE UTILITIES EXIST.
- 6 FINISHED GRADE FOR EDGE OF PAVEMENT IS 1" ABOVE TOP FACE OF RIBBON CURB ELEVATION.
- 7 THE DRAINAGE BASIN SIZES WILL HAVE TO BE DETERMINED IN THE FIELD WITH THE TOWN OF TELLURIDE STAFF COORDINATION. GRADE THE BASINS TO MATCH EXISTING CONDITIONS, MAINTAIN A 0.4% GRADE AND DO NOT CREATE DRAINAGE PROBLEMS FOR EXISTING HOMES.



HEMLOCK ST.
 HORIZ. SCALE: 1" = 10'
 VERT. SCALE: 1" = 5'

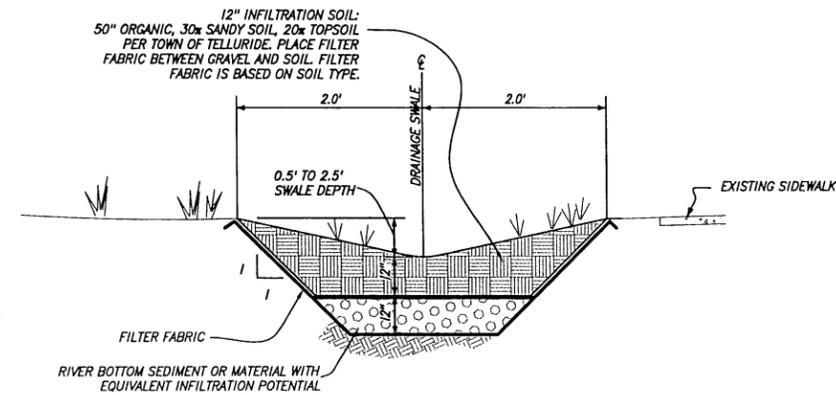
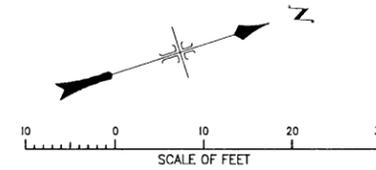
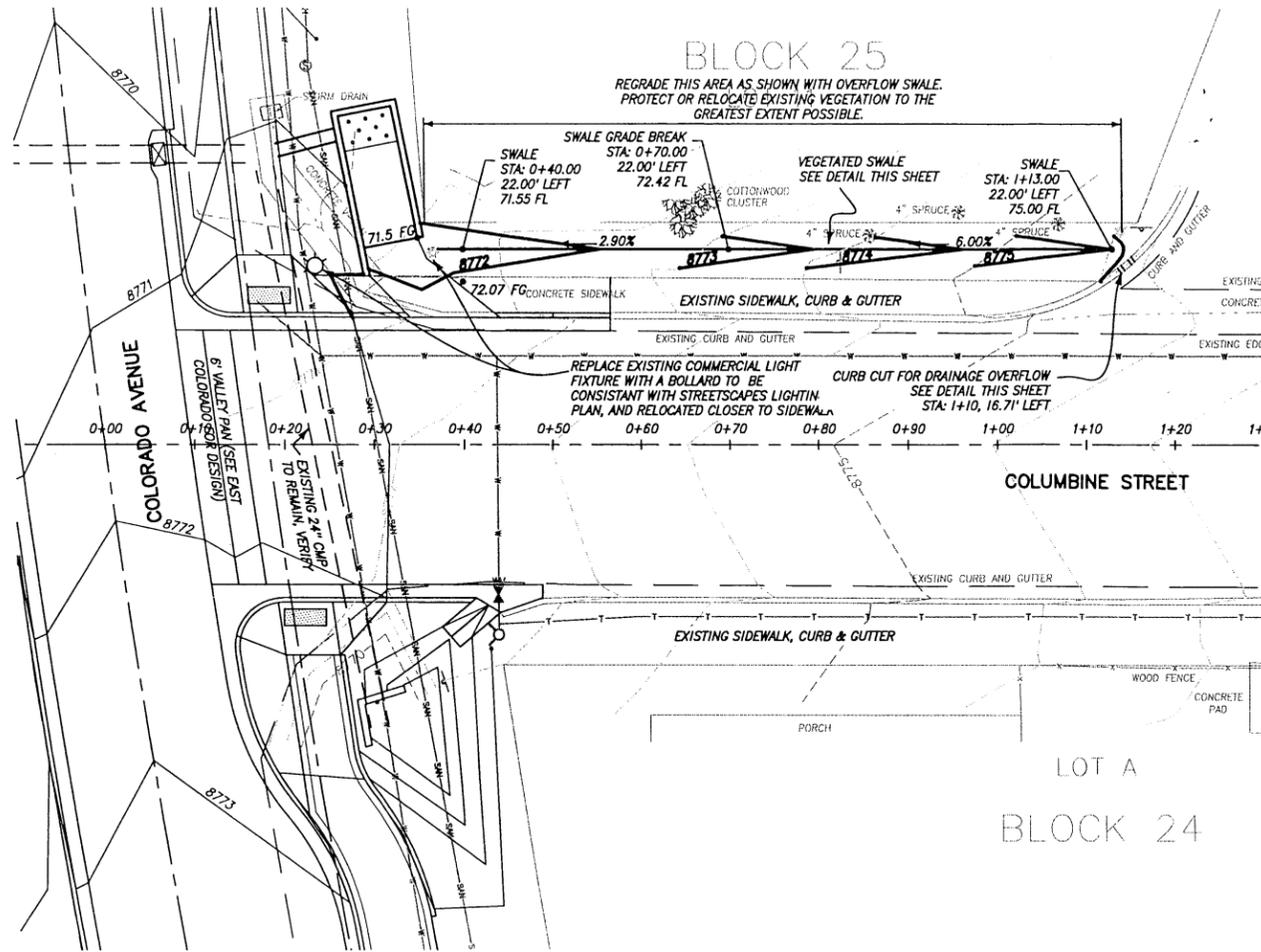
TELLURIDE STREETSCAPES - EAST TELLURIDE
 HEMLOCK STREET PLAN & PROFILE I
 STA. 0+00 TO STA. 2+00

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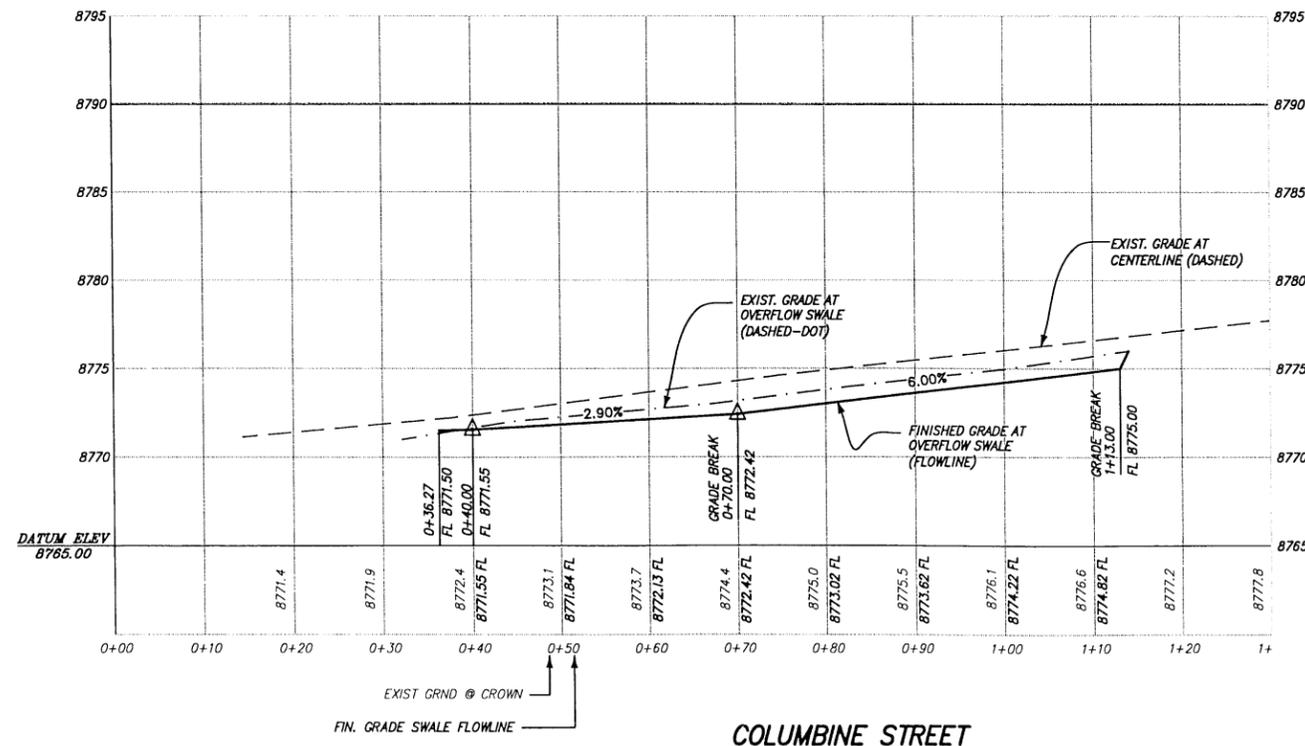
Rev.	date	description
1	4/03/06 DDC	TASK FORCE REVIEW SUBMITTAL
2	5/15/06 DDC	TO FOLEY FOR CURB STAKING
3	5/19/06 DDC	REVISIONS PER TOWN COMMENTS DATED 4/21/06
4	5/31/06 DDC	REVISIONS PER TOWN COMMENTS DATED 5/23/06
5	6/29/06 DDC	REVISIONS TO DRAIN FROM NORTH TO SOUTH
6	7/19/06 DDC	FINAL CONSTRUCTION SET

Project Mgr: ELK
 Drawn by: RDB
 Checked by: ELK
 Start date: 4-16-02
 Dwg path: Colorado.dwg
 Sheet No. **B7**
 Project #: 04-072



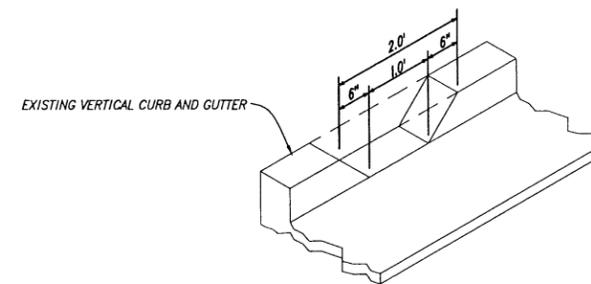
TYPICAL VEGETATED SWALE

N.T.S.



COLUMBINE STREET

HORIZ. SCALE: 1" = 10'
VERT. SCALE: 1" = 5'



TYPICAL CURB CUT

N.T.S.

TELLURIDE STREETSCAPES - EAST TELLURIDE
COLUMBINE STREET PLAN & PROFILE
STA. 0+00 TO STA. 1+30

Contact:
Karan Guglielmino
1370 Black Bear Road
Telluride
Colorado 81435

Client:
Town of Telluride, Colorado
P.O. Box 397
Telluride
Colorado 81435

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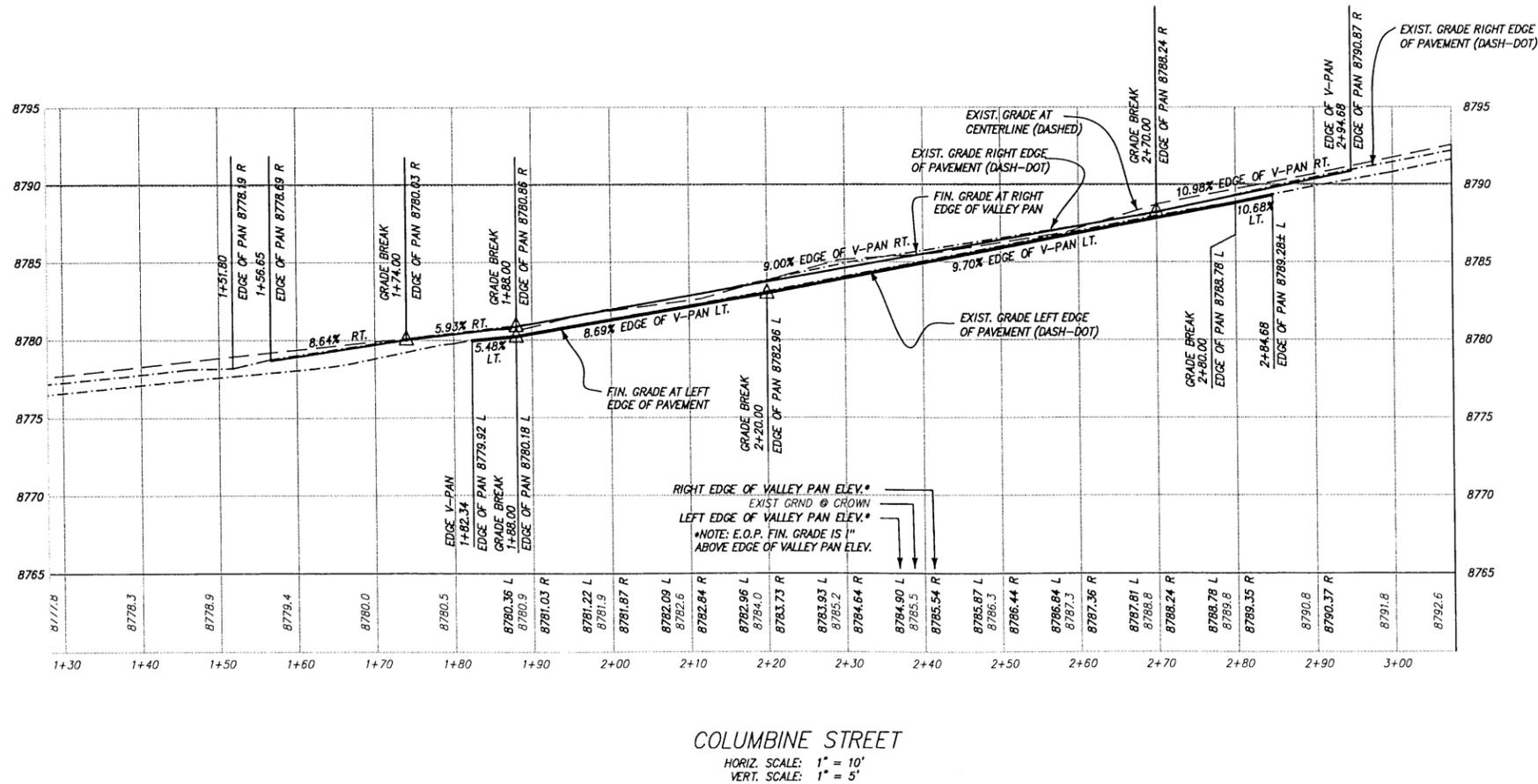
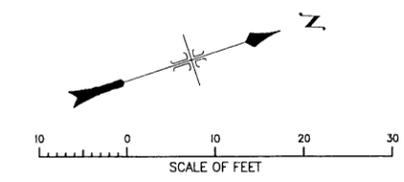
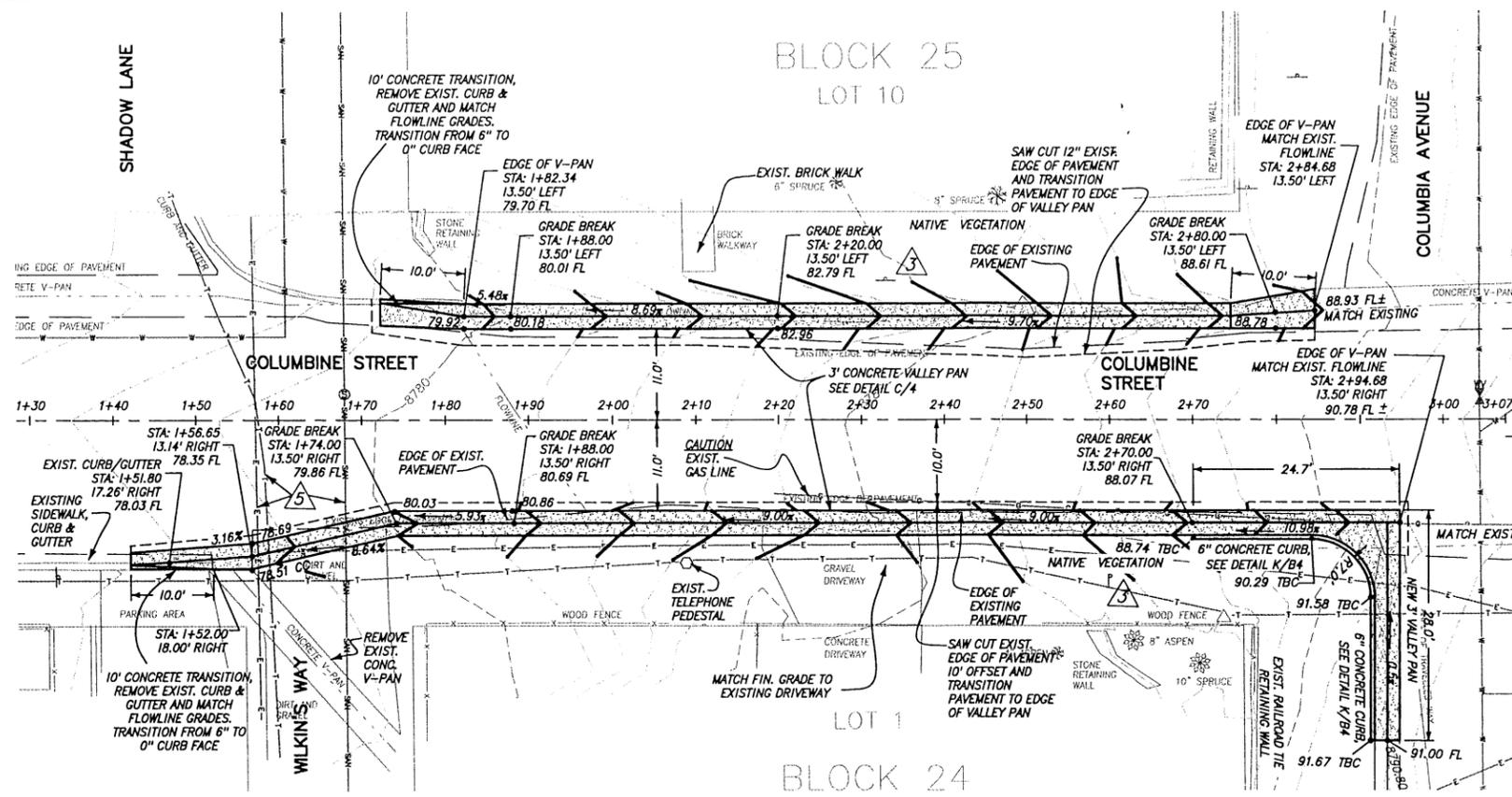
Rev.	Description	Date	By
1	TASK FORCE REVIEW SUBMITTAL	4/03/06	DDC
2	REVISIONS PER TOWN COMMENTS DATED 4/21/06	6/12/06	DDC
3	FINAL CONSTRUCTION SET	7/19/06	DDC

Project Mgr: ELK
Drawn by: DDC
Checked by: ELK
Start date: 4-18-02
Dwg path: Colorado.dwg

Sheet No.
B9

Project #: 04-072

\\2003server\CIVIL\2004\04-072-Task 2 Streetscapes East\dwg\Colorado-X-STREETS.dwg, 7/18/2006 2:07:53 PM, Adobe PDF



- NOTES:
- 1 EXISTING PAVEMENT TO REMAIN, SAW CUT AND REMOVE A PORTION OF ASPHALT TO TRANSITION TO NEW VALLEY PANS
 - 2 LOCATE EXISTING WATER LINE PRIOR TO CONSTRUCTION
 - 3 REINSTALL EXISTING SIGN AFTER REGRADING, SEE NOTE 1 SHEET 2.
 - 4 FINISHED GRADE FOR EDGE OF PAVEMENT IS 1" ABOVE EDGE OF VALLEY PAN ELEVATION.
 - 5 FOR ANY UTILITY CROSSING UNDER VALLEY PAN, THE CONTRACTOR IS TO COORDINATE ANY UTILITY CONFLICTS WITH THE TOWN OF TELLURIDE. USE EXTREME CAUTION DURING CONSTRUCTION IN AREAS OF UTILITIES.

TELLURIDE STREETSCAPES - EAST TELLURIDE
 COLUMBINE STREET PLAN & PROFILE
 STA. 1+30 TO STA. 3+06

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 PO Box 1385 135 W. Pacific Ave. Suite B1
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 www.foleyassoc.com

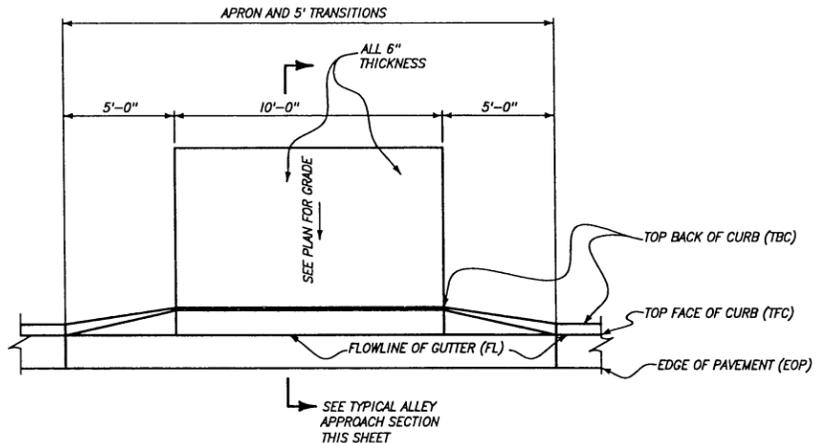
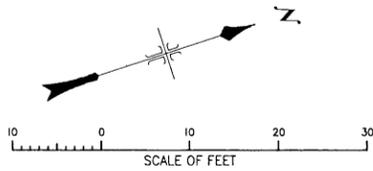
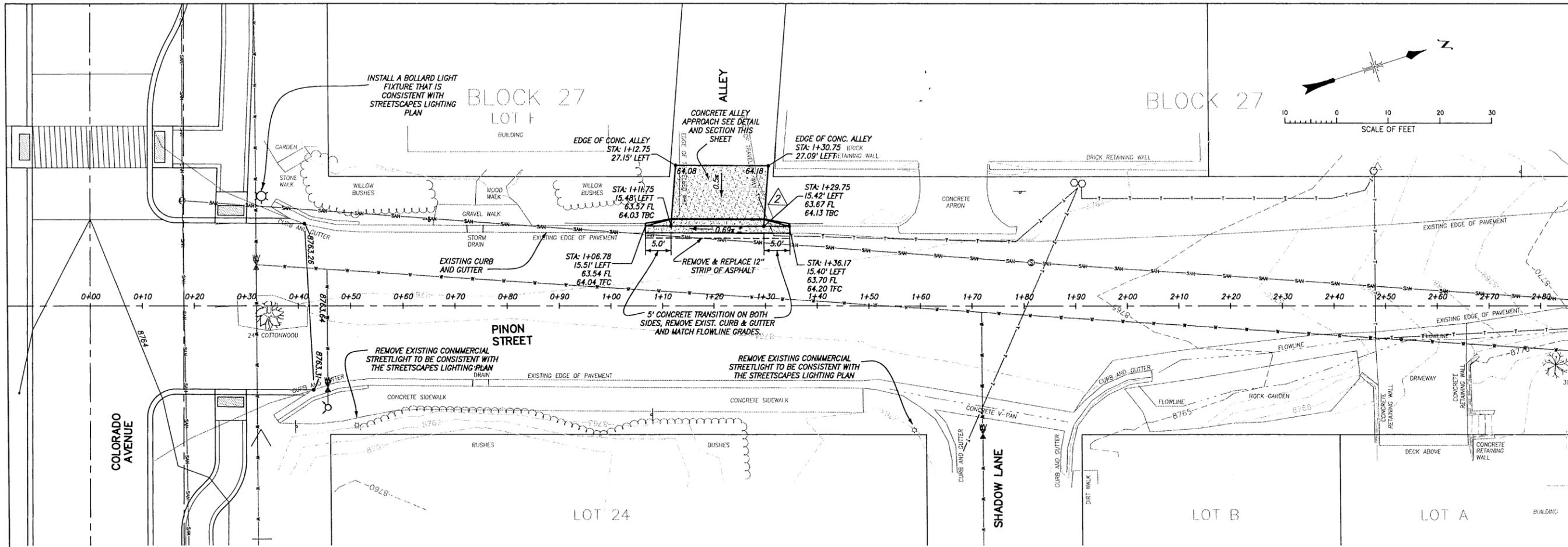
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 Phone (970) 248-6828 Fax (970) 248-0945
 www.buckhorngeo.com

Rev.	Description	Date	By
1	TASK FORCE REVIEW SUBMITTAL	4/25/06 DDC	
2	REVISIONS PER TOWN COMMENTS DATED 4/21/06	6/12/06 DDC	
3	FINAL CONSTRUCTION SET	7/19/06 DDC	

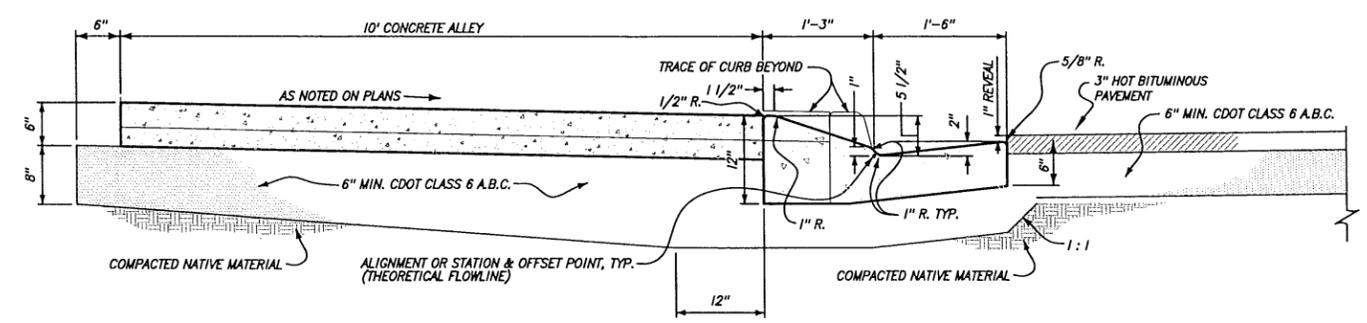
Project Mgr: ELK
 Drawn by: DDC
 Checked by: ELK
 Start date: 4-16-02
 Dwg path: Colorado.dwg
 Sheet No. **B10**
 Project #: 04-072

Contact:
 Karen Gugliemone
 1370 Blackhawk Road
 Colorado 81435

Client:
 Town of Telluride, Colorado
 P.O. Box 397
 Colorado 81435



ALLEY APPROACH DETAIL
N.T.S.



TYPICAL ALLEY APPROACH SECTION
N.T.S.

- NOTES:**
- 1 FINISHED GRADE FOR EDGE OF PAVEMENT IS 1" ABOVE EDGE OF CONCRETE APPROACH ELEVATION.
 - 2 TO RESOLVE ANY UTILITY CONFLICTS WITH THE CONSTRUCTION OF DRAINAGE SWALES OR DRAINAGE BASINS (WATER, SEWER, TELEPHONE, ETC), THE CONTRACTOR IS TO COORDINATE WITH THE TOWN OF TELLURIDE STAFF PRIOR TO CONSTRUCTION. USE EXTREME CAUTION WHEN CONSTRUCTING DRAINAGE SWALES AND DRAINAGE BASINS WHERE UTILITIES EXIST.

TELLURIDE STREETSCAPES - EAST TELLURIDE
PINON STREET PLAN
STA. 1+06 TO STA. 1+36 (100 BLOCK)

Contact:
Karen Gullegime
1370 Black Bear Road
Telluride, Colorado 81435

Client:
Town of Telluride, Colorado
P.O. Box 357
Telluride, Colorado 81435

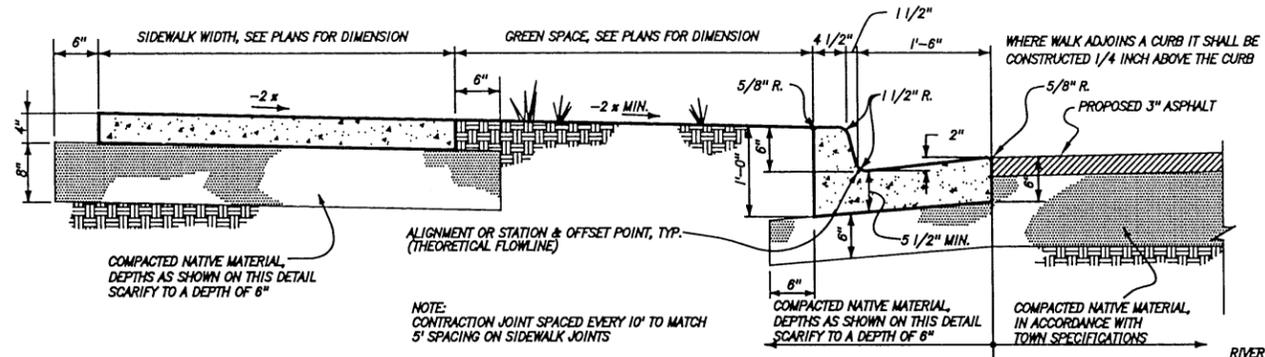
FOLEY ASSOCIATES, INC.
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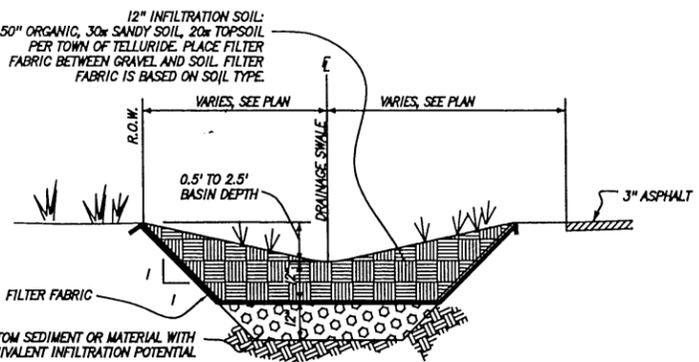
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1	TASK FORCE REVIEW SUBMITTAL	4/20/06 (DDC)
2	REVISIONS PER TOWN COMMENTS DATED 4/21/06	6/12/06 (DDC)
3	FINAL CONSTRUCTION SET	7/19/06 (DDC)

Project Mgr: ELK
Drawn by: DDC
Checked by: ELK
Start date: 4-16-02
Dwg path: Colorado-X-Streets.dwg
Sheet No.
B11
Project #: 04-072-02

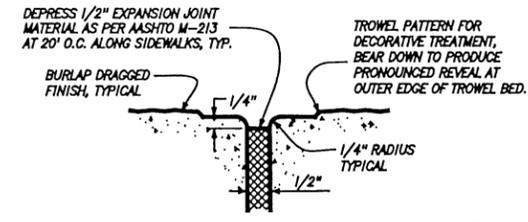
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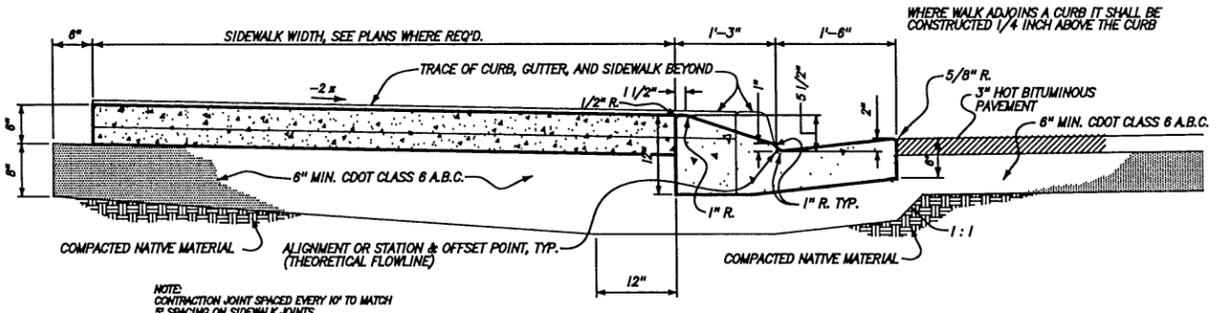
6" VERTICAL CURB, GUTTER AND DETACHED SIDEWALK DETAIL
N.T.S. (A/C4)



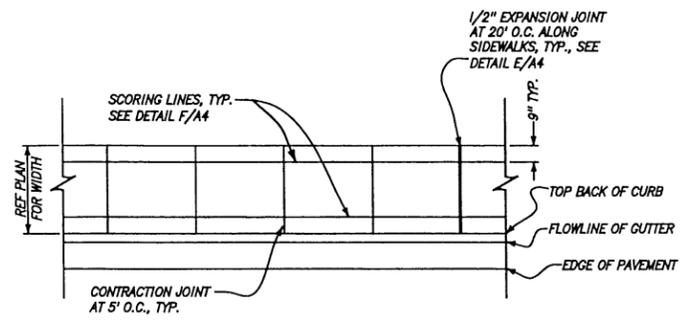
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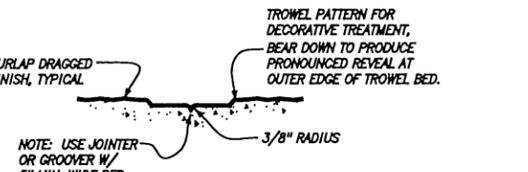
EXPANSION JOINT DETAIL
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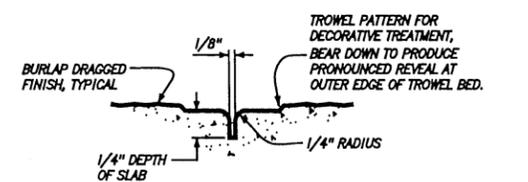
TYPICAL DRIVEWAY APPROACH SECTION
N.T.S. (B/C4)



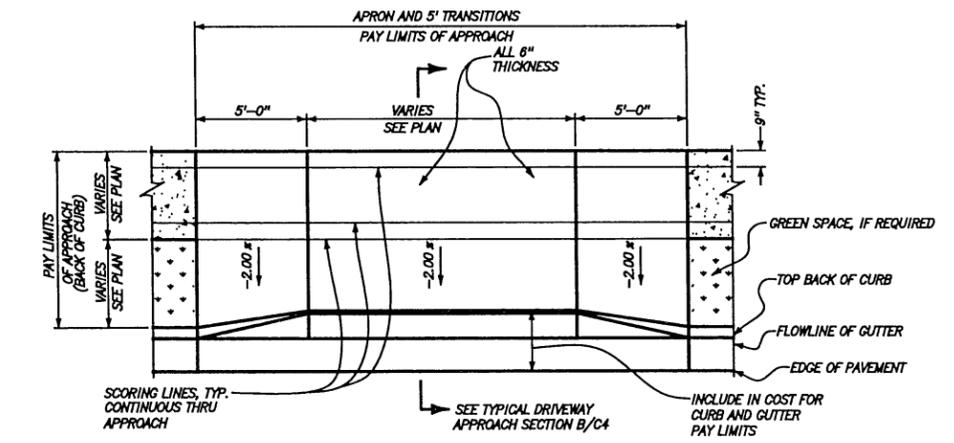
SIDEWALK SCORING PATTERN - ALL WALKWAYS
N.T.S. (F/C4)



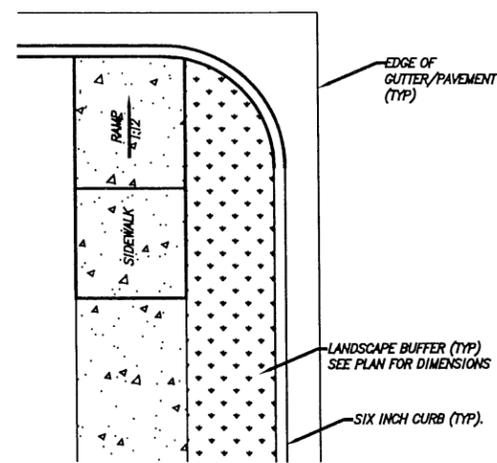
TOOLED JOINT/SCORING DETAIL
N.T.S. (K/C4)



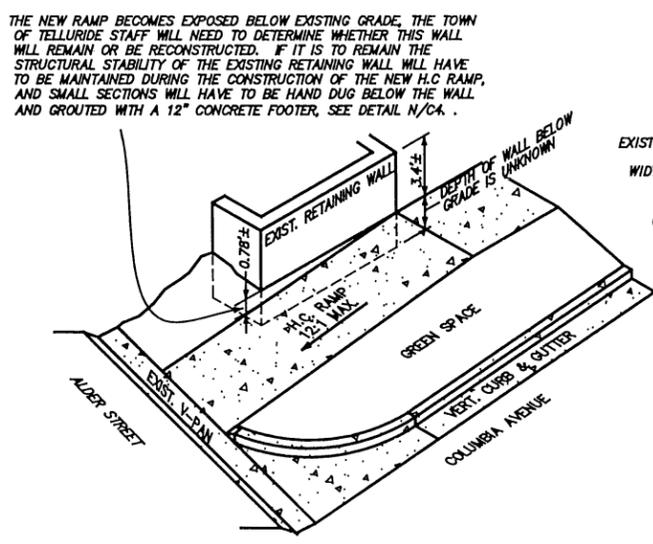
CONTRACTION JOINT DETAIL
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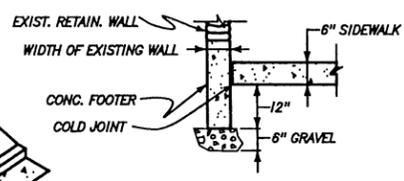
NEW DRIVEWAY OR ALLEY APPROACH DETAIL
N.T.S. (C/C4)



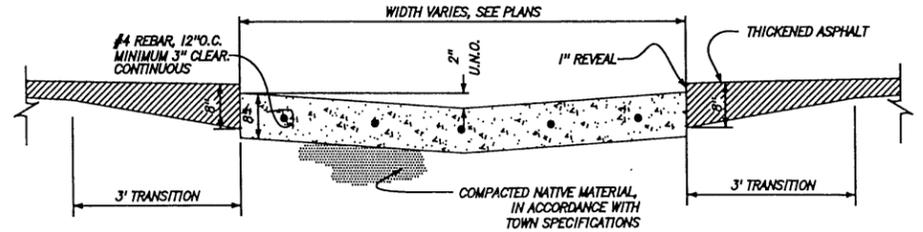
TYPE 5 ADA RAMP
N.T.S. (G/C4)



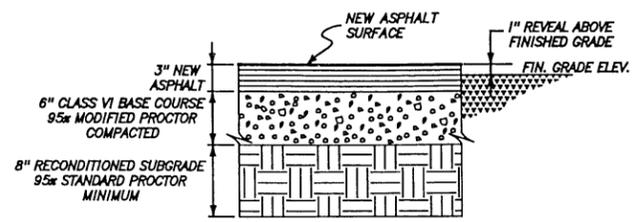
H.C. RAMP AT ALDER & COLUMBIA
N.T.S. (M/C4)



WALL FOOTER
N.T.S. (N/C4)



TYPICAL V-PAN DETAIL
N.T.S. (D/C4)



TYPICAL ASPHALT SECTION
N.T.S. (H/C4)

TELLURIDE STREETSCAPES - EAST TELLURIDE
EAST COLUMBIA AVENUE (400 & 500 BLOCKS)
PROJECT DETAILS

Contact:
Karin Gough
1370 Black Bear Road
Telluride, Colorado
81405

Client:
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P.O. Box 397
Telluride, Colorado
81405

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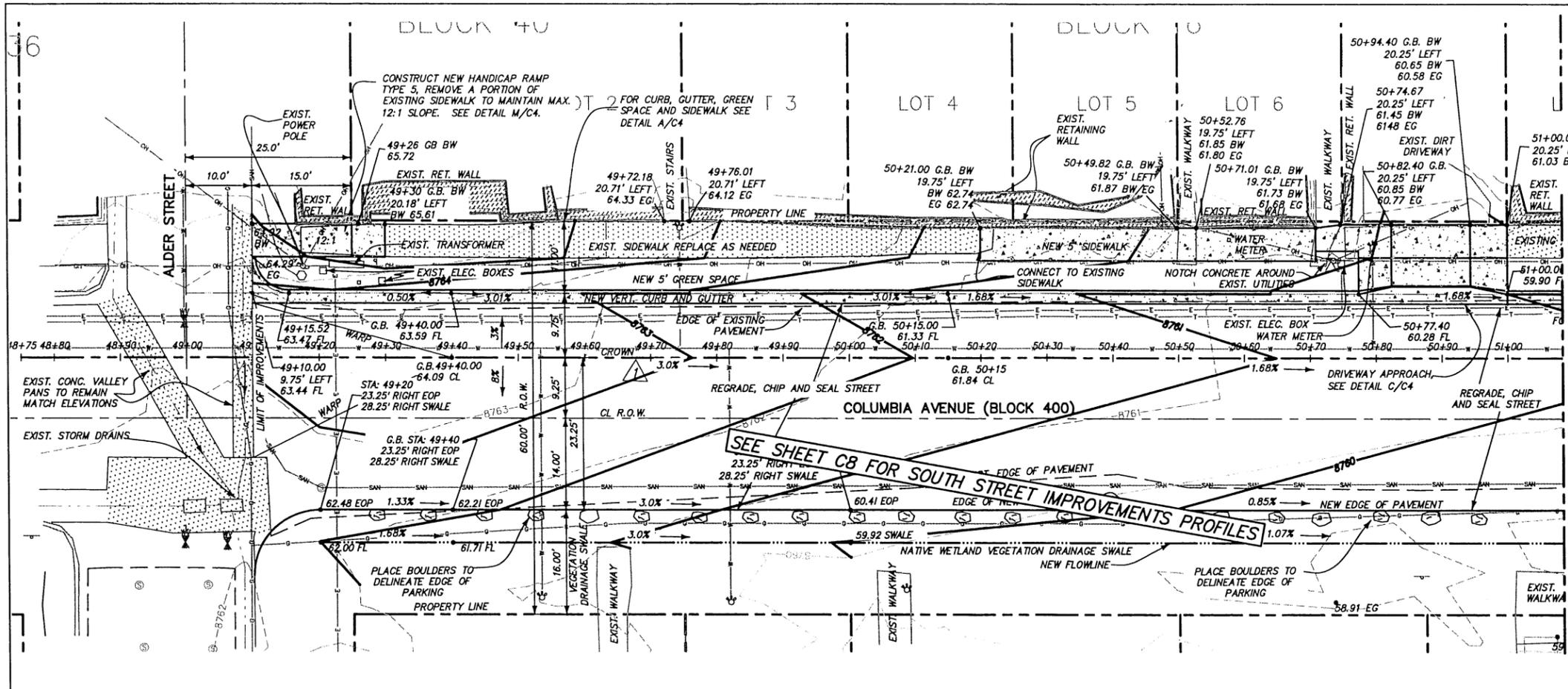
BUCKHORN/OTICH
Civil, Structural and Geotechnical Engineers, Inc.

229 South Park Avenue
Montrose, Colorado 81401
Phone (970) 249-6828 Fax (970) 249-0945
www.buckhorngeo.com

Rev.	description	date	by
1	PLAN & PROFILE SUBMITTAL FOR REVIEW	7/12/06 DDC	
2	FINAL CONSTRUCTION SET	7/19/06 DDC	

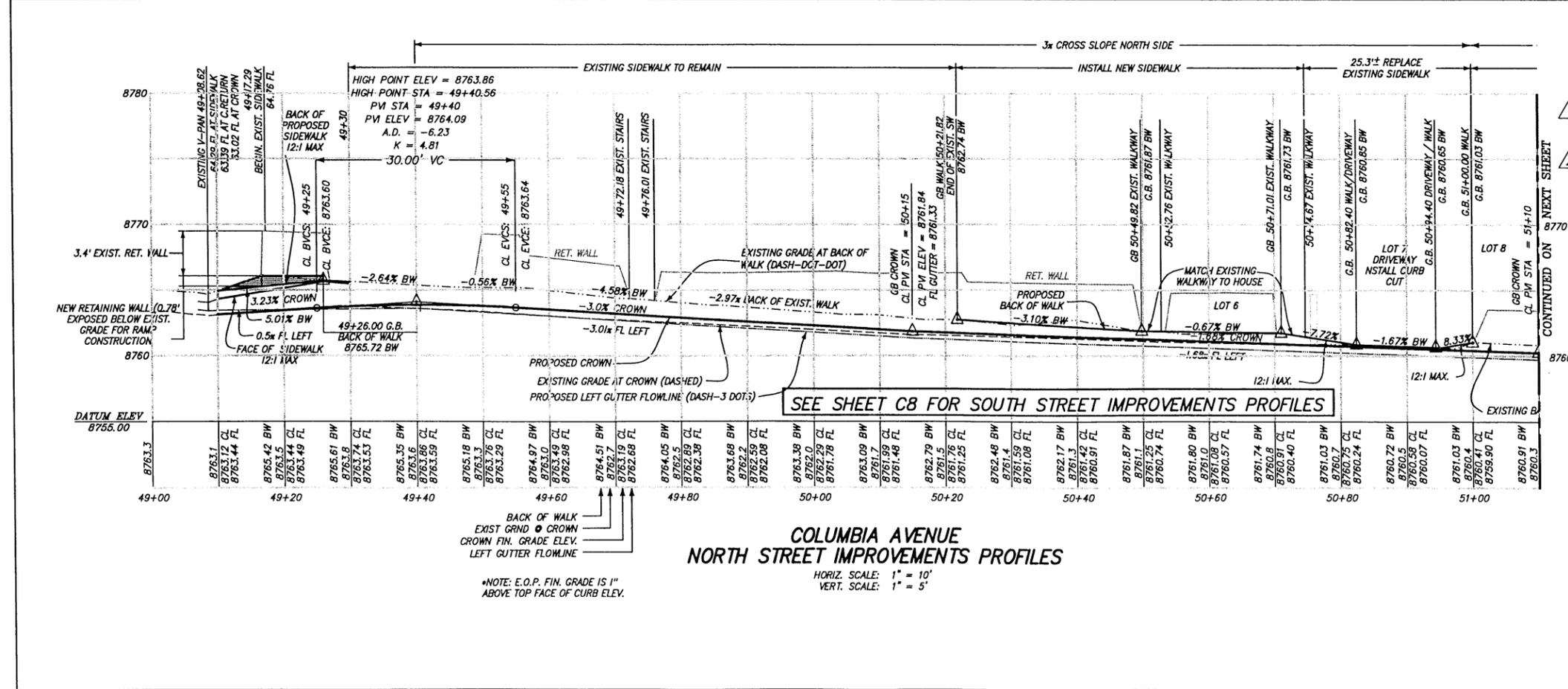
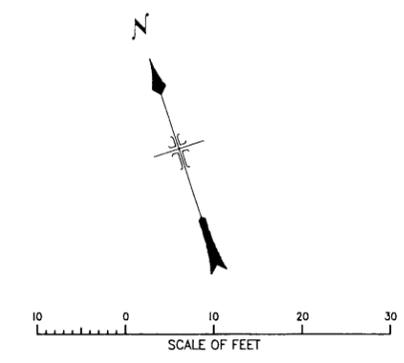
Project Mgr: ELK
Drawn by: DDC
Checked by: ELK
Start date: 4-16-02
Dwg path: Colorado-X-Streets.dwg
Sheet No. **C4**
Project #: 04-072-02

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LEGEND

- Right of Way (R.O.W.) and/or Prc
- HC Handicap Ramp
- Water Valve
- Tree
- Utility Pole
- Sanitary Manhole
- Parking Meter
- Water Service Valve
- Cable Television Pedestal
- Fire Hydrant
- Sign
- Telephone Pedestal
- Transformer
- Streetlamp
- Existing sidewalk / concrete
- Proposed sidewalk / concrete
- Existing retaining wall



NOTES

1. CROWN OF ROAD IS OFFSET FROM CENTER OF R.O.W. TO BE CONSISTENT WITH THE COMPLETED IMPROVEMENTS ON WEST COLUMBIA AVENUE.

2. PROPOSED BACK OF 5'-FT SIDEWALK IS 6" INSIDE R.O.W. WHICH PLACES THE FRONT OF SIDEWALK CONSISTENT WITH THE FRONT OF EXISTING SIDEWALK.

**COLUMBIA AVENUE
NORTH STREET IMPROVEMENTS PROFILES**
HORIZ. SCALE: 1" = 10'
VERT. SCALE: 1" = 5'

TELLURIDE STREETSCAPES - EAST TELLURIDE
EAST COLUMBIA PLAN & NORTH PROFILES
400 BLOCK, STA. 49+00 TO 51+00

Contact:
Karin Gulimone
1370 Black Bear Road
Telluride, Colorado 81435

Client:
Town of Telluride, Colorado
P.O. Box 397
Telluride, Colorado 81435

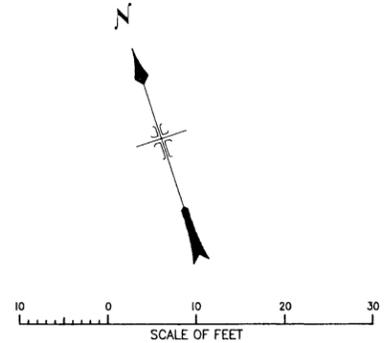
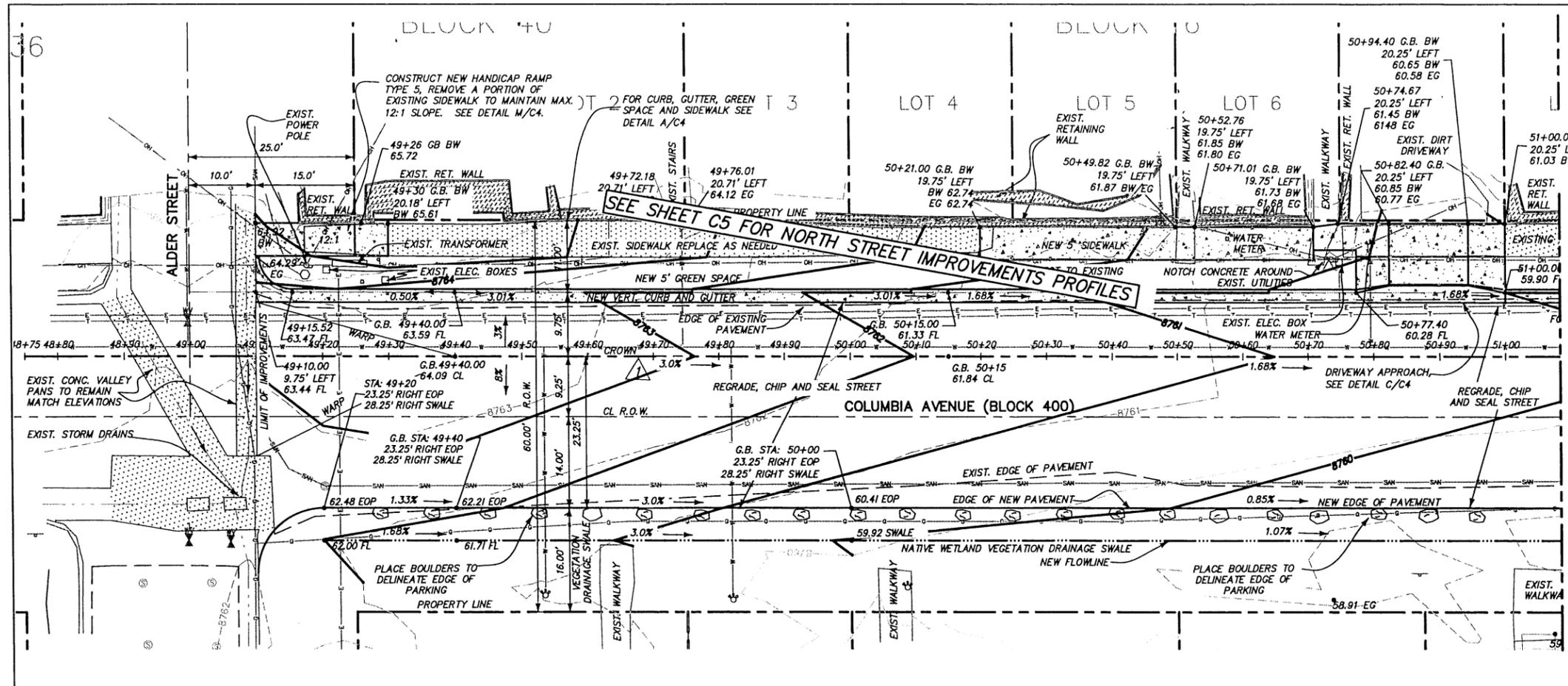
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Rev.	description	date
1	STREET PLAN LAYOUT SUBMITTAL FOR REVIEW	5/04/06 DDC
2	PLAN AND PROFILE SUBMITTAL FOR REVIEW	7/12/06 DDC
3	FINAL CONSTRUCTION SET	7/19/06 DDC

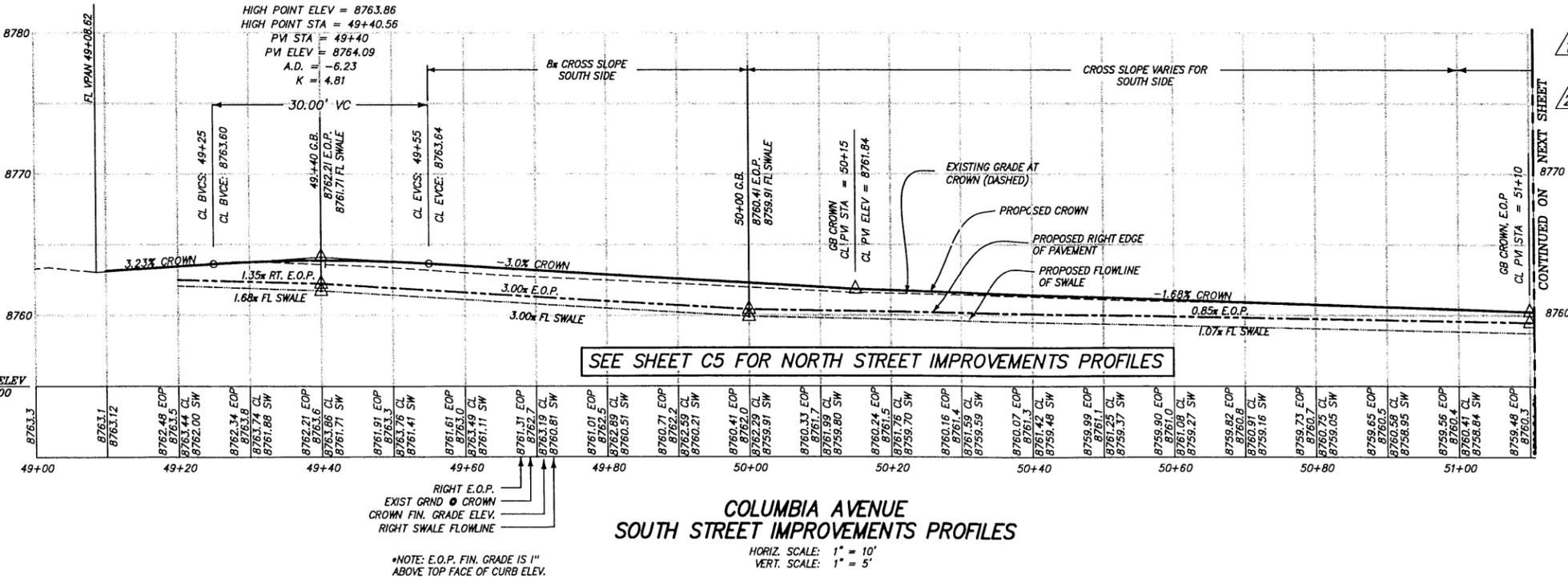
Project Mgr: ELK
Drawn by: DDC
Checked by: ELK
Start date: 4-16-02
Dwg path: Columbia-improv.dwg
Sheet No. **C5**
Project #: 04-072-02

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- LEGEND**
- Right of Way (R.O.W.) and/or Property Line
 - HC Handicap Ramp
 - Water Valve
 - Tree
 - Utility Pole
 - Sanitary Manhole
 - Parking Meter
 - Water Service Valve
 - Cable Television Pedestal
 - Fire Hydrant
 - Sign
 - Telephone Pedestal
 - Transformer
 - Streetlamp
 - Existing sidewalk / concrete
 - Proposed sidewalk / concrete
 - Existing retaining wall

CONTINUED ON NEXT SHEET



- NOTES**
1. CROWN OF ROAD IS OFFSET FROM CENTER OF R.O.W. TO BE CONSISTENT WITH THE COMPLETED IMPROVEMENTS ON WEST COLUMBIA AVENUE.
 2. PROPOSED BACK OF 5-FT SIDEWALK IS 6" INSIDE R.O.W. WHICH PLACES THE FRONT OF SIDEWALK CONSISTENT WITH THE FRONT OF EXISTING SIDEWALK.

CONTINUED ON NEXT SHEET

TELLURIDE STREETSCAPES - EAST TELLURIDE
EAST COLUMBIA PLAN & SOUTH PROFILES
400 BLOCK, STA. 49+00 TO 51+00

Client:
Karen Gulghione
1370 Block Bear Road
Colorado 81435

Client:
Town of Telluride, Colorado
P.O. Box 397
Colorado 81435

FOLEY ASSOCIATES, INC.
ENGINEERING - PLANNING - SURVEYING

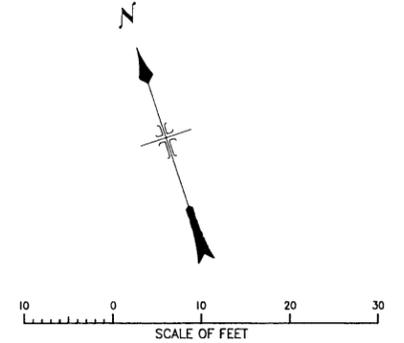
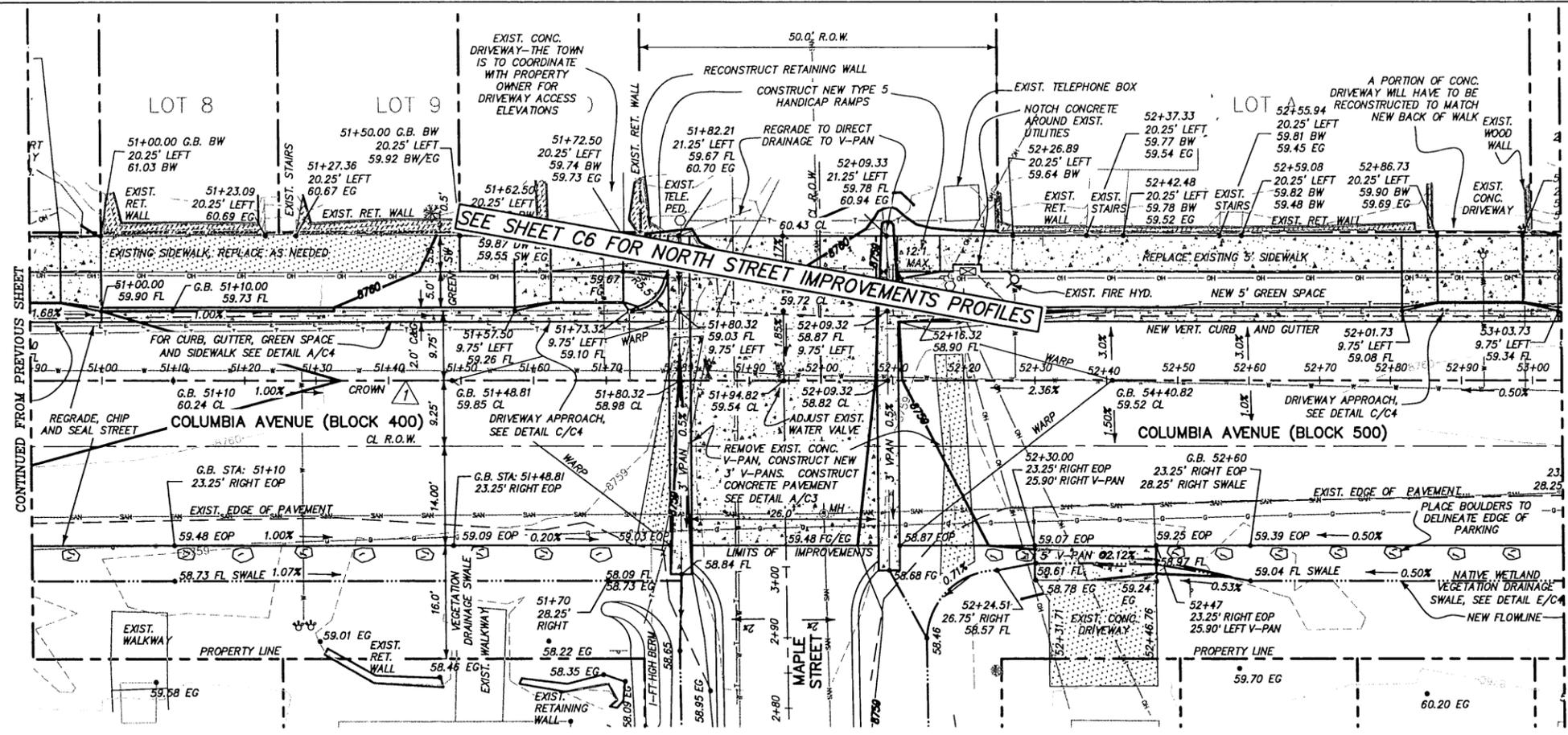
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e-mail: fo.assoc@foleyassoc.com

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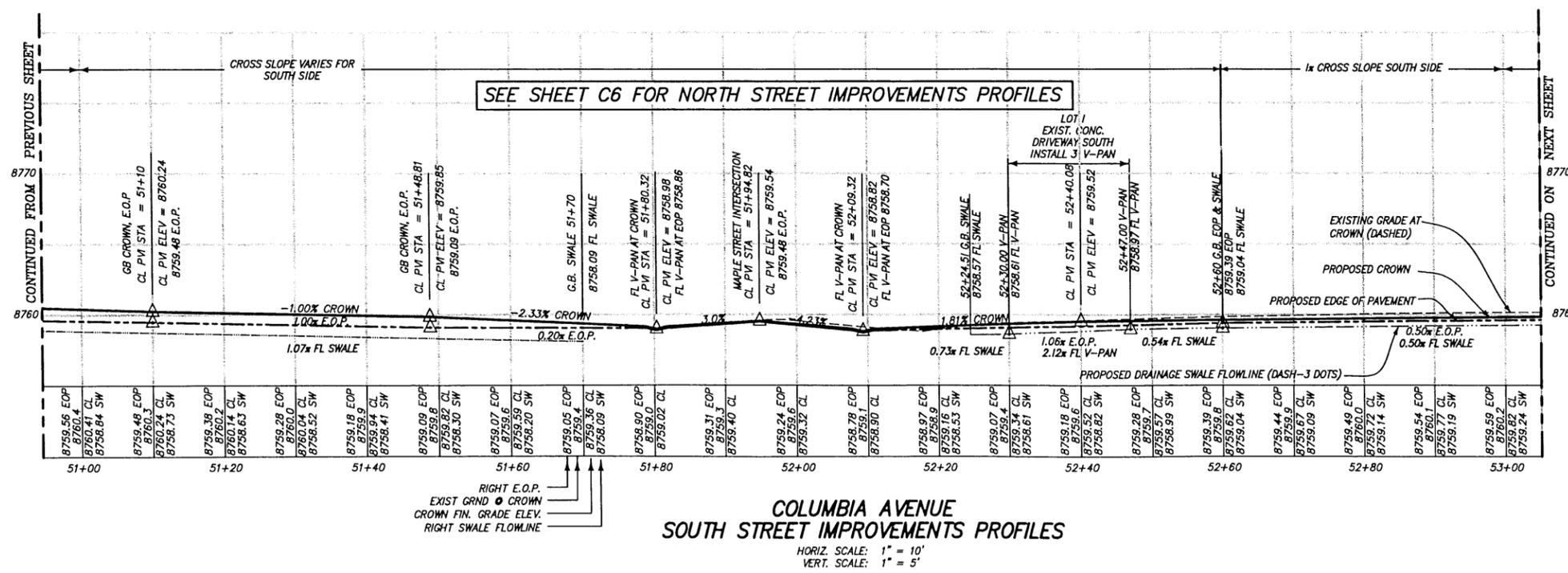
222 South Park Avenue
Montrose, Colorado 81401
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Rev.	Description	Date	By
1	STREET PLAN SCHEMATIC SUBMITTAL FOR REVIEW	5/04/06 DDC	
2	PLAN & PROFILE SUBMITTAL FOR REVIEW	7/17/06 DDC	
3	FINAL CONSTRUCTION SET	7/19/06 DDC	

Project Mgr: ELK
Drawn by: DDC
Checked by: ELK
Start date: 4-18-02
Dwg path: Columbia-Improv.dwg
Sheet No. **C8**
Project #: 04-072-02



- LEGEND**
- Right of Way (R.O.W.) and/or Property Line
 - HC Handicap Ramp
 - Water Valve
 - Tree
 - Utility Pole
 - Sanitary Manhole
 - Parking Meter
 - Water Service Valve
 - Cable Television Pedestal
 - Fire Hydrant
 - Sign
 - Telephone Pedestal
 - Transformer
 - Streetlamp
 - Existing sidewalk / concrete
 - Proposed sidewalk / concrete
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- NOTES**
1. CROWN OF ROAD IS OFFSET FROM CENTER OF R.O.W. TO BE CONSISTENT WITH THE COMPLETED IMPROVEMENTS ON WEST COLUMBIA AVENUE.
 2. PROPOSED BACK OF 5-FT SIDEWALK IS 6" INSIDE R.O.W. WHICH PLACES THE FRONT OF SIDEWALK CONSISTENT WITH THE FRONT OF EXISTING SIDEWALK.

TELLURIDE STREETSCAPES - EAST TELLURIDE
 EAST COLUMBIA PLAN & SOUTH PROFILES
 400 & 500 BLOCKS, STA. 51+00 TO 53+00

Contact:
 Karen Guljamine
 1370 Black Bear Road
 Telluride
 Colorado 81435

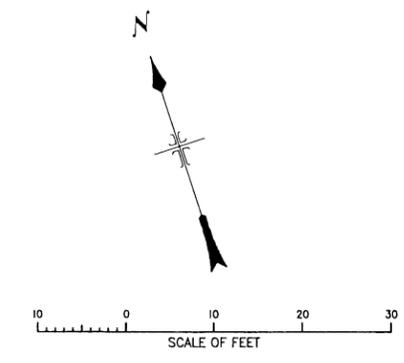
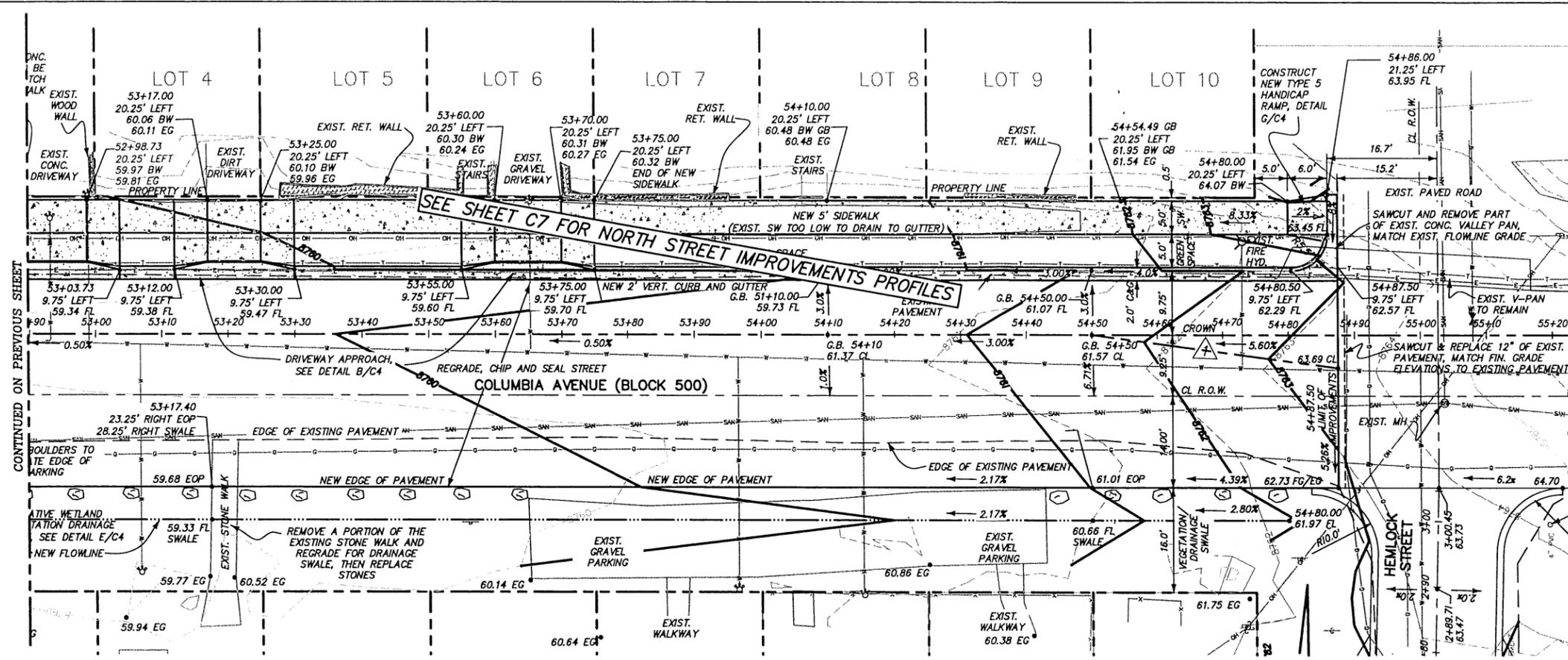
Client:
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 P.O. Box 397
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 Colorado 81435

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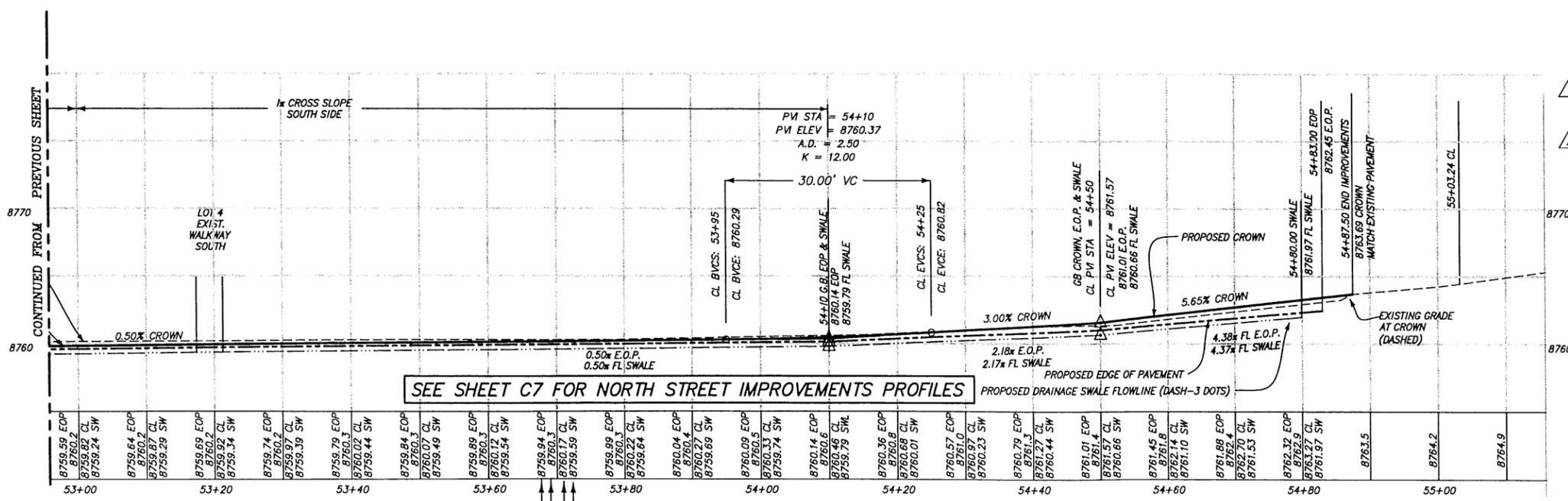
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Rev.	Description	Date	By
1	STREET PLAN SCHEMATIC SUBMITTAL FOR REVIEW	5/04/06 DDC	
2	PLAN & PROFILE SUBMITTAL FOR REVIEW	7/12/06 DDC	
3	FINAL CONSTRUCTION SET	7/19/06 DDC	

Project Mgr: ELK
 Drawn by: DDC
 Checked by: ELK
 Start date: 4-16-02
 Dwg path: Columbia-improv.dwg
 Sheet No. **C9**
 Project #: 04-072-02



- LEGEND**
- Right of Way (R.O.W.) and/or Property Line
 - HC Handicap Ramp
 - Water Valve
 - Tree
 - Utility Pole
 - Sanitary Manhole
 - Parking Meter
 - Water Service Valve
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- NOTES**
1. CROWN OF ROAD IS OFFSET FROM CENTER OF R.O.W. TO BE CONSISTENT WITH THE COMPLETED IMPROVEMENTS ON WEST COLUMBIA AVENUE.
 2. PROPOSED BACK OF 5-FT SIDEWALK IS 6" INSIDE R.O.W. WHICH PLACES THE FRONT OF SIDEWALK CONSISTENT WITH THE FRONT OF EXISTING SIDEWALK.

SEE SHEET C7 FOR NORTH STREET IMPROVEMENTS PROFILES

COLUMBIA AVENUE SOUTH STREET IMPROVEMENTS PROFILES

HORIZ. SCALE: 1" = 10'
VERT. SCALE: 1" = 5'

TELLURIDE STREETSCAPES - EAST TELLURIDE
EAST COLUMBIA PLAN & SOUTH PROFILES
500 BLOCK, STA. 53+00 TO 55+00

Contact:
Korn Gulimone
1370 Black Bear Road
Telluride
Colorado 81435

Client:
Town of Telluride
P.O. Box 307
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Colorado 81435

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Rev.	Description	Date	By
1	STREET PLAN SCHEMATIC SUBMITTAL FOR REVIEW	5/24/06 DDC	
2	PLAN & PROFILE SUBMITTAL	7/12/06 DDC	
3	FINAL CONSTRUCTION SET	7/19/06 DDC	

Project Mgr: ELK
Drawn by: DDC
Checked by: ELK
Start date: 4-18-02
Dwg path: Columbia-improv.dwg
Sheet No. **C10**
Project #: 04-072-02

Right of Way Landscaping Guidelines

RIGHT OF WAY LANDSCAPING GUIDELINES

OVERVIEW

Streetscape improvements are intended to decrease air and water pollution, increase street safety and functionality, and be aesthetically pleasing. As a result, some property owners find themselves with planting areas of various sizes adjacent to their property. For example, North Oak Street planting strips are 2 to 3 feet wide, while the 300 Block of North Spruce Street has a much wider area, or "pocket park," beside the 18-foot wide asphalt travel way. These green strips improve pedestrian safety and the pedestrian experience by increasing the separation between foot-travelers and motorized traffic. They soften the look of the right of way. They decrease the amount of impervious surface, preserving some area in the right of way for infiltration and providing space for snow storage in winter.

In many situations, it may be desirable to place stairs, stepping stone, and/or paths within green strips located in the public right of way to facilitate foot traffic between the travel way and the sidewalk. preserve vegetation, and provide for public safety while not detracting from the landscaping. appropriate locations include paths that align with building entrances or locations where the grade between the sidewalk and street is steep enough to necessitate stairs. Natural materials such as flat boulders, cut stone or flagstone is most desirable but concrete is also suitable. These paths and stairs should be kept to a minimum however, so the vast majority of green strips remains landscaping.

TOPOGRAPHY OR CONTOURS

Unless a berm is specified by the Streetscapes Profile to guide runoff and prevent neighborhood flooding, green strips should be slightly inverted or concave to provide effective infiltration or, if on a slope, they should drain to the adjacent sidewalk or curb. Steeply sloped green strips or pocket parks should be terraced to decrease slope length and better secure soil until plants are established.

SOIL

Initial installation of right of way green strips will require that the subsoil be scarified and that 6 to 12 inches of good, fertile topsoil be installed prior to planting. Expect to add amendments and mulch over time.

PLANTS

Unless other arrangements are made, the Town will seed and mulch new green strips with a native grass (TABLE 4-6) and wildflower seed mix.

Municipal Code Water and Sewer - 13.30.030

Any new lawn, garden or outside area that is to be watered, sprinkled or otherwise irrigated with water supplied through the municipal potable water delivery system shall comply with the provisions of this Section as a condition precedent to connecting any irrigation system or device to the municipal potable water supply.

A. All grasses and plantings to be installed in any yard, garden or other outside area, excluding vegetables or other edible plants cultivated for human consumption, should be of a drought tolerant native species that require minimal water irrigation. The Director of Parks shall promulgate and regularly update an advisory list of drought tolerant native species and acceptable mixtures of such species to guide landscape architects and other interested persons in the selection of acceptable plants and grasses. Xeriscaping shall be encouraged for all landscape plans. A failure to comply with the provisions of this subsection shall not constitute a punishable offense under the Municipal Code, it being the intent of this Section to encourage, but not require, compliance with the provisions hereof.

B. All new irrigation systems to be connected to the municipal potable water supply must incorporate the most water-conserving type equipment and devices commercially available as of the time the system is submitted for approval, and no new irrigation system shall be connected to the municipal potable water supply without having first been approved by the Building Official. ...

Trees

Trees are not permitted in right of way green strips, especially on the south side, where they interfere with snow storage and create shadows that encourage street and sidewalk icing in winter. Trees may be planted as part of pocket park landscaping but must be located 8 feet from the travel way and 3 feet from the sidewalk, at a minimum. TABLE 4-1 offers suggestions for mostly native tree species that do well in Telluride.

Shrubs

Shrubs may be planted in right of way landscaping green strips. They should be situated so they do not block access to fire hydrants or prevent passengers from getting into or out of motor vehicles that are parked. This is especially important for spaces reserved for handicap parking. Shrubs should not block pedestrian access along sidewalks when they are mature. Depending on the species, shrubs should be planted a minimum of 2 to 3 feet from the travel way and the sidewalk. They should also be hardy

enough to withstand burial by plowed snow and the dry, heated conditions created by surrounding concrete sidewalks and paved travel ways. TABLE 4-2 lists native shrubs that appear to survive well in green strips around Town. TABLE 4-3 lists native shrubs that are best to use in other, more protected right of way landscapes, such as pocket parks.

Perennials

Most perennials like lots of sunshine; nevertheless, when selecting perennials to plant in a greenstrip, choose species that can withstand harsh conditions as they will be regularly stepped upon, buried by snow and ice, and subject to pollutants from the adjacent travel way. Recognize that many of the commercially available perennials plants are hybrids of native plants and, most often, are best.

Many more varieties of native wildflowers may be available as seed. Use about 5 ounces of seed for every 100 square feet planting area; handbroadcast; rake lightly; cover with (weed-free) straw or another mulch to retain moisture; water gently once or twice a day for about four weeks; or seed just before the first snowfall and Mother Nature will do the rest. Be prepared to reseed the second season to fill in areas that did not take well.

TABLE 4-4 lists native perennials that have survived well in green strips around Town. TABLE 4-5 lists non-native perennial species that are not invasive and appear to survive well in Telluride green strips.

NOTE: The Town prefers that commercially available, premixed wildflower seeds not be used as they often contain seeds that are non-native and invasive in Colorado. Many seed companies, even those on the Internet, provide their customers with a special mix of seed at no extra charge.

Grasses

Native grasses add greatly to a natural look. Use about 5 ounces of seed for every 100 square feet planting area; handbroadcast; rake lightly; cover with (weed-free) straw or another mulch to retain moisture; water gently once or twice a day for about four weeks; or seed just before the first snowfall and Mother Nature will do the rest. Be prepared to reseed the second season to fill in areas that did not take well. Native grasses require little or no mowing, although, mowing one time during the growing season will encourage deeper root growth.

Kentucky bluegrass is not an appropriate grass for right of way green strips. It is a monoculture whose root mass is too tightly woven to permit adequate infiltration and it requires too much water to survive (up to two times the amount of water needed by native grasses). The native seed mix listed in TABLE 4-6, which is available locally and used by the

Telluride Parks and Recreation and Public Works departments, provides an even, green surface without the disadvantages. TABLE 4-7 lists all of the grass species or varieties considered native at our altitude by at least two reliable reference books or persons.

ROCK GARDENS

Using loose cobbles or gravel in the right of way green strips is an alternative that some fronting property owners have selected, instead of plants. While cobble and gravel is certainly a viable option for the green strips, it is discouraged. Often the cobbles or gravel end up in the travel way to become a hazard for motor vehicles and bicycles. As well, these loose materials, often fall into the gutter, blocking effective drainage and then polluting local waterways. To decrease the potential for either of these undesirable results, make sure that the top of the cobbles or gravel layer is at least 1 inches from the top of the back of curb and sidewalk.

IRRIGATION

Even the most drought-tolerant native plants need water to get established. Ideally, a soaker hose winding its way through the plants will take care of the watering all by itself, if hooked to a faucet equipped with a battery-operated timer. The Town of Telluride prefers that no permanent irrigation be placed in green strips. However, if permanent irrigation is desired, the following constraints must be adhered to:

1. Only drip lines are permitted. No spray heads will be allowed.
2. The irrigation line must be designed to be cut off and permanently capped at the property line, anticipating that future work in the right of way may eliminate the greenstrip.

The following offers property owners bordering green strips alternative planting solutions in accordance with the Telluride Masterplan and Land Use Code, which recommend the use of native plants and a more "rough and natural" look over highly groomed landscapes.

FERTILIZERS AND SOIL AMENDMENTS

It is likely that right of way green strips will require fertilizers and soil amendments to continue to thrive over time. Perennials respond with more vigorous growth to the occasional drink of fertilizer; however, minimize use of these products to prevent excess from running off during precipitation events or snow melt and ending up as pollution in local waterways.

MAINTENANCE & MANAGING INVASIVE WEEDS

Grasses may be cut in the fall, although it is not necessary to do so. Flowers should always be deadheaded, and dead flowerstalks and broken and damaged branches should be removed.

Invasive weeds, such as yellow toadflax, Canada thistle, scentless chamomile, and oxeye daisy can thrive in greenstrip areas, which are subject to frequent stress. The key to effective management is to know what these common invasive weeds look like and to manually remove (i.e., pull) young plants immediately and consistently before they become established. Using an appropriate pesticide is sometimes warranted. Note that most commercially available pesticides are "bareground" chemicals that will kill desirable plants as well as the undesirable weeds. These chemicals will prevent all plant growth where applied for a certain period of time. It may be better to contact Town Staff who may have a more benign alternative chemical available to assist you or can direct you to a professional pesticide applicator. Below are some photos of the most common invasive weeds in Town of Telluride.



Yellow toadflax



Canada Thistle



Scentless Chamomile



Oxeye Daisy

FOOTPATHS

Steps and/or paths might be necessary to facilitate access from the street to the sidewalk. Flat boulders, cut stone, flagstone, or concrete are all suitable materials. When placing steps or paths, consider the location of the front door as well as the spacing of on-street parking to maximize use of the hardscape and minimize disturbance of adjacent vegetation.

WINTER PROTECTION

A thick blanket of (clean) snow will protect greenstrip plants best. Shovel the snow from your sidewalk onto your plants.

TABLE 4-1. Suggestions for mostly native, preferred tree species.

Name	mature height	moist/dry/varied
<u>Evergreens</u> (Subalpine fir and Douglas fir are rarely available at nurseries.)		
White fir (<i>abies concolor</i>)	60'	moist. cool Rocky Mtn. Juniper
Juniper (<i>juniperus scopulorum</i>)	15'	varied
Colorado spruce (<i>picea pungens</i>)	60'	moist
Blue spruce (<i>picea pungens glauca</i>)	60'	moist
Engelmann spruce (<i>picea Engelmannii</i>)	75'	shady, cool
Ponderosa pine (<i>pinus ponderosa</i>)	70'	dry
<u>Deciduous Trees</u>		
quaking aspen (<i>populus tremuloides</i>)	50'	all over
cottonwood (<i>populus angustifolia</i>)	60'	moist
<u>Hardy, non-invasive, but non-native</u>		
crabapple (<i>malus</i> ssp)	25'	tiny red apples
Schubert chokecherry (<i>prunusvirginiana Sch.</i>)	20'	purple leaves

TABLE 4-2. Native shrub species that appear to survive well in green strips around Town.

Name	mature height	moist/dry/varied
chokecherry (<i>prunus virgo melanocarpa</i>)	15'	varied
serviceberry (<i>amelanchier alnifolia</i>)	10'	varied
cliff jamesia, waxflower (<i>jamesia americana</i>)	4'	varied
wild rose (<i>rosa woodsii</i>)	5'	dry
potentilla (<i>potentilla fruticosa</i>)	3'	sunny, dry
currant and gooseberry (<i>ribes</i> ssp)	6'	sunny, dry
rock spirea (<i>holodiscus dumosus</i>)	5'	sunny, dry
raspberry and thimbleberry (<i>rubus</i> ssp)	5'	sunny, dry
scrub oak (<i>quercus gambelii</i>)	5'	sunny, dry
common juniper (<i>juniperus communis</i>)	3'	all over

TABLE 4-3. Native shrub species that can be used in other, more protected right of way landscaping, such as pocket parks.

Name	mature height	moist/dry/varied
mountain snowberry (<i>symphoricarpos oreophilus</i>)	3'	moist, shady
redosier dogwood (<i>cornus sericea</i>)	6'	moist, shady
bush honeysuckle (<i>jonicera involucrata</i>)	6'	moist, shady
red elderberry (<i>sambucus racemosa</i>)	6'	shady
buffalo berry (<i>shepherdia canadensis</i>)	4'	shady
ninebark (<i>physocarpus monogyn!</i> .Js)	5'	shady

TABLE 4-4. Native perennial species that have survived well in green strips around Town. Those marked with "c" grow well from cuttings.

Name	Color	Height	
yarrow (<i>Achillea</i>)	white, pink	18- 24"	c
heartleaf arnica (<i>Arnica cordifolia</i>)	yellow	12 -18"	
pearly everlasting (<i>Anaphali triplinervis</i>)	white	12 -18"	
pussytoes (<i>Antennaria rosea</i>)	lt. Pink	2 -3"	
columbine (<i>Aquilegia ssp</i>)	blue	18 -24"	
fringed sage (<i>Artemisia frigida</i>)	silvery	10 -12"	
butterfly weed (<i>Asclepias tuberosa</i>)	orange	30"	
Alpine aster (<i>Aster alpinus</i>)	white, blue, pink	12 -18"	c
harebell (<i>Campanula rotundifolia</i>)	blue	12 -18"	c
snow in summer (<i>Cerastium tomentosum</i>)	white	8"	
delphinium and larkspur (<i>Delphinium ssp</i>)	blue, white	2 -3'	c
draba (many varieties)	various	2 -8"	
fireweed (<i>Epilobium angustifolia</i>)	pink	12 -24"	
fleabane (<i>Erigeron ssp.</i>)	various	1 -2'	c
Indian blanket (<i>Gaillardia aristata</i>)	red-yellow	12 -24"	c
geranium (<i>Fremontii and richardsonii</i>)	pink/white	12"	
scarlet gilia (<i>Ipomopsis aggregata</i>)	red	12 -30"	
iris (<i>Iris missouriensis</i>)	light blue	18 -24"	c
blue flax (<i>Linnum perenne lewisii</i>)	blue	12 -18"	
lupine (<i>Lupinus ssp.</i>)	many colors	18 -24"	c
forget-me-not (<i>Myosotis alpestris</i>)	blue, pink	5 -6"	c
evening primrose (<i>Oenothera pallida</i>)	white	12"	
alpine poppy (<i>Papaver alpinum</i>)	blue	5 -6"	
firecracker penstemon (<i>P. eatonii</i>)	red	3'	
Rocky Mtn. penstemon (<i>P. strictus</i>)	lavender blue	2 -3'	
Jacob's ladder (<i>Polemonium caeruleum</i>)	blue	12 -24"	c
creeping buttercup (<i>Ranunculus repens</i>)	yellow	6 -8"	
sedum (many varieties)	various	3 -4"	
cushion pink (<i>Silene acaulis</i>)	pink	2 -3"	

TABLE 4-5. Non-native perennial species that are not invasive and appear to survive well in green strips around the Town of Telluride.

Name	Color	Height
Pink pussytoes (<i>Antennaria dioica 'Rubra'</i>)	grey foliage pink flower	1-4"
Purple aster (<i>Aster tongolensis 'Wartburg Star'</i>)	purple	12-18"
White Coneflower (<i>Echinacea purpurea 'White Swan'</i>)	white	24-36"
Dwarf coneflower (<i>Echinacea 'Kim's Knee High'</i>)	purple	18-24"
Dwarf blanket flower (<i>Gaillardia 'Goblin'</i>)	red & yellow	12-18"
Burgundy blanket flower (<i>Gaillardia grandiflora 'Burgundy'</i>)	deep red, burgundy	18-24"
Yellow daylily (<i>Hemerocallis 'hyperion'</i>)	clear yellow, very reliable	18-30"
Lupine (<i>Lupinus polyphylus 'Gallery Mix'</i>)	mixed	18-24"
Red Lupine (<i>Lupinus 'Gallery Red'</i>)	red	18-24"
Petit Delight (<i>Monarda didyma</i>)	lavender, pink	12-18"
Catmint (<i>Nepeta sibirica 'Six Hills Giant'</i>)	grey foliage, lt. purple flower	36" +
Pink catmint (<i>Nepeta grandiflora 'Dawn to Dusk'</i>)	grey foliage, lt. pink flower	24-36"
Icelandic poppy mixed (<i>Papaver n. 'Champagne Bubbles'</i>)	mixed colors	12-18"
Icelandic poppy pink (<i>Papaver n. 'Wonderland Pink'</i>)	pink shades	6-12"
Prairie dusk penstemon (<i>Penstemon pinifolius 'Prairie Dusk'</i>)	deep pink, lavender	18-24"
Pink penstemon (<i>Penstemon 'Elfin Pink'</i>)	med pink	12-15"
Purple salvia (<i>Salvia nemerosa 'May Night'</i>)	purple	12-18"

Pink soapwort (<i>Saponaria ocymoides</i>)	pink	6-12"
Pincushion flower (<i>Scabiosa columbaria</i> 'Butterfly Blue')	blue, lilac	18-24"
Stonecrop dragon's blood (<i>Sedum spurium</i> 'Dragon's Blood')	dark pink	3-6"
Stonecrop autumn joy (<i>Sedum</i> 'Autumn Joy')	pink, red	18-24"
Hens and chicks (<i>Sempervivum tectorum</i>)	white, yellow	0.5-3"
Woolly thyme (<i>Thyme</i> 'Woolly')	pink	0.5-3"

TABLE 4-6. Town of Telluride native grass mix.

<u>Species</u>	<u>Origin</u>	<u>Lbs/Ac</u>
Slender wheat, Pryor	WA	13.57
Mountain brome, Bromar	WA	13.65
Tufted hairgrass	CAN	3.07
Big bluegrass, Sherman	WA	11.82
Streambank wheat, Sodar	WA	13.80
Arizona fescue, Redondo	CO	5.82
Wild rye, Elkton	WA	3.29
		65.00

TABLE 4-7. Grass species or varieties considered native at our altitude by at least two reliable reference books or persons. All are bunch grasses (as opposed to sod-forming grasses), so your future lawn will be a bit irregular and bumpy. (F = foothills, M = montane zone, S = subalpine zone, A = alpine zone)

<u>Name</u>	<u>Range</u>
Alpine bluegrass (<i>poa alpinum</i>)	S, A
Big bluegrass (<i>poa ampla</i>)	
Sandberg bluegrass (<i>poa sandbergii</i>)	F to 11,500'
Muttongrass (<i>poa fendleriana</i>)	
Bottlebrush squirreltail (<i>sitanion hystrix</i>)	to 10,000'
mountain brome (<i>bromus marginatus</i>)	to 10,500'
Arizona fescue (<i>festuca arizonica</i>)	to 11,500'
Idaho fescue (<i>festuca idahoensis</i>)	to 12,000'
Thurber fescue (<i>festuca Thurberi</i>)	
Indian ricegrass (<i>oryzopsis hymenoides</i>)	
Letterman needlegrass (<i>stipa lettermanii</i>)	
Columbia needlegrass (<i>stipacolombiana</i>)	to 10,000'
Junegrass (<i>koeleria cristata</i>)	
Tufted hairgrass (<i>deschampsia caespitosa</i>)	7 – 13,000', WET
Sand dropseed (<i>sporobolus cryptandrus</i>)	F to 8,000'
Bluebunch wheatgrass (<i>agropyron spicatum</i>)	
Slender wheatgrass (<i>agropyron trachycaulum</i>)	M, S
Western wheatgrass (<i>agropyron smithii</i>)	
Blue wild rye (<i>elymus glaucus</i>)	to 11,000'

The following are photographic examples of right of way greenstrip landscaping that is working as intended.



Profile A (example: east side of North Oak Street) – These are narrow, 24 - 36" wide, planting, strips between the sidewalk and the street. The flat stones serve as a step between the sidewalk and parking.



Profile B (example: 300 block of N Spruce Street) -- With the travel way being narrowed to 18 feet and the sidewalks removed, a 22 foot wide greenspace became available directly adjacent to the properties on the E side of Spruce Street. With a total of 4,400 square feet, this area resembles a pocket park more than a planting strip. It constitutes a small victory over the dominance of the car!



Profile C (example: south side of E Columbia)—wide berm for snow storage and flood/runoff management/wetland.

Streetscape Lighting Plan

STREETSCAPE LIGHTING PLAN

*Neighborhood Meetings:
June 10, 1998; January 5, 2000*

OVERVIEW

Street lighting is an important issue as it can greatly affect the aesthetic of a neighborhood, address concerns for safety along pedestrian corridors, and affect nearby wildlife. The Streetscape Task Force conducted numerous meetings with neighborhoods and internally with lighting experts in an effort to address concerns and balance competing interests. As well, this Manual strives to integrate the most recent goals of the Telluride Master Plan (2006) regarding the community's desires regarding lighting. (See the Text Box below.)

Lighting and the Telluride Master Plan (2006)

Environmental Preservation and Restoration. Goal C.14. The Town will adopt guidelines to address night lighting (see also Urban Design #5).

Urban Design. Goal H.5. Adopt guidelines to address night lighting in Town in both public and private spaces. Telluride will strive to become a Dark Skies Community.

Note: The mission of the International Dark Sky Association is to preserve and protect the nighttime environment and our heritage of dark skies through quality outdoor lighting.

Lighting Assessment Results

A May 2000 lighting inventory revealed the following patterns:

- Standard commercial streetlights are positioned in the commercial districts in a consistent pattern on alternate sides of the street. Separation on Colorado Avenue between Alder and Davis ranges from 75 feet to 125 feet. On West Pacific Avenue between Oak and Davis the separation averages about 125 feet. On the block and a half stretch of South Oak, there is a 75' separation between lights.
- Standard commercial streetlights are placed with some regularity along Pacific Avenue west of Davis Street, along Mahoney Drive, along Black Bear Road and Depot Avenue, and on Prospect, Smuggler, Butcher Creek Drive, and Cornet Street. Their placement on the south half of the Warehouse District and in residential areas appears to be random.

- Irregular clusters also exist at the Elementary School, the Viking Lodge, in front of Town Hall, around Shadow Lane and the 100 Block South Pine Street, as well as the RV and Shandoka Parking lots.
- Pole-mounted residential light fixtures are arranged in a loose but fairly consistent pattern in the residential zone along Columbia and Galena from Davis Street to Alder Street. These fixtures are also randomly placed in East Telluride, along Gregory, and in the Warehouse District.
- These randomly-placed fixtures are usually not operable or have been altered.

Revisiting this survey in 2006, revealed some changes:

- In the Commercial Core, standard commercial streetlights remain positioned in a consistent pattern on alternate sides of the street.
- Standard commercial streetlights are placed with regularity along on side of Pacific Avenue west of Davis Street, and along Mahoney Drive, Black Bear Road, Depot Avenue, Prospect, Smuggler, Butcher Creek Drive, and Cornet Street.
- Placement of standard commercial streetlights within the south half of the Warehouse District is becoming more consistent as Streetscape improvements are constructed; however, some areas are over lit, while others lack sufficient lighting.
- Placement of standard commercial streetlights in the East Telluride residential area is anomalous.
- Irregular clusters of standard commercial streetlights still exist at the Elementary School, the Viking Lodge, in front of Town Hall, along Shadow Lane, on the 100 Block South Pine Street and South Willow, and at the RV and Shandoka Parking lots.
- Placement of pole-mounted (i.e., SMPA-supplied) residential light fixtures is fairly consistent in the residential zone along Columbia and Galena from Davis Street to Alder Street. These fixtures have been removed from East Telluride, along Gregory, and in the Warehouse District. All are scheduled to be replaced with a lower-profile, more energy-efficient alternative in 2007.

Lighting Plan Goals

The Streetscape Lighting Plan (Figure 1) is intended to help accomplish the following:

- Revise fixtures so that light is directed downward.

- Replace existing bulbs with more energy efficient alternatives.
- Position streetlights at curbs to minimize impediments to pedestrian flow.
- Be consistent and even with the level of illumination, where illumination is needed.
- Examine the potential for lighting to be consistent along pedestrian corridors.
- Identify areas of Town that do not require any lighting in the right of way.
- Explore options for new fixtures to replace lights on telephone poles at intersections in residential neighborhoods. Select one.
- Make energy efficiency an important selection criterion when selecting new street fixtures.

The Streetscape Task Force believes that these goals are consistent with Telluride desire to become a Dark Skies Community.

STANDARD COMMERCIAL STREETLIGHT FIXTURE

After considerable research to identify a completely different light fixture for the commercial district, the Streetscape Task Force determined that Town residents preferred to keep the existing fixture (Figure 2). Focus then shifted to globe inserts that

would succeed in directing more light down onto the sidewalk and much less into the night sky and neighboring buildings. Figure 3 illustrates the final solution.

PREFERRED RESIDENTIAL FIXTURE

The pole-mounted residential light fixture that was historically provided by the San Miguel Power Association (Figure 4) is aesthetically unpleasing and tends to spill light into buildings that are on adjacent properties. It also creates a notable “glow” in the night sky. Figure 5 illustrates the preferred fixture selected after extensive research into available lighting options. Goals included energy efficiency, aesthetic appeal, and efficient lighting of the intersection.

EAST COLORADO BOLLARDS

Figure 6 illustrates the bollard that was selected for placement at each intersection along East Colorado Avenue from Maple Street to Columbine Street. This bollard is intended to replace the anomalous standard commercial streetlights in the East Colorado residential area.

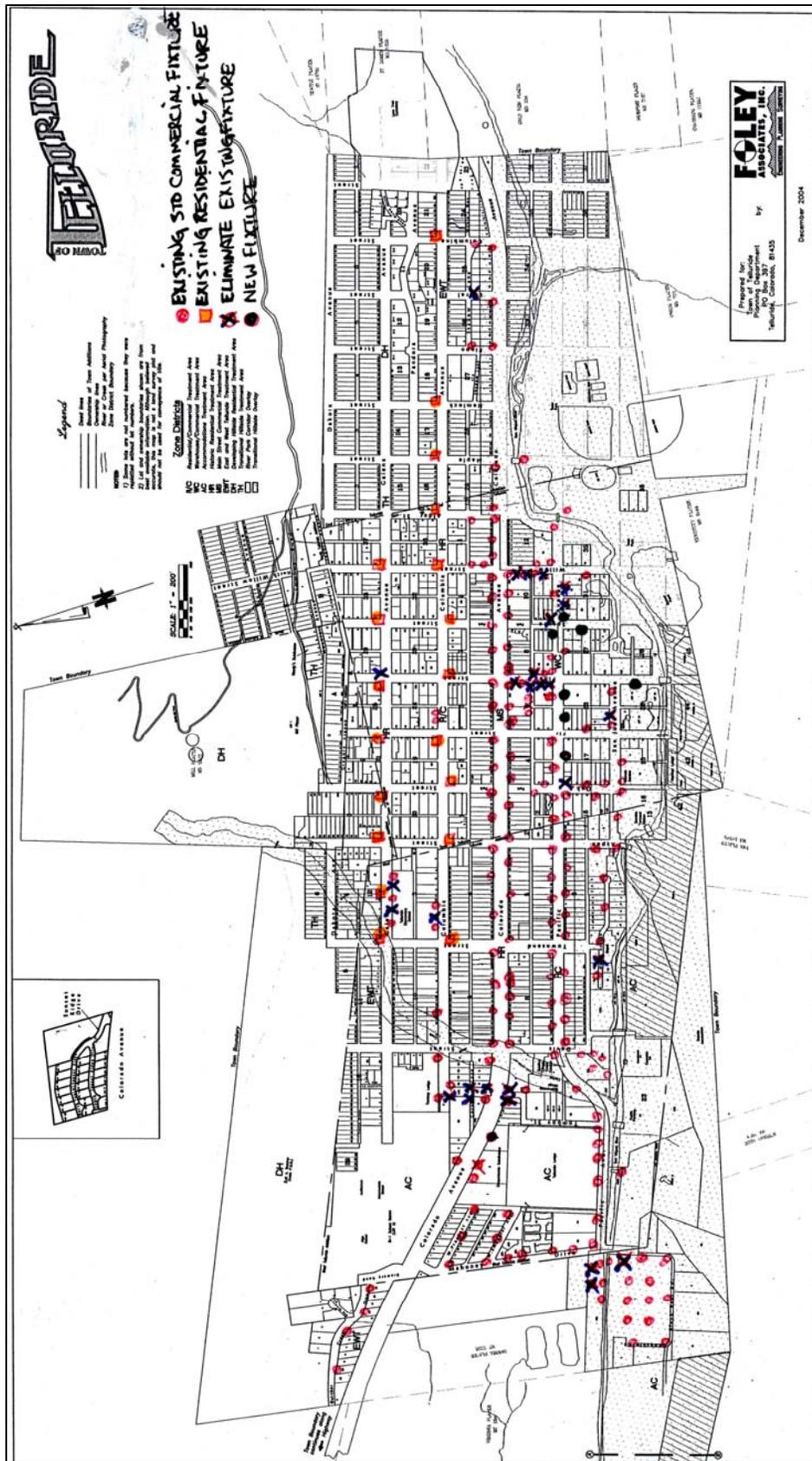


Figure 1. Streetscape Lighting Plan



Figure 2. Telluride's Standard Commercial Streetlight



Figure 3. Globe insert to the Standard Streetlight to decrease light spill and direct more light onto



Figure 4. SMPA-supplies pole mounted residential light



the sidewalk.
Figure 5. Selected residential light fixture



Figure 6. Bollard style selected for East Colorado Avenue intersections from Maple Street to Columbine Street

Appendices

Glossary of Terms

Neighborhood Meeting Notes

Telluride's Sidewalk Construction & Maintenance Ordinance

GLOSSARY OF TERMS

To facilitate a comprehensive understanding of the Manual of Streetscapes Standards, each design element is defined or described in context below.

Alley and Alley Improvements – An alley is a 15- to 20-foot wide right of way serving as public access to the rear of residential and commercial buildings. Alley improvements are intended to include surfacing, drainage management, utility undergrounding, snow removal and storage, and one-way vehicle/service access as design concerns. Not all alleys will be surfaced.

Curb and Gutter – A curb is a continuous raised concrete edge that directs the flow of runoff along a street. The gutter is the area immediately next to the curb that is designed to channel and efficiently transport runoff to a designated discharge location. To protect the curb from potential damage and to increase its intended function, curbs are almost always built with an adjacent gutter.

Drainage – A drainage is either a device used to funnel runoff, or a defined area within which runoff is collected. The device can be open or closed (i.e., covered), consisting of a pipe, channel, gutter, culvert, etc., and may be located above or below grade.

Drainage Infrastructure – Drainage infrastructure is the sum of drainage devices, which are typically located in the public right of way, that work together to serve a drainage area. Drainage infrastructure is typically designed to function using gravity flow.

Engineered Plans – Engineered plans are street construction documents completed and sealed by an engineer registered in the State of Colorado that have been tied topographically to existing and proposed grades and which contain all details necessary to begin construction. Nevertheless, all existing elevations and grades should be verified in the field.

Gateway/Entry Aspects – An entry, or gateway, is the location where the first or primary appearance of a street or the town is made. Aspects of the gateway and entry that must be considered include signs and other defining physical features.

Green Strip – A green strip is the area of land between the back edge of the street curb and the sidewalk or private property, where appropriate plants, furniture, or artwork can be installed and

where snow that is removed from sidewalks or the travel way can be permanently or temporarily stored through the winter.

Handicap Access – Handicap access is the provision of access to and through a building or property for physically impaired individuals, including but not limited to those using wheelchairs or crutches or those with sight and/or hearing impairments.

Hardscape – Hardscape refers to the built or manmade aspects of a land area. Hardscape includes all concrete or asphalt surfaces, as well as wooden decks and building footprints.

Historic Preservation – Historic preservation encompasses a broad range of activities related to the preservation and conservation of the built environment. In Telluride, the aesthetic and visual impact of numerous historic structures and their overall historic context are to be preserved in a manner similar to their original siting and impacts.

Infiltration – Infiltration is the percolation or slow passage of rainwater, snowmelt, or irrigation into and through soil.

Landscaping – Landscaping is the sum of physical features on a parcel of land, e.g., hardscape treatments, plantings, furniture, and artwork. These features are intended to encourage, discourage, or embellish the intended use, adornment, and/or appreciation of a land area.

Landscaping Strip – See green strip.

Lighting – Lighting is a quantity or quality of illumination at a particular location or area, typically measured in lumens.

Light Fixture – A light fixture is a device that supports and typically encloses an electric light, which may include a lens, shade, and/or reflector. It may also support and enclose a gas-powered light. The fixture can be mounted in a variety of ways in a fixed location. The fixture must be distinguished from the light standard.

Light Standard – A light standard is the physical device that supports the light and the light fixture. It typically consists of a freestanding pole or bollard, or an attachment to a building.

Major Street Improvement – A major street improvement is work that cannot be defined as “Minor Maintenance and Infill.”

Managed wetland – A wetland is any land where water saturation is the dominant factor determining the nature of soil development and the types of plant and animal communities living in the surrounding environment. A managed wetland is usually a man-made or restored natural wetland that is used to clean water running off of streets or other polluted water prior to discharge into another water body.

Minor Maintenance and Infill – Minor maintenance and infill is significant work within the street right of way that adjusts the use and function of the street in an insignificant manner. It typically encompasses no more the half the width of the right of way.

Neighborhood Character – Neighborhood character is the quality of a specific neighborhood that makes it unique or gives it “flavor.” This Streetscapes Manual of Standards intends to preserve or enhance neighborhood character, as defined by those who live there. It primarily encompasses physical characteristics of the street, private and public landscaping, public land, and private development.

Parking – Parking is the provision of land for the temporary storage of automobiles.

Pedestrian Path – A pedestrian path is any area designated, formally or informally, for the use of individuals to walk or bike. It is primarily intended to facilitate access for foot or bike traffic outside of the proximity of moving motor vehicles.

Permeability – Permeability describes the ease of movement of water and/or gases through a soil material.

Pocket Park – A pocket park is a very small public park that is situated to relieve the density of the immediate built environment.

Public Art – Public art is artwork designed specifically for, or placed in areas physically accessible to, the general public.

Ribbon Curb – A ribbon curb is a 0.5'- to 1'-wide concrete curb that provides structural support for the edge of asphalt and provides motor vehicle drivers to better see the edge of asphalt, but permits water to sheet flow over it into a vegetated ditch or other natural feature.

Scarify – To scarify means to break the surface of the soil with a narrow bladed instrument.

Scoring Pattern – A scoring pattern is an etched pattern in a concrete sidewalk. The Town of Telluride’s historic scoring pattern should be used on all new concrete sidewalks, alley aprons, and driveways.

Sidewalks – A sidewalk is a walkway near or on the edge of the travel way in the right of way that is improved with material such as concrete or flagstone.

Sign – A sign is fixed graphic display intended to convey information that may be important to the public.

Snow Storage Area – A snow storage area is a parcel of land serving as a repository for snow for the Public Works Department in winter. It should be designed to encourage and appropriately manage the melting of snow. Numerous, scattered snow storage areas that can infiltrate melted snow facilitate its treatment.

Street Furniture – Street furniture refers to the manufactured elements that can be found in the public rights of way, including benches, streetlights, fire hydrants, public art, and signs.

Surfacing – Surfacing is the type of material placed over soils, or earth, that is intended to prevent erosion, manage drainage, and facilitate ease of travel.

Travel Way – The travel way is that area within the right of way used by mechanized vehicles, or when curbs are present, the area between curbs.

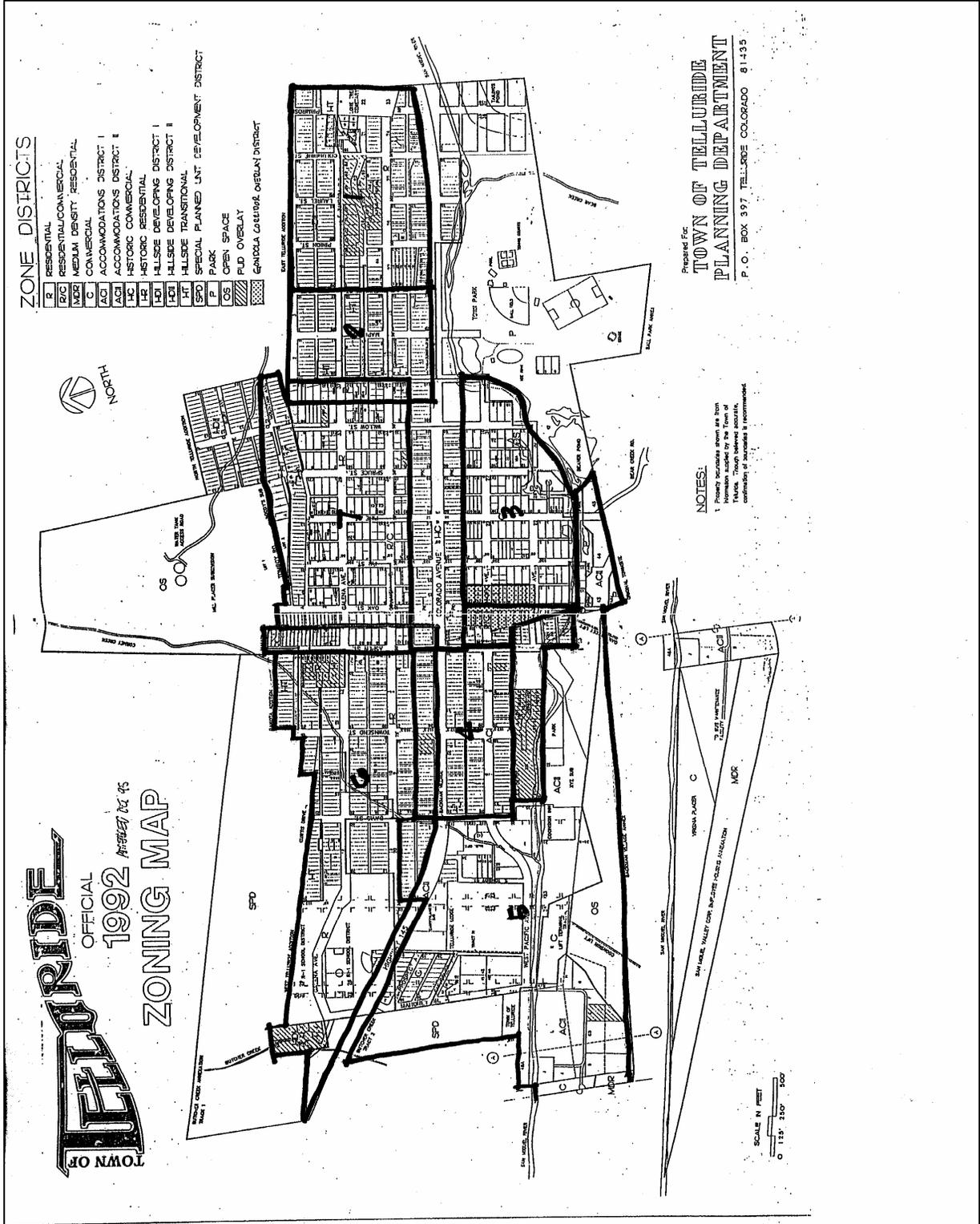
Truck and Traffic Circulation – Truck and traffic circulation is a set of standards and functional application of the same. These standards are applied to determine the adequacy of a particular right of way to accommodate specified types and volumes of vehicles.

Valley Pan – A valley pan is a concave or v-shaped, concrete structure designed to carry a specific volume of water from one location on a right of way to another. It is also called a “drain pan”.

View Preservation – View preservation is the practice of placing physical objects, such as buildings, in a manner that does not change traditional view corridors. Traditional view corridors typically occur within a visual plane that is an actual portion of or an extension of a street right of way strongly linked to Telluride’s historic grid pattern of streets.

NEIGHBORHOOD MEETING NOTES

Streetscapes neighborhoods 1 through 7
 NEIGHBORHOOD 1



The area of Town north of Colorado Avenue between the east boundary of Town and Hemlock Street

- A narrow right of way (travel way) and lack of sidewalks is preferred.
- Maintain any existing mature vegetation in the right of way of Columbia Avenue.
- The snow storage schedule and location should vary and be flexible according to the location of curb cuts to private property in the right of way.
- Are curbs necessary?
- "Status quo" is satisfactory.
- Most things work in this neighborhood.
- There is a drainage problem on the south side of Columbia Avenue.
- There are erosion and drainage problems from the north down Columbine Street from Primrose.
- The wetlands that exist on the north side of Colorado Avenue can be used as a surface drainage filter.
- Consider allocating rights-of-way that will never be used as "traveled ways" for other uses such as trails – Laurel Street is an example.
- Lack of street lighting is a positive thing. Reduce light pollution.
- Columbia Avenue is a significant pedestrian corridor to the east end of Town (to the mine) through the cemetery.
- Traffic calming is necessary on Columbia Avenue.
- Meandering Columbia may be a solution.
- There is a necessity for "doggy bags" in this neighborhood.
- Formalize the "green-space" that already exists on Columbia Avenue.
- Examine the potential for diagonal parking on Columbia.
- Gravel mining on the San Miguel River to the east of the Town Park is a detriment to the health of the river. Public camping should move into this area.
- Town Park blocks south of Colorado Avenue could be used for parking or a managed wetland area - surface drainage could be diverted to this area.
- Drainage could be diverted to wetlands on the north side of the Colorado Avenue right of way (south side of lots).
- Colorado Avenue to the east of Alder Street is a speeding zone.
- Should the bike path and river trail continue on East Colorado Avenue?
- Examine the potential for pocket parks within rights of way that are platted but not used for travel ways because they are too steep.

NEIGHBORHOOD 2

The area of Town north of Colorado Avenue between Hemlock Street and the alley between Willow and Alder Streets.

Problem. End of commercial core – RV and truck turnaround and overnight idling significantly impact the character of the neighborhood.

Solution. Limit truck and RV access times.

- Examine the benefit of a traffic circle at the intersection of Alder Street and Colorado Avenue vs. a turnaround at the Town Park for larger vehicles. A turnaround at the town Park may conflict with the park entrance.
- Slow traffic down east of the business core – to give a more residential feel.
- There is a necessity for pedestrian paths on both sides of east Colorado Avenue. Examine the potential for expanding the River Park Trail to the east.
- Examine the potential for diagonal parking on East Colorado Avenue.
- Snow storage is prohibitive on the south side of Columbia Avenue. Make it functional.
- Alternate parking side to side instead of one side the whole winter.
- Resurface the street with asphalt on Columbia Avenue and the alleys.
- There is a drainage problem on to private property on the 300 block of East Columbia Avenue.
- On east Pandora Avenue, the drainage is too flat – the street needs to be re-graded.
- The Town Park culvert ridge needs to be replaced.
- The culvert at Maple Street and East Colorado Avenue backs up and floods during high water.
- A porous surface on flat areas is recommended.

- An asphalt surface should be placed on Columbia Avenue, alleys should remain dirt.
- Utilities should be undergrounded.
- Acquire snow storage land as future acquisition for P.U.D.'s in this neighborhood.
- Sidewalks on East Colorado Avenue should end at East Maple Street.
- Enhance the wetland buffer on north side of Colorado Avenue as a drainage filtration system.
- Pedestrian safety on East Colorado Avenue is a significant issue.
- Continuous sidewalks are an important part of a good pedestrian experience.
- Paving without drainage features does not work well.
- Curbs are not necessary where the street just drains itself.

General Notes.

- Maintain sidewalks in their existing locations.
- Propose that buses travel in a two-way direction on Pacific Avenue.
- A turnaround at Alder Street would allow for an easy transition to a narrower travel way on Colorado Avenue east of Alder Street (see diagram).
- Consider a low profile curb in some areas.
- The Town should do a grass-crete experiment.

NEIGHBORHOOD 3

The area of Town north and east of the San Miguel River south of the south alley of Colorado Avenue and west of South Aspen Street.

- Isolated areas of green space are appropriate - perhaps on the north side of San Juan Avenue
- Resurface sidewalks and streets
- Improve drainage
- Benches and other public amenities are appropriate
- Public art is encouraged
- Informal landscaping is appropriate
- Need better snow storage and snow removal
- Wider sidewalks are appropriate
- Enhanced pedestrianization is important

NEIGHBORHOOD 4

The area of Town south of Colorado Avenue, west of Aspen Street, and east of Davis Street; and, the area of Town south of the alley between Colorado and Pacific avenues between Oak and Aspen streets.

NEIGHBORHOOD 5

The area of Town south of Colorado Avenue and west of Davis Street; and, the area south of the alley between Pacific Avenue and Depot Avenue and west of Oak Street.

- There is a concern that changing the transit route will increase traffic to Mahoney Avenue
- Tomboy Street cannot handle too much increased traffic.

NEIGHBORHOOD 6

The area of Town north of Colorado Avenue and west of the alley between Aspen and Oak Streets.

- Pedestrian Corridor is desired through Pocket Park at the 500 block of West Galena, perhaps this can be accomplished through signage
- Safety for school pedestrians is a significant issue
- Galena can be much narrower than Columbia

- Pedestrian trail up Cornet Creek

NEIGHBORHOOD 7

The area of Town north of the alley between Colorado Avenue and Columbia Avenue between Aspen and Alder Streets.

- Install conduit before surfacing for future undergrounding – a larger improvements district has economic benefits.
- The general format of the street should develop landscaping on the south (trees), sidewalks on the north (sunny side). The sidewalks should be continuous to promote pedestrianization within the neighborhood.
- Sidewalks should be safer, clean and functional.
- Snow storage should occur on the south side of the street.
- Willow Street and Galena Avenue need better maintenance.
- Don't pave North Willow and East Galena, aesthetically chip/seal is o.k. but not black top.
- Explore the potential for a "grass-crete" surface in various locations and a mixture of hard and permeable surfaces.
- The "rough and natural" look is preferred in this neighborhood.
- Undergrounding of utilities is desired in the neighborhood

TITLE 12

STREETS, SIDEWALKS AND PUBLIC PLACES

Chapter 12.04

Sidewalk Construction and Maintenance

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- 12.04.020 Notice - Contents.
- 12.04.030 Notice - Service.
- 12.04.040 Town Action Upon Default.
- 12.04.050 Assessment.
- 12.04.060 Width.
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Chapter 12.06

Newsracks

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Chapter 12.08

Public Utilities Construction

- 12.08.010 Building Code Compliance.
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Chapter 12.12

Festival Events in Town

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- 12.12.020 Advance Notice of Impacts.
- 12.12.030 Crowd Size Limit for Major Festival Events in Town Park.
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Chapter 12.04

SIDEWALK CONSTRUCTION AND MAINTENANCE

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- 12.04.090 Snow and Ice Removal.
- 12.04.100 Violation – Misdemeanor.
- 12.04.110 Violation – Public Nuisance.

12.04.10 Petition of Council – Council Initiation.

When the owners of the majority of the lots fronting upon either side of any street, or any specified portion of a street, petition the Town Council for the construction or repair of a sidewalk along the street or any portion thereof, or whenever the Town Council deems it necessary, the Town Council may at any regular or special meeting, by an order or resolution to be passed by a majority of the members of the Town Council, order such construction or repair. (Ord. 9,1917)

12.04.020 Notice - Contents.

The order for the construction or repair of any sidewalk shall specify the kind and dimensions of materials to be used, and shall describe the lot or lots in front of which the sidewalk is to be constructed or repaired, and direct the owner of each such lot so described to make the improvement; in case of new construction within twenty (20) days, and in case of repair within five (5) days, from the service of a written copy of such order. (Ord. 9, 1917)

12.04.030 Notice - Service.

A copy of such order shall be served upon the owner or owners of the lots affected, or upon the agent or agents of such owner or owners, either by the Director of Public Works or the Town Marshal, or in case such service cannot be conveniently made, the order shall

be published for one (1) week in a newspaper of general circulation published in the Town, which publication, after the expiration of one (1) week, constitutes due service upon the owners of the lots. (Ord. 9,1917)

12.04.040 Town Action Upon Default.

If any owner of any lot described in the order neglects or refuses to "make the improvements so ordered within twenty (20) days in case of construction or renewal of the walk, or within five (5) days in case of repair of the walk, from the date of service of notice, the Director of Public Works shall cause the walk to be constructed or repaired, as the case may be, and shall make out an itemized statement or bill of the cost of such, construction or repair and present it to the owner or owners, or agent or agents, of the lot or lots or, in case such owner or owners, agent or agents cannot be found or such statement or bill conveniently presented to them, then such statement shall be published for one (1) issue in a newspaper of general circulation published in the Town, which publication is a full and sufficient presentation of such bill or statement. If the bill remains unpaid for twenty (20) days from the date of presentation or date of publication, it shall be presented to the Town Clerk, whose duty it is to bring the matter before the Town Council at the next regular meeting of the Town Council, which may order the bill paid from the Town funds by warrant or warrants thereon. (Ord. 9,1917)

12.04.050 Assessment.

In case the Town has paid for the construction, renewal or repair of the sidewalk as provided in this article, then the Town Council shall, at a regular meeting, assess the cost of such construction, renewal or repair to the lot or lots, together with ten percent (10%) penalty thereon to defray the cost of collection; and it is the duty of the Town Clerk to certify the assessment to the County Clerk and Recorder, or to the official having custody of the tax list at the time such certification is made, to be placed by him or her upon the tax list for the current year and collected in the manner provided by law; provided however, that no such assessment shall be so certified until notice of such assessment has been pub-

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lished in a newspaper of general circulation published in Town, for a period of not less than one (1) week, giving notice to the owner or agent of the lot or lots to appear before the Town Council at a specified meeting, not less than ten (10) days after such publication, where he or she may be heard as to the justness and correctness of the assessment. (Ord. 9, 1917)

12.04.060 Width.

All sidewalks constructed or reconstructed along or upon Colorado Avenue, between Aspen and Willow Streets, shall be ten (10) feet in width, and shall be constructed only of brick, stone, flagging, asphalt or cement. All other sidewalks upon or along Colorado Avenue, and upon or along cross streets from Aspen Street to Willow Street and between Columbia and Pacific Avenues, shall be eight (8) feet in width. All other sidewalks, wherever situated, constructed or reconstructed, shall be six (6) feet in width. (Ord. 9, 1917)

12.04.070 Materials and Standards.

Except as otherwise provided in this article, all sidewalks shall be constructed of cement not less than five (5) inches thick, the bottom four (4) inches of which shall be composed of not more than nine (9) parts of fine gravel to one (1) part of cement, and the top one (1) inch shall be composed of not more than two (2) parts of screened sand to one (1) part of cement; provided always that any property owner who so elects may construct such sidewalk of brick, stone, flagging or asphalt. Repairs upon sidewalks shall be made of the same material as used in the original construction of them. (Ord. 9, 1917)

12.04.080 Grades.

All sidewalks constructed shall be laid to the grades established and shown in detail upon the official profiles in the office of the Town Engineer. The inner edge of all sidewalks shall be two (2) inches higher than the outer edge. (Ord. 9, 1917)

12.04.090 Snow and Ice Removal.

A. Every owner, occupant or agent ("responsible person") of a building, structure,

property or lot ("premises") within the Town shall remove from all sidewalks abutting, adjoining or fronting the premises, all snow and ice within twelve (12) hours after it accumulates thereon. No person shall fail to fulfill this affirmative obligation. For purposes of determining whether a sidewalk is abutting, adjoining or fronting the premises, any intervening right of way shall be excluded. This section does not apply to ice that accumulates for reasons beyond the control of the responsible person.

B. In the event responsible person shall fail to cause snow and ice removal as required by Section 12.04.090.A. above, the Town Manager or designee may cause a written notice to be served on the responsible person. The notice shall require the responsible person to complete the snow and/or ice removal within a reasonable time, not to exceed forty-eight (48) hours. If personal service cannot be made on the responsible person, the notice shall be posted in a prominent place at the premises. If the responsible person so notified fails to remove the snow as required by this section, the Town Manager, or designee, may cause the snow removal to meet the requirements of Section 12.04.090.A and charge the costs thereof, plus an additional amount up to twenty-five dollars for administrative costs, to the responsible person.

C. Any person who violates any provision of this Section shall be subject to a fine not less than One Hundred Dollars (\$100.00) and not to exceed the maximum fine as set forth in Section 1.16.110.A.2. Each such person shall be guilty of a separate violation for each and every day during any portion of which any violation of any provision of this Section is committed, continued or permitted by any such person and such person shall be punished accordingly.

D. Remedies B and C are cumulative. (Ord. 1164,2002)

12.04.100 Violation - Misdemeanor.

If any person violates any of the provisions of this article in the laying or repair of any sidewalk, or interferes with or obstructs any employee of the Town, or any contractor under contract with the Town, engaged in the laying or repair of any such sidewalk, he or she

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is guilty of a misdemeanor and upon conviction shall be subject to the maximum penalty provided for in Section 1.16.110.A.1. (Ord. 9, 1917)

12.04.110 Violation - Public Nuisance.

Any defective or dangerous sidewalk or sidewalks, which by reason of inequalities, holes therein, plank or planks removed therefrom, projecting or protruding nails, nonconformity to the established grade, or which for any other reason is, or is liable to become, dangerous and an impediment to travel, is a nuisance, and the abutting landowner is the author of the nuisance, and after failure to abate the nuisance upon twenty-four (24) hours' notice by the Town Marshal, Director of Public Works or any police officer, and upon conviction shall be subject to the maximum penalty as provided for in Section 1.16.110.A.1. (Ord.9, 1917)

